

Project No.: 17275-1

November 29, 2021

Committee of the Whole
Vaughan City Hall, Level 200
2141 Major Mackenzie Drive
Vaughan, ON L6A1T1

promenadecentre@vaughan.ca

Dear Chair and Members of Committee,

Re: *Proposed Secondary Plan for Promenade Centre (“the Centre”)*
Promenade Limited Partnership (“PLP”)
Summary of Comments

Promenade Limited Partnership (PLP) is the owner of the lands municipally known as 1 and 180 Promenade Circle in the Thornhill Vaughan Community. On its behalf, we are pleased to submit the following comments with respect to the proposed Promenade Centre Secondary Plan which will be considered by your Committee (Committee of the Whole) at a public meeting on Tuesday November 30th, at 7.00 pm.

PLP appreciates the opportunities provided by the City to work with staff on the Secondary Plan over the past two years and is generally supportive of many of the policies in the proposed amendment. These policies clearly encourage intensification of the Centre in accordance with its role as a key site within a Major Transit Station Area (“MTSA”) and provide for substantial growth on the PLP lands.

The proposed Official Plan Amendment and Secondary Plan also include policies which encourage the retention of the existing Promenade Mall, while enhancing its role as a regional shopping destination. This is a fundamental goal not only for PLP but also for the broader community.¹ However, there remain a number of areas of concern. These all relate to PLP’s overriding objective of providing for the continued and successful operation of the Mall while allowing for the intensification of Promenade Centre in accordance with provincial, regional and City policies.

¹ Official Plan Amendment Section I Purpose, and Secondary Plan 11.16.2. Context (1.0) Policy Context (5.0).

PLP's areas of concern are summarized below:

- 1) **Retention of the Promenade Mall as a Regional Shopping Destination**
- 2) **Expansion of opportunities for Intensification**
- 3) **Flexibility with respect to Built Form**
- 4) **Open Space System, Parkland and POPS policies**
- 5) **Cost Sharing Provisions**
- 6) **Transportation and Mobility**
- 7) **Modifications to Schedules and Figures**

While this letter does not highlight every policy which may require clarification or amendment to address PLP's concerns, the above issues are described in detail in this letter. **Attachment 1** illustrates PLP's requested changes to the Schedules and **Attachment 2** is a Technical Transportation Memorandum from BA Consulting with the proposed changes to Figures 1-3.

We have highlighted in bold below specific requests by PLP for modifications to the proposed Official Plan Amendment and Secondary Plan.

A. Retention of the Promenade Mall as a Regional Shopping Destination

- a) Although the policies acknowledge the role of the Mall generally, there are no specific measures to promote its continuation, and no recognition of its significant contribution to the economic vitality of the Secondary Plan area and beyond. In fact, it would almost appear from Schedule D that redevelopment of the Mall is encouraged.

Schedule D provides a height of 35 storeys and a floor space index (FSI) of 6.5 on the entire Mall area. As it is not possible to construct towers over the Mall while continuing to operate it as a regional retail destination, this allocation of density is effectively "frozen". The proposed allocation of so much of the permitted density to the Mall building will effectively prevent the broader Secondary Plan area from achieving the identified population and density targets.

The PLP vision has always been for the retention and strengthening of the Mall while transforming the surface parking lots into a high density mixed use development to form a complete community. **PLP requests that additional height and density be allocated to other more appropriate locations on the PLP lands.** (see Attachment 1 and Paragraph B b. below). The allocation of this additional density, as set forth in Attachment 1, is necessary to both ensure

achievement of the identified population and density targets and address the objective of strengthening the Mall.

- b) The Vision Statement makes no reference to the existing retail uses - only to new development providing a mix of retail, entertainment, community and office uses. The Guiding Principles (2.1 b.) refer only to building upon the current assets, such as retail uses, to allow the evolution of a complete community. The Mall today supports a whole range of functions, activities and uses - such as offices (including medical offices), entertainment (e.g. cinemas) and service uses (e.g. restaurants and fitness) - not just retail. The Secondary Plan should include a specific reference to the significant contributions which the Mall makes to the Thornhill-Vaughan community and the employment opportunities for the area. It is much more than simply a place to gather.
- c) Equally, the Central Square Precinct policies (3.1.1 ii) make no reference to the ongoing role of the Mall - only its function as a gathering place - an important but by no means its only function. The new retail to be provided along High Street, as shown on Attachment 1, will enhance the shopping experience, especially at key entrances to the Mall.

PLP requests that the Central Square Precinct policies be revised to reflect the Mall's continuing role. The Promenade Commercial Assessment Study dated October 6, 2020 by 360 Collective prepared on behalf of PLP has been submitted to City staff and could be of assistance in this regard.

- d) As noted, BA Consultants has prepared a Technical Memorandum to address the specific transportation issues of concern (see Attachment 2). We also note that (Transportation Network, Section 5.2 (f)), does not explicitly reference the Mall as a significant use which will continue. This should be identified as the transportation network needs to reflect the continuing operation of the Mall and its supporting services (e.g. loading and servicing).

B. Expanded Opportunities for Intensification

a) Population and Housing Target

Part A, Preamble Section (5), Policy Context identifies that the Promenade Centre is intended to accommodate an existing population and job range of 17,235 - 21,162. However, this persons and jobs per hectare ("pjh") target is not referred to in Part B of the Secondary Plan and should be referenced in Part B Section 3.1.1 - Density. There is no indication of the distribution between housing (no number of units is provided) and jobs (no gross floor areas are

included). The areas over which the minimum and target densities are to be distributed and assigned are not shown.

While it is recognized that the **minimum** densities for BRT in accordance with the Growth Plan are 150 people and jobs per hectare (pjh) and that a figure of 200 pjh has been endorsed by the Region of York for the broader Disera-Promenade MTSA, the planning for the Secondary Plan area recognizes that a much higher figure is warranted for Promenade Centre.

PLP has made several submissions to the City which underscore the importance of having a target which is well above the regionally endorsed figure of 200 pjh.

PLP requests that the projected higher pjh be specifically identified in the Secondary Plan, and that the number not be a maximum, but simply a target (see Section 3.1.3 Density).

b) Locations for Increased Height and Densities

Given the restrictions on the provision of additional housing and jobs on the Mall itself, we request that the proposed heights and densities on the surrounding blocks be revisited and increased to accommodate more residential units and greater heights as shown on Attachment 1.

In addition, the Secondary Plan should include a policy which allows for the transfer of heights and densities between development parcels through the development approval process without requiring an official plan amendment.

The entire Secondary Plan area falls within an 800 m radius of the Disera Promenade BRT station - thus all uses will be within walking distance of the station. The basis for the limitation of a maximum floor space index (FSI) of 10 within the Transit Hub Precinct and generally only 6-7 for all other buildings shown on Schedule D (Section 3.2.3) is not provided. This restriction is particularly challenging when much of the density allocated to the Mall building (see Schedule D) cannot be used.

There are other locations where the restrictions on heights and densities are not, in our opinion, warranted. For example, given the existing uses (apartments) and heights (15 storeys+) to the southeast and northwest of Promenade Circle, the heights proposed for the developable portions of the PLP lands (20 storeys) could and should be considerably higher while still providing an appropriate transition. In particular, the 20 storey maximum height in the area west of the Mall and east of the existing apartments in the northwest quadrant is too low, given its location within the Transit Precinct.

The restriction on maximum building height to 35 storeys across much of the draft Secondary Plan would seem to be contrary to the policies which encourage a varied skyline (4.2 a)). While all buildings should not be in the 45+ storey range a more flexible approach to building heights should be included in Section 3.1.3b) and shown on Schedule D. Height can be used to advantage in order to highlight certain gateway locations (for example at gateways and where gateway entrances connect to Promenade Circle).

The references to densities in the Secondary Plan (for example Sections 3.1.2 and 3.13 do not define how density is to be calculated. Given the very differing block sizes and the varying amounts of land required for public and private roads for each block it is only appropriate and equitable to calculate densities on a gross not a net basis.

PLP requests that the maximum heights and FSI be increased in key locations as shown on Attachment 1 and that the FSI shown be calculated on a gross not a net basis.

c) Mid-Rise Mixed Use

PLP supports the introduction of a Mid-Rise Mixed Use designation in order to provide a variety of building types and to provide a transition to the existing residential neighbourhood to the east, on the east side of Bathurst Street. However, the lands at the corner of Bathurst and Centre Street, extending south to the first access road, are designated High-Rise Mixed-Use, permitting a height of 35 storeys and a density of 7.0 - and no transition is provided. It is not clear why the lands immediately to the south are restricted to only 12 storeys.

PLP requests that the northerly portion of its lands fronting Bathurst Street (identified as the LCBO lands) be permitted heights greater than 12 storeys and that Policy 3.3.4 be amended to provide a gradation of heights up to 18 storeys at the north end of the Medium-Density Mixed-Use block adjacent to Bathurst (see Attachment 1). The permitted FSI should be 5.5 as is proposed on the other Medium Density blocks extending to the Mall entrance.

C. Flexibility with respect to Built Form

a) Floor Plates and Setbacks

PLP acknowledges that to achieve its urban design objectives the City is seeking to provide direction with respect to such matters as tower separation distances, floor plate sizes and setbacks. However, the inclusion of specific measurements

in an official plan amendment can prove unduly cumbersome in the evaluation of future development applications. For example, there may be situations where a floorplate in the order of 850 sq. m is appropriate and should not trigger an official plan amendment.

PLP asks that the provisions of Section 4.2 c. be reworded to provide that the maximum size of a residential floorplate generally not exceed approximately 850 sq. m. This maximum floor plate size has been approved for the Phase 1 Promenade Centre lands and is a viable standard commonly used in Vaughan and in other municipalities in the GTA.

Equally, minimum building setbacks are set out in Section 4.2.n. **PLP asks that specific standards (3.5 m from edge of right-of-way, 7 m, from Bathurst Street) be deleted, as setbacks are more appropriately addressed in the implementing zoning by-law.**

b) **Gateways**

Gateways are identified on Schedules C/D, and there is a policy reference set out in 4.5. However, there is no clear policy framework outlined as to the key role of these entrances and the opportunities to create key focal points where they intersect with Promenade Circle.

PLP requests an additional policy be included as 4.5.e outlining the importance of these gateways as key focal points for the community, which can support greater densities and heights and offer opportunities for architectural expression and design features at all the gateway entrances and the points of intersection with Promenade Circle.

D. Open Space System, Parkland and POPS Policies

a) **Parkland**

The park designations on Schedule C (and D) are located conceptually throughout the community and include both new and existing parks. Policies 3.8.2 and 6.1 indicate that the **total** parkland (including new and existing) within the Secondary Plan area should “generally” be a minimum of 5.98 ha. There is no policy setting out how the 5.98 ha was determined, nor what existing and /or proposed parks this number includes. With respect to the PLP lands, it would appear that the parkland, as shown on Schedules C/D, currently account for over 10% of their total holdings - a significant amount given the land area devoted to retaining the Mall and recognizing that parkland has already been dedicated on the plan of subdivision for the Mall site (Trudeau Park and Woodlot).

The use of a precise and absolute number such as 5.98 ha is further complicated by the provisions of Section 6.1 b. which provide that to meet or exceed this target the City may require the dedication of parkland in addition to the locations identified on Schedule C, in accordance with the provisions of Policy 7.3.3 of the VOP 2010.

PLP objects to Policies 3.8.2 and 6.1 and also notes that Section 7.3.3 of the VOP has been appealed.

PLP is generally supportive of the parks locations shown. However, it suggests that some minor adjustments are in order as shown on Attachment 1. For example, the park on the PLP lands adjacent to Bathurst Street should be increased and extended north, as shown on Schedules C and D. While conversely, the park shown at the northwest corner of the PLP lands south of Promenade Circle should be deleted as its size and location severely restrict the developability of the remaining parcel to the south.

b) POPS

PLP is supportive of the overall intent of the Secondary Plan that POPS should be incorporated at strategic locations, generally as described in 3.10.2 and 4.3 c. However, it is premature to identify specific locations for POPS on Schedules C/D. POPS should be identified through the development approval process in accordance with the general policy direction.

PLP requests that that the POP locations as shown on Schedules C and D be deleted (see Attachment 1).

c) Pathways and Trails

There are a number of references to pathways and trails (e.g. policies 4.3 and 6.0), but no explanation of how these pathways are to be secured through the development parcels in a strata condition.

PLP requests that Figures 4 and 5 which show pathway configurations be deleted as the location, function and width of shared paths in an urban setting should be determined at the time of site plan approval.

d) Cost Sharing

PLP appreciates the provisions of Policy 11.7.3 indicating that cost sharing agreements and other measures will be required, as a condition of development approvals to ensure that new development in the Promenade Centre is coordinated and that the required commitment of funds, lands and services are secured and/or in place. These agreements and other measures will be needed

to address, among other things, an owner's over dedication of parkland. The assistance of the City will be required to confirm that all parkland, road, servicing and other infrastructure requirements are fairly and equitably shared through cost-sharing, development charges and other measures.

F. Transportation and Mobility

These policies and corresponding adjustments to the Schedules and Figures are addressed by BA Consulting in the letter included as **Attachment 2**.

A summary of the transportation and mobility policies which PLP requests be amended is set out below:

5.2 b.i - in order to identify interior Local streets as private streets on Schedule D.

5.2 b.iii - in order to provide reduced right-of-way widths for Collector streets, Local Streets (with parking on one side) and Shared Use Paths.

5.2.b.iv.b – in order to provide flexibility with respect to the cross-section elements required within private Local streets.

5.2.b.iv.c – in order to provide flexibility in establishing the cross-sectional dimensions and elements within Shared Use Paths

5.2.f ii and iii - in order to clarify that the Long Term Future Road Connection on Schedules C, D and E would only occur in the event of a very long term redevelopment of the Mall.

5.2.f v - in order to modify the wording that permits the dedication of new public streets through the site plan process, and not necessarily through the subdivision process to reflect the wording more closely in Section 11.9, Conveyance of Lands.

5.4.b.i – in order to clarify cycle tracks be provided on all public Collector streets with flexibility to configure cycling facilities on private Local streets.

5.5.b Parking Supply Requirements - in order to allow flexibility in establishing minimum and maximum parking requirements, and reduced parking standards for residential visitors, retail, personal service and office uses to reflect those in the VMC. The policy framework should also refer to a shared parking formula and the potential for a co-mingling of residential visitor and Mall parking.

5.7 Streetscaping – in order to reflect flexibility in configuring cycling facilities along private Local streets to address multi-modal travel needs.

G. Modifications to Schedules

PLP requests that Schedules B, C, D and E be revised in accordance with the Revised Schedules included in Attachment 1.

PLP requests that Schedule F, Street Network Implementation Plan be deleted as the detailed phasing of the implementation of the Street Network should be determined at the time of detailed development applications.

PLP requests that the Title Page include a statement that Figures 1-3 (Appendix 2) are not to form part of the Secondary Plan and are for illustrative purposes only, and that the proposed revisions set in Attachment 2 be accepted by the City and that Figures 4-5 should be deleted as the width and configuration of shared paths in an urban setting should be determined at the site plan stage.

We thank you for your consideration of these requests and we look forward to discussing the concerns of PLP with you and City staff in the near future.

Yours truly,

Bousfields Inc.

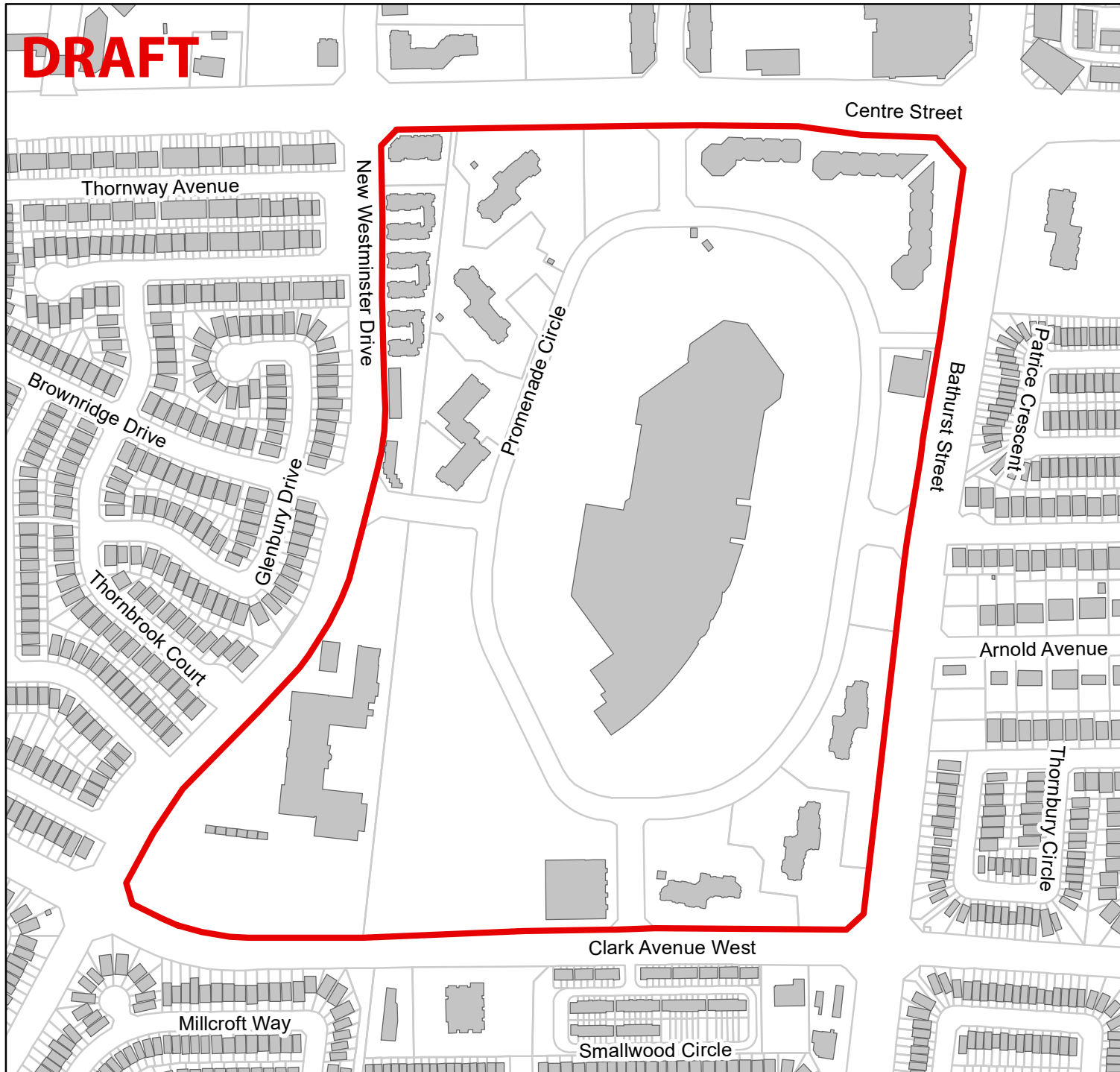


Lindsay Dale-Harris M.Sc.Pl., FCIP, RPP

LDH/rml:jobs

cc: Michael Uster, michael@libertydevelopment.ca
Fred Davish, fred@libertydevelopment.ca
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Len Abelman, labelman@wzmmh.com
Mark Noskiewicz, mnoskiewicz@goodmans.ca




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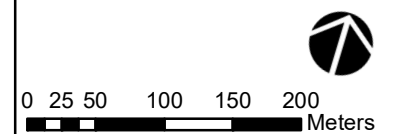


Schedule A

SECONDARY PLAN AREA

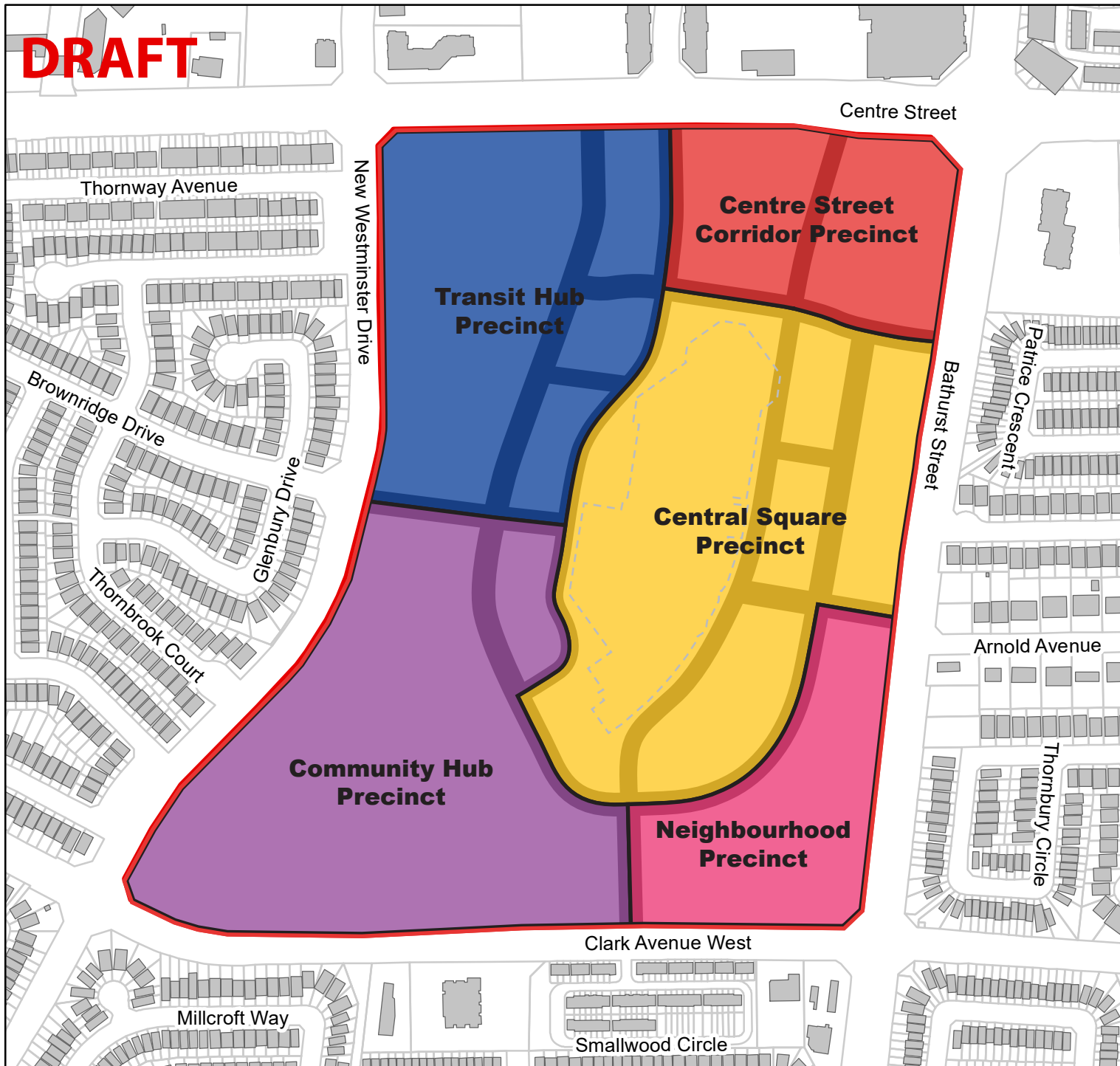
Legend

-  Secondary Plan Area
-  Existing Buildings
-  Existing Property Lines



November 2021







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Schedule B

LAND USE PRECINCTS

Legend

-  Secondary Plan Area
-  Transit Hub Precinct
-  Central Square Precinct
-  Centre Street Corridor Precinct
-  Neighbourhood Precinct
-  Community Hub Precinct

0 25 50 100 150 200 Meters

















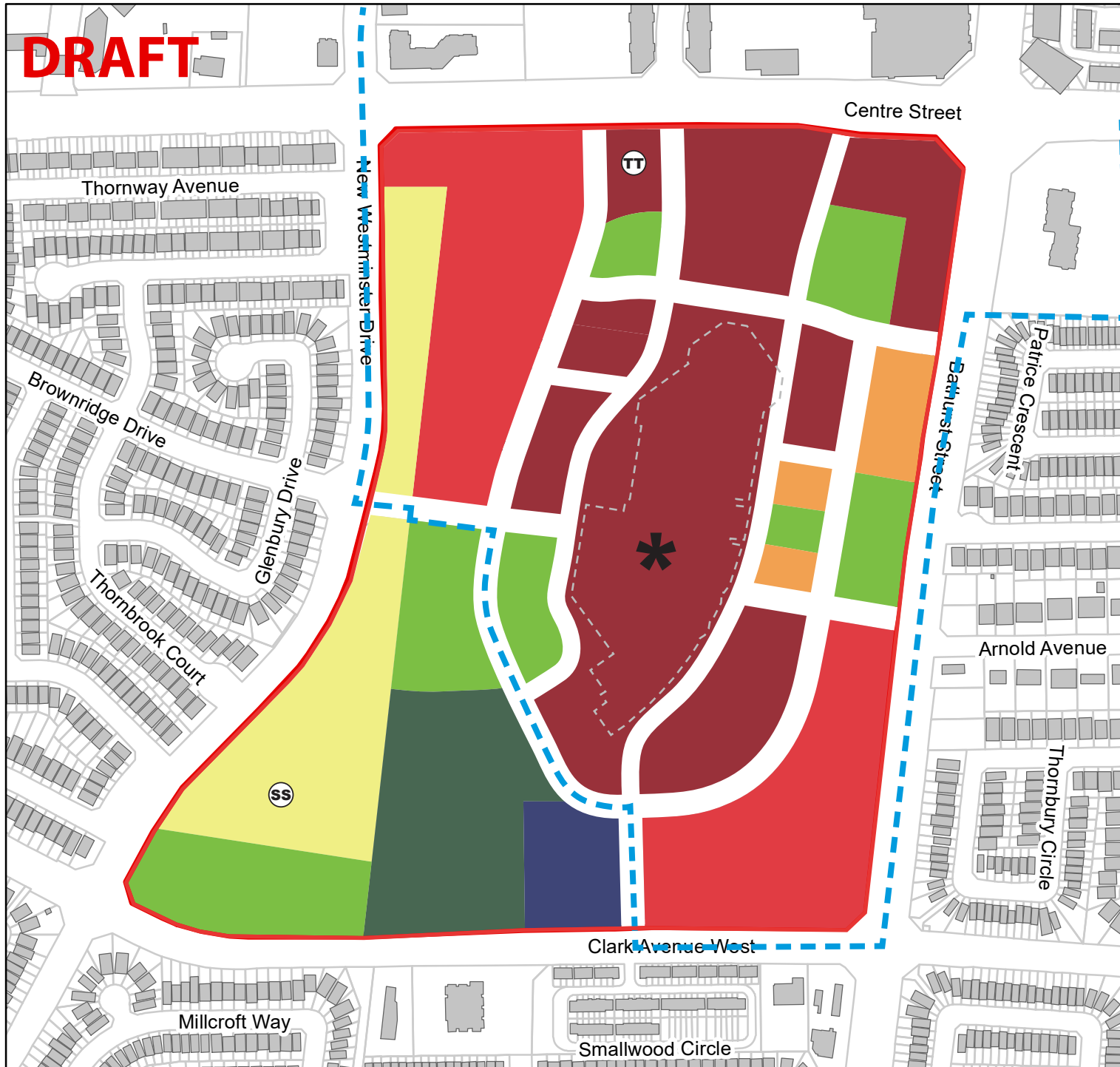
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Schedule C LAND USE PLAN

Legend

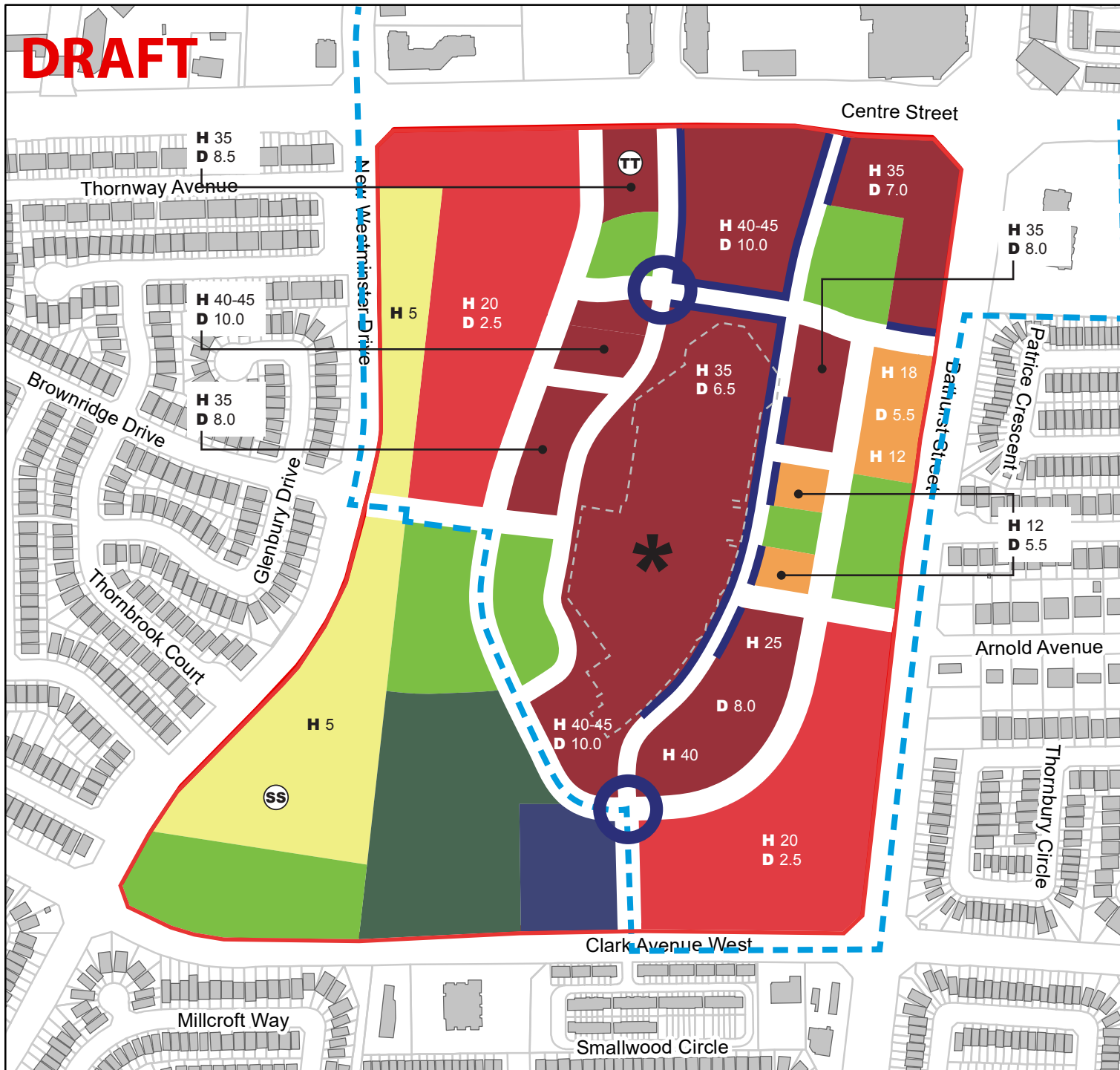
-  Secondary Plan
-  High-Rise Mixed-Use
-  High-Rise Residential
-  Mid-Rise Mixed-Use
-  Low-Rise Mixed-Use
-  Major Institutional
-  Park
-  Natural Area
-  Privately Owned Publicly-Accessible Spaces (POPS)
-  Transit Terminal
-  Secondary School
-  Existing Mall & Phase 1 Development
-  Disera-Promenade MTSA (including areas extending beyond the Secondary Plan)
-  See Policies 5.2.f.ii., 5.2.f.iii.



0 25 50 100 150 200 Meters

November 2021

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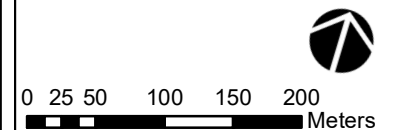


Schedule D

HEIGHT, DENSITY, & USE PARAMETERS

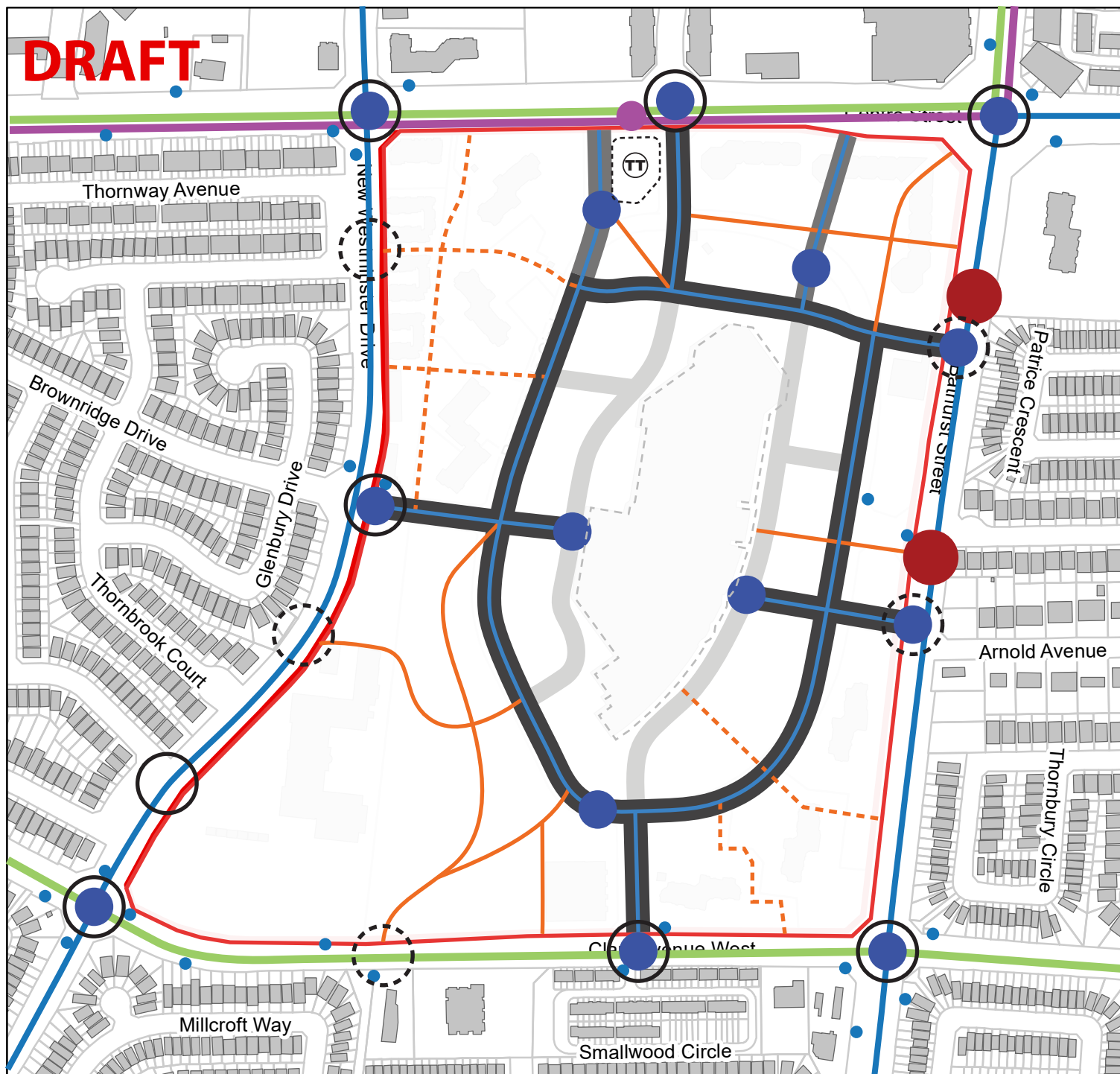
Legend

- Secondary Plan
- High-Rise Mixed-Use
- High-Rise Residential
- Mid-Rise Mixed-Use
- Low-Rise Mixed-Use
- Major Institutional
- Park
- Natural Area
- Retail or Service Commercial Frontage
- Gateway
- Privately Owned Publicly-Accessible Spaces (POPS)
- TT Transit Terminal
- SS Secondary School
- Existing Mall & Phase 1 Development
- Disera-Promenade MTSA (including areas extending beyond the Secondary Plan)
- * See Policies 5.2.f.ii., 5.2.f.iii.



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Schedule E

MULTI-MODAL TRANSPORTATION NETWORK

Legend

- Secondary Plan Area
- Collector Street
- Local Street
- Private Local Street / Publicly Accessible
- * See policies 5.2.f.ii, 5.2.f.iii.
- Existing Cycle Track
- Cycle Track
- Shared Use Path¹
- Long Term Future Shared Use Path²
- Regional Rapid Transit Corridor
- Bus Rapid Transit (BRT) Stop
- Bus Stop
- Shared Mobility Hub
- Transit Terminal
- Traffic Signal
- Existing Traffic Signal
- Existing Traffic Signal to be relocated
- Existing Mall & Phase 1 Development
- Existing Transit Terminal
- Conceptual Shared Use Path *

¹ Refer to Policy 5.2.e.ii.

² Refer to Policy 5.2.e.v.

* To be co-ordinated with Site Plan of Block

0 25 50 100 150 200 Metres

November 2021

Memorandum

TO:

Jim Baird, Promenade Limited Partnership (PLP)

COPIES:

Michael Uster, PLP

FROM:

Timothy J. Arnott, MCIP RPP
Principal

PROJECT:

7411-07
Promenade Shopping Centre –
Master Plan

DATE:

November 29, 2021

SUBJECT: REVIEW OF PUBLIC/PRIVATE STREETS AND CORRESPONDING RIGHTS-OF-WAY –
PROMENADE SHOPPING CENTRE, MASTER PLAN

1.0 INTRODUCTION

BA Group is retained by Promenade Limited Partnership (PLP) to provide transportation consulting input towards the development of the Promenade Shopping Centre Master Plan and its mobility characteristics.

PLP and its consulting team have been participating in the ongoing Promenade Secondary Plan Study since it was initiated in the 3rd Quarter of 2019. Amongst the matters that PLP and its consulting team have been providing is input into the Master Plan street network necessary to support the proposed Secondary Plan and PLP's proposed development levels. PLP and its consulting team have been consistent in their position that the proposed streets within the Promenade Shopping Centre lands should consist of a combination of Public (Collector) and Private (Local) streets that exhibit Rights-of-Way (ROW) that contain key multi-modal elements to equitably support pedestrian, cycling, transit vehicle, service vehicle, private vehicle and emergency vehicle needs.

2.0 SUMMARY OF THE PROPOSED PUBLIC AND PRIVATE STREET CHARACTERISTICS

Collector and Local streets noted below are illustrated in Drawings FD-01, FD-02 and TS-01, in **Appendix A**.

2.1 COLLECTOR STREETS:

Promenade Circle – Collector Street

- Public Street exhibiting a 24 m Right-of-Way (ROW) with dedicated cycle tracks in boulevard

Connector Streets – Collector Streets Linking Promenade Circle to surrounding Public Streets;

- East Promenade Connector to Bathurst Street – Two Public streets exhibiting 30 m ROW's with dedicated cycle tracks in boulevard;
- South Promenade Connector to Clark Avenue – Public street exhibiting a 26 m ROW with dedicated cycle tracks in boulevard;
- West Promenade Connector to New Westminster Drive – Public street exhibiting a 26 m ROW with dedicated cycle tracks in boulevard;
- North Promenade Connector to Centre Street – Public street exhibiting a 32 m ROW with dedicated cycle tracks in boulevard;

Future East-West Collector Street linking West Promenade to the southerly East Connector to Bathurst Street

- Public Street Collector exhibiting a 20 m ROW with dedicated cycle tracks in boulevard
- Sections of this street would be implemented in phases as development proceeds.
- The central section of this Collector Street between High Street (Local Street on the east side of the Promenade Shopping Centre building – which is being constructed presently) and a future Private N-S Local Street on the west side of Promenade Shopping Centre building would only be implemented if the Promenade Shopping Centre building were redeveloped in the long term future.
- The westerly and easterly segments of this Public Collector Street, between Promenade Circle and the two Local Private N-S Streets would be implemented as development in those areas proceeds.

2.2 PRIVATE LOCAL STREETS

High Street

- A Private Local N-S Street on the east side of Promenade Shopping Centre that has been approved as part of the Phase 1 development and is under construction would exhibit a 20 m ROW with cycling Sharrows

West N-S Local Private Street

- A Private Local N-S Street aligned on the west side of Promenade Shopping Centre that would intersect with Promenade Circle opposite the North Promenade Connector and extend south to intersect with Promenade Circle between South Promenade and West Promenade.
- This Private Street would exhibit an 18 m ROW with cycling facilities:
 - Alternative cross-sections are possible with this 18 m ROW that would vary the curb to curb dimension between 6.6 m and 8.5 m depending upon how cycling facilities are implemented within the ROW. In the 8.5 m pavement width condition, cycling would be accommodated within Sharrows similar to "High Street". Alternatively, within the 6.6 m pavement dimension condition, a two-way 3.0 metre dedicated cycle track facility on one side of the vehicular travelled section would accommodate cycling. Both conditions would have dedicated sidewalks of at least 2.0 metres in width, landscaping provisions, and opportunities for layby on-street parking in strategically located segments to address pick-up/drop-off needs related to adjacent land use intensification.

E-W Private Local Streets

- These two connecting Private Local Streets link Promenade Circle to the two N-S Private Local Streets – High Street on the east side of the Shopping Centre Building and West N-S Local Private Street on the east side of the Shopping Centre Building.
- These two Private Local Streets would exhibit an 18 m ROW's with either cycling Sharrows or dedicated cycle tracks on one side of the street.

Attached are preliminary functional design drawings of the proposed Collector and Private Local Street alignments illustrating the ROW's (Drawing FD-01) and Lane Configurations (Drawing FD-02) and cross-sections (Drawing TS-01).

The following memorandum summarizes the rationale for the proposed Private Street designations for Local Streets and the proposed public and private street ROW dimensions.

3.0 RATIONALE FOR PRIVATE AND PUBLIC STREETS

3.1 PUBLIC STREETS

PLP has proposed that the **Collector Streets** that form part of the Master Plan street network **will be Public Streets**. This is consistent with the Secondary Plan assumptions proposed by the City of Vaughan and their consulting team. Public Streets are illustrated in a blue colour on Drawing FD-01 in **Appendix A**.

The Collector streets proposed as part of the Master Plan (and Secondary Plan) included:

- Promenade Circle;
- The 'Connector' streets that link Promenade Circle to the surrounding existing public arterial and collector streets:
 - North Promenade connector street(links to Centre Street);
 - Two East Promenade connector streets – north and south (that link to Bathurst Street);
 - South Promenade connector street (that links to Clark Avenue); and,
 - West Promenade connector street (that links to New Westminster Drive).
- Provision has been made for a future extension of West Promenade and the southerly East Promenade connector streets through what is the existing Promenade Shopping Centre building. This would not happen unless long term future redevelopment of the Promenade Shopping Centre building is contemplated. There are no plans at this time within the context of the Master Plan proposed by PLP to affect change to the Promenade Shopping Centre building in a way that would facilitate this mid-block E-W collector street within the foreseeable future.

There is one Local Street that has been designated as a Public Street. This Local Street is the extension of High Street, north of the east-west segment of Promenade Circle, between Promenade Circle and Centre Street. This section of the Promenade Secondary Plan area travels through the PLP lands and the Torgan lands. It links the Secondary Plan street network to the surrounding existing public arterial and collector streets. The "function" of this street segment serves a Local Street role (i.e., mainly access to adjacent development, but also a limited role of distributing Secondary Plan development traffic to the adjacent Centre Street arterial corridor, within the network.

These Collector Streets provide an important role in both accommodating the planned intensification associated with the Promenade Master Plan and the Secondary Plan and appropriately distributing all pedestrian, cycling and vehicular traffic (transit vehicle, service vehicle, private vehicle and emergency vehicle) to their various destinations within the Master Plan area as well as providing public street linkages between the aforementioned surrounding existing public arterial and collector streets. In only limited circumstances will the Collector Streets within the PLP Master Plan provide any direct access to individual development parcels.

As noted in Section 4.0, the Collector Streets provide for all of the essential ROW elements and do so by providing typical dimensional requirements for same.

3.2 PRIVATE STREETS

Private streets are illustrated in a pale yellow colour in Drawing FD-01, in **Appendix A**.

The Phase 1 portion of the PLP intensification of the Promenade Shopping Centre was approved in advance of the Secondary Plan process being initiated. As part of the Phase 1 development application, a Private Local Street was proposed adjacent to the Promenade Shopping Centre Building. This is referred to as High Street for two reasons; 1) it was a Local Street that was envisioned to be lined with development that fronted onto the Local Street providing an active frontage that would accommodate interaction with the Local Street itself (akin to a shopping street or what has been referred to as a “high street” in an urban context). 2) The second reason it was called High Street is there is a vehicular facility beneath High Street sometimes referred to as Low Street. Low Street is a private vehicular facility that facilitates service vehicle access and loading vehicle manoeuvring as well as access and circulation related to parking facilities associated with the Phase 1 development and potentially subsequent development parcels.

This inherent flexibility of putting the functional loading and parking access below-grade enables the High Street context to reflect a significantly enhanced urban environment that is absent the “back-of-house” facilities associated with the Phase 1 development as well as a significant amount of existing Promenade Shopping Centre loading requirements that had existed along the east side of the Shopping Centre prior to the Phase 1 development. This was possible, in part, due to the grading opportunities that the east side of the shopping centre presented. The flexibility that making High Street a Private Local Street – with the appropriate public easements in favour of the general public being able to use High Street unimpeded – and making it “look” like a public street enabled the Promenade Shopping Centre significant advantages in terms of urban design, efficiency of loading and parking access operations and in terms of its constructability and maintenance.

These same principles of a Private Street are being applied to the balance of the Local Streets identified within the Master Plan (and Secondary Plan) as proposed by PLP and its consulting team.

The Private Local Streets within the Master Plan are set out in Section 2.2 above.

The principal reasons that these Local Streets have been designated as Private Streets include:

- **Public Easements** would be granted in favour of the City of Vaughan permitting the use of the Local Street by the general motoring public and would “appear” (at-grade) to be public given they would be designed to surface public street standards having appropriate operating design criteria (i.e., design and posted speeds and corresponding horizontal and vertical design criteria). Easements could also obligate the owners of the Private Streets to maintain them to a minimum standard that would equal public ownership conditions to ensure they retain their intended role in the overall Master Plan street network;
- The Private Street ownership better facilitates the ability to **locate parking beneath the Private Local Street ROW**. There is significantly less complexity in the strata ownership arrangements that result from retaining the Local Street under Private ownership. Similarly, the liability associated with the ability to locate parking beneath a Private Street is more manageable relative to the same arrangements beneath a Public Street;
- Being able to park beneath the Local Streets will enable a **more efficient and more cost effective below-grade parking garage layout** for development blocks around the Shopping Centre building;
- A Private Local Street will **better facilitate potential below-grade pedestrian and service connections** between development intensification on the perimeter of the Promenade Shopping Centre building and the Shopping Centre building itself. This is evidenced by the design efficiencies and advantages that have been achieved on the east side of the Shopping Centre building with Phase 1 development and High Street (a Private Local Street).
- **Phasing of development** around the perimeter of the Shopping Centre building would be more efficient when the Local Streets are in Private Ownership. Implementing segments of the Private Streets – given an overall street network plan to work towards – would be more efficiently and cost effectively undertaken given Private Local Street control.
- **Cost effectiveness for the Municipality** – Long term maintenance of the Private Streets would be the responsibility of PLP, lessening the financial obligations of the Municipality and linking them to the planned development;
- Having the Local Streets under Private ownership would enable flexible conditions to be implemented and operated along the Local Street segments. This would **better facilitate the retention and operation of surface loading facilities that exist on the west side of the Shopping Centre building** and that are planned in the southwest component of Phase 1, that are essential to the day to day operating conditions associated with the Promenade Mall. Service vehicles that use these loading facilities along the east side of the Mall require the use of the planned West N-S Private Local Street for day to day vehicle manoeuvring requirements. Retaining ownership would permit PLP to safely and effectively manage those loading operations while allowing the Private Local Street to function as a street during other times of the weekdays and weekend days.
- Private Local Street designation would **permit a more flexible design and construction of key civil and structural elements associated with the ROW** while maintaining appropriate engineering and urban design requirements;

- Private Ownership of the Local Streets would also facilitate a **more flexible and higher standard of urban design and maintenance program to be implemented** along the Private Street ROW. It would also enable better integration of the urban design elements of the Promenade Mall, central to the intensification plans associated with the PLP lands, with the adjacent Private Local Streets.

A Private Street designation for the Local Streets within the PLP lands is more conducive to the operational requirements of successfully sustaining and supporting the Promenade Mall.

For these reasons, PLP and its consulting team continue to propose Private Local Street ownership conditions within the context of the proposed Master Plan for Promenade Shopping Centre and for the Secondary Plan area.

We should also note that the inclusion of the Shared Use Paths within the Secondary Plan can be achieved through a combination of public and private segments, depending upon which block within the Secondary Plan area they happen to be located; i.e., public park block, POPS, or development block.

The attributes that contribute to a successfully designed active transportation path system can be implemented through public ownership (e.g., within a public park block) or by way of easements across a privately owned block (e.g., a POPS or development block). Their implementation, however, would best be achieved through the Site Plan approval process where the details of the requisite elements to support the shared use path's needs and context (precise location, function and dimensional requirements) are most appropriately established and designed.

4.0 RATIONALE FOR PROPOSED RIGHTS-OF-WAY

PLP and its consultants have also reviewed and considered the make-up of the proposed ROW's and the associated dimensions that have been proposed for all new streets within the Master Plan PLP has prepared. The Master Plan area is consistent with the Secondary plan boundaries.

In general, the composition of each ROW was driven by its functional needs and a sense of hierarchy within the Secondary Plan (PLP Master Plan) mobility network.

Collector Streets provide a connecting or collecting role within the hierarchy and must be able to accommodate the forecast multi-modal travel demands associated with the planned intensification as well as connect appropriately to the boundary arterial and collector streets just beyond the Secondary Plan area.

Local Streets provide the principal role of providing access to and from development parcels and publicly accessible uses such as parks and open space. Their role in providing dedicated facilities for each mode is less and in some cases is used strategically to provide a slower operating environment for vehicular traffic and to facilitate activities like curb-side pick-up and drop-off, on-street parking and access to below-grade parking, loading facilities and of course emergency vehicle access to all development parcels.

Shared Use Paths for active transportation uses serve as an added level of mobility for non-vehicular modes and are generally incorporated into development parcels to facilitate direct access to land uses who's travel

origins and destinations are well served by such proximity. The alignment, function and dimensional requirements of Shared Use Paths are, however, best addressed within their contextual situation – depending upon which block within the Secondary Plan they are situated (public park block, POPS block or development block). This component of the overall mobility network should be determined at the time of Site Plan approval.

In general the reduced ROW dimensions set out herein facilitate a greater efficiency in the use of land, given the amount of land within the Secondary Plan area that is dedicated to the retention of the Promenade Mall building as a community asset while also providing a finely grained network of multi-modal streets and shared use paths (as highlighted in the Secondary Plan) and a system of parks and open space. This balance provides for more efficient development parcels that can be readily accessed on a multi-modal basis from an appropriate hierarchy of public and private streets and shared use paths.

A summary of the ROW elements and their dimensions for the Master Plan streets follows. Drawings FD-01, FD-02 and TS-01 illustrate the corresponding designations and dimensions in **Appendix A**.

These proposed ROW characteristics compare to the City of Vaughan and their consultants' ROW dimensions of 26 m for Collector Streets and 20 to 22 metres for Local Streets. These are presented in **Appendix B** for comparative purposes.

All of the functional requirements illustrated within the City's cross-sections can be appropriately met within the reduced ROW dimensions discussed herein.

4.1 COLLECTOR/CONNECTOR STREET ROW CHARACTERISTICS:

Promenade Circle – Collector Street

- Ownership: Public Street
- ROW: 24 m Right-of-Way
- Posted Speeds: 30 km/h to 40 km/h
- Pavement width: 10 m including a 3-lane cross-section composed of 3.5 m “thru lanes” and a 3.00 m centre left turn lane.
- Boulevard Elements: pedestrian sidewalks, cycle tracks, landscaping area, setbacks from travelled lanes and from property lines
- Boulevard Element dimensions:
 - Sidewalks – 2.00 metre minimum;
 - Dedicated Cycle Tracks – 1.5 metres
 - Landscaping area – 2.2 metres
 - Setback between travelled lanes and Cycle Track – 1.00 metre
 - Setback between Property Line and Sidewalk – 0.3 metres
- **Comments –**
 - The provision of a 3-Lane cross-section is consistent with the City and their consultants' proposed requirements.
 - The Sidewalk and Cycle Track dimensions meet the typical required dimensions for these elements as evidenced in many urban environments around the Greater Toronto Area.

- The Landscaping area provides for sufficient dimension to accommodate alternating sections of boulevard trees and utility poles/street lighting.
- The narrower ROW relative to the proposed Secondary Plan Collector Street ROW facilitates a more efficient use of land within the Secondary Plan area.

Connector Streets – Collector Streets Linking Promenade Circle to surrounding Public Streets;

- These streets include:
 - East Promenade to Bathurst Street – Two east-west Public streets;
 - South Promenade Connector to Clark Avenue;
 - West Promenade Connector to New Westminster Drive;
 - North Promenade Connector to Centre Street
 - All exhibit signalized connections to the surrounding Public Streets
- Ownership: Public Streets
- ROW: ROW's vary from 26 metres (West Promenade and South Promenade) to 30 metres (the two East Promenade connector streets) to 32 metres (North Promenade);
- Posted Speeds: 30 km/h to 40 km/h
- Pavement width: vary as follows:
 - South Promenade - 16 m existing pavement width retained – 4 lane cross-section composed of 3.3 m “outer lanes” and a 3.00 m “inner lanes” and on-street bike lanes each at 1.7 m in width
 - West Promenade – 14 metre pavement width - 4 lane cross-section composed of 3.3 m “outer lanes” and a 3.00 m “inner lanes”;
 - East Promenade – both E-W connector streets – 5 lane cross-section with centre median – composed of four 3.3 metre lanes and one 3.0 metre lane plus a 1.5 m centre median island;
 - North Promenade – 5 lane cross-section with centre median – composed of four 3.3 metre lanes and one 3.0 metre lane plus a 1.5 m centre median island;
- Boulevard Elements: pedestrian sidewalks, bike lanes (South Promenade), cycle tracks (West, East (x2), North Promenade), landscaping area, setbacks from travelled lanes and from property lines
- Boulevard Element dimensions:
 - Sidewalks – existing (South Promenade) or 2.00 metre minimum;
 - Dedicated Cycle Tracks – 1.5 metres
 - Landscaping area – Existing (South Promenade) to 1.7 m to 2.2 metres
 - Setback between travelled lanes and Cycle Track – 0.5 m to 1.00 metre
 - Setback between Property Line and Sidewalk – 0.3 metres
- **Comments –**
 - The lane requirements on the Connector streets are driven by the functional and operational needs of each street segment, including forecast queuing, lane continuity, and capacity requirements.
 - This is generally consistent with the City and their consultants' assessed requirements.
 - The Sidewalk and Cycle Track dimensions meet typical required dimensions for these elements.
 - The Landscaping areas provides for sufficient dimension to accommodate alternating sections of boulevard trees and utility poles/street lighting.

Future East-West Collector Street linking West Promenade to the southerly East Connector to Bathurst Street

- Ownership: Public Street
- ROW: 20 m Right-of-Way
- Posted Speeds: 30 km/h
- Pavement width: 7.0 m including a 2-lane cross-section composed two 3.5 m “thru lanes”.
- Boulevard Elements: pedestrian sidewalks, cycle tracks, landscaping area, setbacks from travelled lanes and from property lines
- Boulevard Element dimensions:
 - Sidewalks – 2.00 metre minimum;
 - Dedicated Cycle Tracks – 1.5 metres
 - Landscaping area – 2.2 metres
 - Setback between travelled lanes and Cycle Track – 0.5 metre
 - Setback between Property Line and Sidewalk – 0.3 metres
- **Comments** – Sections of this street would be implemented in phases as development proceeds.
 - **The Central Section of this Collector Street** between High Street (Local Street on the east side of the Promenade Shopping Centre building – which is being constructed presently) and a future Private N-S Local Street on the west side of Promenade Shopping Centre building **would only be implemented if the Promenade Shopping Centre building were redeveloped in the long term future. There are no plans to redevelop the Promenade Shopping Centre Building in this area for the foreseeable future.**
 - The westerly and easterly segments of this Public Collector Street, between Promenade Circle and the two Local Private N-S Streets would be implemented as development in those areas proceeds.
 - The narrower ROW relative to the proposed Secondary Plan Collector Street ROW facilitates a more efficient use of land within the Secondary Plan area.

4.2 PRIVATE LOCAL STREETS ROW CHARACTERISTICS

High Street

- Ownership: Private Street – PLP Ownership
- ROW: 20 m Right-of-Way
- Posted Speeds: 30 km/h
- Pavement width: 9.0 m including a 2-lane cross-section composed two 4.5 m “thru lanes” including Sharrow Lane Markings for shared cycling and motor vehicle use.
- Boulevard Elements: pedestrian sidewalks, landscaping area, setbacks from travelled lanes for landscaping with sidewalks extending up to the property lines given the active commercial frontage.
- Boulevard Element dimensions:
 - Sidewalks – 2.00 metre minimum;
 - Landscaping area – varies depending upon commercial frontage and on-street parking locations
 - On-Street Parking – laybys at 2.45 m in width
 - Setback between Property Line and Sidewalk – 0.0 metres given the active commercial frontage and need to accommodate direct access to commercial floor space from High Street.
- **Comments** –

- A Private Local N-S Street on the east side of Promenade Shopping Centre that has been approved as part of the Phase 1 development and is under construction.
- This Private Street would eventually be extended north to intersect with Promenade Circle. Further extension of High Street, as a Public Street, north of Promenade Circle to Centre Street, would be facilitated with development on the Torgan Lands.
- It is anticipated that a similar cross-section would be continued north to Centre Street where it would intersect at a right-in/right-out (RI/RO) intersection and operate under Stop Control in the NB direction.

West N-S Local Private Street

- A Private Local N-S Street aligned on the west side of Promenade Shopping Centre that would intersect with Promenade Circle opposite the North Promenade Connector and extend south to intersect with Promenade Circle between South Promenade and West Promenade.
- Ownership: Private Street – PLP Ownership
- ROW: 18 m Right-of-Way
- Posted Speeds: 30 km/h
- Pavement width: 8.5 m pavement width exhibiting a 2-lane cross-section composed two 4.25 m “thru lanes” including Sharrow Lane Markings for shared cycling and motor vehicle use. Alternative Condition: 6.6 m pavement width with added boulevard features.
- Boulevard Elements: pedestrian sidewalks, landscaping area, setbacks from travelled lanes for landscaping with sidewalks extending up to the property lines given the active commercial frontage. Alternative Condition would also include a two-way cycle track on one side of the ROW and strategically located on-street parking lay-bys.
- Boulevard Element dimensions:
 - Sidewalks – 2.00 metre minimum;
 - Landscaping area – varies depending upon commercial frontage and on-street parking locations
 - On-Street Parking – Intermittent laybys at 2.0 m in width on potentially alternating sides of the Street. Alternative Condition would implement on-street parking on one side of the street in layby segment between landscaping and utility (street lighting, hydrant, etc.) locations.
 - Setback between Property Line and Sidewalk – 0.0 metres given the active development frontages contemplated and need to accommodate direct access to residential / commercial floor space from the Local Street.
- **Comments –**
 - The Private 18.0 metre Private Local Streets would provide direct multi-modal access to development parcels including below-grade parking garages, pu/do activities, strategically located on-street parking, and access to existing Promenade Shopping Centre loading areas.
 - The 18.0 m ROW would appropriately accommodate emergency service accessibility.
 - Dedicated cycling facilities are not required on every street within the network. The proximity of development intensification on the future PLP development lands relative to the dedicated cycling facilities along all surrounding Public Collector street ROW's and the Path network will appropriately link pedestrian and cycling originating within or travelling through the PLP lands to the broader cycling infrastructure planned for the area, in general.
 - However, an Alternative Condition cross-section is possible with this 18 m ROW that could include the following characteristics:

- curb to curb dimensions generally 6.6 m with two travelled lanes one in each direction;
- dedicated cycling facilities within the boulevard – options including:
 - one, 3.0 metre two-way cycling facility on one side of the vehicular travelled section.
- Dedicated sidewalks of between 2.0 metres and 3.0 in width.
- Setbacks between travelled lanes and cycle tracks of 2.4 metres and 1.0 m between Cycle track and the sidewalk;
- 2.4 metre Utility allowance shared with landscaping area (i.e., setback between travelled lanes and the 2-way cycle track.
- On-street layby parking 2.0 m in dimension alternating with landscaped/utility areas.
- The narrower ROW relative to the proposed Secondary Plan Collector Street ROW facilitates a more efficient use of land within the Secondary Plan area.

E-W Private Local Streets

- There are two east-west Private Local Streets that link the north-south segments of Promenade Circle to the two N-S Private Local Streets described above (i.e., High Street on the east side of the Shopping Centre Building and the West N-S Local Private Street on the east side of the Shopping Centre Building)
- Ownership: Private Street – PLP Ownership
- ROW: 18 m Right-of-Way
- Posted Speeds: 30 km/h
- Pavement width: 8.5 m pavement width exhibiting a 2-lane cross-section composed two 4.25 m “thru lanes” including Sharrow Lane Markings for shared cycling and motor vehicle use. Alternative Condition: 6.6 m pavement width with added boulevard features.
- Boulevard Elements: pedestrian sidewalks, landscaping area, setbacks from travelled lanes for landscaping with sidewalks extending up to the property lines given the active commercial frontage. Alternative Condition would also include a two-way cycle track on one side of the ROW and strategically located on-street parking lay-bys.
- Boulevard Element dimensions:
 - Sidewalks – 2.00 metre minimum;
 - Landscaping area – varies depending upon commercial frontage and on-street parking locations
 - On-Street Parking – Intermittent laybys at 2.0 m in width on potentially alternating sides of the Street. Alternative Condition would implement on-street parking on one side of the street in layby segment between landscaping and utility (street lighting, hydrant, etc.) locations.
 - Setback between Property Line and Sidewalk – 0.0 metres given the active development frontages contemplated and need to accommodate direct access to residential / commercial floor space from the Local Street.
- **Comments –**
 - The Private 18.0 metre Private Local Streets would provide direct multi-modal access to development parcels including below-grade parking garages, pu/do activities, strategically located on-street parking, and access to existing Promenade Shopping Centre loading areas.
 - The 18.0 m ROW would appropriately accommodate emergency service accessibility.

- Dedicated cycling facilities are not required on every street within the network. The proximity of development intensification on the future PLP development lands relative to the dedicated cycling facilities along all surrounding Public Collector street ROW's and the Path network will appropriately link pedestrian and cycling originating within or travelling through the PLP lands to the broader cycling infrastructure planned for the area, in general.
- However, an Alternative Condition cross-section is possible with this 18 m ROW that could including the following characteristics:
 - curb to curb dimensions generally 6.6 m with two travelled lanes one in each direction;
 - dedicated cycling facilities within the boulevard – options including:
 - one, 3.0 metre two-way cycling facility on one side of the vehicular travelled section.
 - Dedicated sidewalks of between 2.0 metres and 3.0 in width.
 - Setbacks between travelled lanes and cycle tracks of 2.4 metres and 1.0 m between Cycle track and the sidewalk;
 - 2.4 metre Utility allowance shared with landscaping area (i.e., setback between travelled lanes and the 2-way cycle track.
- The narrower ROW relative to the proposed Secondary Plan Collector Street ROW facilitates a more efficient use of land within the Secondary Plan area.

5.0 SUMMARY

In summary, the combination of the proposed Public Collector and Private Local street system within the PLP Lands and the balance of the Promenade Secondary Plan area can effectively and appropriately accommodate the planned intensification within the PLP Master Plan and the balance of the Secondary Plan lands.

Furthermore, the reduced ROW characteristics incorporated with the PLP Master Plan can be implemented while providing for the appropriately designed and necessary ROW elements. This includes:

- Vehicular travelled lanes of appropriate dimension and configurations;
- Shared cycling/travelled lanes on Local Streets;
- On-street parking and pu/do areas in selected locations on Local Streets;
- Boulevard elements including cycle tracks on Collector Streets, sidewalks, landscaping and appropriate setbacks where necessary.
- Option to include cycling tracks on Local Streets within the boulevard.

The proposed ROW dimensions provide for a well connected and efficient combined Public / Private street network that can accommodate the planned intensification across the PLP lands and the balance of the Secondary Plan area. These proposed ROW's by PLP provide the same basic lane configuration opportunities and functionality. Exceptions to "typical" ROW dimensions would occur at key intersections where operational and functional design conditions may warrant additional width / lanes.

In general, the reduced ROW dimensions set out herein facilitate a greater efficiency in the use of land, given the amount of land within the Secondary Plan area that is dedicated to the retention of the Promenade Mall building as a community asset while also providing a finely grained network of multi-modal streets and paths (as highlighted in the Secondary Plan) and a system of parks and open space. This balance provides for more efficient development parcels that can be readily accessed on a multi-modal basis from an appropriate hierarchy of public and private streets and paths.

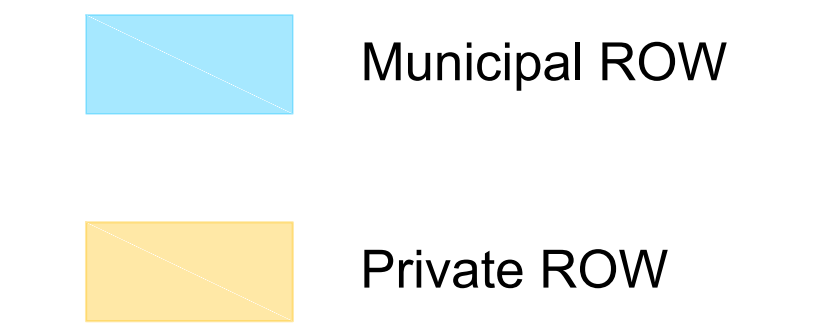
Private Local Street tenure offers considerable support to more efficiently and cost effectively deliver a publicly accessible street system that forms a part of the overall multi-mobility network of streets and paths envisioned within the Secondary Plan area.

APPENDIX A: Functional Design Plans – BA Group

Drawing FD-01, Road Network – Rights-of-Way, June 2021

Drawing FD-02, Road Network – Lane Configurations, June 2021

Drawing TS-01, Typical Cross-Section, June 2021



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MOVEMENT
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Promenade Mall Redevelopment

Master Plan

Road Network - Right-of-Ways

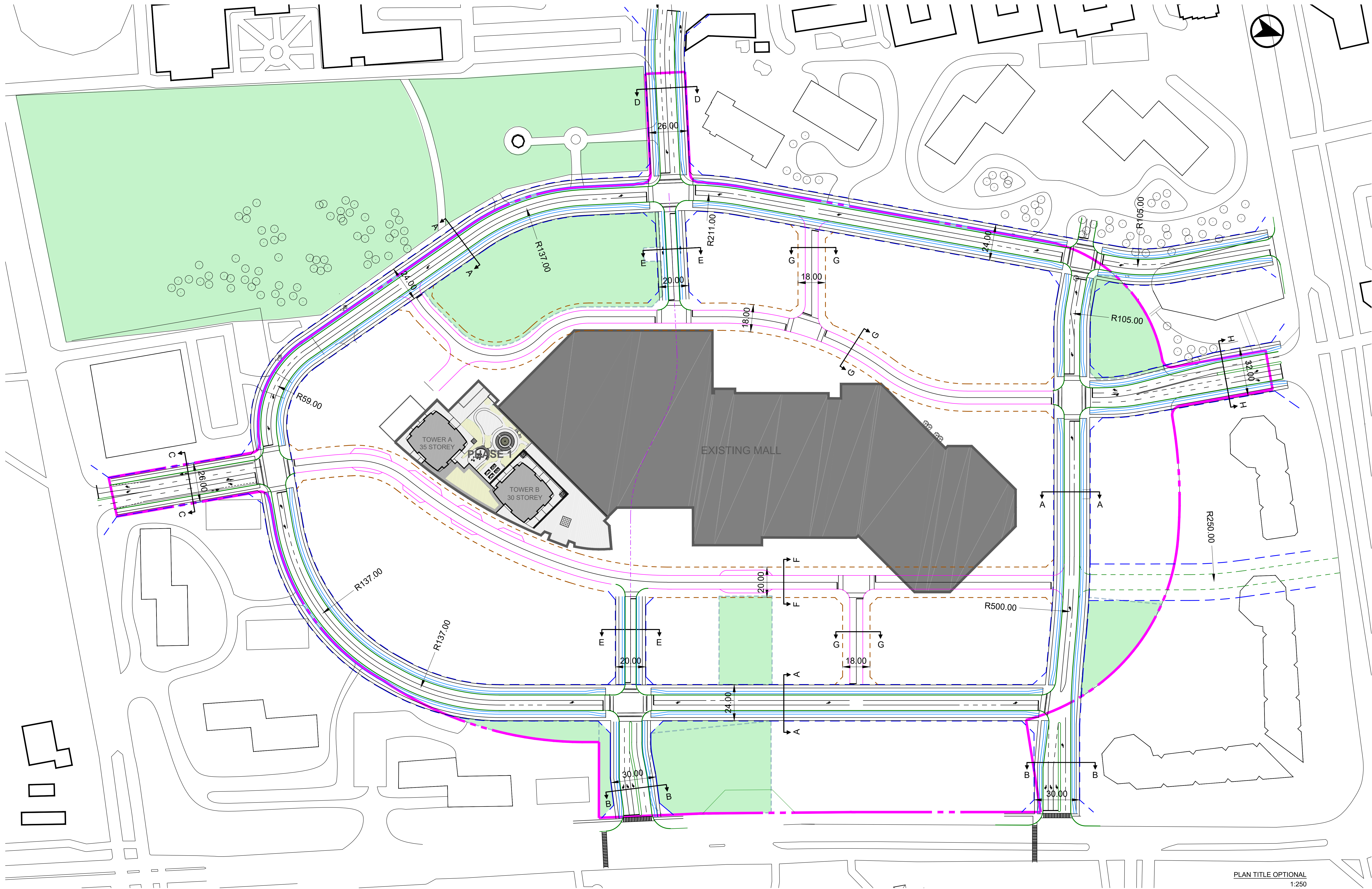
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Project No.: 7411-07

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PLAN TITLE OPTIONAL
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FD-01



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Promenade Mall Redevelopment

Master Plan
Road Network - Lane Configurations

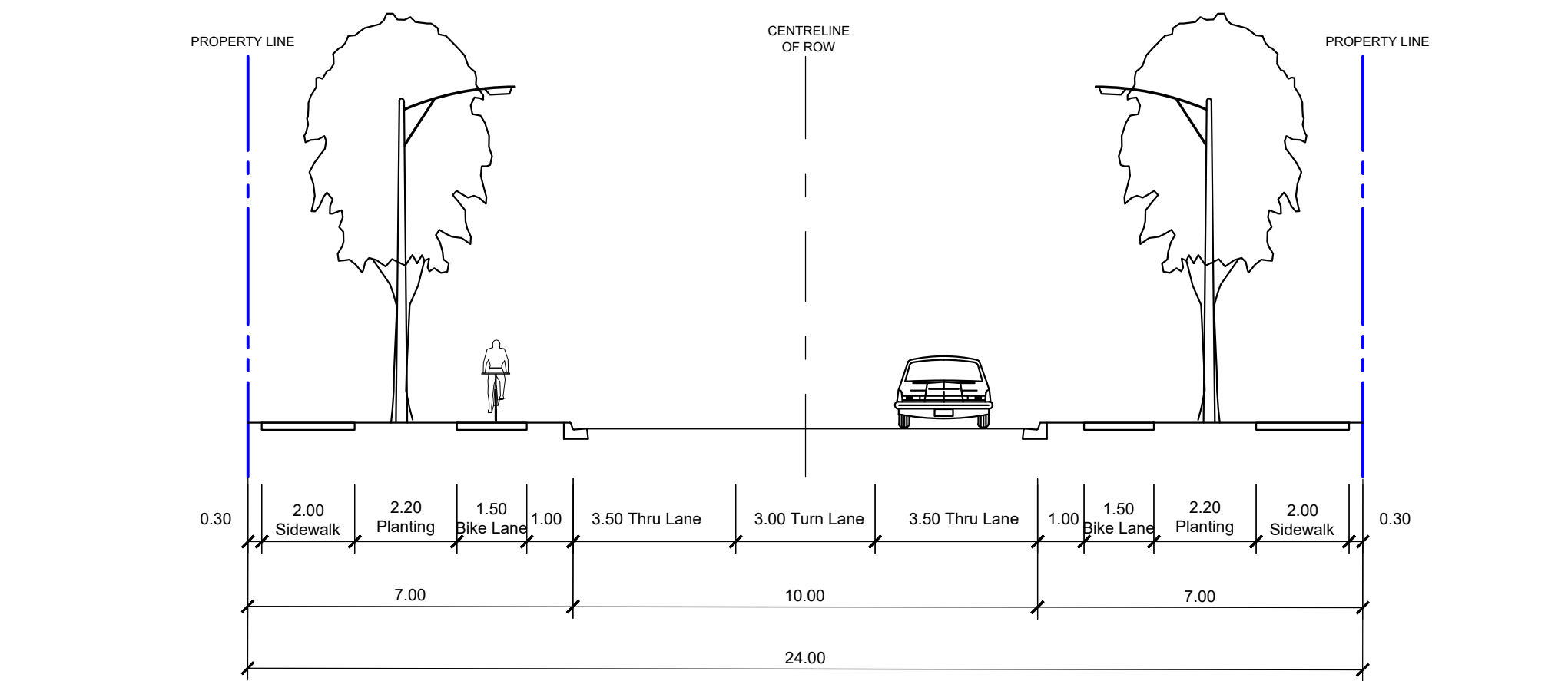
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FD-02

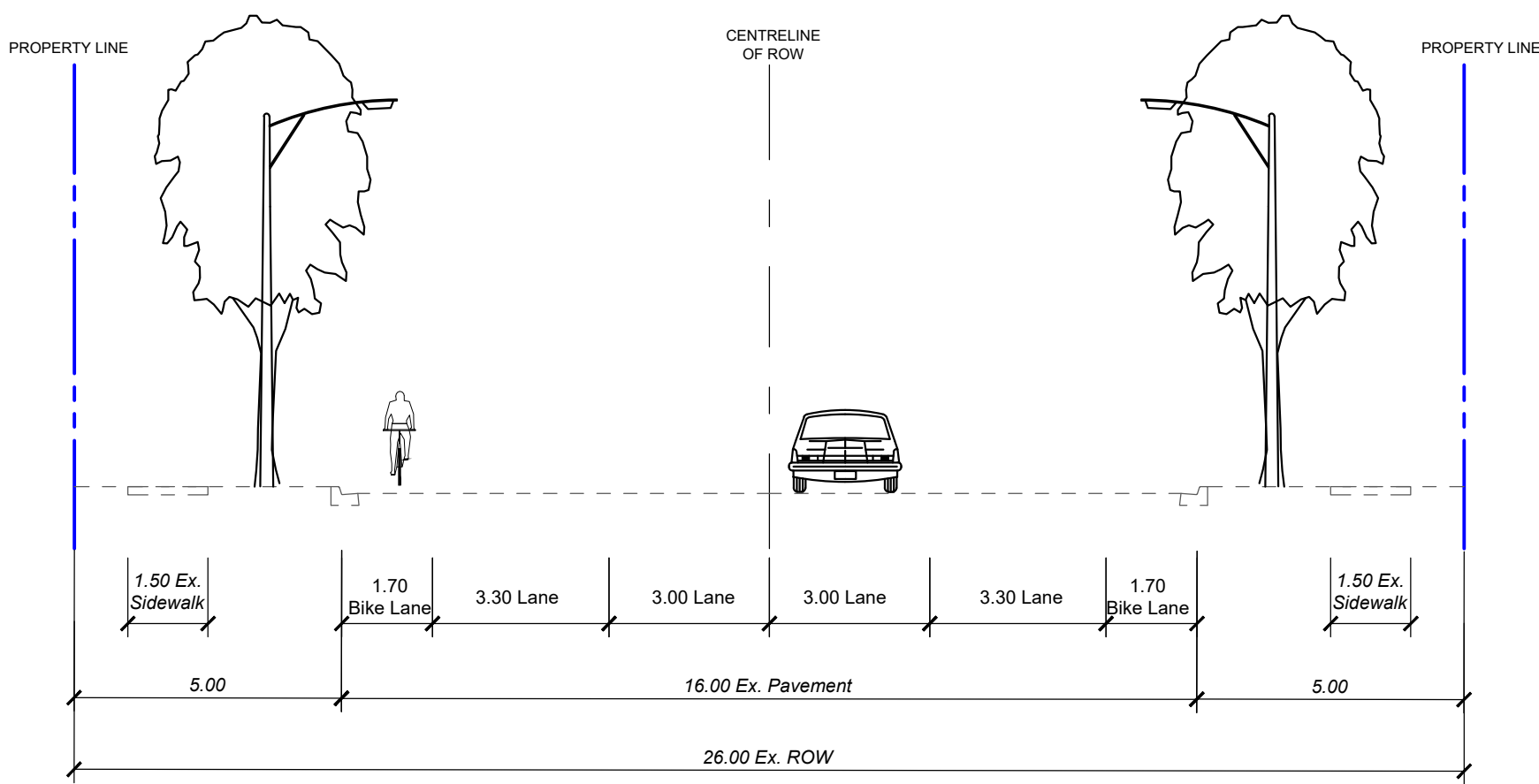
SECTION A -- A
PROMENADE CIRCLE

24.0m ROW - 3 Traffic Lanes & Cycle Tracks



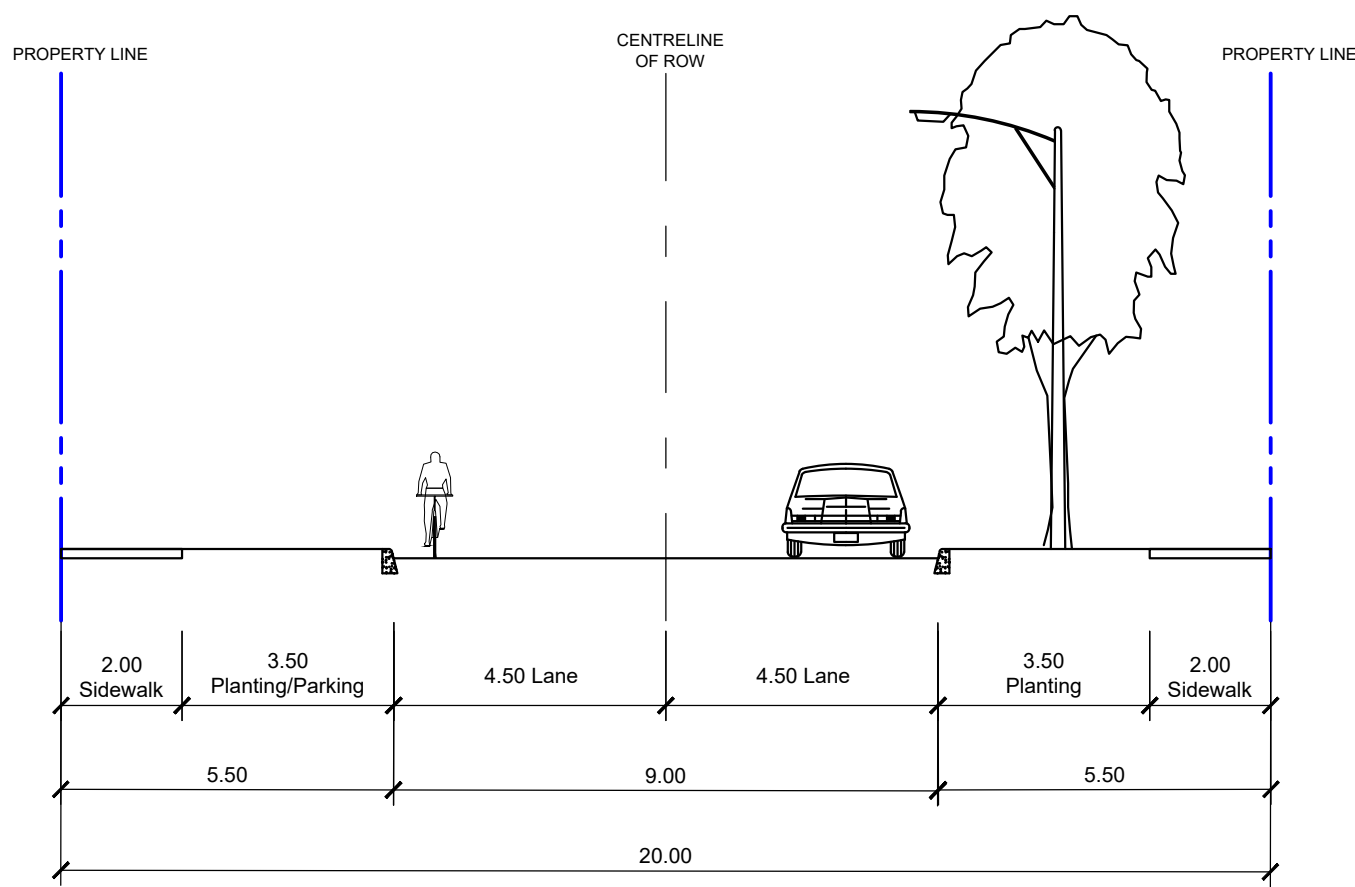
SECTION C -- C
SOUTH PROMENADE

26.0m ROW - 4 Traffic Lanes & Bike Lanes



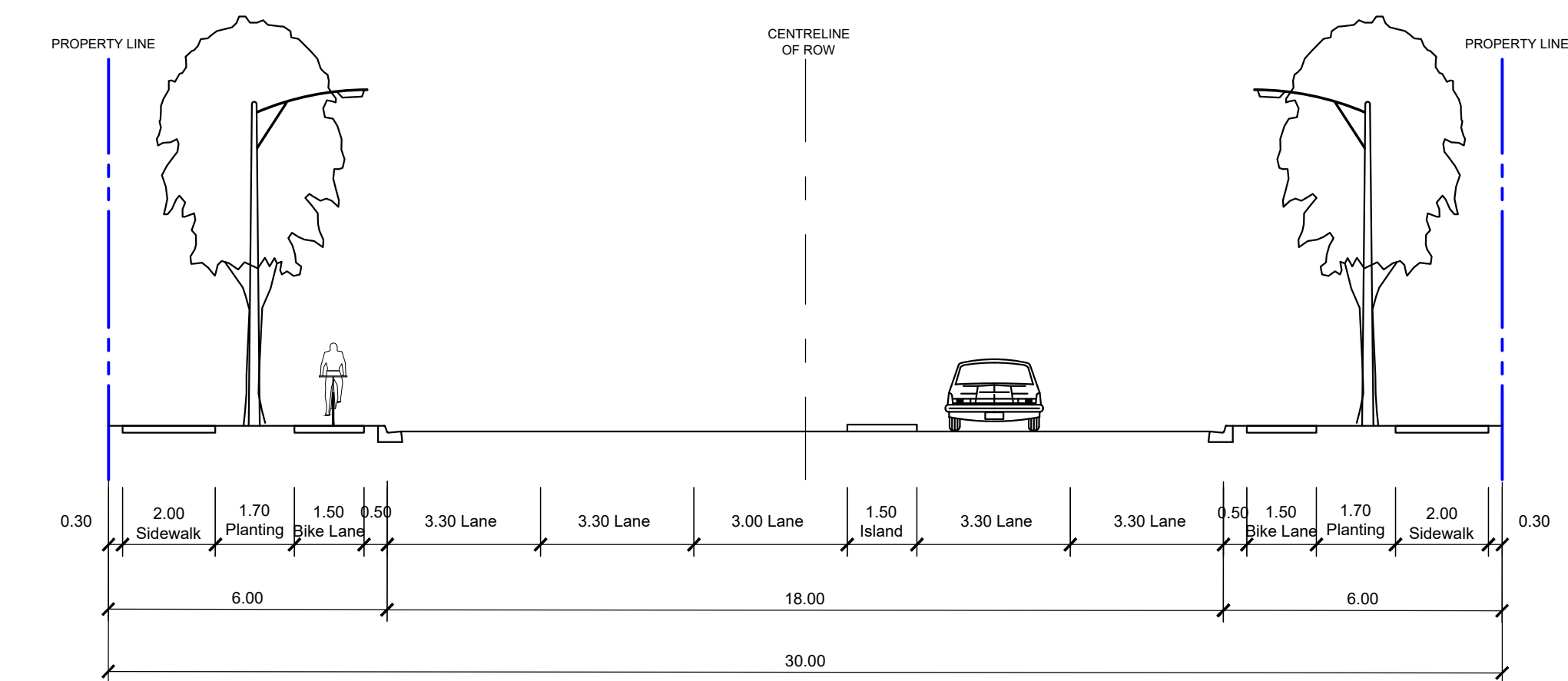
SECTION F -- F
PRIVATE STREETS

20.0m ROW - 2 Traffic Lanes with Sharrows



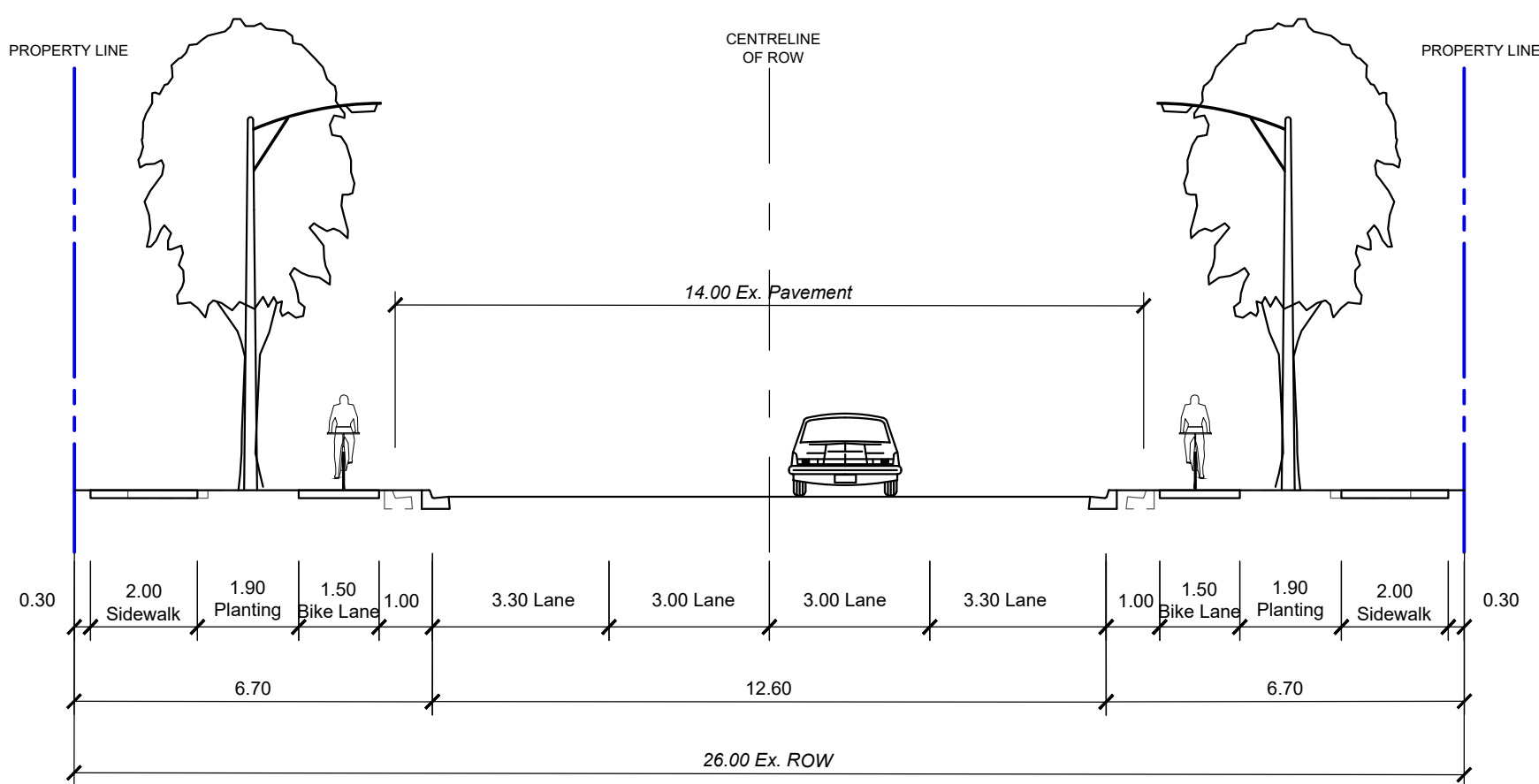
SECTION B -- B
EAST PROMENADE

30.0m ROW - 5 Traffic Lanes & Cycle Tracks



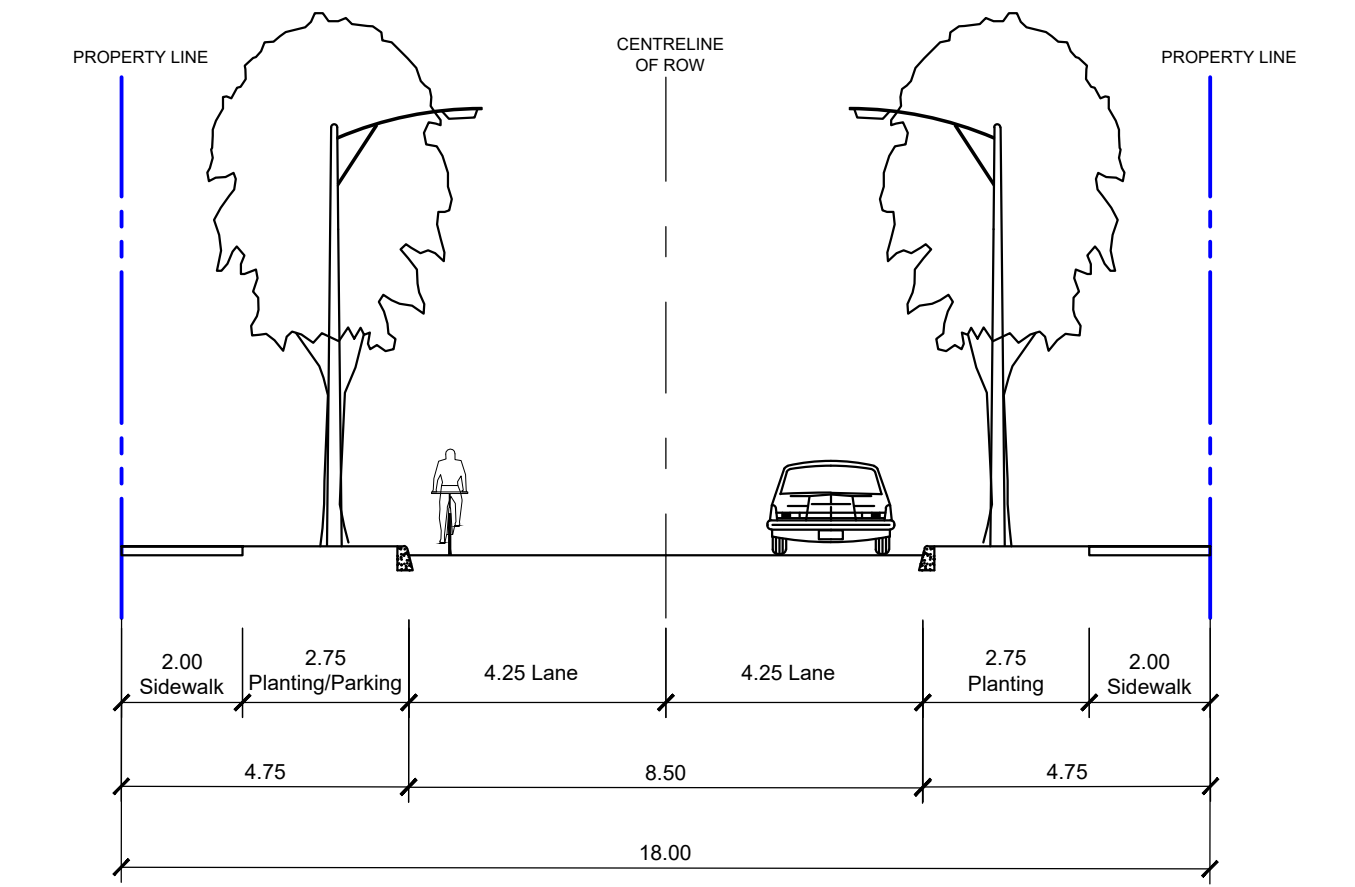
SECTION D -- D
WEST PROMENADE

26.0m ROW - 4 Traffic Lanes & Cycle Tracks



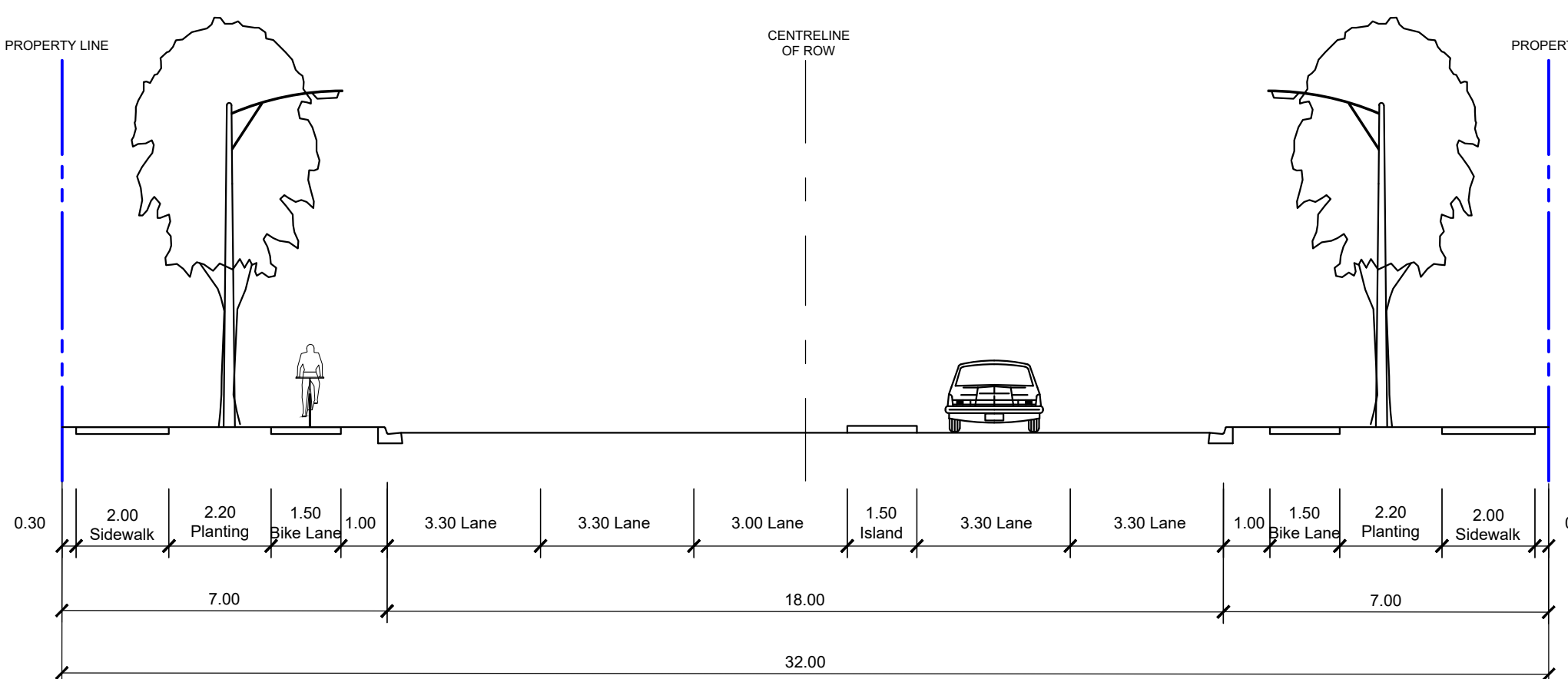
SECTION G -- G
PRIVATE STREETS

18.0m ROW - 2 Traffic Lanes with Sharrows



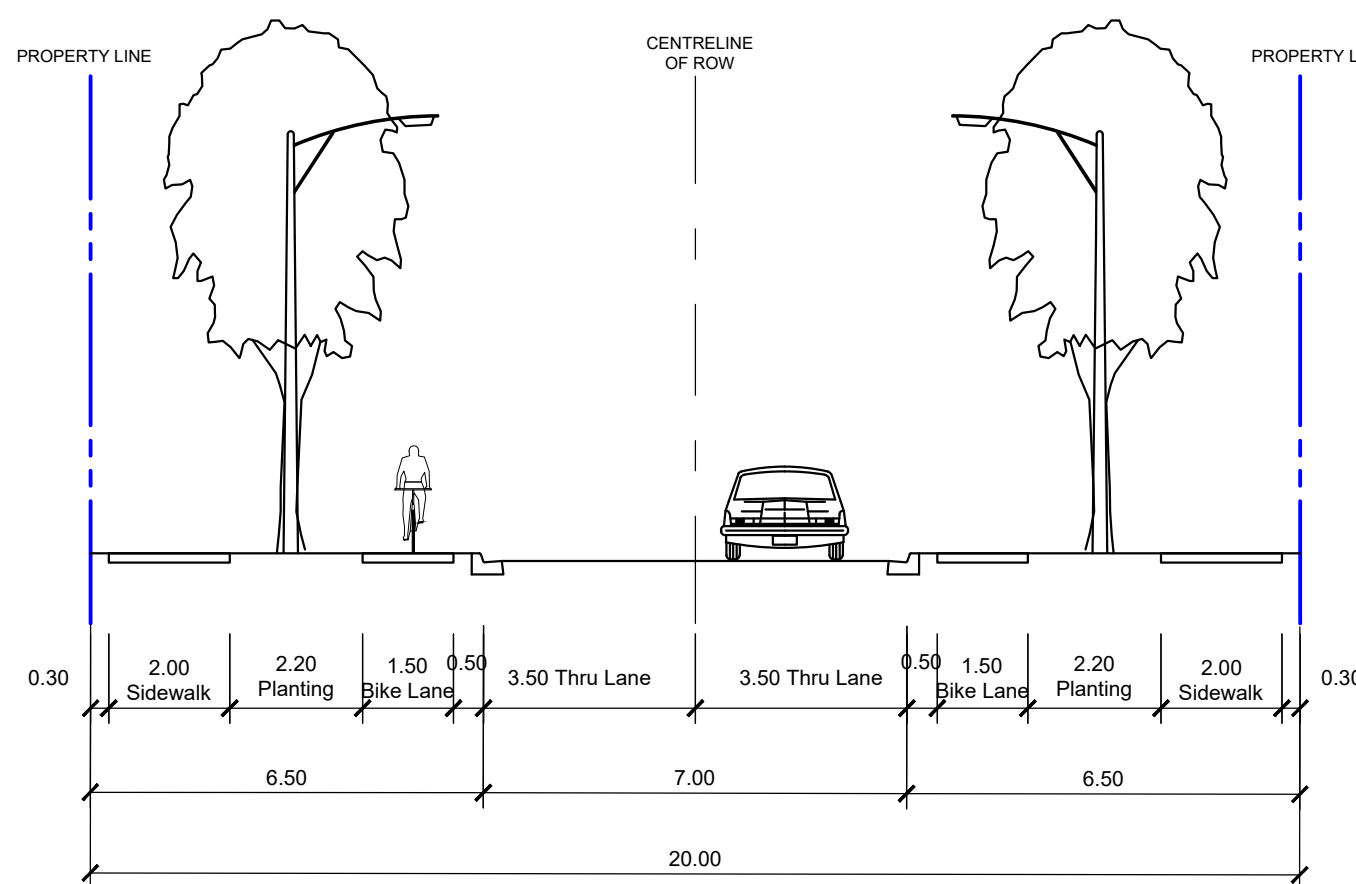
SECTION H -- H
NORTH PROMENADE

32.0m ROW - 5 Traffic Lanes & Cycle Tracks



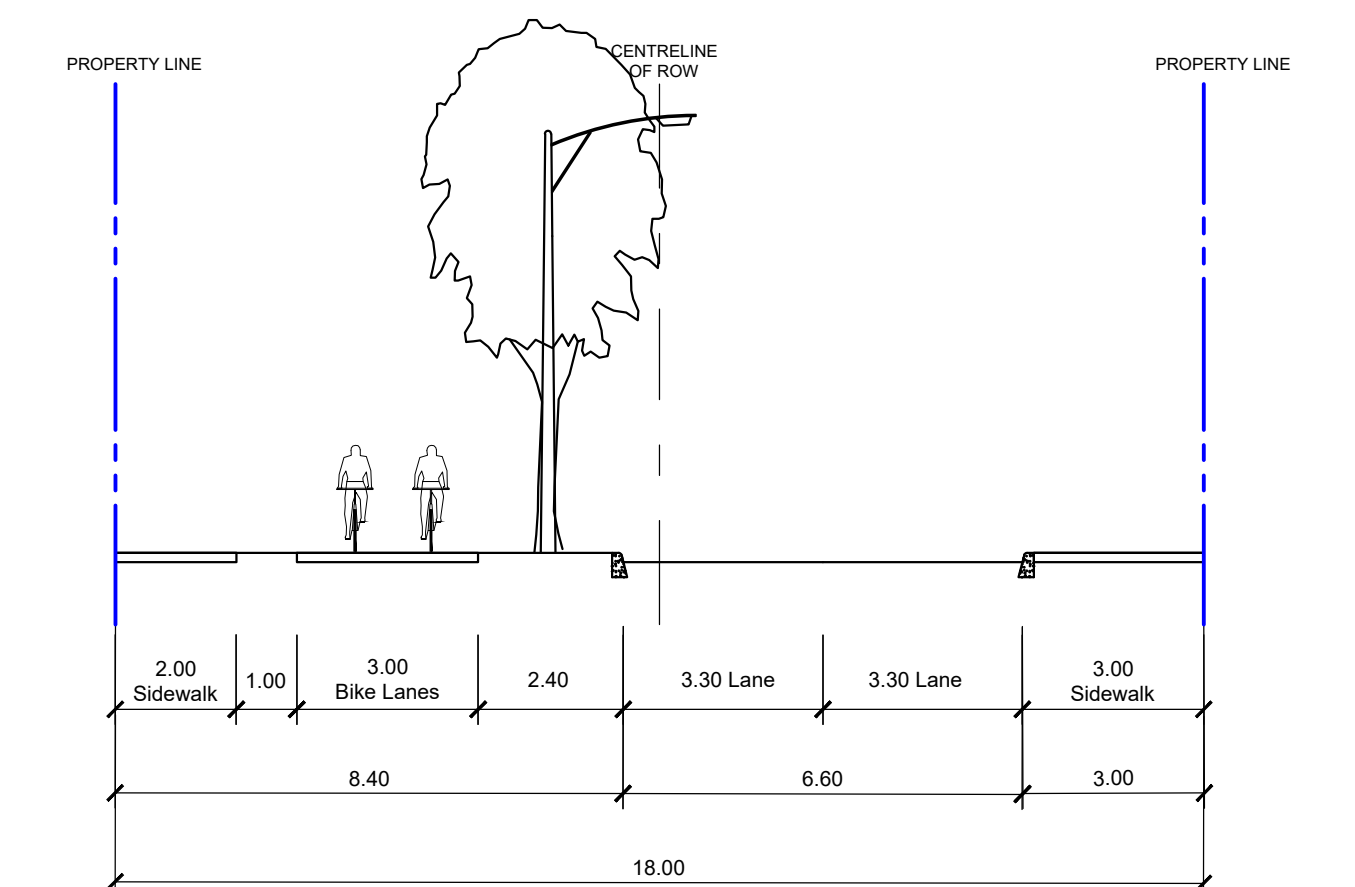
SECTION E -- E
PUBLIC COLLECTOR ROAD

20.0m ROW - 2 Traffic Lanes & Cycle Tracks

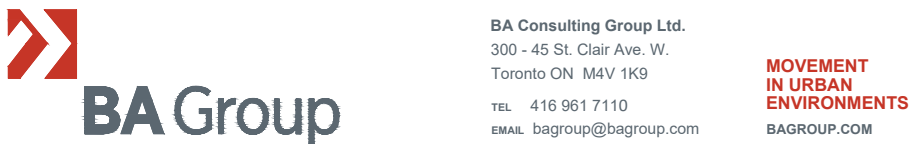


SECTION G -- G
PRIVATE STREETS - ALTERNATE

18.0m ROW - 2 Traffic Lanes & 2-Way Cycle Path



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Promenade Mall Redevelopment

Master Plan

Typical Sections

Date: November 29, 2021

Project No.: 7411-07

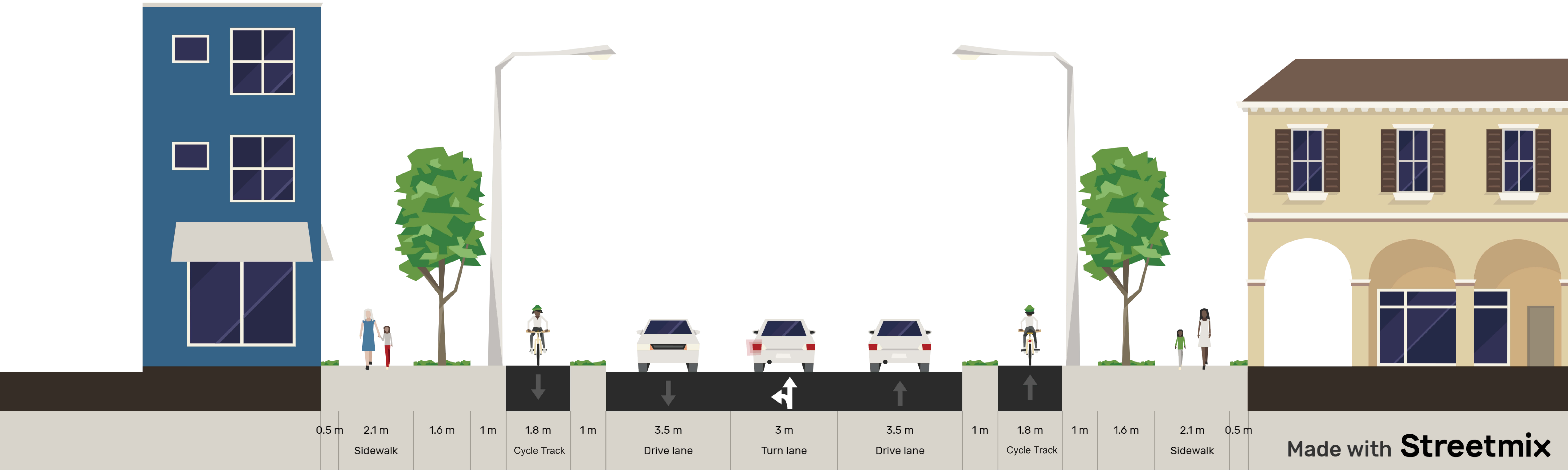
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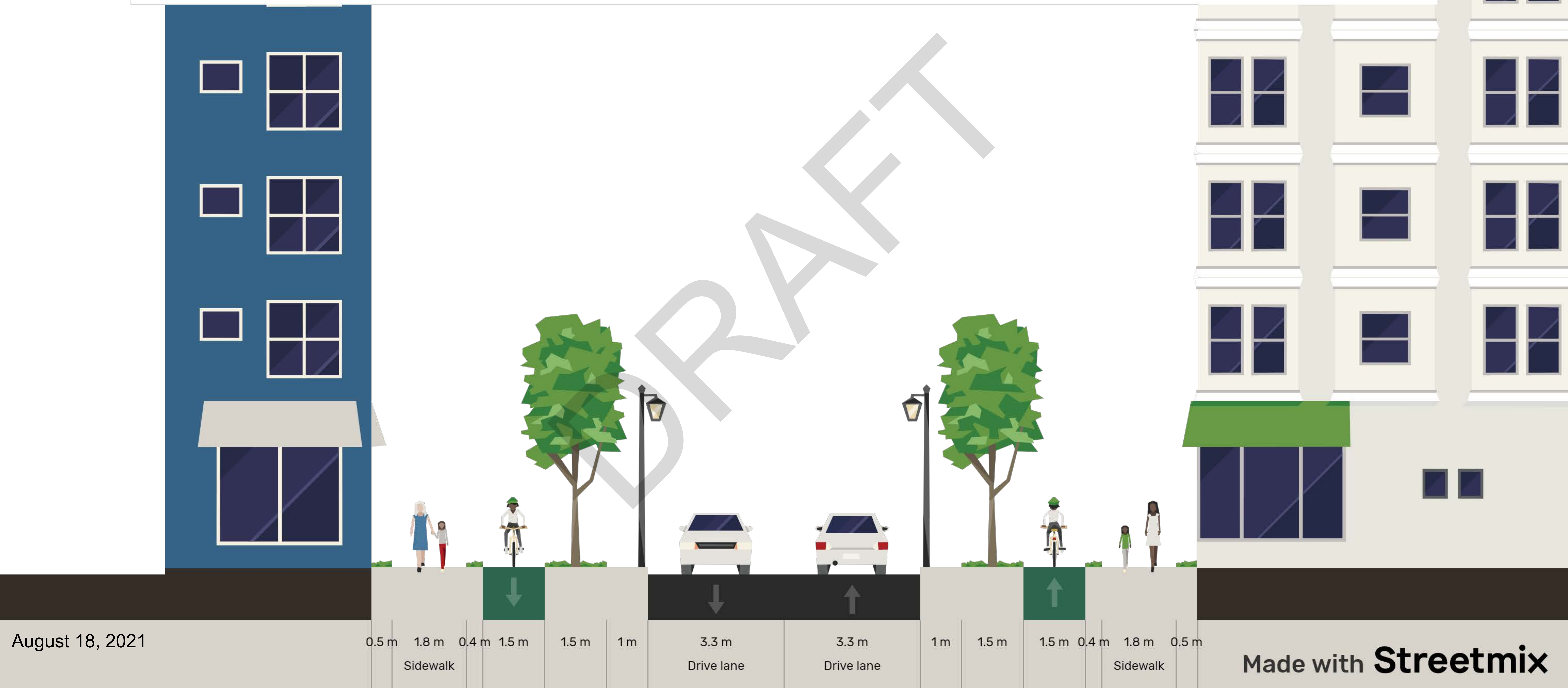
APPENDIX B:

City of Vaughan / HDR Proposed ROW Cross-Sections, August 2021

Collector, 26m ROW (Draft) 2021-03-08



Promenade - 20m Local w/o Parking



Promenade - 22m Local w/Parking One Sid...

