

COMMITTEE OF THE WHOLE (1) – MAY 4, 2021**COMMUNICATIONS****Distributed May 3, 2021**

| | <u>Item(s)</u> |
|---------------------------------------|-----------------------|
| C1. Mr. Dino Risi, dated May 3, 2021. | 7 |
| C2. Mr. Dino Risi, dated May 3, 2021. | 7 |
| C3. Mr. Dino Risi, dated May 3, 2021. | 7 |

Distributed May 4, 2021

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| C4. Kevin Bechard and Hoordad Ghandehari, presentation material. | 1 |
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Please note there may be further Communications.

From: Clerks@vaughan.ca
To: [Bellisario, Adelina](#)
Subject: FW: [External] Fwd: Zoning By-Law Amendment File Z.20.010 & DA.20.015
Date: May-03-21 9:24:04 AM

From: Dino Risi <[REDACTED]>
Sent: Monday, May 03, 2021 8:03 AM
To: Clerks@vaughan.ca
Cc: Dino Risi <[REDACTED]>
Subject: Fwd: [External] Fwd: Zoning By-Law Amendment File Z.20.010 & DA.20.015

My initial opposition to the development.

Thank-you

----- Forwarded message -----

From: DeGasperis, Daniela <Daniela.DeGasperis@vaughan.ca>
Date: Fri, Oct 16, 2020 at 1:27 PM
Subject: Re: [External] Fwd: Zoning By-Law Amendment File Z.20.010 & DA.20.015
To: Dino Risi <[REDACTED]>

Hello Dino,

Thank you for your email.

On September 22, 2020, the applications were heard by the Committee of the Whole for a Public Hearing. The purpose of the Public Hearing was to receive input from the members of the public and the Committee of the Whole on these applications. Please see link which contains a link to the Public Hearing Report which contains additional background information:

<https://pub-vaughan.escribemeetings.com/Meeting.aspx?Id=f8a646fd-4fec-4f82-8a57-da23d8a6c9fc&Agenda=Agenda&lang=English&Item=5>

The applications are still under review by City of Vaughan. Once the review has been completed, the applications will be forwarded to a future Committee of the Whole meeting for a decision. You will also be informed of this meeting via a hearing notice in the mail.

With respect to the specific concerns you raised in your email below, they will be formally addressed in a Technical Report prepared by staff. The Technical Report will be available for viewing online 1 week prior to the future Committee of the Whole meeting date.

If you have any additional questions please do not hesitate to contact me directly.

Thank you and hope you have a nice weekend,

Daniela

Daniela DeGasperis, BUR.PI

Planner

905-832-8585 ext. 8382 | daniela.degasperis@vaughan.ca

City of Vaughan | Development Planning Department

2141 Major Mackenzie Drive, Vaughan, ON L6A 1T1

vaughan.ca

From: "Manocchio, Frances" <Frances.Manocchio@vaughan.ca>

Date: Friday, October 16, 2020 at 8:35 AM

To: "DeGasperis, Daniela" <Daniela.DeGasperis@vaughan.ca>

Cc: "Adamo, Frances" <Frances.Adamo@vaughan.ca>, "Cortese, Marisa" <Marisa.Cortese@vaughan.ca>, "Germano, Derek" <Derek.Germano@vaughan.ca>

Subject: FW: [External] Fwd: Zoning By-Law Amendment File Z.20.010

Morning Daniela!

Please see below and kindly respond.

Thanks!

Frances M

From: Dino Risi <[REDACTED]>

Sent: Friday, October 16, 2020 2:31 AM

To: Bevilacqua, Maurizio <Maurizio.Bevilacqua@vaughan.ca>; Shefman, Alan <Alan.Shefman@vaughan.ca>; DevelopmentPlanning@vaughan.ca; Parks Planning <parksplanning@vaughan.ca>; environmental@vaughan.ca

Subject: [External] Fwd: Zoning By-Law Amendment File Z.20.010

Good morning,

I sent a deposition regarding the Council meeting held September 29th and would appreciate some answers/comments. What I do not understand is that the reviews, studies, plans appear to be finalized and the sales office is up and running prior to the meeting? What was the result of the meeting and how are my concerns being dealt with.

Thank-you in advance

----- Forwarded message -----

From: **Dino Risi** <[REDACTED]>

Date: Mon, Sep 21, 2020 at 7:37 AM

Subject: Zoning By-Law Amendment File Z.20.010

To: clerks@vaughan.ca <clerks@vaughan.ca>, Dino Risi <[REDACTED]>, Dino Risi <[REDACTED]>

I strongly oppose the Re-zoning of C1 and R1V lands to RA3.

My name is Dino Risi and am the owner of the resident at [REDACTED] Helen Avenue. I purchased the property and built my dwelling in Thornhill with the understanding that R1V zoning would not allow the lots to be sub-divided as has been the trend within other residential neighbourhoods and to live in a community with a rural setting as a result of the R1V zoning restrictions. Approval of this zoning will definitely impede my family's privacy and accustomed life style.

My issues of concern are as follows :

1. Re-zoning of C1 to RA3 should not be permitted due to the density increase and the negative effects on the R1V lands. The massing of a 12 story building, including penthouse, adjacent to rural single family dwellings will have substantial impact resulting from noise, traffic, safety, sun/shading constrictions, privacy, visual and de-valuing of the affected properties. Residents in this neighbourhood have settled in R1V area for a reason and pay taxes based on that comfort level.
2. Re-Zoning of the R1V parcel of land should be strictly prohibited nor considered. This parcel sits directly behind my residence in my back yard. My home and family will be surrounded by the activities of this building removing the privacy which we have been accustomed to and subject to the daily activities associated with a commercial/retail and 282 resident dwelling occupants.
3. The rear yard site plan deems a portion of the property as privately owned park which will not serve the community but the building occupants. Park activities will attract people with activities and unwanted noise through-out the day. Of major concern is the access to the building underground parking garage, approximately 343 spaces, is located off Uplands Avenue R1V property. This increase in traffic of vehicles 24 hours a day creating noise, dust, shining headlights directly into my back yard is not acceptable. Not to mention the deliveries for the commercial stores along Yonge street and the garbage disposal noise. I experience it presently on a limited basis but can imagine the timing and frequency of garbage disposal for commercial/retail tenants and 283 dwelling units. The access from Uplands will definitely reduce any rear yard living activities which my family should be entitled to and in my estimation, will have the same vehicular traffic as Yonge street.
4. The building will definitely affect my exposure to sunlight through-out the year, I have had a chance to review the shadow study and do not find them accurate. The earliest model is indicated at 9:18 am which is not indicative of the sunrise in the summer. This building, 38.55 metres in total height, over 126 feet, towers over all dwellings located in R1V lands. I would like council to review the section elevation as prepared by Constantine/Truelife Urban Design Brief dated July 20, 2020 on page 50 which clearly indicates the mass of the building in relation to my residence depicted I assume, to scale. Does that indicate a relationship between Zoning parcels which are deemed to be in character with the neighbourhood. My privacy has been taken away from my family due to the cascading terraces abutting my residence.

5. Review of the site grading plan also indicates that the rear and east property lines have a 3:1 slope towards my property. I already have issues with a rear yard neighbour which adjusted his grades which now affect my rear yard.
6. As for construction, the lower the density the lesser the impact. This building will require at least 24-36 months to complete. This extended construction time-line will cause havoc on traffic, increased unwanted noise, dust and pollution, a negative visual impact to the neighbourhood, safety concerns, construction deliveries off hours, increase parking on Helen and Uplands Ave. to name a few of the more obvious. To note, the location of the water holding tank is indicated to be placed directly behind my rear yard. I understand that dewatering systems will require 24 hour pumps to be active and removal of said water from the tanks will entail untimely discharge from tanker truck and constant noise on a basis as dictated by the aquifer strength.
7. Shoring diagrams provided by Terraprobe, drawing SH-2 indicates shoring along the east property line probably for a duration of 12 months. Section 5/SH5 indicate a tie back encroachment of 16,200 mm underneath my residence. This installation and demobilization duration will also have a noise, visual and vibration impact subject to damages.

In closing, I strongly object to this densification proposal due to the impact this will have on my family and lifestyle. I chose to reside in R1V area specifically to avoid exactly this massing of dwelling units adjacent and in the rear of my property. I believe the City of Vaughan also taxes me accordingly but now it seems the taxes received from the Constructor and 282 proposed family units are appealing. This proposal will definitely de-value my property and I will be prepared to hold the City of Vaughan accountable, retain proper council and prepare for an appeal to the Local Planning Appeal Tribunal.

Trusting this will be taken into consideration,

Dino Risi
■ Helen Avenue
Thornhill ONT
■

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From: Clerks@vaughan.ca
To: [Bellisario, Adelina](#)
Subject: FW: [External] Re: 8188 Yonge Street Proposal
Date: May-03-21 11:07:30 AM

From: Dino Risi [REDACTED] >

Sent: Monday, May 03, 2021 11:05 AM

To: Shefman, Alan <Alan.Shefman@vaughan.ca>; Bevilacqua, Maurizio <Maurizio.Bevilacqua@vaughan.ca>; daniela.desgasperis@vaughan.ca; Clerks@vaughan.ca

Subject: [External] Re: 8188 Yonge Street Proposal

Revise

1. 5 Helen should be 5 Uplands.

Thank-you

On Mon, May 3, 2021 at 11:00 AM Dino Risi <[REDACTED]> wrote:

Gentlemen,

This density and type of development adjacent to RV1 zoning properties is totally unacceptable to myself and the community. We attended one meeting years ago regarding a proposal and were never notified nor invited to discuss nor provide input to the development ever again. We received a notice of a "new" proposal only after the City and Owners had already determined the density, height and site plan of the condominium structure with zero consideration of the neighbourhood. Shortly thereafter, signage for the sales office was erected. This is democracy?

I have provided notice of my objection and concerns to no avail. I did receive

a

partial explanation back on November 20/2020 and provide the following comments:

1. 5 Uplands Avenue was zoned RV1 in conformance with the neighbourhood. Once you rezone the property to allow services and driveways to support the commercial development, it is considered to be Commercial property, period.

Review of the site plan clearly indicates that a portion of 5 Helen Ave. will be used as the building footprint which does not conform to the LPAT decision rendered on October 7 2019. Only a portion has been allocated to park space.

Please refer to the site plan and landscape drawings provided. If it is classified as privately owned publicly accessible open space, what by-laws are applicable to 5 Uplands Ave.

2. You indicated that the Vaughan Official Plan 2010 speaks to angular planes. Unfortunately, review of the building section provided by Constatine clearly shows that the 45 degree angular plane falls within my property. Interesting that the paragraph I was sent refers only to mid rise buildings which abuts the rear yards, my dwelling is a side yard condition.

How has the development maintained the Vaughan Official Plan..."in order to provide appropriate privacy"...when the units all have terraced balconies overlooking my home and rear yard. Was that the intent of providing privacy to the residents?

3. As for the overall height and density, I do not agree that the land area of 5 Uplands should be used for the permitted floor space Index. It is either low rise residential, RV1 or commercial C1. Therefore, the GFA should be reduced with emphasis on the height restriction.

The building is in fact 11 stories high including a Mechanical penthouse which renders the building 12 stories high, not 10 stories as noted on the OBC matrix.

In conclusion, I am extremely disappointed in the manner the City of Vaughan failed to notify and listen to the residents of the neighbourhood. Development is necessary but an eye sore adjacent to RV1 is not the answer. Personally,

I believe that access to the building from Uplands Avenue will be highly congested with vehicles and delivery trucks.

The entrance to the parking structure for the 343 plus vehicles will point directly at my rear yard elevation. In the evening, there will be a constant parade of head lights and noise. Delivery and disposal vehicles are also to use this access, both in and out, causing unwanted noise and exhaust fumes. I can only assume that disposal trucks will operate at all hours of the evening and early morning.

The parkette also incorporates a dog park....I was visited numerous times by the by-law officer due to a neighbour complaint of my dog barking. Now, I have to endure a dog park?

A park also will also invite the public to gather and make use of the facilities increasing the noise level factor and decreasing my privacy.

This development has surrounded my property, home, family and privacy and the City of Vaughan has let this occur. The goal and intent of building my home which I designed in Thornhill RV1 zoning was to be near the Thornhill CC and allow my family to enjoy the rural surroundings. This space and lifestyle has been taken from me and my family due to the inconsideration decisions made by the officials in collaboration with the developers.

Lastly, the value of my property will and has been greatly affected by this development. I will seek further council on this matter and proceed accordingly.

Thank-you,

Dino Risi

From: [Dino Risi](#)
To: [Bellisario, Adelina](#); Clerks@vaughan.ca; [Bevilacqua, Maurizio](#); [Shefman, Alan](#)
Subject: [External] 8188 Yonge Street
Date: May-03-21 12:22:56 PM

And as a final note, will this set a precedent for the same thing to happen to 5 Helen Avenue when that property is destined to be developed? I will definitely become landlocked between the two condos?

Thank-you



COMMITTEE OF THE WHOLE

TOWNHOUSE DEVELOPMENT
300 ATKINSON INC.

MAY 4TH, 2021
CITY OF VAUGHAN

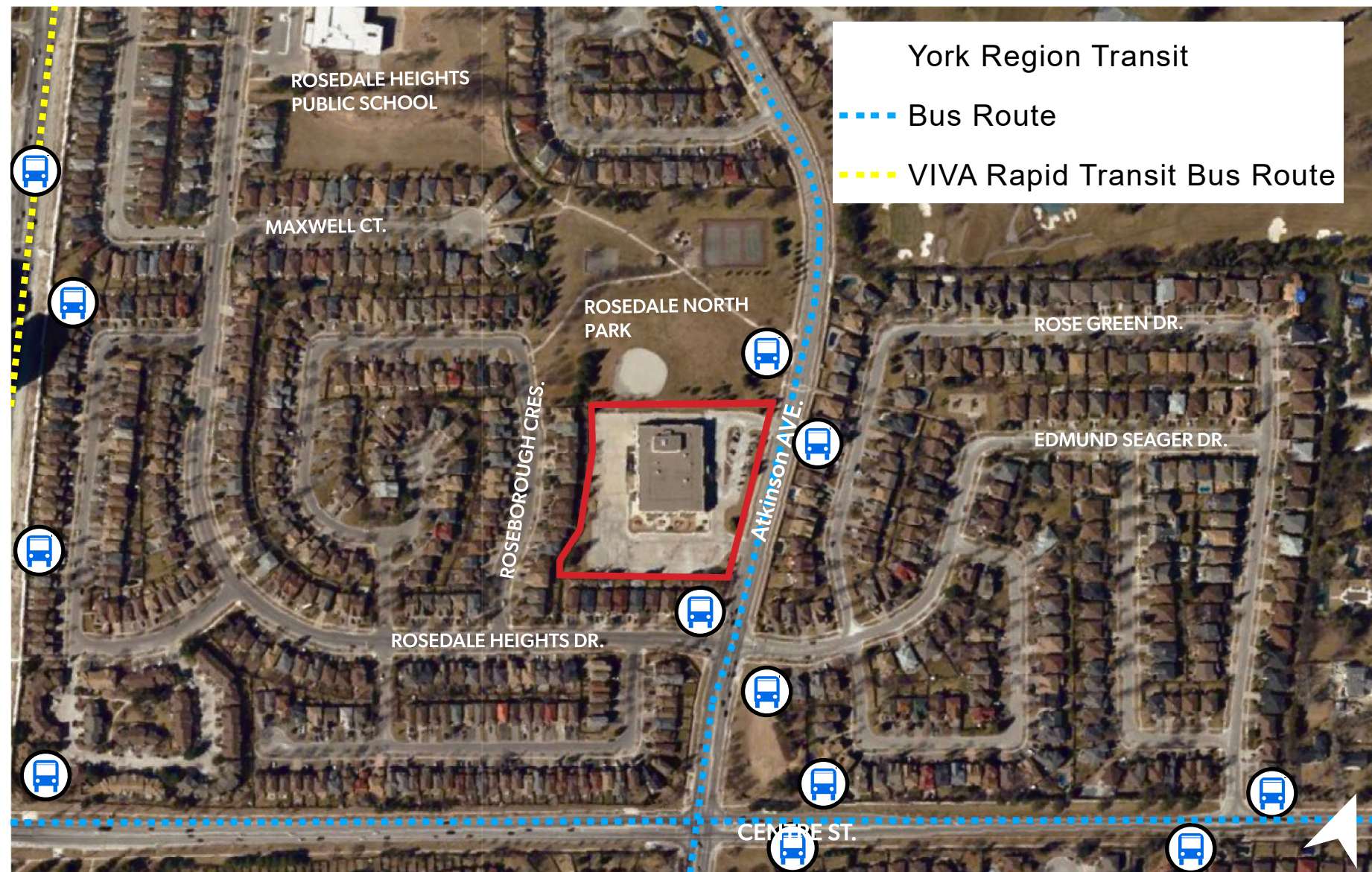
OFFICIAL PLAN AMENDMENT (FILE OP. 19.011)
ZONING BY-LAW AMENDMENT (FILE Z.19.002)
SITE DEVELOPMENT (FILE DA.19.083)
RELATED FILES (Z.19.028)



300 Atkinson Inc. Development Proposal



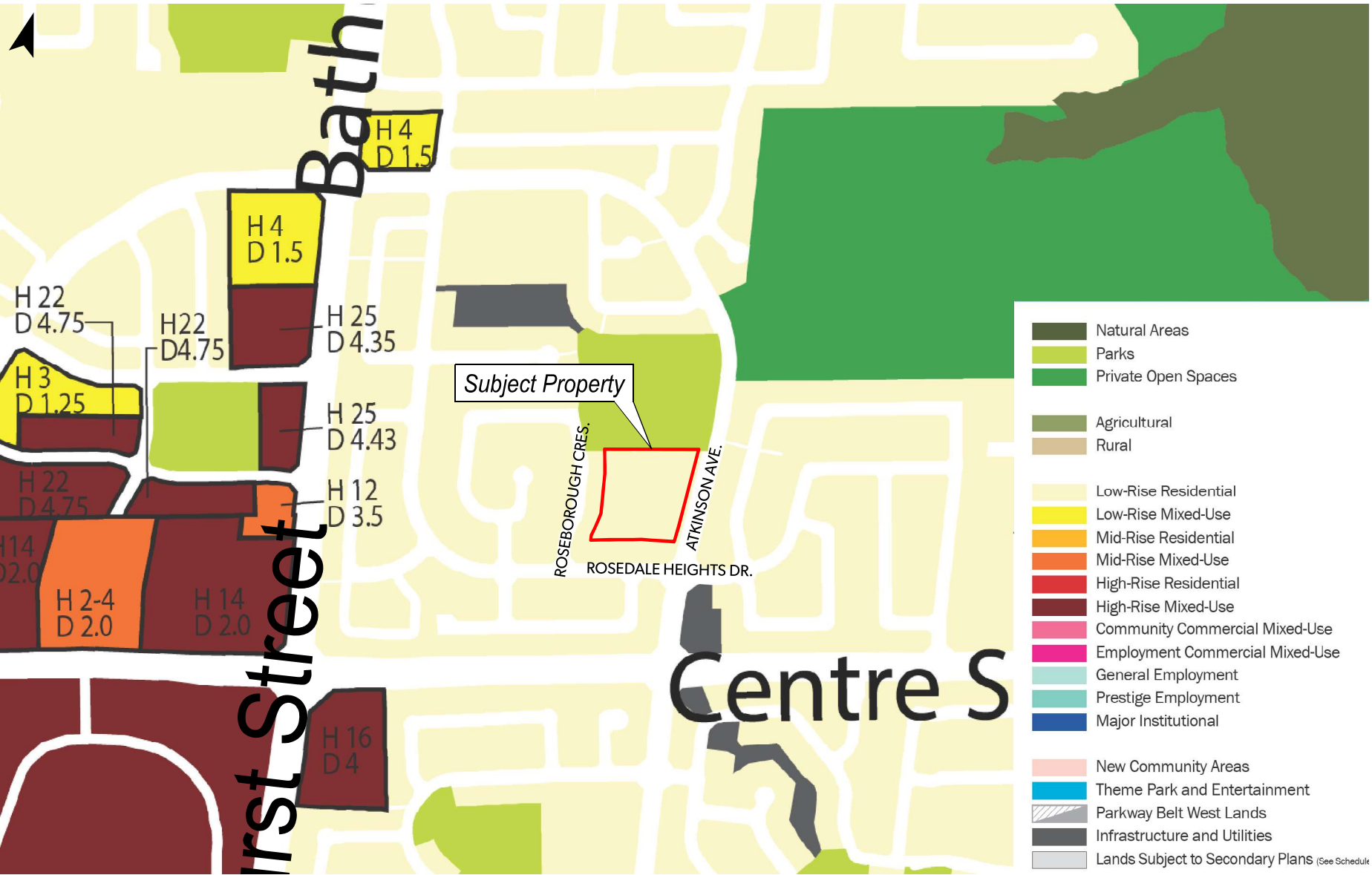
Subject Lands



Aerial Image of the Subject Site

- Site area - 1.7 ha (4.2 acres)
- A school building occupies the site and is surrounded by parking and outdoor play areas
- Frontage of 140 m along Atkinson Avenue
- Change in grade from west to east
- Existing vehicle access from Atkinson Avenue
- Abuts Rosedale North Park to the north
- Serviced by a number of local YRT transit stops along Atkinson Avenue, with higher order facilities along Bathurst Street, Centre Street and at Promenade Mall

Policy Framework Review - Vaughan Official Plan



Subject property designated “Low-Rise Residential”, by VOP 2010 which permits low-rise residential and community uses, including:

- Residential units (Detached Houses, Semi-Detached Houses, Townhouses)
- Public and Private Institutional Buildings

Townhouses are permitted in the Low-Rise Residential designation, back-to-back townhouses are not explicitly recognized in the VOP 2010

Official Plan Amendment submitted to recognize back-to-back townhouses

Maximum permitted height in Low-Rise Residential designation is 3 storeys

Consistent with the policies and direction of the PPS

Land Use Schedule

Policy Framework Review - Zoning By-law 1-88



Zoning Map

- Subject property zoned “R3 – Residential”
- Zoning By-law Amendment submitted to rezone the subject lands from “R3 Single Family Detached Dwelling” to “RM2 Apartment Residential Zone”.
- Site specific provisions to facilitate a residential development comprised of 15 blocks of traditional and back-to-back townhouse units.

Site Plan and Statistics

Site Plan Statistics

- Total GFA: 18,183.70 m²
- Net FSI: 1.12
- Net Coverage: 5,963.70 (36.71%)
- Maximum Height: 12.91 m (3 storeys)
- Total Units: 114

Parking Required

- Resident (1.5/unit): 171 spaces
- Visitor (0.2/unit): 23 spaces
- Total: 194 spaces

Parking Proposed

- Resident: 222 spaces
- Visitor: 26 spaces
- Total: 248 spaces

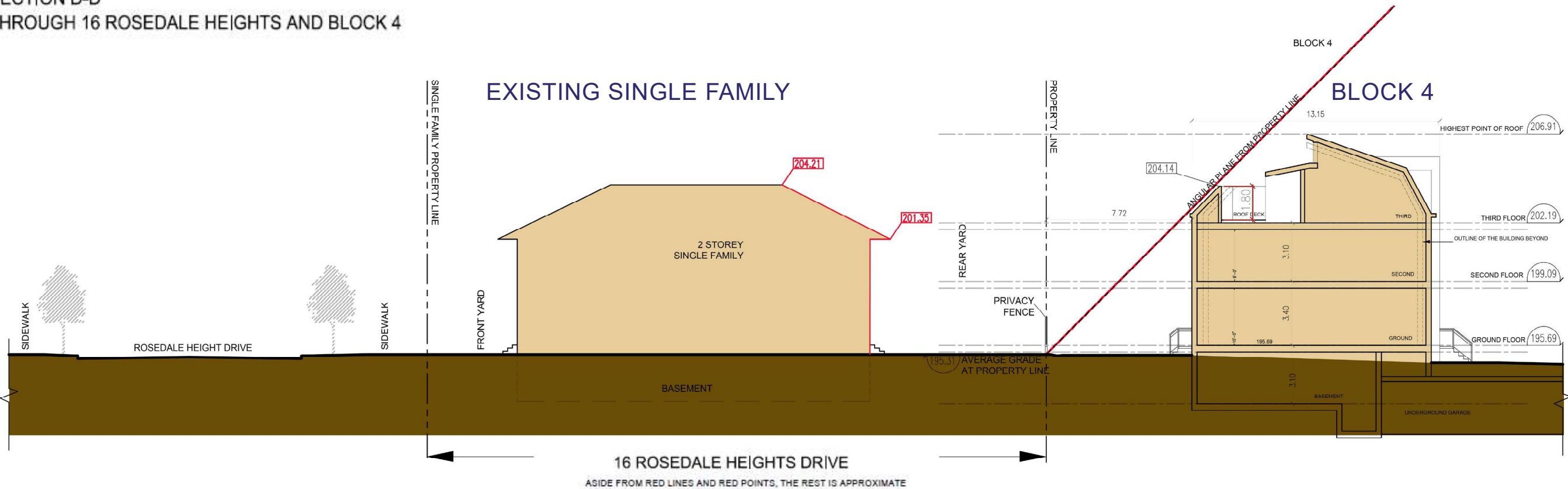
Regular Townhouses

B2B Townhouses



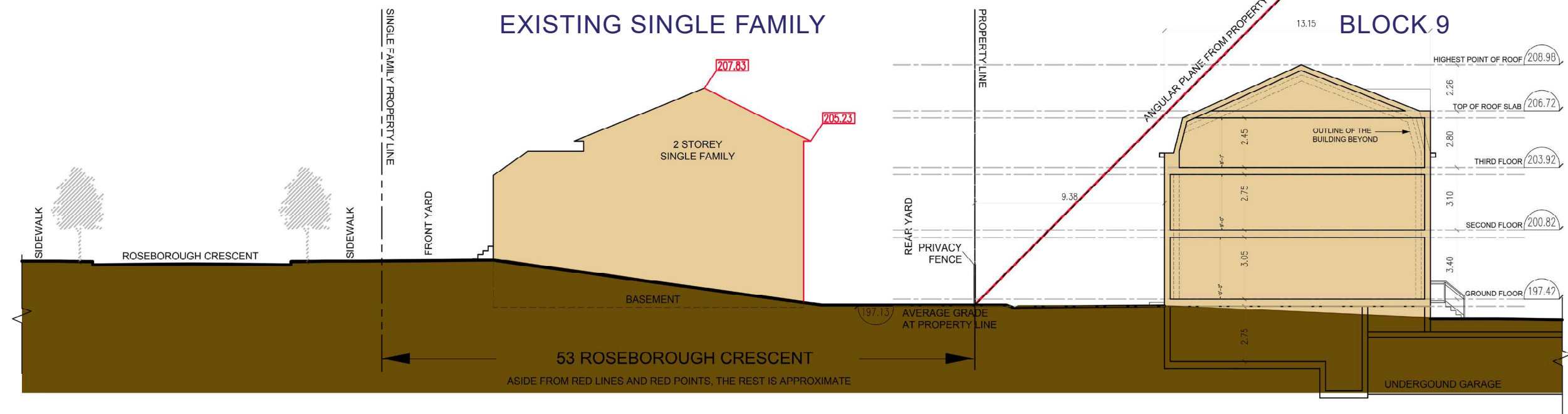
Cross Section - South Side (Flanking Rosedale Heights Drive)

SECTION D-D
THROUGH 16 ROSEDALE HEIGHTS AND BLOCK 4



Cross Section - West Side (Flanking Roseborough Cres.)

SECTION A-A
THROUGH 53 ROSEBOROUGH CRESENT AND BLOCK 9



Elevation



300 ATKINSON AVENUE, ON

WESTON
CONSULTING



ICON
ARCHITECTS

Bird's Eye View



Thank You
Comments and Questions?