



**COMMITTEE OF THE WHOLE (PUBLIC HEARING) – JULY 7, 2020**

**COMMUNICATIONS**

**Item**

**Distributed July 2, 2020**

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| C1 | Ms. Sonya Fiorini, La Neve Ave, Woodbridge, dated June 23, 2020  | 4 |
| C2 | Ms. Andrea Caruso, dated June 22, 2020                           | 3 |
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| C4 | Mr. Gaetano Franco, Castlepoint Investments, dated June 25, 2020 | 4 |
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| C8  | Mr. Robert Piecuch, dated July 5, 2020.                       | 4 |
| C9  | Rose, Stephen, and Joseph Mignogna, dated July 3, 2020.       | 4 |
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**Distributed July 7, 2020**

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Please note there may be further Communications.

## COMMUNICATION – C1

### ITEM 4

Committee of the Whole (Public Hearing)

July 7, 2020

City of Vaughan  
Office of the City Clerk  
2141 Major Mackenzie Dr.  
Vaughan, ON L6A 1T1  
[clersk@vaughan.ca](mailto:clersk@vaughan.ca)

June 23, 2020

### **Re: 9630 Islington Inc. – Written Comments Prior to Public Hearing July 7<sup>th</sup>, 2020**

Dear Office of the City Clerk,

I am writing regarding the notice of public hearing for the zoning by-law application submitted by 9630 Islington Inc. to construct a 5-storey residential apartment building with 89 units and one level of underground parking.

I have resided in the City of Vaughan for more than 20 years, currently a resident of the Sonoma Heights area at Islington and Major Mackenzie since 2010. I have strong concerns regarding this development for several reasons detailed below:

- Traffic and congestion – The intersection of Islington and Napa Valley is very congested as it stands currently. Adjacent to 9630 Islington, is a very busy plaza at all times of the day and evenings
- Recent approved developments already in the area - Across the street from 9630 Islington, is another approved development which will add even more residents and traffic to the area (OP.16.010 two 6-storey apartments)
- Capacity of the proposed 5-storey apartment - This is simply too many people for what this space can accommodate. 89 units is a lot of people being added to this area from one apartment complex in what is already a tight space
- Conservation and natural habitat - The area has a lot of conservation land and located within the greenbelt. Wildlife depends on conservation and dedicating parts of the city to its preservation. One of the many reasons residents live here is because of that natural beauty. Adding more concrete buildings does not allow for nature to thrive and conflicts with “Green Direction Vaughan” plan which is supposed to guide the community to a more sustainable future by addressing environmental, cultural, social, and economic values

[https://www.vaughan.ca/cityhall/environmental\\_sustainability/GreenDirections/Pages/Our-Green-Direction.aspx](https://www.vaughan.ca/cityhall/environmental_sustainability/GreenDirections/Pages/Our-Green-Direction.aspx)

My primary concern is that this development will lead to more subsequent development and this area is already too congested as it is. We do not want to be another HWY 7 and Weston Road.

One of the reasons I choose to live in this neighbourhood was that it was away from traffic and somewhat quiet. There are already new townhome developments being constructed down the road on Islington and Sonoma Heights and another across the street from this proposed one. I am afraid this intersection will be over capacity and seriously conflict with the way residents of this community interact on daily basis. This development will cause a lot of extra traffic in an intersection which already has traffic today.

The notice of public hearing did not specify what the applicant is proposing to give back to the community or how to mitigate some of the adverse impacts this development would undoubtedly cause overall and too the natural environment. First and foremost, the City of Vaughan must protect its residents and have their best interests in mind.

Thank you for the opportunity to share my viewpoints during this important process. I have spoken to many of my neighbours about this who also feel the same as I do. I hope they participate by voicing their concerns and taking the time as I have done to write a letter. As a proud resident of Vaughan, I will be extremely upset should this development be approved.

Sincerely,

Sonya Fiorini

■ La Neve Ave, Woodbridge, ON

COMMUNICATION – C2  
ITEM 3  
Committee of the Whole (Public Hearing)  
July 7, 2020

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**From:** Andrea Caruso <Rockinfitmomma@outlook.com>  
**Sent:** Monday, June 22, 2020 8:06 PM  
**To:** Clerks@vaughan.ca  
**Subject:** [External] Belmont properties file z.20.003

Hello,

We would like to formally dispute this zoning application. I would definitely like to make a deputation for the meeting as we are strongly against this property being approved. This is directly behind our property which will lose value if this subdivision is put up, will invade our privacy not to mention invade our backyard. All of our neighbors we have spoken to are also angry that this is now being changed from a commercial site to residential. This is not what we were told was being built and will harshly affect our family home and any possibly to resell. Please advise how I can get my voice heard at this meeting as I will not sit aside and let this monstrosity be built in my backyard! I will do whatever it takes to stop this from happening.

Andrea Caruso  
Certified STOTT Pilates instructor  
Canadian Yoga Alliance IR Yoga instructor  
Fitness instructor & Personal Trainer  
Total Barre instructor  
Diet and lifestyle coach  
Belly dance performer and instructor  
416-561-0651  
[Rockinfitmomma@outlook.com](mailto:Rockinfitmomma@outlook.com)  
@rockinfitmomma  
@rockandrollingpin1

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**From:** Amir Farag [REDACTED]  
**Sent:** Wednesday, June 24, 2020 2:18 PM  
**To:** Clerks@vaughan.ca  
**Subject:** [External] Zoning By-Law Amendment File Z20.001 - comment

Dear Sir,

I'm writing to you to raise my objection to the planned development of 5 storey residential apartment building (zoning by law Amendment file Z20.001).  
As a resident in [REDACTED] San Vito drive I don't like to have such building that doesn't match up to the area as the community is only houses.

Thank you,  
Amir Farag  
[REDACTED] San Vito Drive, Vaughan [REDACTED]  
Phone No: [REDACTED]

**COMMUNICATION – C4**  
**ITEM 4**  
**Committee of the Whole (Public Hearing)**  
**July 7, 2020**

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**From:** Gaetano Franco <gaetano.franco@castlepointinvestments.ca>  
**Sent:** Thursday, June 25, 2020 12:00 PM  
**To:** Clerks@vaughan.ca  
**Cc:** 'Tom Davies (tdavies@tldspc.ca)' <tdavies@tldspc.ca>; 'MAURICE STEVENS' (mstevens90@hotmail.com) <mstevens90@hotmail.com>  
**Subject:** [External] Notice of Public Hearing Zoning By-Law Amendment File Z.20.001

Good Afternoon,

Our office has received the attached notice of public hearing for the above noted matter.

The subject property is located within the Woodbridge Expansion Area Landowners Group, as such, any approvals should include a condition of approval requiring the applicant to obtain clearance letter from the Woodbridge Expansion Area Landowners Group Trustee such the landowners group financial requirements can be satisfied.

Please continue to notify our office of any information and approvals related to this file.

Thank-you

**COMMUNICATION – C5**  
**ITEM 4**  
**Committee of the Whole (Public Hearing)**  
**July 7, 2020**

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**From:** liae [REDACTED] >  
**Sent:** Sunday, June 28, 2020 11:48 AM  
**To:** Clerks@vaughan.ca  
**Subject:** [External] File Z.20.001

Hi, I am opposed to the zoning bylaw amendment regarding the proposed apartment building development at the northwest corner of Islington and Napa Valley. I have lived in the area for almost 20 years and have seen a steady increase in terms of noise and congestion. I think the proposed development would make a bad situation worse. I believe that the city should buy the land from the developer and create a green space that would provide an environmental balance to the busy plaza across the street.

Thank you.

Sent from my Samsung Galaxy smartphone.

**COMMUNICATION – C6**  
**ITEM 3**  
**Committee of the Whole (Public Hearing)**  
**July 7, 2020**

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**From:** Elle Volanis [REDACTED] >  
**Sent:** Monday, June 29, 2020 12:47 PM  
**To:** Clerks@vaughan.ca  
**Subject:** [External] File: Z.20.003

Hello,

I am writing in regards to my concerns on File: Z.20.003 zoning bylaw amendment.

I am a resident in Vaughan and there is a public hearing that would effect myself and all neighbouring houses that back on to the property that is requesting a zoning by-law amendment. This property was zoned as commercial and now requesting to be residential.

This is not a request against the applicant to not build homes - This is simply a request that the city does not allow or extend their request to build taller than 11.3 Meters. 12 meters is an additional 4 feet towering over our neighbourhood. The back of our homes on Wardlaw Place appear as semi detached homes (they're Linked townhomes). These large stacked townhomes are going to look like a lowrise apartment building in our yards obnoxiously taller than our homes and it is not fair.

Properties (Weston) Inc. should present the council with past and current developments that show their heights of structures. They should figure out a plan to fit within the building guidelines.

They are proposing that the structures will be 11.3 Meters but are requesting 12 meters.... Approval height is clearly what they are after so they can do as they please later... This is not okay and giving them a full meter is like turning a blind eye.

This impacts all homes that back onto the lot. We are residence, homeowners, pay property taxes and don't want a visually noticeable obstruction to view. There are 29 houses that have been here since 2011 and by you amending the zoning height by 4 feet for 30 new townhomes you are implying that these homes are more important than ours. It is okay for their property to have an increased value and to depreciate our homes by what we will be faced with in our yards. The developer is already requesting to change from commercial to residential. The least they can do is follow some rules and guidelines to keep the citizens who have been living here happy.

To be clear I am not opposed to having the lot go from commercial to residential. I am requesting that they do not get approval for 12 meters. If their proposal for the structures is 11.3 Meters which is a request for one foot. No one will be against 1 foot difference so I believe that is what the extension should be. They would never request that much more without the intention of using it once you have approved it.

With the city of Vaughan being in phase two and a lot of things have reopened with measures, I also feel like city hall should be open for proper discussion on this matter.

Thank you,

Elle  
[REDACTED]

**COMMUNICATION – C7  
ITEM 4  
Committee of the Whole  
(Public Hearing)  
July 7, 2020**

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**From:** A F [REDACTED]  
**Sent:** Monday, June 29, 2020 9:40 PM  
**To:** Clerks@vaughan.ca  
**Subject:** [External] 9630 Islington Inc.

In response to the 5-story residential apartment building, my family and many of my neighbours do not want it constructed. It would be an eye-sore for many of us who live just off of Napa Valley and wish that the plaza next to the construction zone does not become surrounded with residential apartments in the future. Especially with how many elderly we have, as it would be healthier to leave them clearer horizons. The plaza is also a big hangout place for teenagers and large buildings surrounding it will heavily change the atmosphere.



Virus-free. [www.avast.com](http://www.avast.com)

**C8**  
**COMMUNICATION**  
**CW (Public Hearing) - July 7, 2020**  
**Item # 4**

**From:** [Robert Piecuch](mailto:Robert.Piecuch@vaughan.ca)  
**To:** [Clerks@vaughan.ca](mailto:Clerks@vaughan.ca); [Robert Piecuch](mailto:Robert.Piecuch@vaughan.ca)  
**Subject:** [External] Zoning By-Law Amendment File Z.20.001  
**Date:** July-05-20 10:57:53 AM

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**I oppose the proposed zoning by-law change to the existing property for a 5 Story 89 Unit apartment complex.**

1. No where around the area including down Major Mackenzie to Weston Rd. have any 5 story apartments been built with such a small property foot print.
2. 89 units are proposed which leads me to believe they will be on average approx. 500 sqft. which will mean these units will be **bought for investment purposes and rented which will lead to airbnb rentals and parties and noise.** City of Vaughn should have a system in place that if an apartment or condominium is **bought it cannot be rented for 3 years** this will allow serious home buyers a chance to purchase something and not pay extreme rent.
3. **Vaughn zoning cannot enforce noise complaints and loitering at the shopping plaza 9600 Islington Ave.** so how will Vaughn enforce complaints in regards to the proposed apartment.
4. Napa Valley and Islington Ave. will become more congested due to the possible 89 cars trying to exit the apartments, the intersection already has congestion during rush hours this will just add more gas to the fire. I also can see people living in the apartment that don't have enough parking for their additional vehicles, family and friends visiting parking on Napa Valley, Islington Ave. and surrounding neighborhoods. The possible parking on Napa Valley with restrict the movement of Emergency Vehicles to access the area in the event of an emergency.
5. A Front Loader Garbage Truck will have to turn into head on traffic on Napa Valley to turn into the tight entrance way and stay in his lane in according to the highway traffic act. The drawing shows the truck turning into the exit lane of the apartments entrance, I would like to know if Vaughn approves the proposed apartment based on these drawings will **Vaughn be held liable** in the event of accident due to the garbage truck having to use opposing traffic lanes.
6. Has a study been done of the shadow / blockage of natural southern sunlight effect on the houses to the rear of the proposed apartment building. Have these citizens / neighbors been contacted in person or contacted by registered mail prior to the proposed design by the Developers or the City of Vaughn, it appears that a design was completed prior to zoning by-law change and citizens in the neighborhood only get informed 2 weeks before a public hearing via mail in which some citizens didn't receive the notification.

**In my opinion if the City of Vaughn approves this zoning proposal without taking in consideration of the existing neighborhood conditions and sets a presentence with a 5 story building on such a small foot print with no green space it is surely for GREED.**

C9  
**COMMUNICATION**  
**CW (Public Hearing) - July 7, 2020**  
**Item # 4**

**From:** [Clerks@vaughan.ca](mailto:Clerks@vaughan.ca)  
**To:** [Bellisario, Adelina](#)  
**Subject:** FW: [External] 9630 Islington Avenue  
**Date:** July-06-20 9:35:36 AM

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**From:** Rose Mignogna [REDACTED]  
**Sent:** Friday, July 3, 2020 4:34 PM  
**To:** Clerks@vaughan.ca  
**Subject:** [External] 9630 Islington Avenue

To whom it may concern,

My name is Rose Mignogna and we had received a letter regarding a Zoning By-law Amendment Z.20.001. Our family has been living in the area for 20 years and each year the area has slowly been changing with new construction projects which bring in more and more people, more congestion, and fewer green spaces, which affects the well-being of all residents including me and my family.

It is a shame that such a beautiful and safe area has changed so much and these new projects are obviously not for the well-being of the residents of the community but only benefit large building companies and bring in an excessive amount of congestion. The intersection between Napa-Valley and Islington is already congested enough between the fast-food restaurants like Tim Hortons and McDonald's as well as Food Basics and it is even difficult to navigate in that specific intersection of Napa Valley and Islington.

There is already a new construction of townhomes down the road at Sonoma and Islington which are not even completed yet. Once all these residential construction projects are completed and life resumes back to normal when Covid-19 eventually slows down, what will be of this area?

Again, I find this very disappointing and I oppose to see the area in which we live become a congested cosmopolitan zone. Between more traffic, more people, especially with all the units being built, I find this very upsetting.

So in that note, my family and I oppose to the building of these units. Many of my neighbors whom I have spoken to are very dissatisfied as well and we urge that this project does not go through and gets denied as soon as possible. We live in a beautiful area, it will be a shame to see all of these new residential buildings. This is a big mistake. The only person who benefits from residential projects like these is the builder, not the residents of the area.

We oppose, please take our letter into consideration.

Thank you,

Rose, Stephen, and Joseph Mignogna

at [REDACTED]. Woodbridge, Ontario [REDACTED]

**C10**  
**COMMUNICATION**  
**CW (Public Hearing) - July 7, 2020**  
**Item # 4**

**From:** [Clerks@vaughan.ca](mailto:Clerks@vaughan.ca)  
**To:** [Bellisario, Adelina](mailto:Bellisario, Adelina)  
**Subject:** FW: [External] Fw: Zoning By-Law Amendment File Z.20.001  
**Date:** July-06-20 9:36:33 AM

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**From:** Carella, Tony <Tony.Carella@vaughan.ca>  
**Sent:** Friday, July 3, 2020 6:07 PM  
**To:** Joe Cirasella <[REDACTED]>; Clerks@vaughan.ca  
**Cc:** Cardile, Lucy <Lucy.Cardile@vaughan.ca>  
**Subject:** Re: [External] Fw: Zoning By-Law Amendment File Z.20.001

Joe, by copy of your email to the City Clerk, your comments become part of the public record on this file. Thank you for registering your concerns.

Sent from my iPhone

On Jul 3, 2020, at 5:11 PM, Joe Cirasella <[REDACTED]> wrote:

Sorry Tony please see below

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**From:** Joe Cirasella  
**Sent:** July 3, 2020 6:06 PM  
**To:** [tonycarella@vaughan.ca](mailto:tonycarella@vaughan.ca) <[tonycarella@vaughan.ca](mailto:tonycarella@vaughan.ca)>  
**Subject:** Zoning By-Law Amendment File Z.20.001

Hi Tony,  
Thank you for calling me today, your quick response was greatly appreciated.  
I am very concerned about the building proposal at 9630 Islington Ave.  
I feel that with the building going up on the east side of Islington, this new building and the plaza there will be a great deal of traffic and havoc. The following points should be carefully researched.

1. Traffic
2. Entrance and exit being from Napa Valley will cause a great deal of traffic with the entrance of the plaza being so close
3. Noise garbage pickup, deliveries entrance and exit to underground parking. Also the entrance should not be on Napa Valley.

4. Littering. The garbage from the plaza who by the way do a very poor job in maintaining it clean. Garbage from the plaza is everywhere. This apartment building will just add more garbage.

I recommend that instead of allowing for an apartment building the developers change to Townhomes. This would fall well within the surrounding community.

I have been speaking to many of my neighbours and they are very angry at council for even entertaining these developers request.

I humbly request that council adhere to the request of the community who have elected you all to represent us in matters such as this.

As our councilor I am requesting your support and be our voice in stopping this project from proceeding.

Thank you

Joe Cirasella (jr)

**C11**  
**COMMUNICATION**  
**CW (Public Hearing) - July 7, 2020**  
**Item # 4**

**From:** [Clerks@vaughan.ca](mailto:Clerks@vaughan.ca)  
**To:** [Bellisario, Adelina](mailto:Bellisario, Adelina)  
**Subject:** FW: [External] 9630 Islington  
**Date:** July-06-20 9:42:16 AM

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**From:** Lou Specialx <[REDACTED]>  
**Sent:** Saturday, July 4, 2020 9:59 PM  
**To:** Clerks@vaughan.ca  
**Subject:** [External] 9630 Islington

Hello,

In response to the 9630 Islington "Notice of Public Hearing Committee of the Whole" letter received, I have the following questions:

1. Will this building be a condominium with individually owned units or will this building be an apartment building with a single owner/corporation.
2. Has there been a traffic assessment completed pre-COVID and during the Elementary School year?
3. Will the parking entrance to the building be located on Islington Avenue or Napa Valley?
4. How will the additional traffic be dealt with?
5. Is this a social housing project?

Thanks,

Lou (Local resident).

**C12**  
**COMMUNICATION**  
**CW (Public Hearing) - July 7, 2020**  
**Item # 4**

**From:** [Clerks@vaughan.ca](mailto:Clerks@vaughan.ca)  
**To:** [Bellisario, Adelina](mailto:Bellisario, Adelina)  
**Subject:** FW: [External] Deputation  
**Date:** July-06-20 11:15:55 AM

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**From:** JAVIER aragon <[REDACTED]>  
**Sent:** Monday, July 6, 2020 11:13 AM  
**To:** Clerks@vaughan.ca  
**Subject:** [External] Deputation

To whom may concern,

My name is Javier Aragon from [REDACTED] Woodbridge ON, and we are not agree with the planing building construction located on 9630 Islington avenue, northwest corner of Islington ave and Napa valley ave, being block 6, Plan 65M-3467 (ward2).

Without more to say is a total disappointed about that construction and joining the rest of the neighbourhood for any legal process that we can.

Thank you so much, have a nice day.

Sent from myMail for iOS

**From:** [Clerks@vaughan.ca](mailto:Clerks@vaughan.ca)  
**To:** [Bellisario, Adelina](mailto:Bellisario, Adelina)  
**Subject:** FW: Electronic Deputation Zoning By-Law Amendment File Z.20.001  
**Date:** July-06-20 11:51:23 AM

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**From:** John Mora <[REDACTED]>  
**Sent:** Monday, July 6, 2020 11:42 AM  
**To:** Clerks@vaughan.ca  
**Cc:** [REDACTED]; Nancy Mora [REDACTED]  
**Subject:** [External] Electronic Deputation Zoning By-Law Amendment File Z.20.001

**I oppose the proposed zoning by-law change to the existing property for a 5 Story 89 Unit apartment complex.**

1. Nowhere around the area, including down Major Mackenzie to Weston Rd. have any five-story apartments been built with such a small property footprint with NO GREEN SPACE.
2. 89 units are proposed, which leads me to believe they will be, on average, approx. 500 sqft. Which will mean these units will be **bought for investment purposes and rented, which will lead to Airbnb rentals and parties and noise.** The city of Vaughn should have a system in place that if an apartment or condominium is **bought, it cannot be rented for three years** this will allow serious home buyers a chance to purchase something and not pay extreme rent.
3. Napa Valley and Islington Ave. will become more congested due to the possible 89 cars trying to exit the apartments, the intersection already has congestion during rush hours this will just add more gas to the fire. I also can see people living in the apartment that don't have enough parking for their additional vehicles, family, and friends visiting parking on Napa Valley, Islington Ave. And surrounding neighborhoods. The possible parking on Napa Valley with restricting the movement of Emergency Vehicles to access the area in the event of an emergency.
4. Has a study been done of the shadow/blockage of natural southern sunlight effect on the houses to the rear of the proposed apartment building? Have these citizens/neighbors been contacted in person or contacted by registered mail prior to the proposed design by the Developers or the City of Vaughn, it appears that a design was completed prior to zoning by-law change and citizens in the neighborhood only get informed two weeks before a public hearing via mail-in which some citizens didn't receive the notification.
5. The letter states that the proposed building will be five stories, which would have an overall height of 54.13 feet (1 story = 14.3 feet). Although looking at the plan, it seems like the total height of the structure is closer to 75 feet, making it a 6.9 story building.
6. Also, it looks like from the plan; there is only one entrance in and out of the building. How will this affect the response of emergency vehicles should there be, and emergency and the one entry is blocked for whatever reason?
7. Being that the structure is "5 stories," the fire department will require the use of a Ladder truck should there be a fire on the 4<sup>th</sup> or 5<sup>th</sup> floors if that fire happens to be at the front of the building. Will the ladder truck be able to enter and get to the front of the building, given the tight driveway and turning radius?
8. The location of the garbage bins is also of concern, given that it seems to be placed at the property line between the existing homes and the building. This will cause an increase in rodents, which will affect the neighboring houses. Also, the added noise when the garbage is going to be picked up. We are already

complaining about the garbage pick up at the plaza across the street. Given that they have their garbage pick up t around 4:30 or 5:00 AM. what will be done to prevent this?

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**C14**  
**COMMUNICATION**  
**CW (Public Hearing) - July 7, 2020**  
**Item # 3**

File Z.20.003

DEPUTATION: ELLEN VOLANIS

Tues Jul 7, 2020

Ellen Volanis  


Belmont properties (Weston Inc) wants to change their permit from commercial zoning to residential.

I am not opposed to this request I am however opposed to any bylaw height amendments that the developer has requested.

There are currently 29 homes all at 36 feet (11 meters) that surround this lot off of two streets.

KLM the designer for Belmont Properties has two current design developments in Vaughan. Both had requests to amend the height bylaw in Vaughan. The difference in both cases is that there was no residential homes existing behind those lots.

My deputation tomorrow will be requesting city council to not allow the height bylaws to be amended in this specific case.

Thank you,

Elle

***PUBLIC MEETING***  
***July 7<sup>th</sup>, 2020***

C15  
COMMUNICATION  
CW (Public Hearing) - July 7, 2020  
Item #4

***9630 Islington Avenue***  
*9630 Islington Inc.*  
Block 6, Registered Plan 65M-3467

Zoning By-law Amendment Z.20.001  
Site Plan Approval Application DA.20.002

# Aerial Overview & Site Characteristics



2, 6 Storey Residential Apartments (252 units)

Low-Rise Residential

**SUBJECT SITE**

Commercial

Low-Rise Residential

*Lot Area:*  
3,925.45m<sup>2</sup>

*Frontage:*  
(Islington)  
39.10m

*Depth:*  
(Napa Valley)  
90.79m

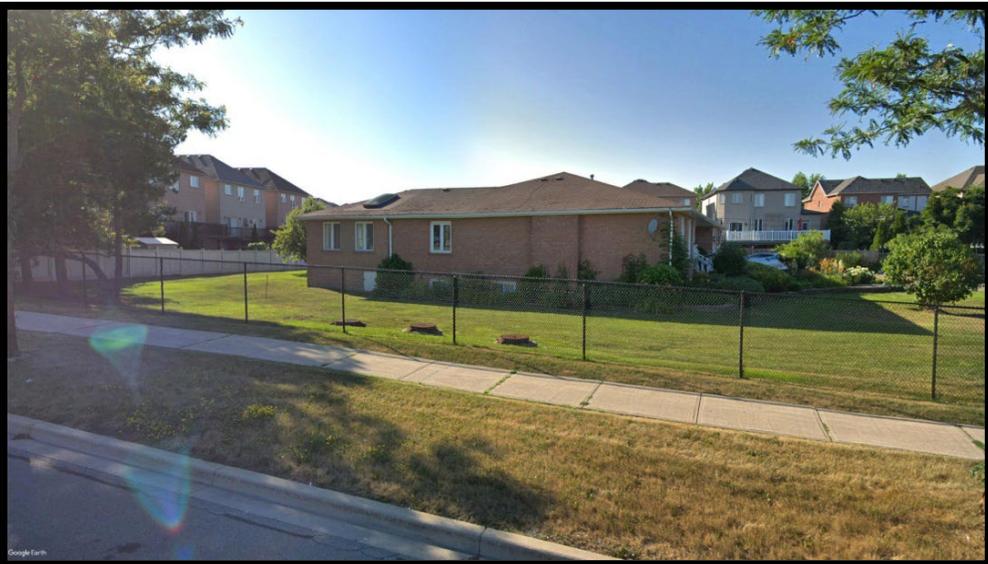
Google Earth

© 2016 Google

# Site and Surrounding Area Photographs



1. Looking West from Islington Ave



2. Looking North West From Napa Valley

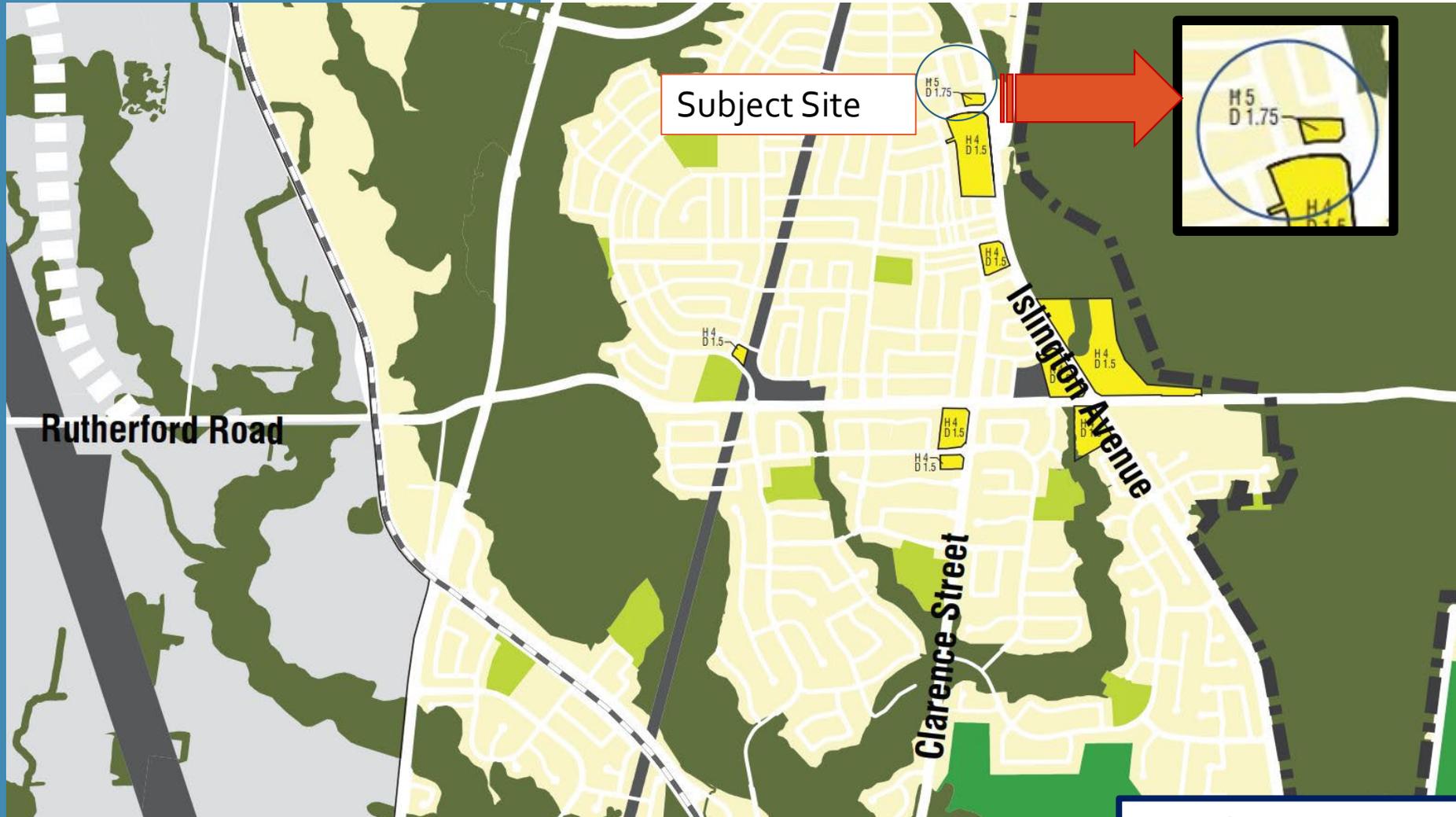


3. Looking North West from Islington Ave



4. Looking South from Napa Valley

# City of Vaughan Official Plan

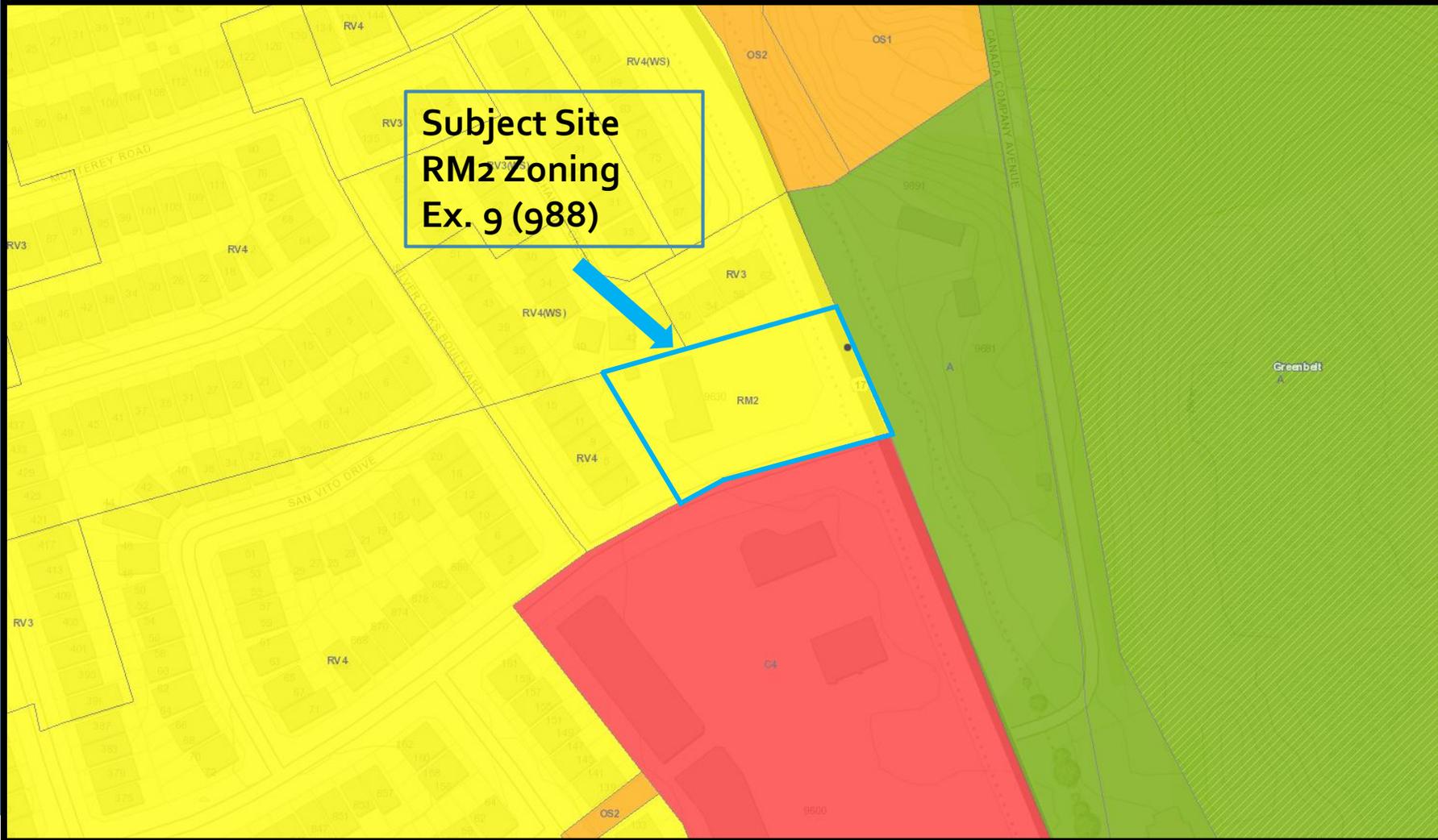


## SCHEDULE 13 Land Use



Permits:  
- Height: 5 storeys  
- Density: 1.75 FSI

# Zoning- By-law 1-88



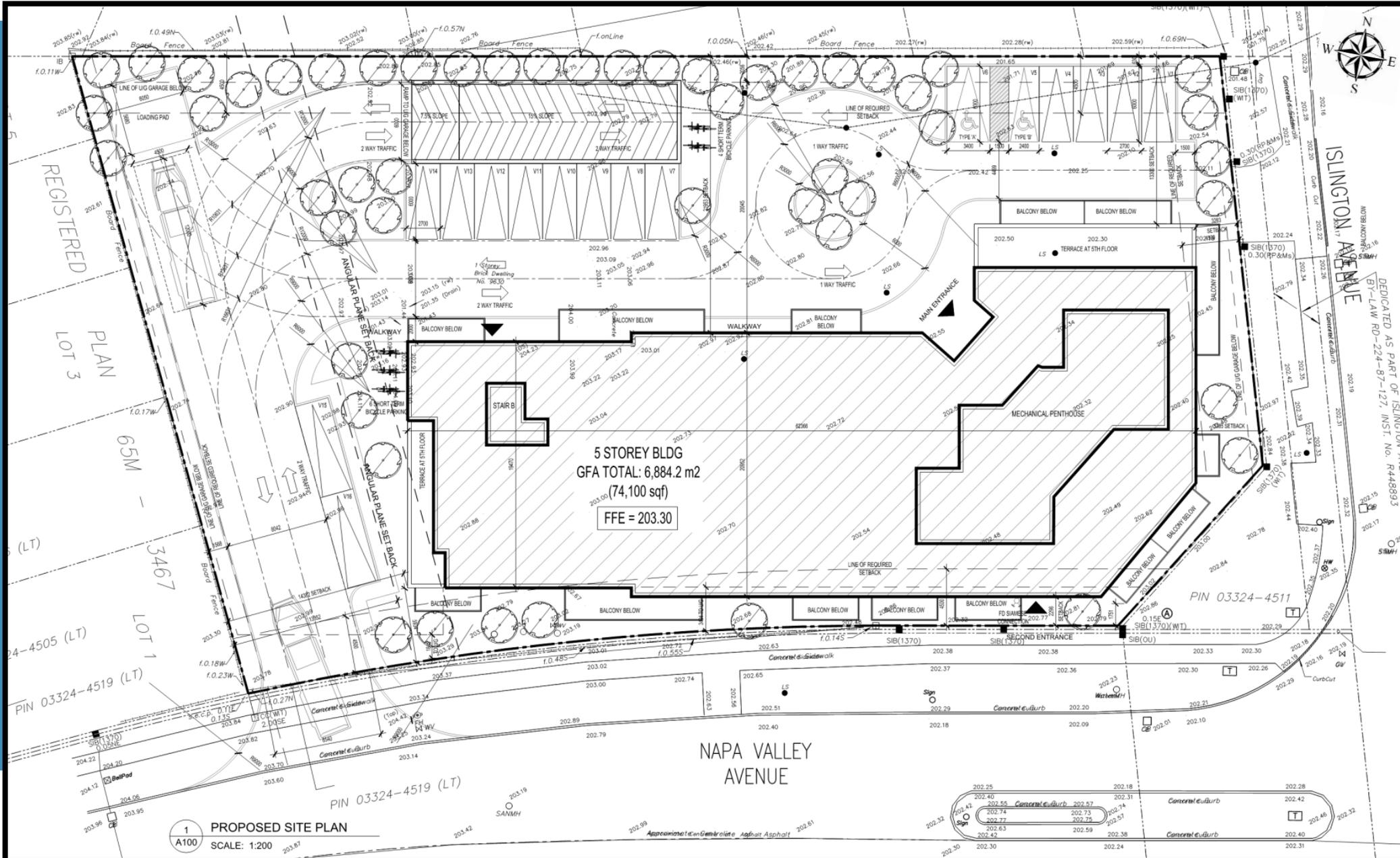
### RM2 Current Permitted Uses:

- Apartment Dwelling
- Multiple Family Dwelling
- Block Townhouse Dwelling
- Day Nursery

### Ex. 9 (988):

- Additional Uses Permitted:*
- Bank or Financial Institution
  - Retail Store
  - Business or Professional Offices
  - Personal Service Shop
  - Photography Studio
  - Eating Establishment\*

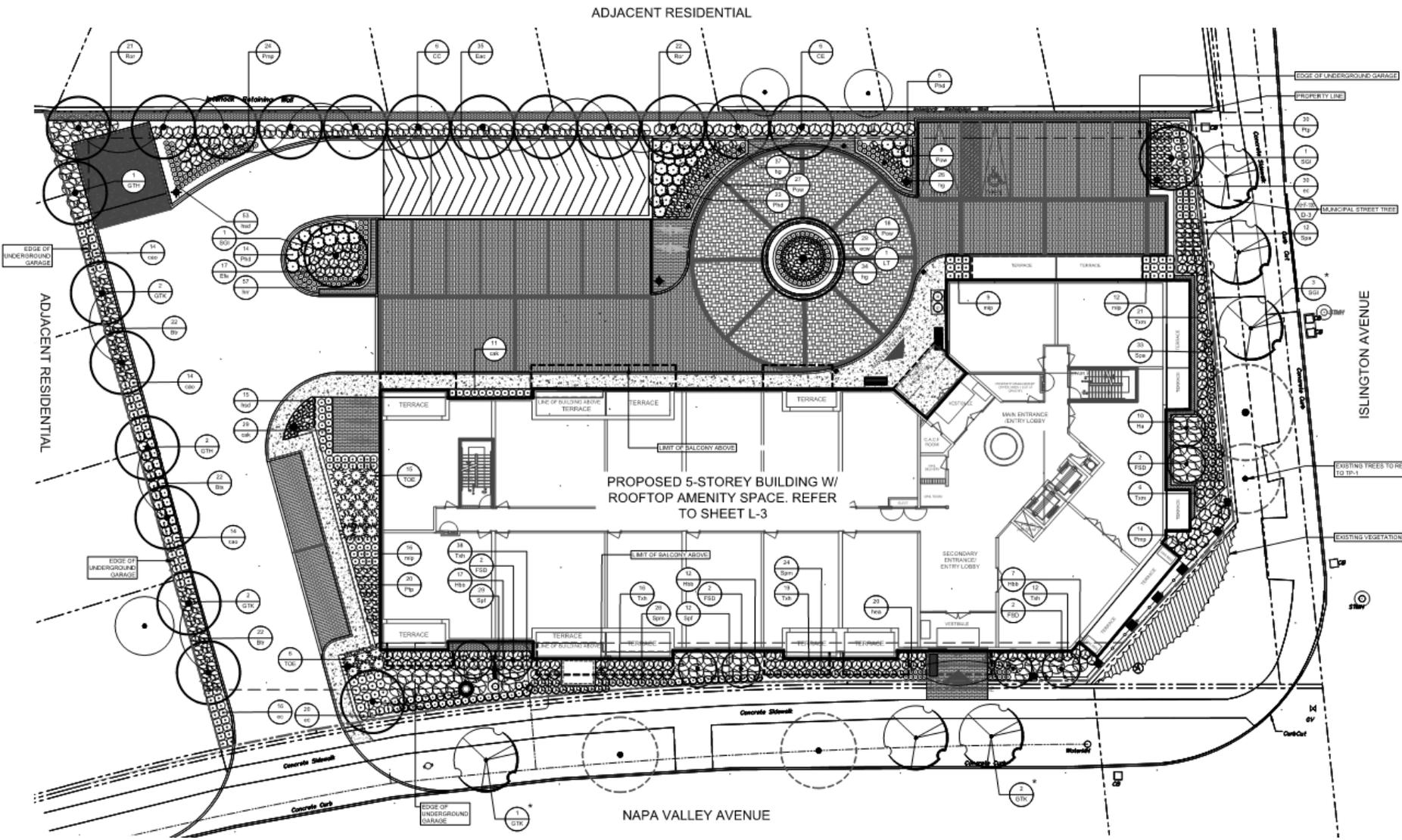
# Site Plan



- 5 Storeys
- 89 Units
- 1 Level U/G Parking
- Rooftop Amenity Area
- Gross Floor Area (GFA) 6,884.2m<sup>2</sup>
- Floor Space Index (FSI) = 1.75

1 PROPOSED SITE PLAN  
A100 SCALE: 1:200

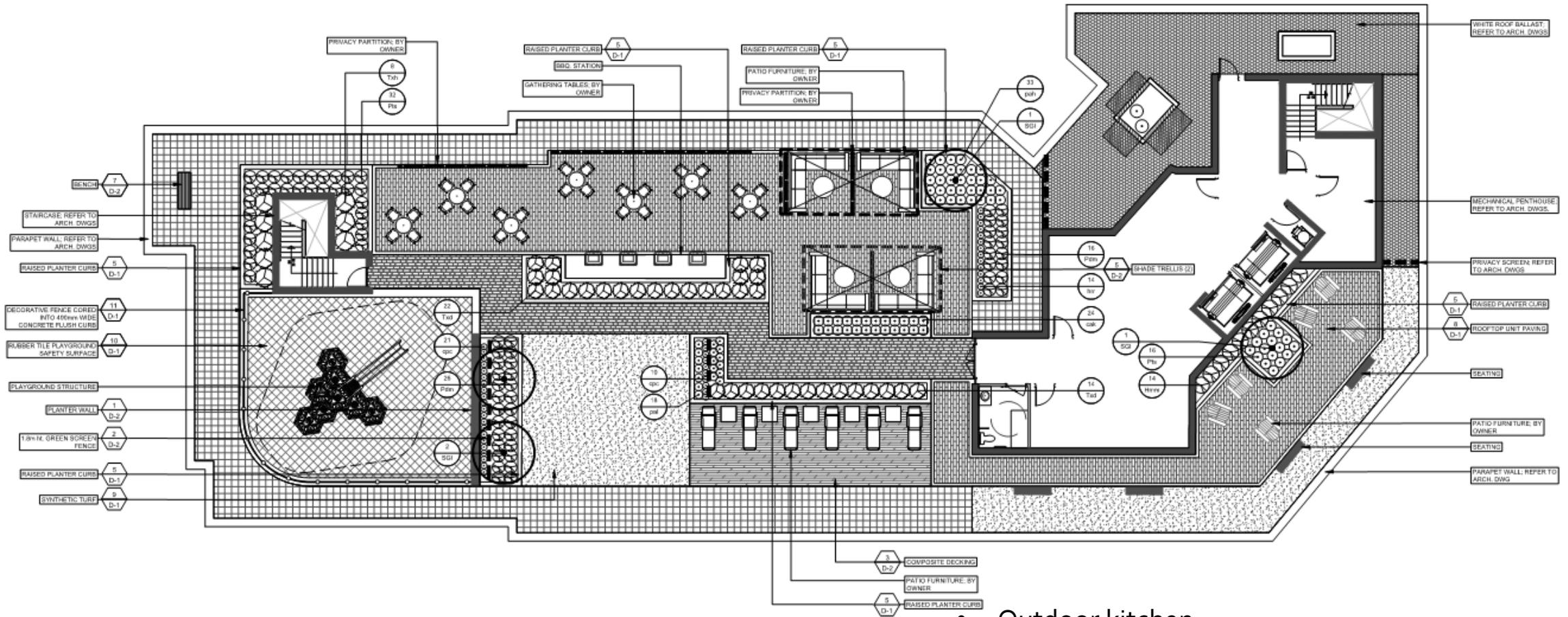
# Landscape Plan



**LEGEND:**

	DETAIL #
	SHEET #
	QUANTITY
	TYPE
	PROPOSED DECIDUOUS TREE
	PROPOSED MUNICIPAL STREET TREE
	PROPOSED SHRUBS AND PERENNIALS
	EXISTING PRIVATE TREE TO REMAIN
	EXISTING MUNICIPAL TREE TO REMAIN
	LIGHT POLE; REFER TO ELECTRICAL DWGS
	BOLLARD LIGHT; REFER TO ELECTRICAL DWGS

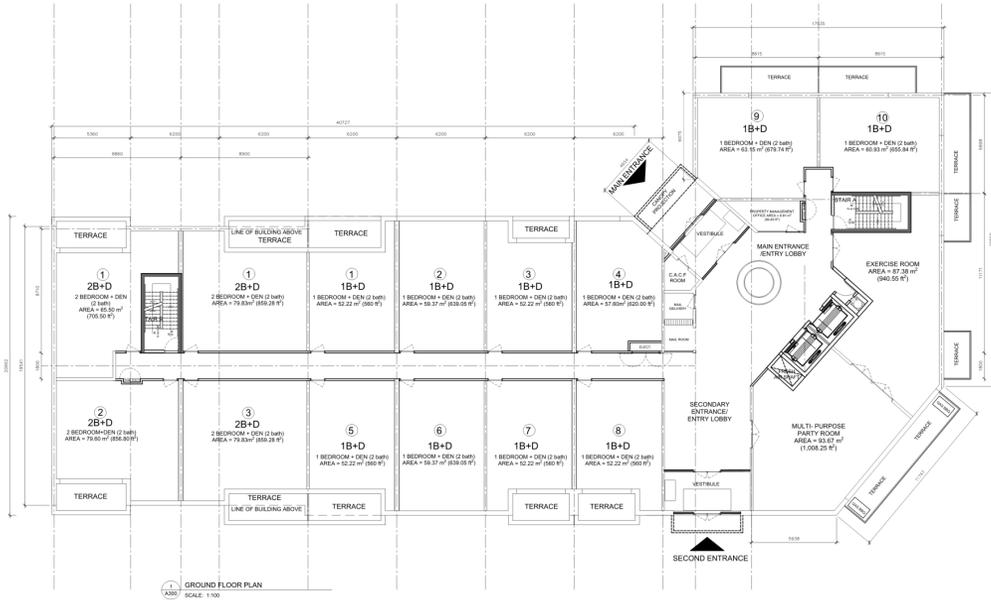
# Roof Plan – Amenity Space



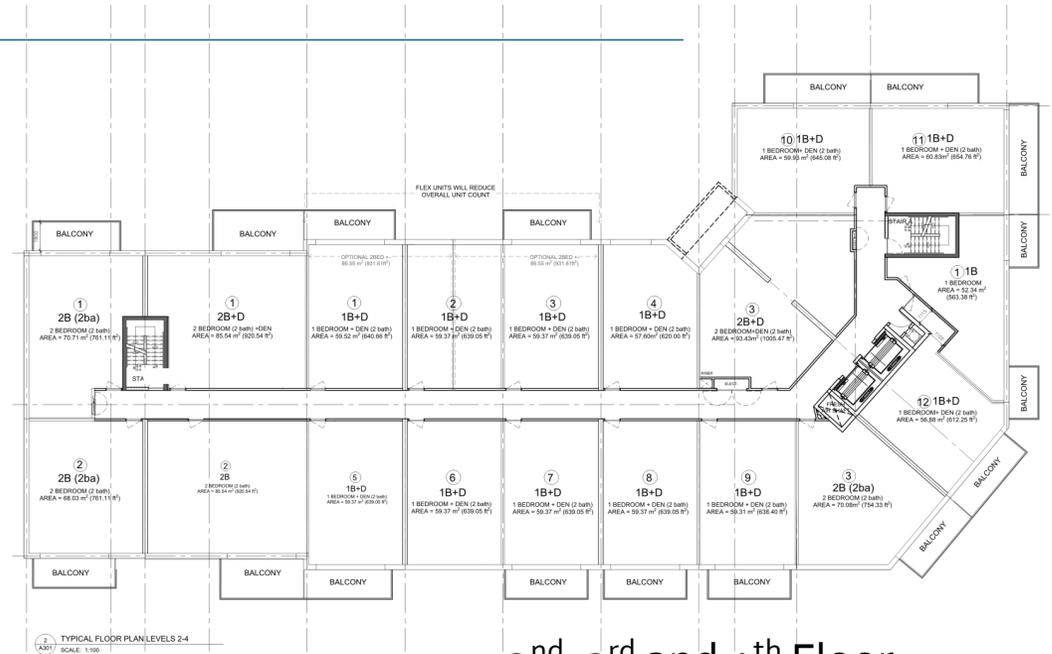
Rooftop Amenity Space = 751m<sup>2</sup>

- Sun deck
- Workout space
- Eating area
- Outdoor kitchen
- Playground area
- Athletic walking path
- Landscape plantings

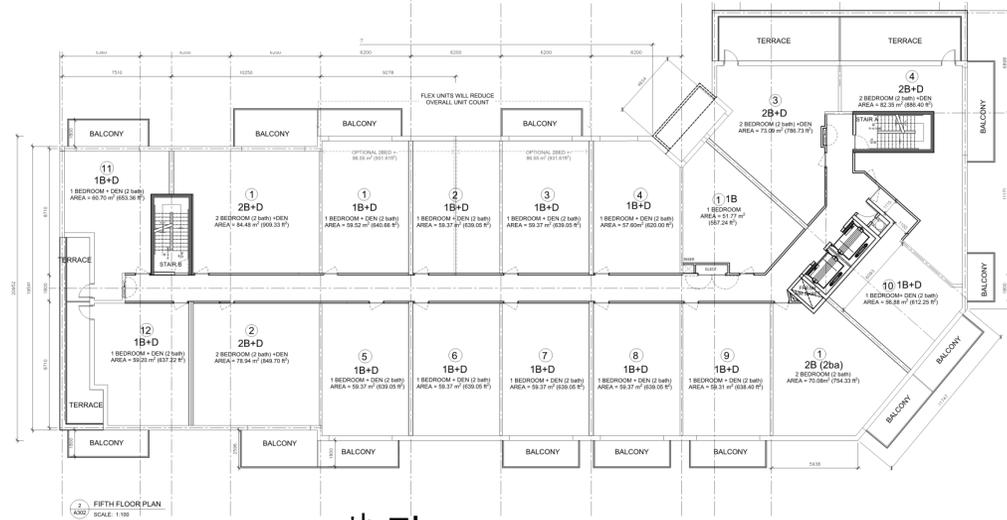
# Floor Plans



Ground Floor Plan

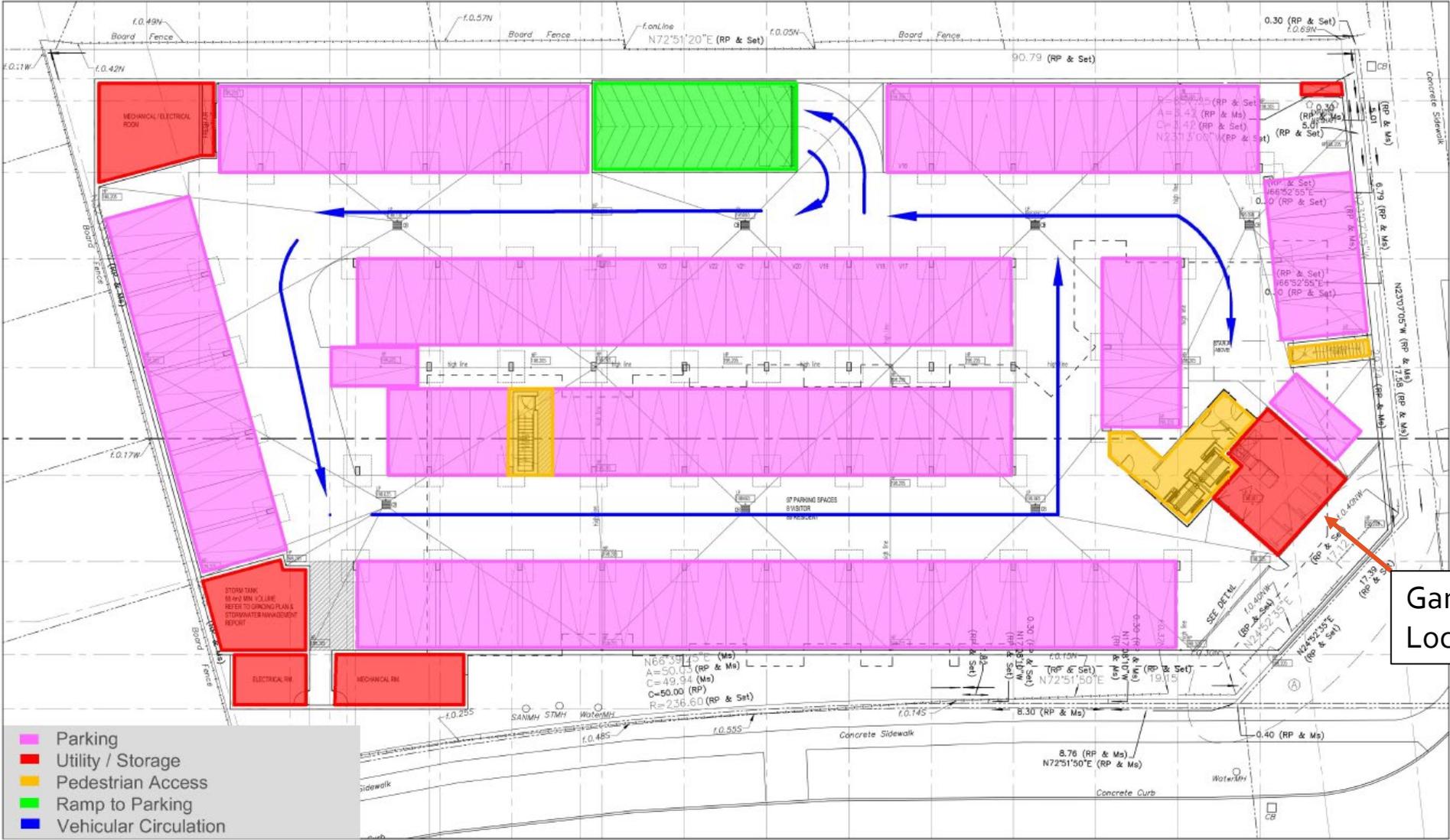


2<sup>nd</sup>, 3<sup>rd</sup> and 4<sup>th</sup> Floor



5<sup>th</sup> Floor

# Underground Parking Plan



Garbage Room Location

- Parking
- Utility / Storage
- Pedestrian Access
- Ramp to Parking
- Vehicular Circulation

# Colour Elevations



View looking North from Napa Valley Avenue



View Looking West from Islington Avenue

# Streetscape Elevations



1 NAPA VALLEY AVENUE STREETScape ELEVATION  
SCALE: NTS



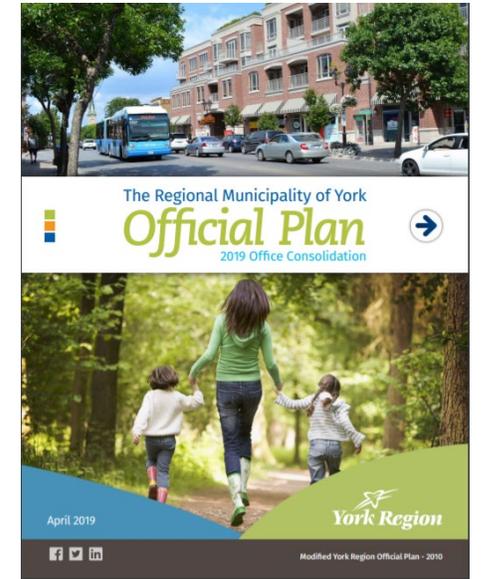
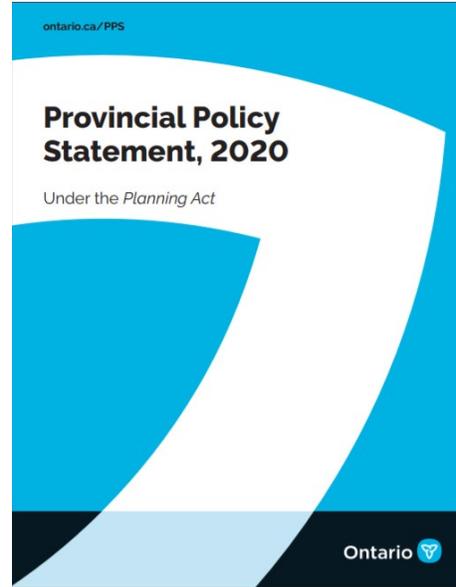
2 ISLINGTON AVENUE STREETScape ELEVATION  
SCALE: NTS



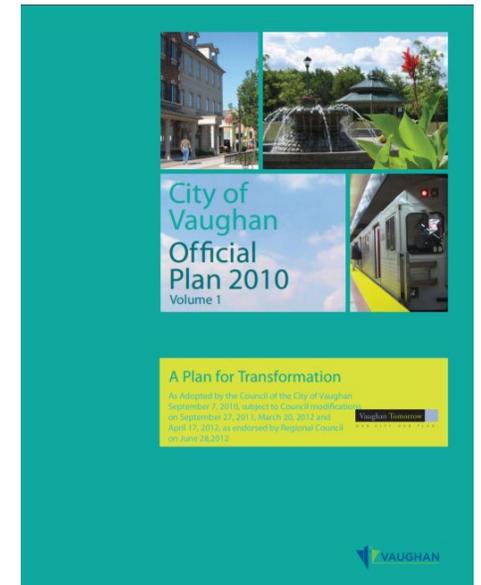
# 3D Architectural Rendering



# Regulatory Framework



- The proposal demonstrates compliance with Provincial, Regional and Local Statutes



# Supporting Technical Documentation

Studies and Drawings submitted in support of the application to determine the overall feasibility of development.



- *Topographic and Boundary Survey*
- *Site Plan and Architectural Details*
- *Planning Justification Report*
- *Community Services & Facilities Report*
- *Urban Design & Sustainability Brief*
- *Site Servicing , Grading Plan & Erosion & Sediment Control Plan*
- *Functional Servicing Brief & SWM Report*
- *Landscape Plans and Details*
- *Transportation and Parking Study*
- *Exterior Lighting & Photometric Plan*
- *Geotechnical Study*
- *Hydrogeological Report*
- *Water Balance Report*
- *Tree Inventory & Preservation Study*
- *3D Renderings*

# Northwest Corner of Weston Road and Chatfield Drive

## Belmont Properties

Application for Zoning By-law Amendment  
File No.: Z.20.003

C16  
COMMUNICATION  
CW (Public Hearing) - July 7, 2020  
Item # 3



## SUBJECT LANDS

- Subject Lands are located at NW corner of Weston Road and Chatfield Drive
- Block within Registered Plan of Subdivision
- +/-0.77 Hectares (+/-1.9 acres)
- Relatively flat
- Sparsely vegetated due to clearing and grading of subdivision
- Currently occupied by temporary sales offices
  - To be removed to facilitate proposed development
- North: existing residential townhouses
- East: Weston Road and existing stormwater management pond
- South: Chatfield Drive and future residential
- West: existing residential townhouses

**Official Plan:**

- **'Low Rise Mixed Use'** designation
  - Maximum height of 4 storeys
  - Maximum density of 1.5 FSI
  - Permits mix of commercial and residential uses, including townhouses

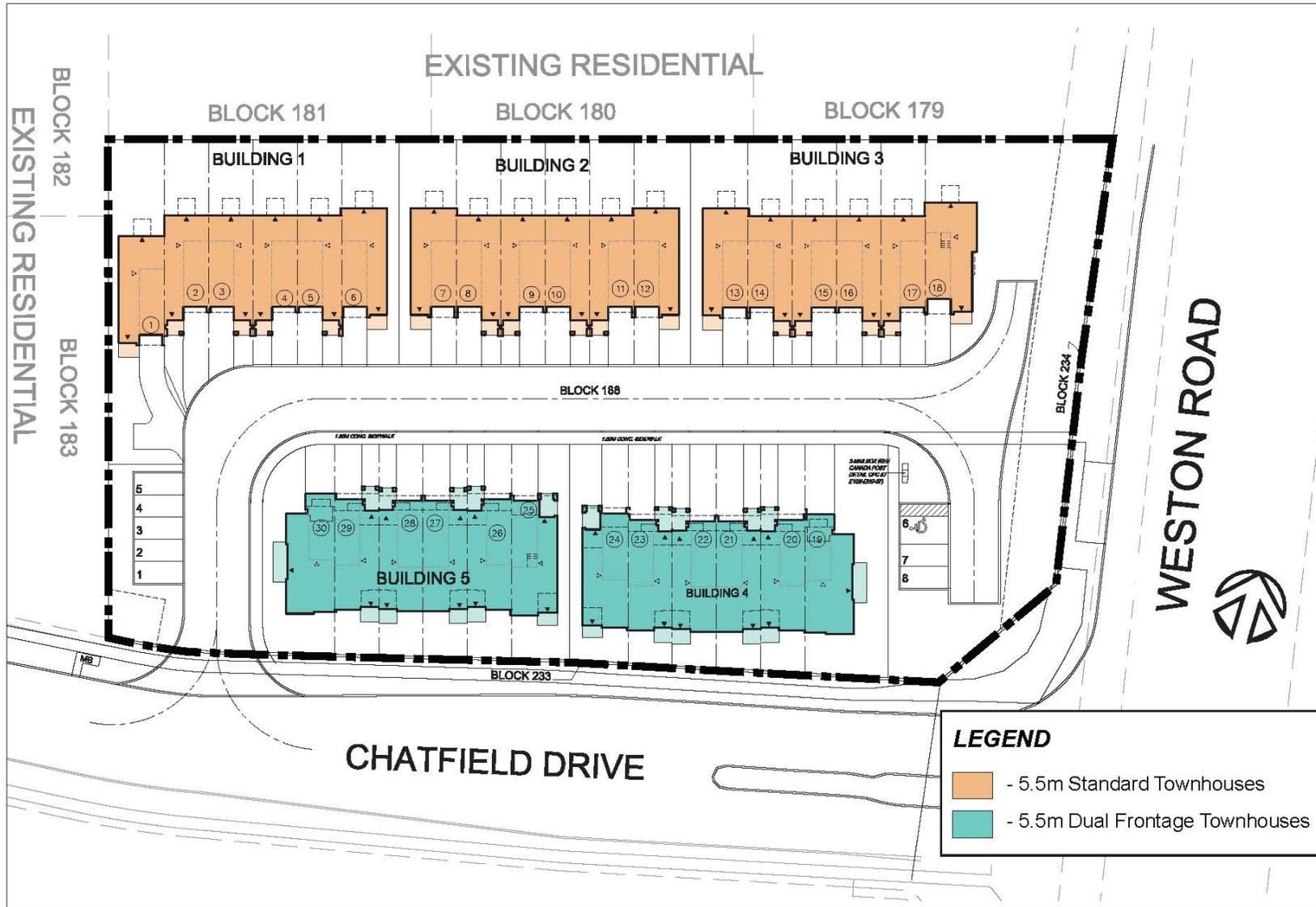
**No amendments to the Official Plan are required**

**Current Zoning:**

- **C3(H) – Local Commercial Zone** with Hold provision
  - Maximum height of 3 storeys
  - Permits Bank or Financial Institution; Business or Professional Office; Personal Service Shop; Pharmacy; Photography Studio; Retail Store; Video Store

**Amendment to Zoning By-law 1-88  
required to permit proposed residential use**

# PROPOSED SITE PLAN



- Total of 30 units on private road
- Minimum unit width of 5.5m
- 18 standard townhouses
- 12 dual frontage townhouses
- 2 parking spaces per unit
- 8 visitor parking spaces

- Reduced unit width from 6.0m to 5.5m
- Reduced minimum front and rear yards for dual frontage units
- Increased maximum building height from 11.0m to 12.0m
- Reduced minimum lot depth from 27m to 26m
- Permission for proposed townhouses to have frontage on private road
- Reduced landscaping requirements

# PROPOSED ELEVATIONS

## Dual Frontage Units



Proposed Dual Frontage Townhouses (prepared for Belmont by VA3) - FRONT ELEVATION



Proposed Dual Frontage Townhouses (prepared for Belmont by VA3) - REAR ELEVATION

# PROPOSED ELEVATIONS

## Standard Unit Elevations



Proposed Front-Loaded Townhouses (prepared for Belmont by VA3) - FRONT ELEVATION



Proposed Rear Upgrade  
(Block 3)