

ITEM: 6.6	REPORT SUMMARY MINOR VARIANCE APPLICATION FILE NUMBER A054/25
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Report Date: Friday, June 13, 2025

THIS REPORT CONTAINS COMMENTS FROM THE FOLLOWING
DEPARTMENTS & AGENCIES (SEE SCHEDULE B):

Additional comments from departments and agencies received after the publication of the report will be made available on the City’s [website](#).

Internal Departments *Comments Received	Conditions Required		Nature of Comments
Committee of Adjustment	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
Building Standards (Zoning)	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
Development Planning	Yes <input type="checkbox"/>	No <input type="checkbox"/>	Application Under Review
Development Engineering	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	General Comments w/Conditions
Development Finance	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
By-law & Compliance	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
Forestry	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	General Comments w/Conditions

External Agencies *Comments Received	Conditions Required		Nature of Comments *See Schedule B for full comments
Alectra	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
Region of York	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
TRCA	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
Metrolinx	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments

PUBLIC & APPLICANT CORRESPONDENCE (SEE SCHEDULE C)				
All personal information collected because of this public meeting (including both written and oral submissions) is collected under the authority of the Municipal Act, the Municipal Freedom of Information and Protection of Privacy Act (MFIPPA), the Planning Act and all other relevant legislation, and will be used to assist in deciding on this matter. All personal information (as defined by MFIPPA), including (but not limited to) names, addresses, opinions and comments collected will become property of the City of Vaughan, will be made available for public disclosure (including being posted on the internet) and will be used to assist the Committee of Adjustment and staff to process this application.				
Correspondence Type	Name	Address	Date Received (mm/dd/yyyy)	Summary
N/A				

BACKGROUND (SCHEDULE D, IF REQUIRED)	
* Background Information contains historical development approvals considered to be related to this file. This information should not be considered comprehensive.	
Application No. (City File)	Application Description (i.e. Minor Variance Application; Approved by COA / OLT)
N/A	N/A

ADJOURNMENT HISTORY	
* Previous hearing dates where this application was adjourned by the Committee and public notice issued.	
Hearing Date	Reason for Adjournment (to be obtained from NOD_ADJ)
N/A	N/A

SCHEDULES	
Schedule A	Drawings & Plans Submitted with the Application
Schedule B	Comments from Agencies, Building Standards & Development Planning
Schedule C (if required)	Public & Applicant Correspondence
Schedule D (if required)	Background



MINOR VARIANCE APPLICATION FILE NUMBER A054/25

CITY WARD #:	5
APPLICANT:	Lijia Gao
AGENT:	Kyutae Kim (LHW ENGINEERING LTD)
PROPERTY:	91 Crestwood Road, Thornhill
ZONING DESIGNATION:	See below.
VAUGHAN OFFICIAL PLAN (2010) DESIGNATION:	Vaughan Official Plan 2010 ('VOP 2010'): "Low Rise Residential"
RELATED DEVELOPMENT APPLICATIONS:	N/A
PURPOSE OF APPLICATION:	Relief from the Zoning By-law is being requested to permit a proposed second storey and attic addition to the existing dwelling and increased driveway width.

The following variances have been requested from the City's Zoning By-law:

The subject lands are zoned R2A(EN) – 2nd Density Residential Zone (Established Neighbourhood) and subject to the provisions of Exception 14.205 under Zoning By-law 001-2021, as amended.

#	Zoning By-law 001-2021	Variance requested
1	The minimum required interior side yard is 1.8m where there is a door providing entry into the sideyard. Exception 14.205 E-390	To permit a minimum interior side yard of 1.45m on the west side.
2	The minimum required interior side yard is 1.2m. Exception 14.205 E-390	To permit a minimum interior side yard of 1.03m on the east side.
3	In a Residential Zone, the maximum cumulative width of both accesses to a circular driveway, as measured at the street line, shall be 9.0 m. Section 6.7.4 4	To permit a maximum cumulative driveway width of 10.31m measured at the street line.

HEARING INFORMATION

DATE OF MEETING: Thursday, June 19, 2025

TIME: 6:00 p.m.

MEETING LOCATION: Vaughan City Hall, Woodbridge Room (2nd Floor), 2141 Major Mackenzie Drive

LIVE STREAM LINK: [Vaughan.ca/LiveCouncil](https://vaughan.ca/LiveCouncil)

PUBLIC PARTICIPATION

If you would like to speak to the Committee of Adjustment at the meeting, either remotely or in person, please complete the [Request to Speak Form](#) and submit to cofa@vaughan.ca

If you would like to submit written comments, please quote file number above and submit by mail or email to:

Email: cofa@vaughan.ca

Mail: City of Vaughan, Office of the City Clerk, Committee of Adjustment, 2141 Major Mackenzie Drive, Vaughan, ON, L6A 1T1

To speak electronically, pre-registration is required by completing the [Request to Speak Form](#) on-line and submitting it to cofa@vaughan.ca no later than NOON on the last business day before the meeting.

THE DEADLINE TO REGISTER TO SPEAK ELECTRONICALLY OR SUBMIT WRITTEN COMMENTS ON THE ABOVE NOTED FILE(S) IS NOON ON THE LAST BUSINESS DAY BEFORE THE MEETING.

INTRODUCTION	
Staff and Agencies act as advisory bodies to the Committee of Adjustment. The comments contained in this report are presented as recommendations to the Committee.	
Section 45(1) of the Planning Act sets the criteria for authorizing minor variances to the City of Vaughan’s Zoning By-law. Accordingly, review of the application may consider the following:	
That the general intent and purpose of the by-law will be maintained. That the general intent and purpose of the official plan will be maintained. That the requested variance(s) is/are acceptable for the appropriate development of the subject lands. That the requested variance(s) is/are minor in nature.	
Public written and oral submissions relating to this application are taken into consideration by the Committee of Adjustment as part of its deliberations and final decision on this matter.	

COMMITTEE OF ADJUSTMENT	
Date Public Notice Mailed:	June 5, 2025
Date Applicant Confirmed Posting of Sign:	June 2, 2025
Applicant Justification for Variances: <small>*As provided in Application Form</small>	The existing side setback is against zoning by law
Was a Zoning Review Waiver (ZRW) Form submitted by Applicant: <small>*ZRW Form may be used by applicant in instances where a revised submission is made, and zoning staff do not have an opportunity to review and confirm variances prior to the issuance of public notice.</small>	Yes <input type="checkbox"/> No <input checked="" type="checkbox"/>
COMMENTS:	
N/A	
Committee of Adjustment Recommended Conditions of Approval:	None

BUILDING STANDARDS (ZONING)	
**See Schedule B for Building Standards (Zoning) Comments	
Building Standards Recommended Conditions of Approval:	None

DEVELOPMENT PLANNING	
**See Schedule B for Development Planning Comments. Application under Review.	
Development Planning Recommended Conditions of Approval:	TBD

DEVELOPMENT ENGINEERING	
Link to Grading Permit Link to Pool Permit Link to Curb Curt Permit Link Culvert Installation	
The proposed work by the Owner/Applicant is increasing the lot coverage on the subject property. The added hardscape may have impacts on the City’s Storm Water management system. Development Engineering strongly encourages the Owner / Applicant introduce Low-Impact Development (LID) measures (e.g., bioswales, permeable pavers, rain gardens, rain barrels etc.) to reduce the impacts to the stormwater system. Should further information be required, please contact the Development Engineering COA reviewer. The Owner / Applicant shall ensure that the proposed (fill in proposed development) does not negatively impact neighbours due to surface water runoff. The property should be properly graded, ensuring that surface water from the (fill in proposed development) does not go onto adjacent lots in accordance with the City's Engineering Standards. It's important to note that (fill in proposed development based on Section 2.2.1 - Table 1.0) necessitates a Grading Permit. Once the Grading Permit is obtained, please reach out to the Development Engineering Reviewer to clear the Condition imposed on this application. (Condition attached)	
Development Engineering Recommended Conditions of Approval:	The Owner/Applicant shall submit an application and obtain an approved Grading Permit before initiating any work on the property. The Final Lot Grading and/or Servicing Plan will be required for the Grading Permit Application. Please visit the Permits page of the City of Vaughan’s website: Permits City of Vaughan to apply for a Grading Permit. For any inquiries regarding the Grading Permit, please email DEPermits@vaughan.ca

PARKS, FORESTRY & HORTICULTURE (PFH)	
Recommended condition of approval listed below.	
PFH Recommended Conditions of Approval:	The Applicant/owner shall obtain a “Private Property Tree Removal & Protection Permit - Construction or Infill” through the forestry division prior to any construction works on the subject property.

DEVELOPMENT FINANCE	
No comment no concerns	
Development Finance Recommended Conditions of Approval:	None

BY-LAW AND COMPLIANCE, LICENSING AND PERMIT SERVICES	
No comments.	
BCLPS Recommended Conditions of Approval:	None

BUILDING INSPECTION (SEPTIC)	
No comments received to date.	
Building Inspection Recommended Conditions of Approval:	None

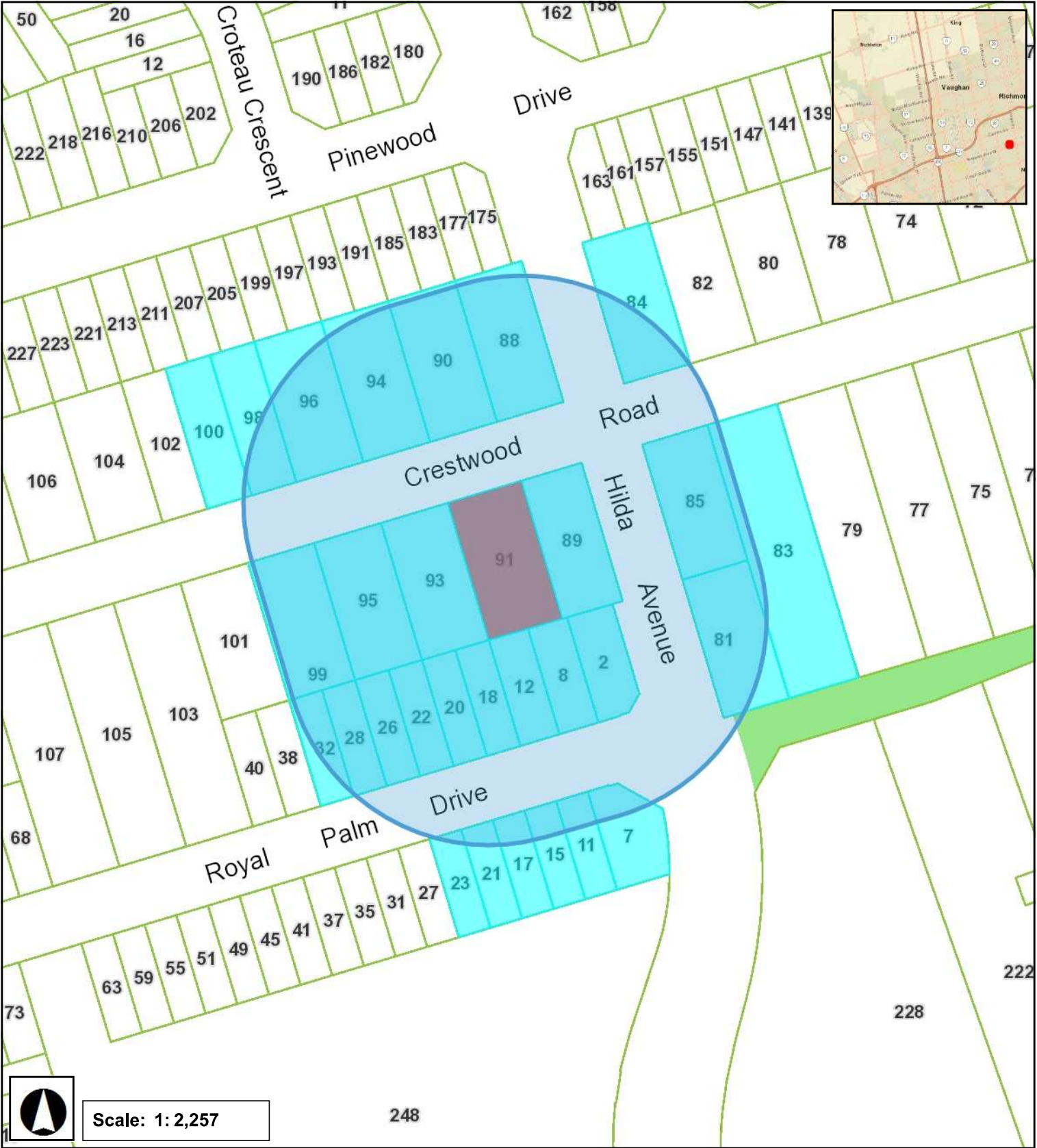
FIRE DEPARTMENT	
No comments received to date.	
Fire Department Recommended Conditions of Approval:	None

RECOMMENDED CONDITIONS OF APPROVAL SUMMARY		
Should the Committee find it appropriate to approve this application in accordance with request and the sketch submitted with the application, as required by Ontario Regulation 200/96, the following conditions have been recommended:		
#	DEPARTMENT / AGENCY	CONDITION
1	Development Planning Alyssa.pangilinan@vaughan.ca	TBD
2	Development Engineering jonal.hall@vaughan.ca	The Owner/Applicant shall submit an application and obtain an approved Grading Permit before initiating any work on the property. The Final Lot Grading and/or Servicing Plan will be required for the Grading Permit Application. Please visit the Permits page of the City of Vaughan’s website: Permits City of Vaughan to apply for a Grading Permit. For any inquiries regarding the Grading Permit, please email DEPermits@vaughan.ca
3	Parks, Forestry and Horticulture Operations kari.sthyrhansen@vaughan.ca	The Applicant/owner shall obtain a “Private Property Tree Removal & Protection Permit - Construction or Infill” through the forestry division prior to any construction works on the subject property.
<i>All conditions of approval, unless otherwise stated, are considered to be incorporated into the approval “if required”. If a condition is no longer required after an approval is final and binding, the condition may be waived by the respective department or agency requesting conditional approval. A condition cannot be waived without written consent from the respective department or agency.</i>		

IMPORTANT INFORMATION
<p>CONDITIONS: It is the responsibility of the owner/applicant and/or authorized agent to obtain and provide a clearance letter from respective department and/or agency (see condition chart above for contact). This letter must be provided to the Secretary-Treasurer to be finalized. All conditions must be cleared prior to the issuance of a Building Permit.</p>
<p>APPROVALS: Making any changes to your proposal after a decision has been made may impact the validity of the Committee's decision.</p> <p>An approval obtained from the Committee of Adjustment, where applicable, is tied to the building envelope shown on the plans and drawings submitted with the application and subject to the variance approval.</p> <p>A building envelope is defined by the setbacks of the buildings and/or structures shown on the plans and drawings submitted with the application, as required by Ontario Regulation 200/96. Future development outside of an approved building envelope, where a minor variance was obtained, must comply with the provisions of the City's Zoning By-law.</p> <p>Elevation drawings are provided to reflect the style of roof (i.e. flat, mansard, gable etc.) to which a building height variance has been applied. Where a height variance is approved, building height is applied to the style of roof (as defined in the City's Zoning By-law) shown on the elevation plans submitted with the application.</p> <p>Architectural design features that are not regulated by the City's Zoning By-law are not to be considered part of an approval unless specified in the Committee's decision.</p>
<p>DEVELOPMENT CHARGES: That the payment of the Regional Development Charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the Regional Development Charges By-law in effect at the time of payment.</p> <p>That the payment of the City Development Charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the City's Development Charges By-law in effect at the time of payment.</p> <p>That the payment of the Education Development Charge if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the Boards of Education By-laws in effect at the time of payment</p> <p>That the payment of Special Area Development charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and The City's Development Charge By-law in effect at the time of Building permit issuance to the satisfaction of the Reserves/Capital Department.</p>
<p>NOTICE OF DECISION: If you wish to be notified of the decision in respect to this application or a related Ontario Land Tribunal (OLT) hearing you must complete a Request for Decision form and submit to the Secretary Treasurer (ask staff for details). In the absence of a written request to be notified of the Committee's decision you will not receive notice.</p>



Minor Variance Applicaiton A054/25





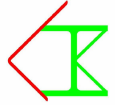
1 3D View (FRONT)



2 3D View (REAR)

NOTE:
FOR REFERENCE ONLY

LHW ENGINEERING LTD.
Unit 405, 2347 Kennedy Rd
Toronto, ON M1T 3T8
T: 416-299-8837
LHW.ENGINEERING@GMAIL.COM


LHW.ENGINEERING@GMAIL.COM

MAY/22/25

No.	Description	Date
1	CLIENT REVIEW	APR 4 , 2025
2	CLIENT REVIEW	APR 14 , 2025
3	ISSUED FOR COA	APR 24 , 2025
4	ISSUED FOR COA	MAY 22 , 2025
5		
6		
7		

PROJECT TITLE	3-D VIEW
91 Crestwood Rd, Thornhill, ON L4J 1A7	

Date	Drawn By
MAY/22/25	KT
Scale	Checked By
	BW
Proj. No.	DWG. No.
-	A0

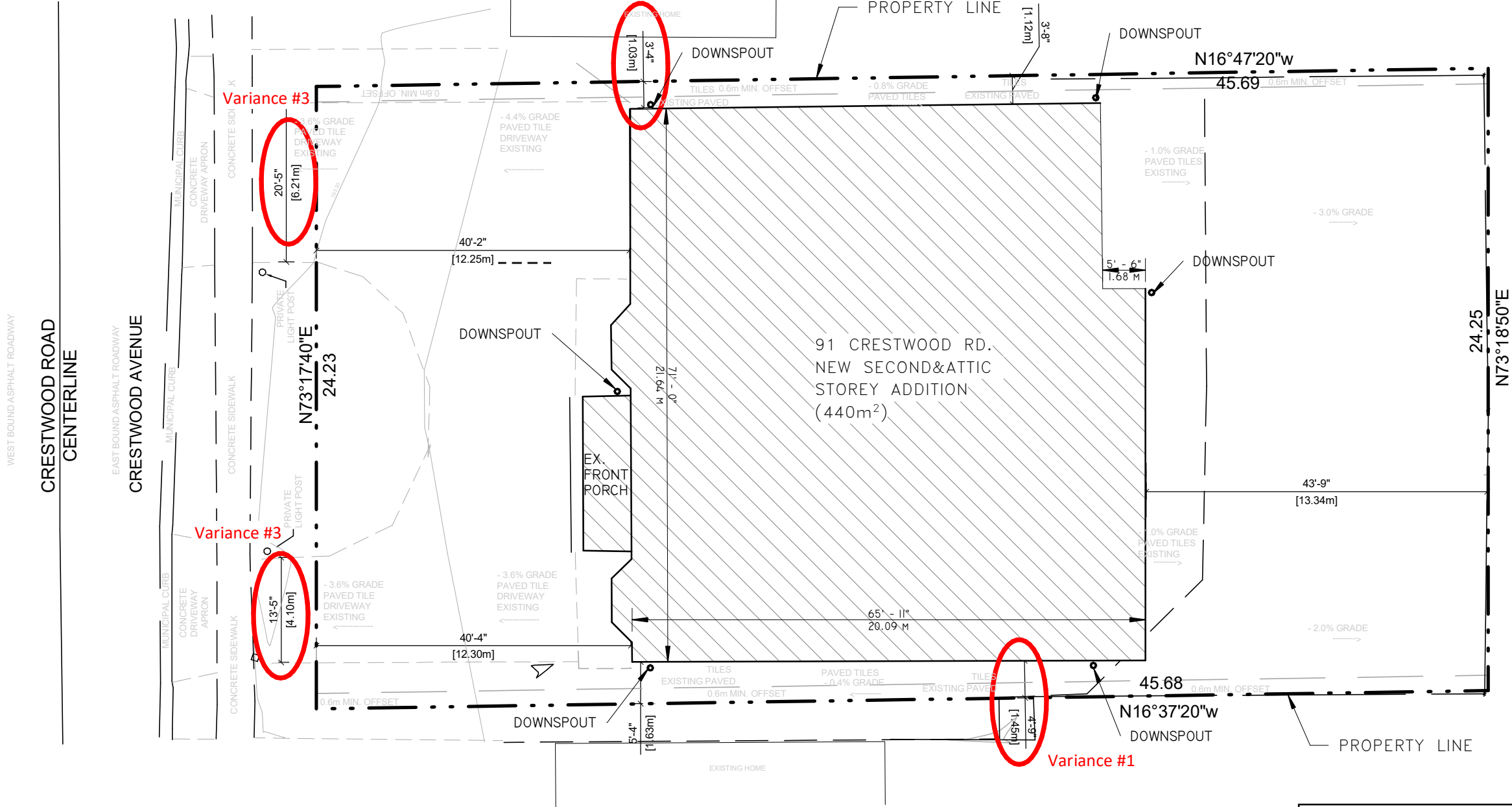
THE SCOPE OF WORK

- 1. SECOND STOREY & REAR ADDITION
- 2. NEW ROOF
- 3. NEW ATTIC FLOOR



<GOOGLE MAP>

SITE STATISTIC			
ZONING	R2A (EXCEPTION 205)	LOT NO.	65
PLAN NO.	REGISTERED PLAN 3205	LOT AREA	1,108 M²
LOT FRONTAGE	24.23 M	LOT DEPTH	45.68 M
ESTABLISHED GRADE			
	EXISTING	PROPOSED	
STOREY	2	2 1/2	
HEIGHT	-	10.5M (MAX . IIM)	
BUILDING DEPTH	-	23.6M	
LOT COVERAGE	393M² (35.5%)	440M² (39.7%)	
AREA			
	EXISTING	PROPOSED	
MAIN FLOOR	374.9M²	412.3M²	
2ND FLOOR	375.5M²	424.9M²	
THIRD FLOOR (LIVING AREA)	-	118.8M²	
GROSS FLOOR AREA	750.4M²	956M²	
SETBACK			
	EXISTING	PROPOSED	
FRONT (NORTH)	12.25M	NO CHANGE	
SIDE (EAST)	1.03M	NO CHANGE	
SIDE (WEST)	1.45M	NO CHANGE	
REAR (SOUTH)	13.34M	NO CHANGE	



GENERAL NOTE.
1.CONTRACTOR TO VERIFY ALL DIMENSIONS AND
CONDITIONS PRIOR TO CONSTRUCTION

1

SITE PLAN

1/16" = 1'-0"

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T: 416-299-8837
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No.

Description

Date

1

CLIENT REVIEW

APR 4 , 2025

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CLIENT REVIEW

APR 14 , 2025

3

ISSUED FOR COA

APR 24 , 2025

4

ISSUED FOR COA

MAY 22 , 2025

5

6

7

PROJECT TITLE

91 Crestwood Rd, Thornhill, ON
L4J 1A7

DRAWING TITLE

SITE PLAN

Drawn By

KT

Date

MAY/22/25

Checked By

BW

Scale

1/16" = 1'-0"

Proj. No.

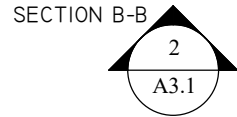
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DWG. No.

A1.0



<div>LHW ENGINEERING LTD.</div> <div><div><div></div><div></div></div><div>Unit 405, 2347 Kennedy Rd Toronto, ON M1T 3T8 T: 416-299-8837 LHW.ENGINEERING@GMAIL.COM</div></div>	MAY/22/25	No.	Description	Date	PROJECT TITLE 91 Crestwood Rd, Thornhill, ON L4J 1A7	Date MAY/22/25	Drawn By KT
		1	CLIENT REVIEW	APR 4 , 2025			
		2	CLIENT REVIEW	APR 14 , 2025	DRAFTING TITLE PROPOSED MAIN FLOOR PLAN	Proj. No. -	DWG. No. A1.4
		3	ISSUED FOR COA	APR 24 , 2025			
		4	ISSUED FOR COA	MAY 22 , 2025			
		5					
		6					
		7					

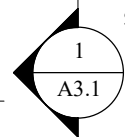


SECTION B-B

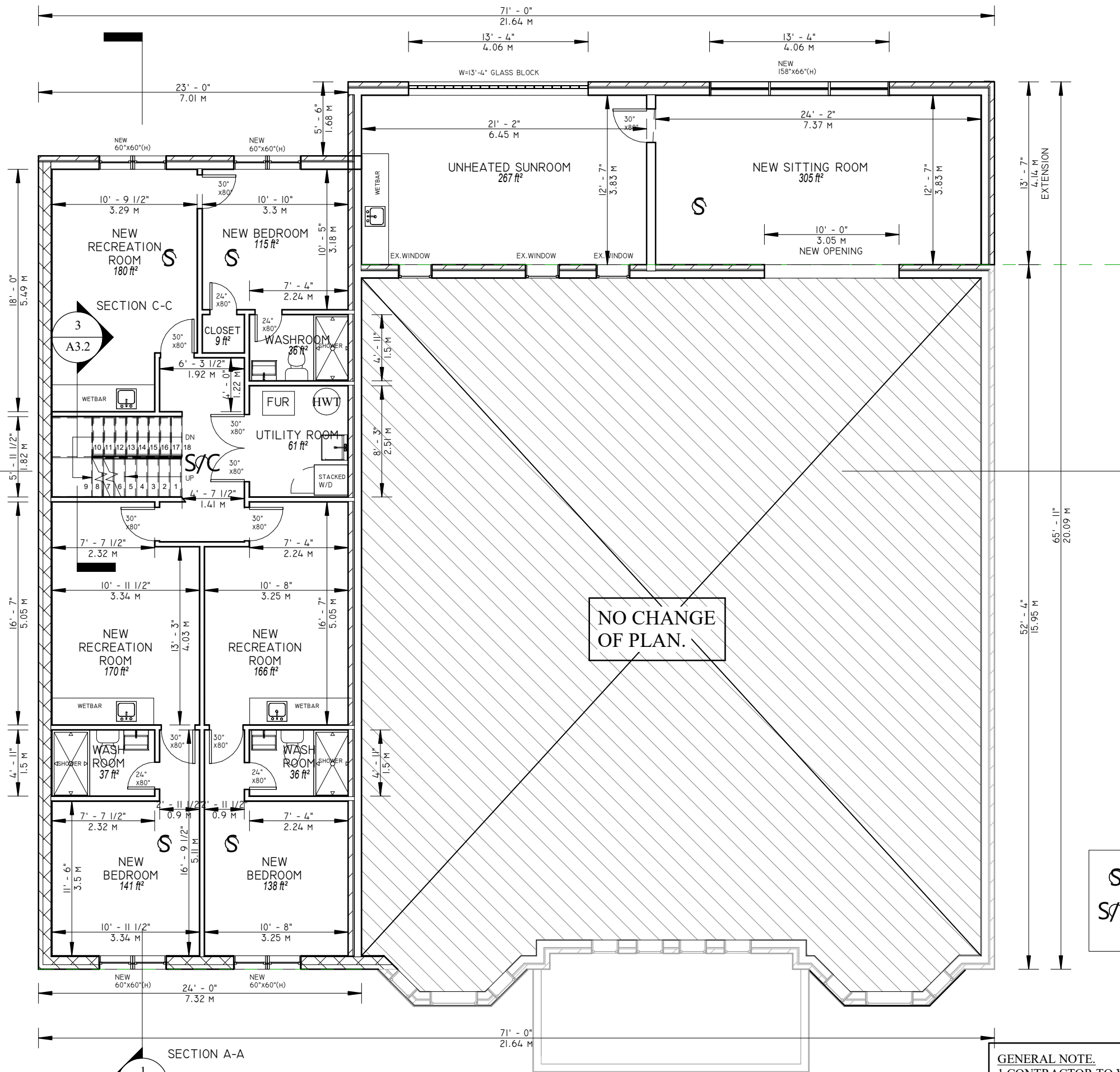
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SECOND FLOOR (ADDITION)

1/8" = 1'-0"



SECTION A-A



S SMOKE ALARM
S/C SMOKE ALARM / CARBON MONOXIDE ALARM

GENERAL NOTE.
1.CONTRACTOR TO VERIFY ALL DIMENSIONS AND CONDITIONS PRIOR TO CONSTRUCTION

LHW ENGINEERING LTD.

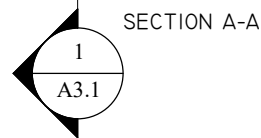
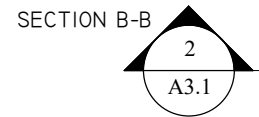
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LHWENGINEERING@GMAIL.COM





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MAY/22/25

PROJECT TITLE	Date	Drawn By
91 Crestwood Rd, Thornhill, ON L4T 1A7	MAY/22/25	KT
DRAWING TITLE	Scale	Checked By
PROPOSED SECOND FLOOR	1/8" = 1'-0"	BW
	Proj. No.	DWG. No.
	-	A1.5



1

	SMOKE ALARM
	SMOKE ALARM / CARBON MONOXIDE ALARM


EX.ROOF
TO BE REMOVED

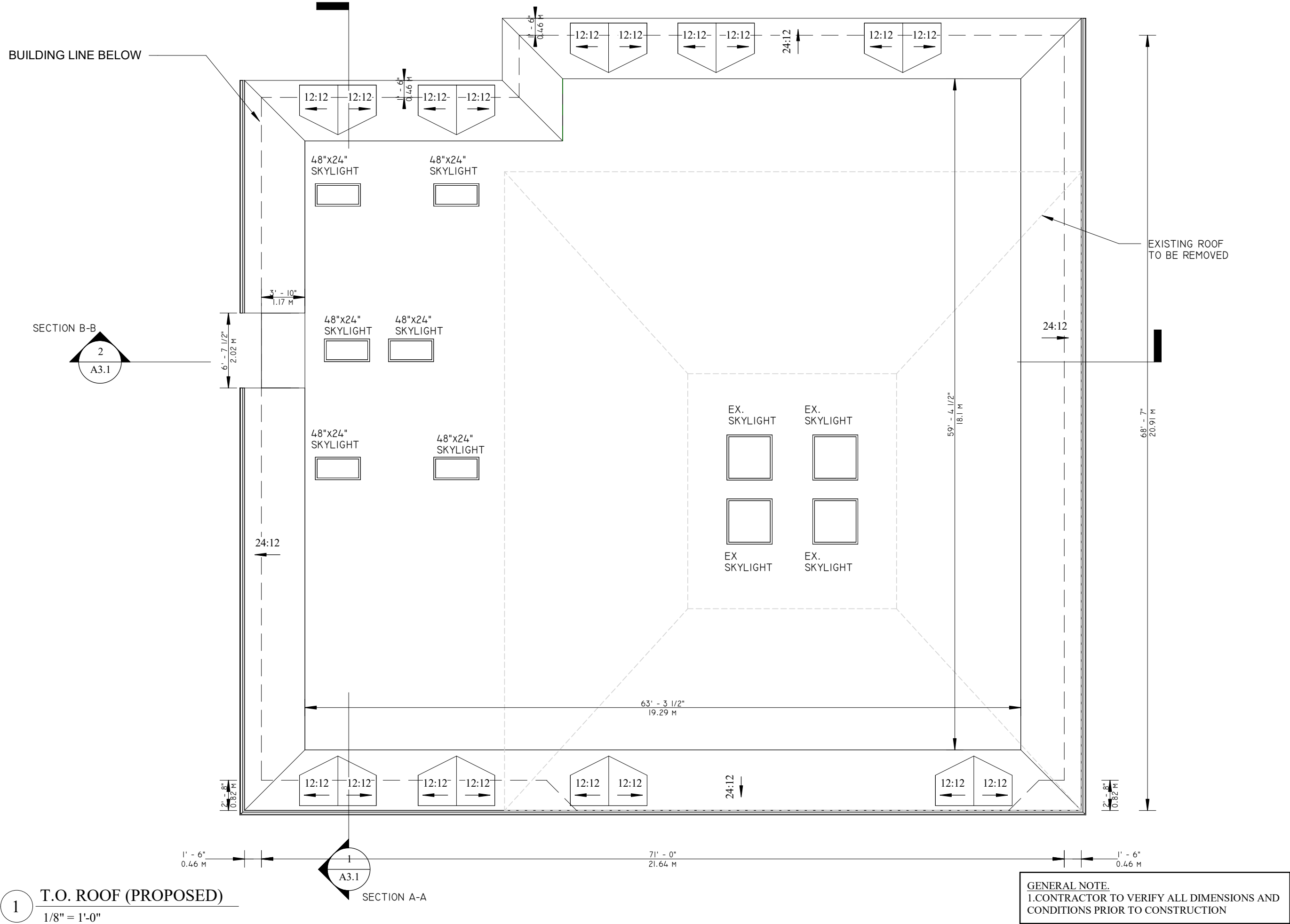
EX.2x6 @ 16" O.C
TO BE EXTENDED

HEATED NEW ATTIC
SPACE(STORAGE)

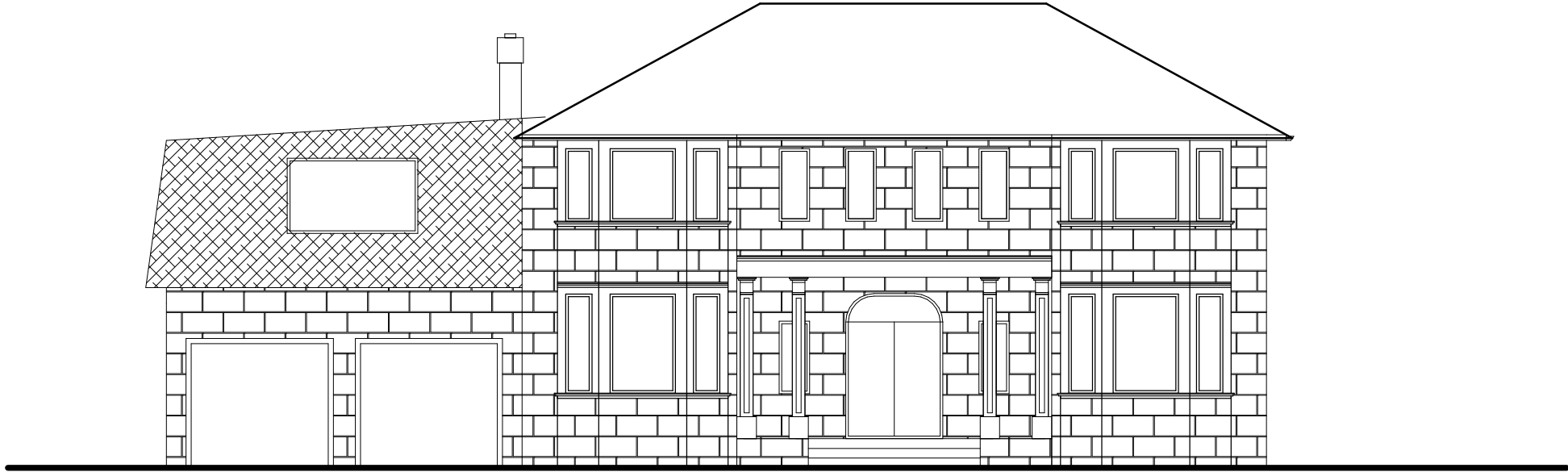
— NEW 2x4 @ 16" O.C

NEW 2x6 @16"O.C
- WALL TO BE ADDED.
ON THE TOP OF EX.WALL

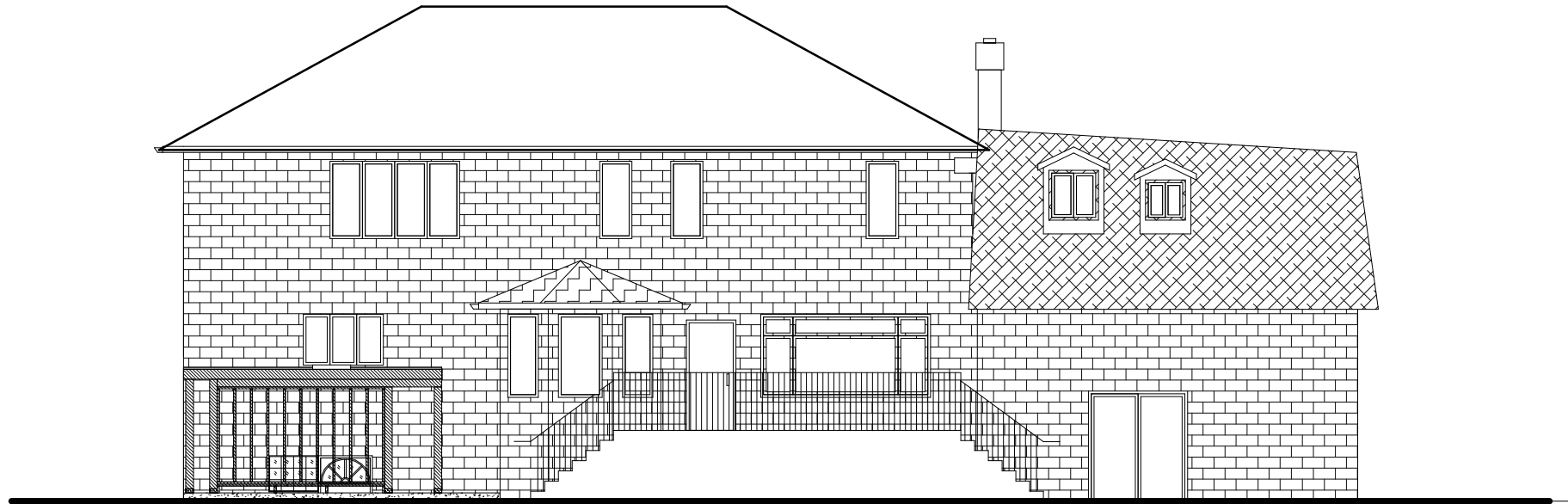
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		5					
		6					
		7					



LHW ENGINEERING LTD. Unit 405, 2347 Kennedy Rd Toronto, ON M1T 3T8 T: 416-299-8837 LHW.ENGINEERING@GMAIL.COM		MAY/22/25		No.		Description		Date		PROJECT TITLE		Date	Drawn By
				1		CLIENT REVIEW		APR 4, 2025				MAY/22/25	KT
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				5									
				6									
				7									
										91 Crestwood Rd, Thornhill, ON L4J 1A7		Scale 1/8" = 1'-0"	Checked By BW
										DRAWING TITLE PROPOSED ROOF		Proj. No. -	DWG. No. A1.7



1 FRONT ELEVATION (EXISTING)
1" = 10'-0"



2 REAR ELEVATION (EXISTING)
1" = 10'-0"

NOTE:
FOR REFERENCE ONLY

LHW ENGINEERING LTD.

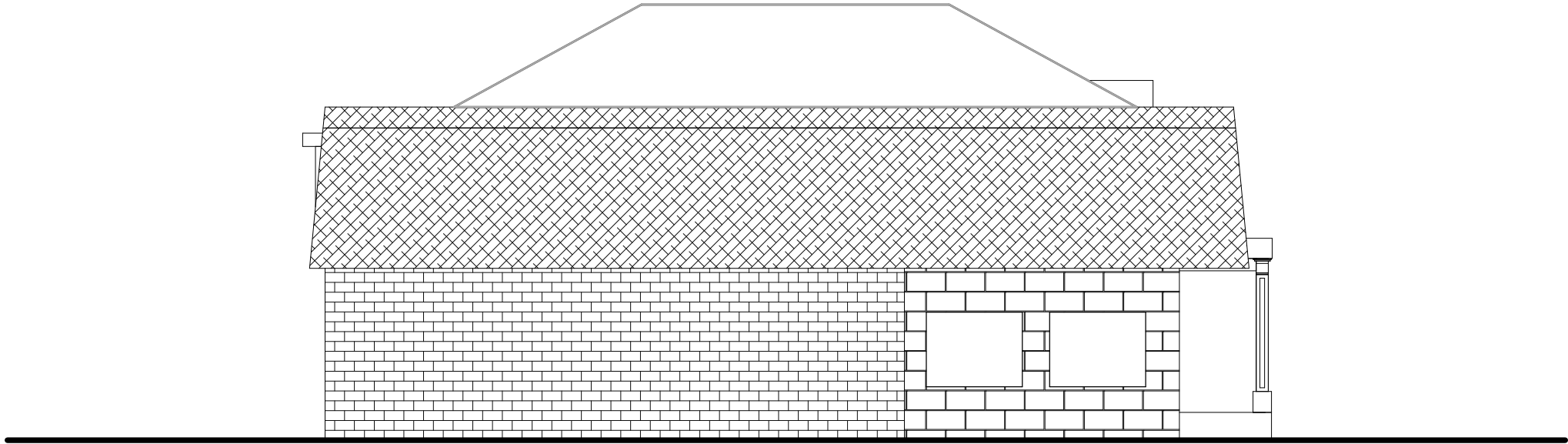
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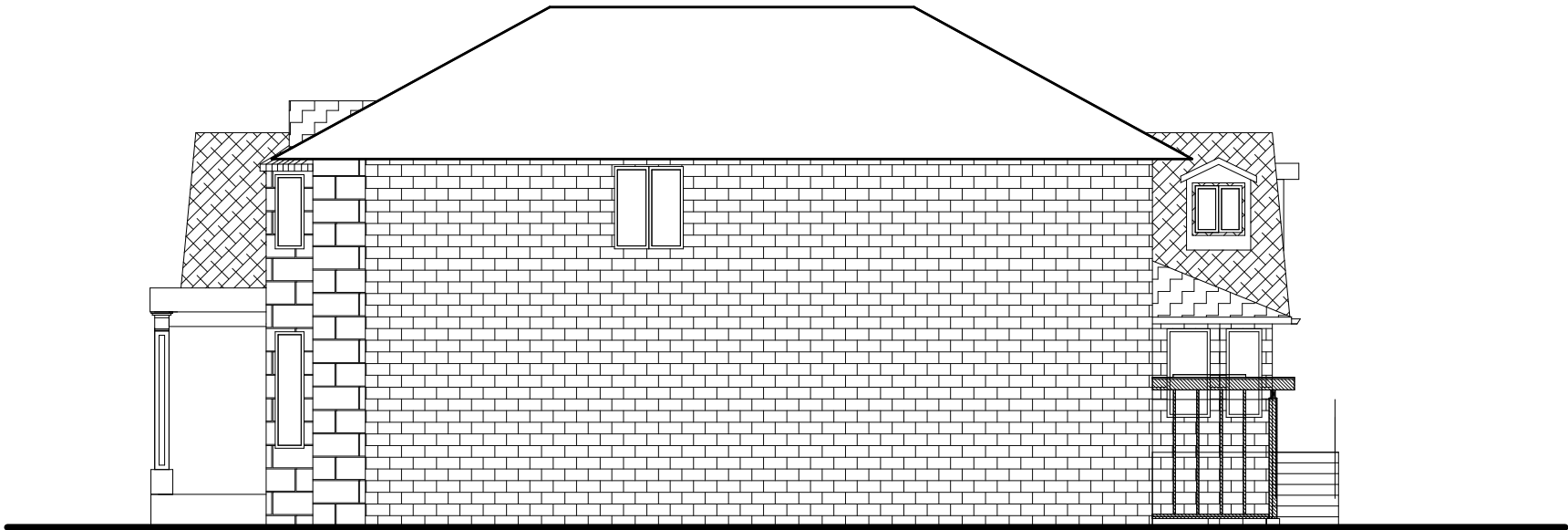
PROJECT TITLE	Date	Drawn By
91 Crestwood Rd, Thornhill, ON L4J 1A7	MAY/22/25	KT
DRAWING TITLE EX. FRONT & REAR ELEVATION	Scale 1" = 10'-0"	Checked By BW
	Proj. No. -	DWG. No. A2.1



1

EAST ELEVATION (EXISTING)

1" = 10'-0"



2

WEST ELEVATION (EXISTING)

1" = 10'-0"

NOTE:

FOR REFERENCE ONLY

LHW ENGINEERING LTD.

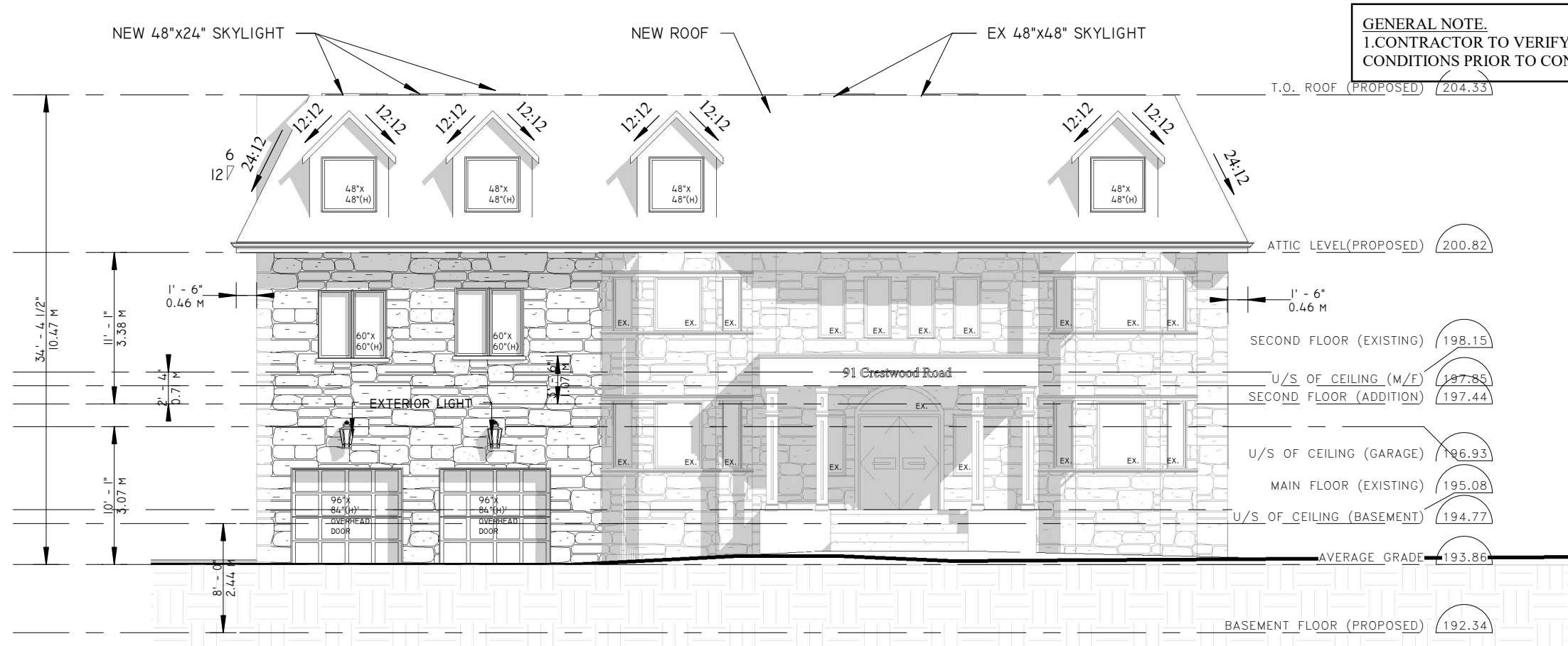
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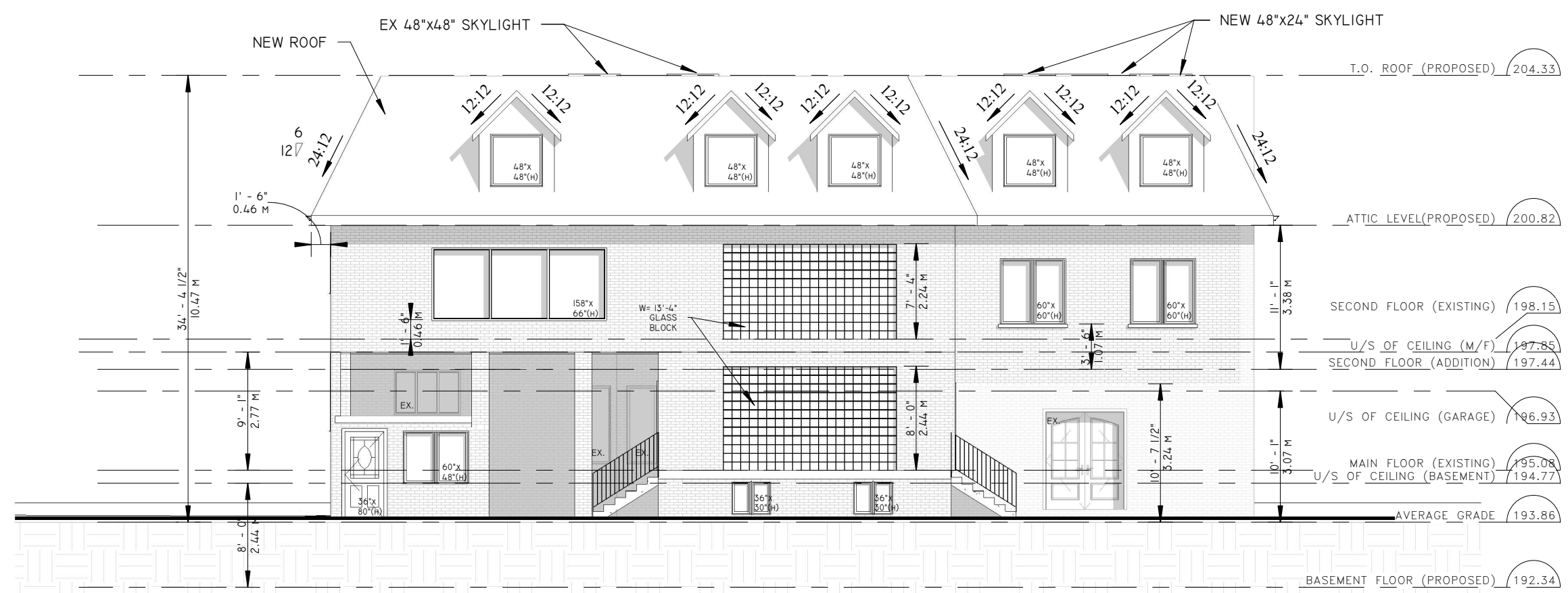
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PROJECT TITLE	Date	Drawn By
91 Crestwood Rd, Thornhill, ON L4J 1A7	MAY/22/25	KT
DRAWING TITLE	Scale	Checked By
EX. EAST & WEST ELEVATION	1" = 10'-0"	Checker
	Proj. No. -	DWG. No. A2.2



1 FRONT ELEVATION(PROPOSED)
1" = 10'-0"



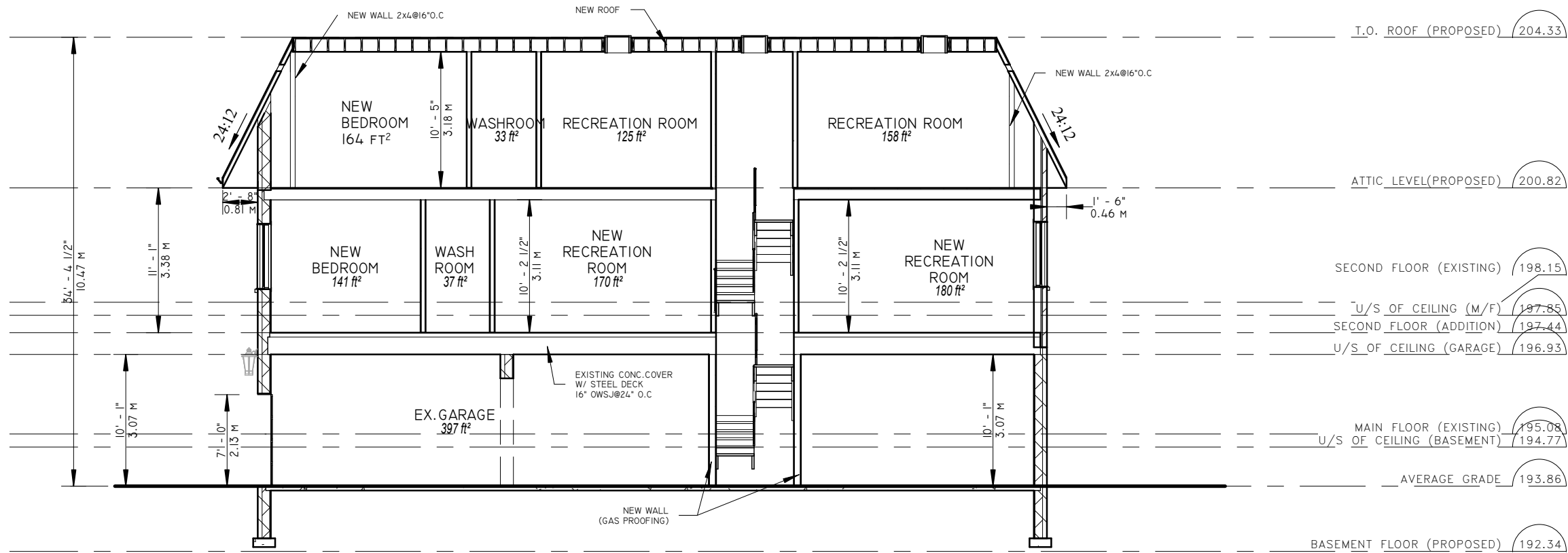
2 REAR ELEVATION(PROPOSED)
1" = 10'-0"

GENERAL NOTE.
1.CONTRACTOR TO VERIFY ALL DIMENSIONS AND
CONDITIONS PRIOR TO CONSTRUCTION

Drawn By	KT	Checked By	BW	DWG. No.	A2.3
Date	MAY/22/25	Scale	1" = 10'-0"	Proj. No.	-
PROJECT TITLE		91 Crestwood Rd, Thornhill, ON L4J 1A7		DRAWING TITLE PROPOSED FRONT & REAR ELEVATION	
Date	APR 4 , 2025	Description	CLIENT REVIEW	No.	1
Date	APR 14 , 2025	Description	CLIENT REVIEW	No.	2
Date	APR 24 , 2025	Description	ISSUED FOR COA	No.	3
Date	MAY 22 , 2025	Description	ISSUED FOR COA	No.	4
Date		Description		No.	5
Date		Description		No.	6
Date		Description		No.	7

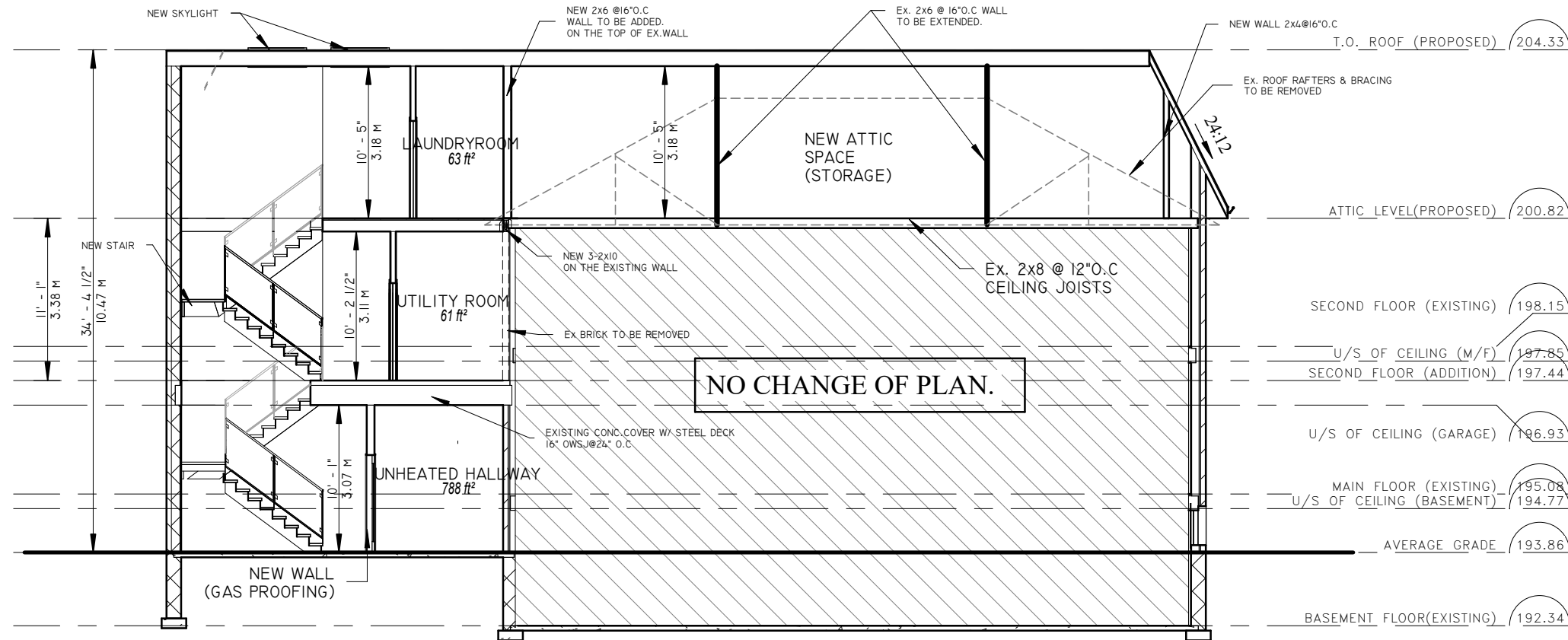
LHW ENGINEERING LTD.

Unit 405, 2347 Kennedy Rd
Toronto, ON M1T 3T8
T: 416-299-8837
LHWENGINEERING@GMAIL.COM

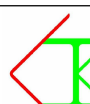


1 SECTION A-A
1" = 10'-0"

GENERAL NOTE:
1.CONTRACTOR TO VERIFY ALL DIMENSIONS
AND CONDITIONS PRIOR TO CONSTRUCTION



2 SECTION B-B
1" = 10'-0"

<div>LHW ENGINEERING LTD.</div> <div>Unit 405, 2347 Kennedy Rd Toronto, ON M1T 3T8 T: 416-299-8837 LHW.ENGINEERING@GMAIL.COM</div> <div></div>				MAY/22/25
	No.	Description	Date	
	1	CLIENT REVIEW	APR 4 , 2025	
	2	CLIENT REVIEW	APR 14 , 2025	
	3	ISSUED FOR COA	APR 24 , 2025	
	4	ISSUED FOR COA	MAY 22 , 2025	
	5			
	6			
	7			
PROJECT TITLE		Date	Drawn By	
91 Crestwood Rd, Thornhill, ON L4J 1A7		MAY/22/25	KT	
DRAWING TITLE		Scale	Checked By	
SECTION VIEW		1" = 10'-0"	BW	
		Proj. No.	DWG. No.	
		-	A3.1	

SCHEDULE B:
COMMENTS FROM AGENCIES, BUILDING STANDARDS &
DEVELOPMENT PLANNING

Internal Departments *Comments Received	Conditions Required		Nature of Comments
Building Standards (Zoning)	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
Development Planning	Yes <input type="checkbox"/>	No <input type="checkbox"/>	Application Under Review

External Agencies *Comments Received	Conditions Required		Nature of Comments *See Schedule B for full comments
Alectra	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
Region of York	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
TRCA	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
Metrolinx	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments

Date: May 7th 2025

Attention: **Christine Vigneault**

RE: Request for Comments

File No.:

Related Files: **A054-25**

Applicant: Lhw Engineering Ltd

Location 91 Crestwood Road

COMMENTS:

- ☐ We have reviewed the proposed Variance Application and have no comments or objections to its approval.
- ☒ We have reviewed the proposed Variance Application and have no objections to its approval, subject to the following comments (attached below).
- ☐ We have reviewed the proposed Variance Application and have the following concerns (attached below).

Alectra Utilities (formerly PowerStream) has received and reviewed the proposed Variance Application. This review, however, does not imply any approval of the project or plan.

All proposed billboards, signs, and other structures associated with the project or plan must maintain minimum clearances to the existing overhead or underground electrical distribution system as specified by the applicable standards, codes and acts referenced.

In the event that construction commences, and the clearance between any component of the work/structure and the adjacent existing overhead and underground electrical distribution system violates the Occupational Health and Safety Act, the customer will be responsible for 100% of the costs associated with Alectra making the work area safe. All construction work will be required to stop until the safe limits of approach can be established.

In the event construction is completed, and the clearance between the constructed structure and the adjacent existing overhead and underground electrical distribution system violates the any of applicable standards, acts or codes referenced, the customer will be responsible for 100% of Alectra's cost for any relocation work.

References:

- Ontario Electrical Safety Code, latest edition (Clearance of Conductors from Buildings)
- Ontario Health and Safety Act, latest edition (Construction Protection)
- Ontario Building Code, latest edition (Clearance to Buildings)
- PowerStream (Construction Standard 03-1, 03-4), attached
- Canadian Standards Association, latest edition (Basic Clearances)

If more information is required, please contact either of the following:

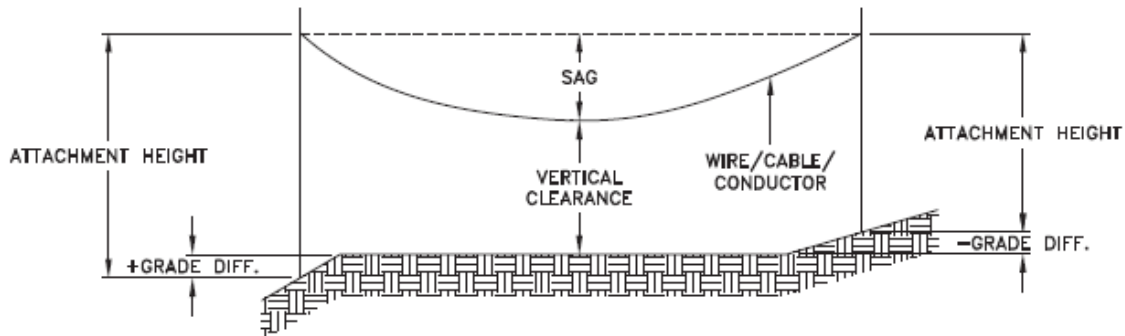
Mr. Stephen Cranley, C.E.T
Supervisor, Distribution Design, ICI & Layouts (North)
Phone: 1-877-963-6900 ext. 31297

E-mail: stephen.cranley@alectrautilities.com

Mitchell Penner
Supervisor, Distribution Design-Subdivisions
Phone: 416-302-6215

Email: Mitchell.Penner@alectrautilities.com

LOCATION OF WIRES, CABLES OR CONDUCTORS	SYSTEM VOLTAGE			
	SPAN GUYS AND COMMUNICATIONS WIRES	UP TO 600V AND NEUTRAL	4.16/2.4kV TO 27.6/16kV (SEE NOTE 1)	44kV
	MINIMUM VERTICAL CLEARANCES (SEE NOTE 2)			
OVER OR ALONGSIDE ROADS, DRIVEWAYS OR LANDS ACCESSIBLE TO <u>VEHICLES</u>	442cm	442cm	480cm	520cm
OVER GROUND ACCESSIBLE TO <u>PEDESTRIANS AND BICYCLES ONLY</u>	250cm	310cm	340cm	370cm
ABOVE TOP OF RAIL AT <u>RAILWAY CROSSINGS</u>	730cm	730cm	760cm	810cm



MINIMUM ATTACHMENT HEIGHT = MAXIMUM SAG
 + MINIMUM VERTICAL CLEARANCE (FROM ABOVE TABLE)
 + GRADE DIFFERENCE
 + 0.3m (VEHICLE OR RAILWAY LOCATION)
 + SNOW DEPTH (PEDESTRIAN LOCATION, SEE NOTE 3)

NOTES:

1. THE MULTIGROUNDED SYSTEM NEUTRAL HAS THE SAME CLEARANCE AS THE 600V SYSTEM.
2. THE VERTICAL CLEARANCES IN THE ABOVE TABLE ARE UNDER MAXIMUM SAG CONDITIONS.
3. REFER TO CSA STANDARD C22.3 No.1, ANNEX D FOR LOCAL SNOW DEPTH VALUES.
4. ALL CLEARANCES ARE IN ACCORDANCE TO CSA STANDARD C22.3.

CONVERSION TABLE	
METRIC	IMPERIAL (APPROX)
810cm	27'-0"
760cm	25'-4"
730cm	24'-4"
520cm	17'-4"
480cm	16'-0"
442cm	15'-5"
370cm	12'-4"
340cm	11'-4"
310cm	10'-4"
250cm	8'-4"

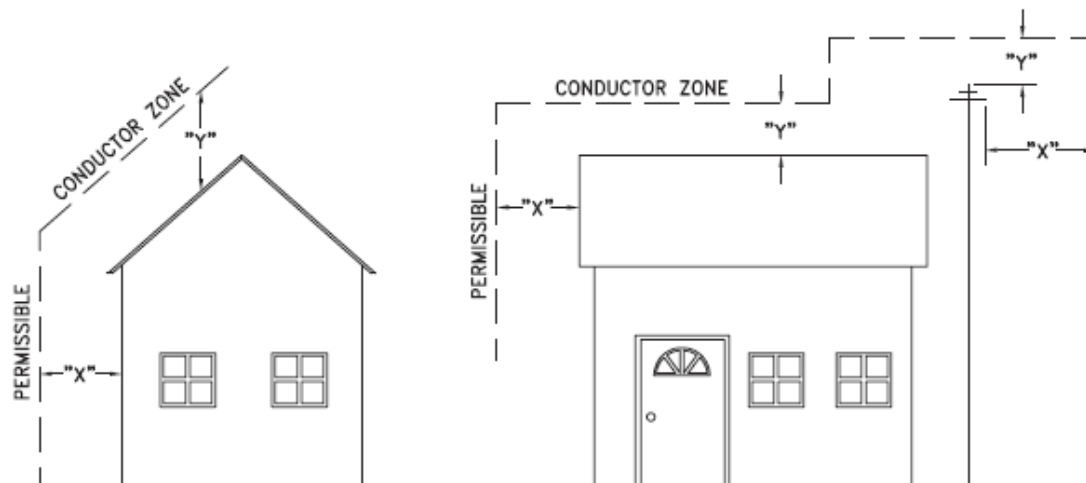
REFERENCES

SAGS AND TENSIONS | SECTION 02

MINIMUM VERTICAL CLEARANCES OF WIRES, CABLES AND CONDUCTORS ABOVE GROUND OR RAILS

ORIGINAL ISSUE DATE: 2010-DEC-24 REVISION NO: R1 REVISION DATE: 2012-JAN-09

Certificate of Approval	
This construction Standard meets the safety requirements of Section 4 of Regulation 22/04	
Joe Crozier, P.Eng.	2012-JAN-09
Name	Date
P.Eng. Approval By:	Joe Crozier



VOLTAGE	MINIMUM HORIZONTAL CLEARNACE UNDER MAXIMUM SWING CONDITIONS DIMENSION "X" (SEE NOTES 1, 3 & 4)	MINIMUM VERTICAL CLEARANCE UNDER MAXIMUM DESIGN SAG CONDITIONS DIMENSION "Y" (SEE NOTES 1, 2, 4 & 5)
0-600V AND NEUTRAL	100cm	250cm
4.16/2.4 TO 44kV	300cm	480cm

NOTES

1. UNDER NO CIRCUMSTANCES SHALL A CONDUCTOR BE PERMITTED TO PENETRATE THE ENVELOPE SHOWN BY THE DOTTED LINE.
2. THE VERTICAL CLEARANCES ARE UNDER CONDITIONS OF MAXIMUM DESIGN SAG.
3. THE HORIZONTAL CLEARANCES ARE UNDER CONDITIONS OF MAXIMUM SWING. WHERE THE CONDUCTOR SWING IS NOT KNOWN A HORIZONTAL CLEARANCE OF 480CM SHALL BE USED.
4. BUILDINGS THAT EXCEED 3 STOREYS OR 15M IN HEIGHT, THE MINIMUM HORIZONTAL CLEARANCE OF THE SECONDARY CONDUCTORS SHOULD BE INCREASED TO 300cm WHERE IT IS NECESSARY TO ALLOW FOR THE RAISING OF LADDERS BY LOCAL FIRE DEPARTMENTS.
5. IN SITUATIONS SUCH AS MULTI-LEVEL GARAGES, WHERE ROOFS ARE NORMALLY USED BY PERSONS AND VEHICLES, THE VERTICAL CLEARANCES OF POWERSTREAM STANDARD 03-1 SHALL APPLY.
6. DISTRIBUTION LINES CONSTRUCTED NEAR BUILDINGS SHALL BE BUILT TO AVOID OVERHANG WHEREVER POSSIBLE. WHERE LINES MUST BE CONSTRUCTED OVER OR ADJACENT TO BUILDINGS THE APPLICABLE HORIZONTAL AND VERTICAL CLEARANCES SHALL BE AT CONDITIONS OF MAXIMUM CONDUCTOR SWING AND MAXIMUM SAG. THE ABOVE CLEARANCES ARE DESIGNED TO PREVENT PERSONS ON OR IN BUILDINGS AS WELL AS EXTERNAL MACHINERY USED IN CONJUNCTION WITH A BUILDING TO COME IN CONTACT WITH CONDUCTORS. EFFORTS SHOULD BE MADE TO INCREASE THESE CLEARANCES WHERE POSSIBLE.
7. ALL CLEARANCES ARE IN ACCORDANCE TO CSA C22.3 NO.1-06 (TABLE-9).

CONVERSION TABLE	
METRIC	IMPERIAL (APPROX)
480cm	16'-0"
300cm	10'-0"
250cm	8'-4"
100cm	3'-4"

MINIMUM VERTICAL & HORIZONTAL CLEARANCES OF CONDUCTORS FROM BUILDINGS OR OTHER PERMANENT STRUCTURES (CONDUCTORS NOT ATTACHED TO BUILDINGS)

ORIGINAL ISSUE DATE: 2010-MAY-05 REVISION NO: REVISION DATE:

PS:\System Planning and Standards\Standard Design\PowerStream Standards\PowerStream Standards working folder\Section 3\3-4\DWG 03-4 R0 May 5, 2010.dwg, 5/5/2010 8:22:02 AM, Adobe PDF

Certificate of Approval
This construction Standard meets the safety requirements of Section 4 of Regulation 22/04
Debbie Dadwani, P.Eng. 2010-MAY-05
Name Date
P.Eng. Approval By: D. Dadwani

To: Committee of Adjustment

From: Bernd Paessler, Building Standards Department

Date: May 21, 2025

Applicant: LHW ENGINEERING LTD

Location: 91 Crestwood Road
PLAN RP3205 Part of Lot 65

File No.(s): A054/25

Zoning Classification:

The subject lands are zoned R2A(EN) – 2nd Density Residential Zone (Established Neighbourhood) and subject to the provisions of Exception 14.205 under Zoning By-law 001-2021, as amended.

#	Zoning By-law 001-2021	Variance requested
1	The minimum required interior side yard is 1.8m where there is a door providing entry into the sideyard. Exception 14.205 E-390	To permit a minimum interior side yard of 1.45m on the west side.
2	The minimum required interior side yard is 1.2m . Exception 14.205 E-390	To permit a minimum interior side yard of 1.03m on the east side.
3	In a Residential Zone, the maximum cumulative width of both accesses to a circular driveway, as measured at the street line, shall be 9.0 m . Section 6.7.4 4	To permit a maximum cumulative driveway width of 10.31m measured at the street line.

Staff Comments:

Stop Work Order(s) and Order(s) to Comply:

There are no outstanding Orders on file

Other Comments:

General Comments	
1	The applicant shall be advised that additional variances may be required upon review of detailed drawing for building permit/site plan approval.

Conditions of Approval:

If the committee finds merit in the application, the following conditions of approval are recommended.

* Comments are based on the review of documentation supplied with this application.

From: [Tetyana Bailey](#)
To: [Committee of Adjustment Mailbox](#)
Subject: [External] RE: A054/25 - 91 CRESTWOOD ROAD - REQUEST FOR COMMENTS, CITY OF VAUGHAN
Date: Thursday, May 15, 2025 11:01:55 AM
Attachments: [image001.png](#)

CAUTION! This is an external email. Verify the sender's email address and carefully examine any links or attachments before clicking. If you believe this may be a phishing email, please use the Phish Alert Button.

Hi there,

The subject property for this application is positioned outside Metrolinx jurisdiction, and as such we have no comments.

If you are unsure about Metrolinx interest areas, please refer to this interactive map:

https://maps.metrolinx.com/Third_Party_Coordination_Permitting/

If you have any questions, please don't hesitate to contact our team at

development.coordinator@metrolinx.com

Best,

Tetyana Bailey MCIP, RPP, AIC Candidate Member

Project Manager, Adjacent Construction Review (Subways)

Real Estate & Development

Metrolinx | 20 Bay Street | Toronto | Ontario | M5J 2W3

T: 416.432.9975 | tetyana.bailey@metrolinx.com



From: [Cameron McDonald](#)
To: [Committee of Adjustment Mailbox](#)
Subject: [External] RE: A054/25 - 91 CRESTWOOD ROAD - REQUEST FOR COMMENTS, CITY OF VAUGHAN
Date: Monday, May 12, 2025 9:54:44 AM

CAUTION! This is an external email. Verify the sender's email address and carefully examine any links or attachments before clicking. If you believe this may be a phishing email, please use the Phish Alert Button.

Hello,

Based on a review of our screening mapping, I can confirm that the subject property is not located within TRCA's Regulated Area. As such, any site alteration or development on the property would not require a permit from the TRCA.

Based on the above, we have no comments/requirements.

Regards,

Cameron McDonald

Planner I

Development Planning and Permits | Development and Engineering Services

T: [\(437\) 880-1925](tel:(437)880-1925)

E: cameron.mcdonald@trca.ca

A: [5 Shoreham Drive, Toronto, ON, M3N 1S4](#) | trca.ca



From: [Development Services](#)
To: [Committee of Adjustment Mailbox](#)
Subject: [External] RE: A054/25 - 91 CRESTWOOD ROAD - REQUEST FOR COMMENTS, CITY OF VAUGHAN
Date: May-08-25 2:08:55 PM

CAUTION! This is an external email. Verify the sender's email address and carefully examine any links or attachments before clicking. If you believe this may be a phishing email, please use the Phish Alert Button.

Good afternoon,

The Regional Municipality of York has completed its review of the above minor variance and has no comment.

Regards,

Gabrielle

Gabrielle Hurst MCIP, RPP | Associate Planner, Development Planning, Economic and Development Services Branch | The Regional Municipality of York | 1-877 464 9675 ext 71538 | gabrielle.hurst@york.ca | www.york.ca

SCHEDULE C: PUBLIC & APPLICANT CORRESPONDENCE

Correspondence Type	Name	Address	Date Received (mm/dd/yyyy)	Summary
N/A				

SCHEDULE D: BACKGROUND

Application No. (City File)	Application Description (i.e. Minor Variance Application; Approved by COA / OLT)
N/A	N/A