

ITEM: 6.14	REPORT SUMMARY MINOR VARIANCE APPLICATION A051/25
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Report Date: May 30, 2025

THIS REPORT CONTAINS COMMENTS FROM THE FOLLOWING  
DEPARTMENTS & AGENCIES (SEE SCHEDULE B):

Additional comments from departments and agencies received after the publication of the report will be made available on the City’s [website](#).

Internal Departments *Comments Received	Conditions Required		Nature of Comments
Committee of Adjustment	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
Building Standards (Zoning)	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
Development Planning	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	Recommend Approval w/Conditions
Development Engineering	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
Forestry	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
Development Finance	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments

External Agencies *Comments Received	Conditions Required		Nature of Comments *See Schedule B for full comments
Alectra	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
TRCA	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
Region of York	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
Region of Peel	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments

PUBLIC & APPLICANT CORRESPONDENCE (SEE SCHEDULE C)				
All personal information collected because of this public meeting (including both written and oral submissions) is collected under the authority of the Municipal Act, the Municipal Freedom of Information and Protection of Privacy Act (MFIPPA), the Planning Act and all other relevant legislation, and will be used to assist in deciding on this matter. All personal information (as defined by MFIPPA), including (but not limited to) names, addresses, opinions and comments collected will become property of the City of Vaughan, will be made available for public disclosure (including being posted on the internet) and will be used to assist the Committee of Adjustment and staff to process this application.				
Correspondence Type	Name	Address	Date Received (mm/dd/yyyy)	Summary
Applicant			04/28/2023	Application Cover Letter
Applicant			05/02/2025	Parking Study

BACKGROUND (SCHEDULE D, IF REQUIRED)	
* Background Information contains historical development approvals considered to be related to this file. This information should not be considered comprehensive.	
Application No. (City File)	Application Description (i.e. Minor Variance Application; Approved by COA / OLT)
N/A	N/A

ADJOURNMENT HISTORY	
* Previous hearing dates where this application was adjourned by the Committee and public notice issued.	
Hearing Date	Reason for Adjournment (to be obtained from NOD_ADJ)
None	N/A

SCHEDULES	
Schedule A	Drawings & Plans Submitted with the Application
Schedule B	Comments from Agencies, Building Standards & Development Planning
Schedule C (if required)	Public & Applicant Correspondence
Schedule D (if required)	Background

	<b>MINOR VARIANCE APPLICATION</b> <b>FILE NUMBER A051/25</b>
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CITY WARD #:	2
APPLICANT:	Highway 50 Nominee Inc.
AGENT:	Weston Consulting
PROPERTY:	30 Labourers Way, Vaughan
ZONING DESIGNATION:	See Below
VAUGHAN OFFICIAL PLAN (2010) DESIGNATION:	Vaughan Official Plan 2010 ('VOP 2010'): "Prestige Employment"
RELATED DEVELOPMENT APPLICATIONS:	Z.21.054 & DA.21.063
PURPOSE OF APPLICATION:	Relief from zoning by-law is being requested to permit reduced parking requirements for a manufacturing/processing facility.

The following variances are being requested from the City’s Zoning By-law to accommodate the above proposal:

The subject lands are zoned EM1 and subject to the provisions of Exception 14.1005 and EM2 and subject to the provisions of Exception 14.1005 under Zoning By-law 001-2021, as amended.

#	Zoning By-law 01-2021	Variance requested
1	A minimum of 1.0 parking spaces per 100 m <sup>2</sup> of gross floor area is required for a Manufacturing or Processing Facility. [Section 6.3.5, Table 6-2]	To permit a minimum of 0.5 parking spaces per 100m <sup>2</sup> of gross floor area for a Manufacturing or Processing Facility.

HEARING INFORMATION
<b>DATE OF MEETING:</b> Thursday, June 5, 2025 <b>TIME:</b> 6:00 p.m. <b>MEETING LOCATION:</b> Vaughan City Hall, Woodbridge Room (2 <sup>nd</sup> Floor), 2141 Major Mackenzie Drive <b>LIVE STREAM LINK:</b> <a href="https://vaughan.ca/LiveCouncil">Vaughan.ca/LiveCouncil</a>
PUBLIC PARTICIPATION
<p>If you would like to speak to the Committee of Adjustment at the meeting, either remotely or in person, please complete the <a href="#">Request to Speak Form</a> and submit to <a href="mailto:cofa@vaughan.ca">cofa@vaughan.ca</a></p> <p>If you would like to submit written comments, please quote file number above and submit by mail or email to:</p> <p><b>Email:</b> <a href="mailto:cofa@vaughan.ca">cofa@vaughan.ca</a></p> <p><b>Mail:</b> City of Vaughan, Office of the City Clerk, Committee of Adjustment, 2141 Major Mackenzie Drive, Vaughan, ON, L6A 1T1</p> <p>To speak electronically, pre-registration is required by completing the <a href="#">Request to Speak Form</a> on-line and submitting it to <a href="mailto:cofa@vaughan.ca">cofa@vaughan.ca</a> no later than NOON on the last business day before the meeting.</p> <p><b>THE DEADLINE TO REGISTER TO SPEAK ELECTRONICALLY OR SUBMIT WRITTEN COMMENTS ON THE ABOVE NOTED FILE(S) IS NOON ON THE LAST BUSINESS DAY BEFORE THE MEETING.</b></p>

INTRODUCTION
<p>Staff and Agencies act as advisory bodies to the Committee of Adjustment. The comments contained in this report are presented as recommendations to the Committee.</p> <p>Section 45(1) of the Planning Act sets the criteria for authorizing minor variances to the City of Vaughan’s Zoning By-law. Accordingly, review of the application may consider the following:</p>

INTRODUCTION	
<p>That the general intent and purpose of the by-law will be maintained.</p> <p>That the general intent and purpose of the official plan will be maintained.</p> <p>That the requested variance(s) is/are acceptable for the appropriate development of the subject lands.</p> <p>That the requested variance(s) is/are minor in nature.</p> <p>Public written and oral submissions relating to this application are taken into consideration by the Committee of Adjustment as part of its deliberations and final decision on this matter.</p>	

COMMITTEE OF ADJUSTMENT	
Date Public Notice Mailed:	May 22, 2025
Date Applicant Confirmed Posting of Sign:	May 2, 2025
Applicant Justification for Variances: <small>*As provided in Application Form</small>	See cover letter.
Was a Zoning Review Waiver (ZRW) Form submitted by Applicant: <small>*ZRW Form may be used by applicant in instances where a revised submission is made, and zoning staff do not have an opportunity to review and confirm variances prior to the issuance of public notice.</small>	Yes <input type="checkbox"/> No <input checked="" type="checkbox"/>
COMMENTS:	
None	
Committee of Adjustment Recommended Conditions of Approval:	None

BUILDING STANDARDS (ZONING)	
**See Schedule B for Building Standards (Zoning) Comments	
Building Standards Recommended Conditions of Approval:	None

DEVELOPMENT PLANNING	
**See Schedule B for Development Planning Comments.	
Development Planning Recommended Conditions of Approval:	That the Owner obtains site plan approval from the Development and Parks Planning Department by way of a note to file DA.21.063 to the satisfaction of Development and Parks Planning staff.

DEVELOPMENT ENGINEERING	
<a href="#">Link to Grading Permit</a> <a href="#">Link to Pool Permit</a> <a href="#">Link to Curb Curt Permit</a> <a href="#">Link Culvert Installation</a>	
<p>The following Minor Variance application addresses a technical adjustment in the use classification for Building B on the Highway 50 site. While the building’s designation is proposed to shift from "Warehousing and Distribution Facility" to "Manufacturing or Processing Facility" to accommodate a new window manufacturing tenant, the applicant is seeking to retain the originally approved parking ratio. This request is based on the operational needs of the incoming tenant, the continued presence of warehousing functions, and the availability of surplus parking elsewhere on the site. The application aims to demonstrate that the existing parking configuration remains suitable and sufficient, even with the change in use classification. The Development Engineering Department does not object to the Minor Variance application A051/25.</p>	
Development Engineering Recommended Conditions of Approval:	None

PARKS, FORESTRY & HORTICULTURE (PFH)	
No comments received to date.	
PFH Recommended Conditions of Approval:	None

DEVELOPMENT FINANCE	
No comment no concerns.	
Development Finance Recommended Conditions of Approval:	None

BY-LAW AND COMPLIANCE, LICENSING AND PERMIT SERVICES	
No comments received to date.	
BCLPS Recommended Conditions of Approval:	None

BUILDING INSPECTION (SEPTIC)	
No comments received to date.	
Building Inspection Recommended Conditions of Approval:	None

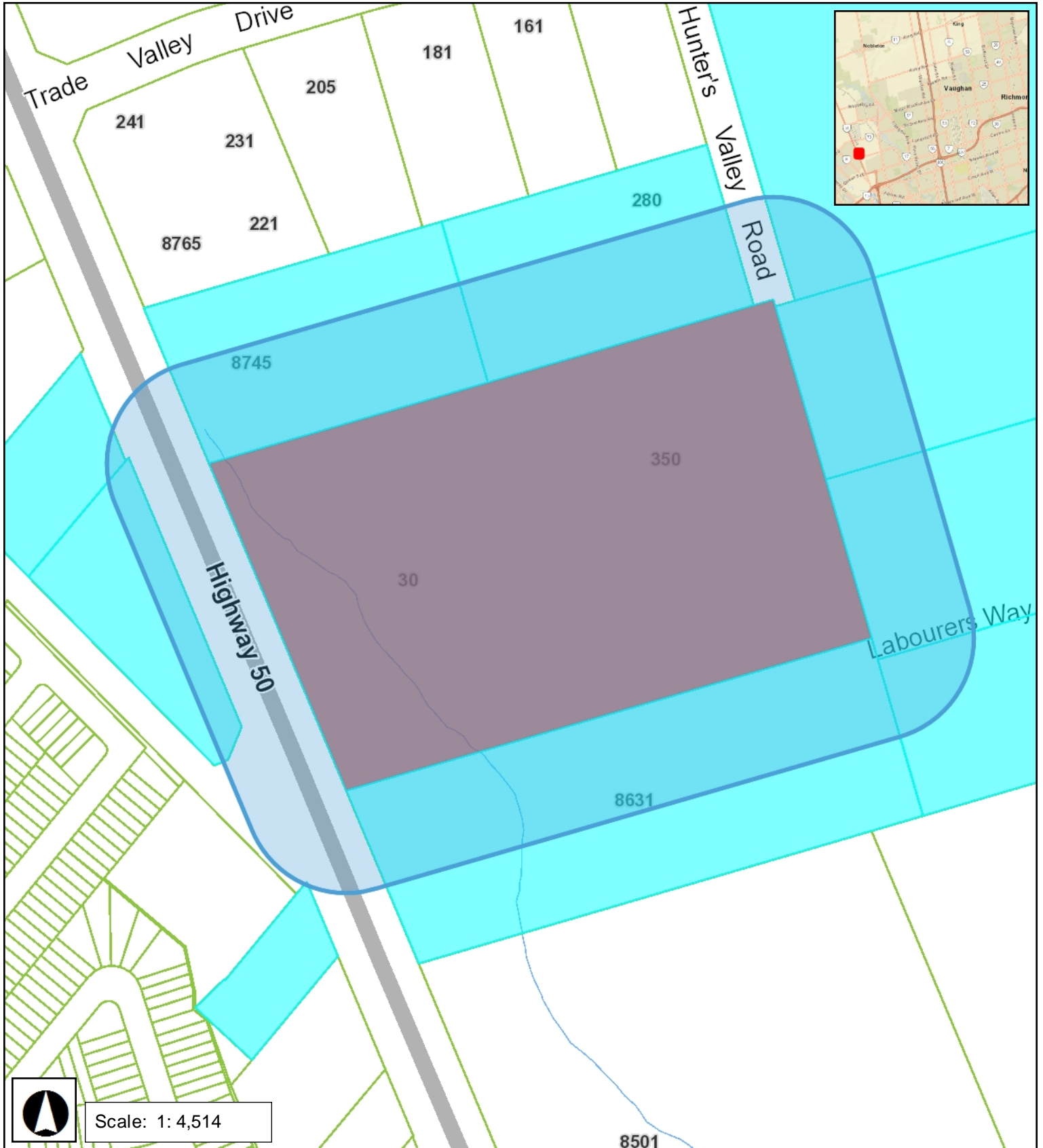
FIRE DEPARTMENT	
No comments received to date.	
Fire Department Recommended Conditions of Approval:	None

RECOMMENDED CONDITIONS OF APPROVAL SUMMARY		
Should the Committee find it appropriate to approve this application in accordance with request and the sketch submitted with the application, as required by Ontario Regulation 200/96, the following conditions have been recommended:		
#	DEPARTMENT / AGENCY	CONDITION
1	Development Planning <a href="mailto:Harry.Zhao@vaughan.ca">Harry.Zhao@vaughan.ca</a>	That the Owner obtains site plan approval from the Development and Parks Planning Department by way of a note to file DA.21.063 to the satisfaction of Development and Parks Planning staff.
<i>All conditions of approval, unless otherwise stated, are considered to be incorporated into the approval “if required”. If a condition is no longer required after an approval is final and binding, the condition may be waived by the respective department or agency requesting conditional approval. A condition cannot be waived without written consent from the respective department or agency.</i>		

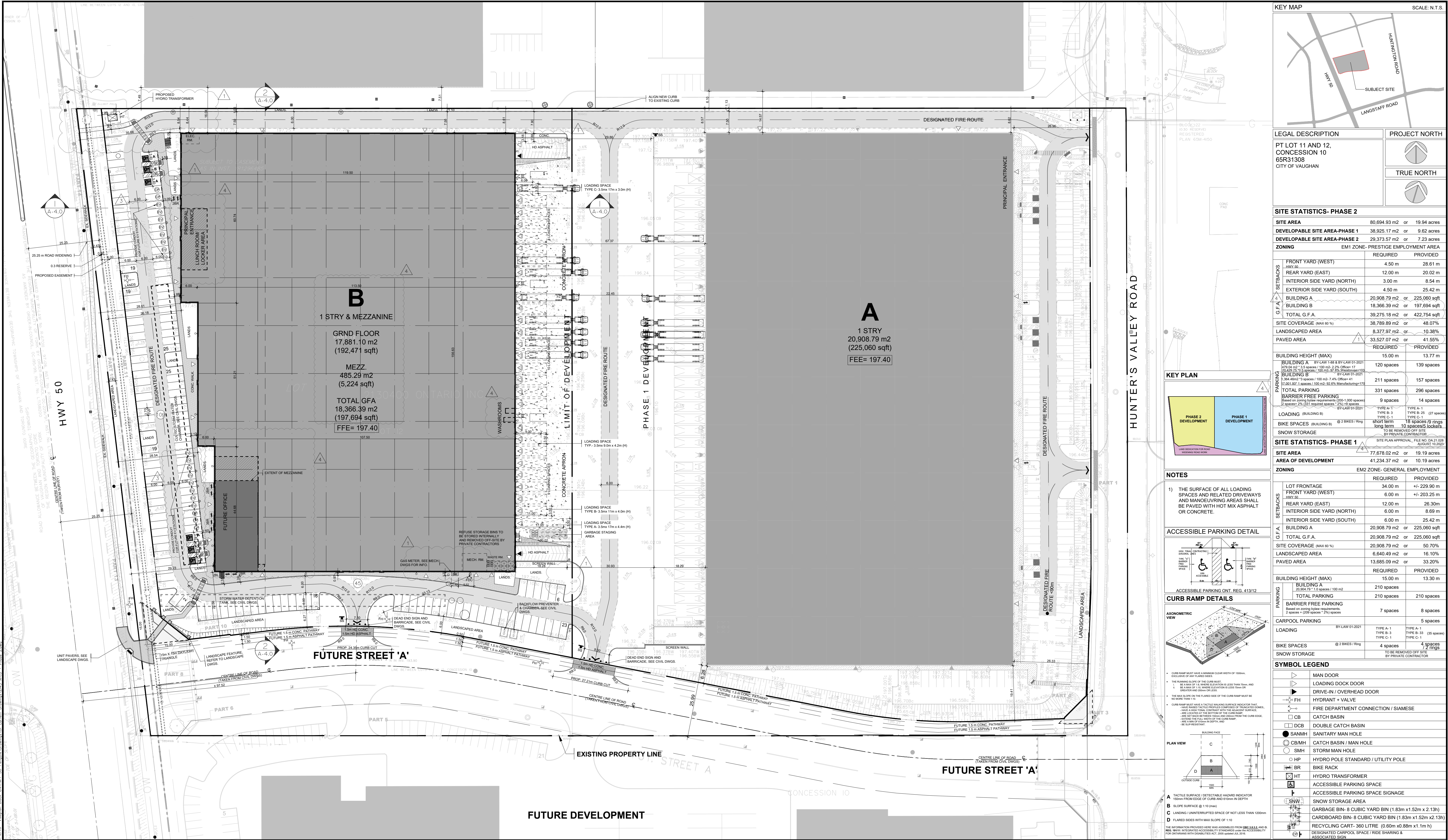
IMPORTANT INFORMATION
<b>CONDITIONS:</b> It is the responsibility of the owner/applicant and/or authorized agent to obtain and provide a clearance letter from respective department and/or agency ( <b>see condition chart above for contact</b> ). This letter must be provided to the Secretary-Treasurer to be finalized. All conditions must be cleared prior to the issuance of a Building Permit.
<b>APPROVALS:</b> Making any changes to your proposal after a decision has been made may impact the validity of the Committee’s decision.  An approval obtained from the Committee of Adjustment, where applicable, is tied to the building envelope shown on the plans and drawings submitted with the application and subject to the variance approval.  A building envelope is defined by the setbacks of the buildings and/or structures shown on the plans and drawings submitted with the application, as required by Ontario Regulation 200/96. Future development outside of an approved building envelope, where a minor variance was obtained, must comply with the provisions of the City’s Zoning By-law.  Elevation drawings are provided to reflect the style of roof (i.e. flat, mansard, gable etc.) to which a building height variance has been applied. Where a height variance is approved, building height is applied to the style of roof (as defined in the City’s Zoning By-law) shown on the elevation plans submitted with the application.  Architectural design features that are not regulated by the City’s Zoning By-law are not to be considered part of an approval unless specified in the Committee’s decision.
<b>DEVELOPMENT CHARGES:</b> That the payment of the Regional Development Charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the Regional Development Charges By-law in effect at the time of payment.  That the payment of the City Development Charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the City’s Development Charges By-law in effect at the time of payment.  That the payment of the Education Development Charge if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the Boards of Education By-laws in effect at the time of payment

IMPORTANT INFORMATION
That the payment of Special Area Development charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and The City's Development Charge By-law in effect at the time of Building permit issuance to the satisfaction of the Reserves/Capital Department.
<b>NOTICE OF DECISION:</b> If you wish to be notified of the decision in respect to this application or a related Ontario Land Tribunal (OLT) hearing you must complete a Request for Decision form and submit to the Secretary Treasurer (ask staff for details). In the absence of a written request to be notified of the Committee's decision you will <b>not</b> receive notice.

**SCHEDULE A: DRAWINGS & PLANS**







**KEY MAP**  
SCALE: N.T.S.  
[Map showing location relative to Hwy 50 and Langstaff Road]

**LEGAL DESCRIPTION**  
PT LOT 11 AND 12,  
CONCESSION 10  
65R31308  
CITY OF VAUGHAN

**PROJECT NORTH**  
[North arrow pointing up]

**TRUE NORTH**  
[North arrow pointing slightly right]

**SITE STATISTICS - PHASE 2**

SITE AREA	80,694.93 m <sup>2</sup>	or	19.94 acres
DEVELOPABLE SITE AREA-PHASE 1	38,925.17 m <sup>2</sup>	or	9.62 acres
DEVELOPABLE SITE AREA-PHASE 2	29,373.57 m <sup>2</sup>	or	7.23 acres
ZONING	EMI ZONE- PRESTIGE EMPLOYMENT AREA		

**SETBACKS**

FRONT YARD (WEST)	4.50 m	28.61 m
REAR YARD (EAST)	12.00 m	20.02 m
INTERIOR SIDE YARD (NORTH)	3.00 m	8.54 m
EXTERIOR SIDE YARD (SOUTH)	4.50 m	25.42 m

**G.F.A.**

BUILDING A	20,908.79 m <sup>2</sup>	or	225,060 sqft
BUILDING B	18,366.39 m <sup>2</sup>	or	197,694 sqft
TOTAL G.F.A.	39,275.18 m <sup>2</sup>	or	422,754 sqft

**SITE COVERAGE (MAX 60%)**

LANDSCAPED AREA	8,377.97 m <sup>2</sup>	or	10.38%
PAVED AREA	33,527.07 m <sup>2</sup>	or	41.55%

**BUILDING HEIGHT (MAX)**

BUILDING A	15.00 m	13.77 m
BUILDING B	12.00 spaces	139 spaces

**PARKING**

TOTAL PARKING	331 spaces	296 spaces
BARRIER FREE PARKING	9 spaces	14 spaces

**LOADING (BUILDING B)**

TYPE A-1	17 spaces
TYPE B-3	27 spaces
TYPE C-1	18 spaces

**BIKE SPACES (BUILDING B)**

TYPE A-1	18 spaces
TYPE B-3	10 spaces
TYPE C-1	10 spaces

**SNOW STORAGE**

TYPE A-1	10 spaces
TYPE B-3	10 spaces
TYPE C-1	10 spaces

**SITE STATISTICS - PHASE 1**

SITE AREA	77,678.02 m <sup>2</sup>	or	19.19 acres
AREA OF DEVELOPMENT	41,234.37 m <sup>2</sup>	or	10.19 acres
ZONING	EM2 ZONE- GENERAL EMPLOYMENT		

**SETBACKS**

FRONT FRONTAGE	34.00 m	+/- 229.90 m
FRONT YARD (WEST)	6.00 m	+/- 203.25 m
REAR YARD (EAST)	12.00 m	26.30 m
INTERIOR SIDE YARD (NORTH)	6.00 m	8.69 m
INTERIOR SIDE YARD (SOUTH)	6.00 m	25.42 m

**G.F.A.**

BUILDING A	20,908.79 m <sup>2</sup>	or	225,060 sqft
TOTAL G.F.A.	20,908.79 m <sup>2</sup>	or	225,060 sqft

**SITE COVERAGE (MAX 60%)**

LANDSCAPED AREA	6,640.49 m <sup>2</sup>	or	16.10%
PAVED AREA	13,085.09 m <sup>2</sup>	or	33.20%

**BUILDING HEIGHT (MAX)**

BUILDING A	15.00 m	13.30 m
BUILDING B	210 spaces	210 spaces

**PARKING**

TOTAL PARKING	210 spaces	210 spaces
BARRIER FREE PARKING	7 spaces	8 spaces

**CARPOOL PARKING**

TYPE A-1	5 spaces
TYPE B-3	5 spaces
TYPE C-1	5 spaces

**BIKE SPACES**

TYPE A-1	4 spaces
TYPE B-3	4 spaces
TYPE C-1	4 spaces

**SNOW STORAGE**

TYPE A-1	4 spaces
TYPE B-3	4 spaces
TYPE C-1	4 spaces

**SYMBOL LEGEND**

MAN DOOR	DRIVE-IN / OVERHEAD DOOR
HYDRANT + VALVE	FIRE DEPARTMENT CONNECTION / SIAMESE
CATCH BASIN	DOUBLE CATCH BASIN
DCB	SANITARY MAN HOLE
SANMH	CATCH BASIN / MAN HOLE
CBMH	STORM MAN HOLE
SMH	HYDRO POLE STANDARD / UTILITY POLE
HP	BIKE RACK
BR	HYDRO TRANSFORMER
HT	ACCESSIBLE PARKING SPACE
APSP	ACCESSIBLE PARKING SPACE SIGNAGE
APSS	SNOW STORAGE AREA
SSA	GARBAGE BIN - CUBIC YARD BIN (1.83m x 1.52m x 2.13m)
GB	CARDBOARD BIN - 8 CUBIC YARD BIN (1.83m x 1.52m x 2.13m)
CB	RECYCLING CART - 360 LITRE (0.60m x 0.88m x 1.1m h)
RC	DESIGNATED CARPOOL SPACE / TRIDE SHARING & ASSOCIATED SIGN

No.	ISSUED	DATE
6	RE-ISSUED FOR SPA NO. 5	OCT. 20, 2023
7	RE-ISSUED FOR SPA NO. 6	DEC. 12, 2023
8	RE-ISSUED FOR SPA NO. 7	JAN. 08, 2024
9	RE-ISSUED FOR SPA NO. 8	MAR. 8, 2024
10	ISSUED FOR CONDITIONAL PERMIT	APRIL 11, 2024

No.	REVISION	DATE
1	REVISED AS NOTED	FEB. 21, 2024
2	REVISED AS NOTED	JUNE 13, 2023
3	REVISED AS NOTED	AUG. 23, 2024
4	REVISED AS NOTED	MAR. 12, 2025

**BALDASSARRA**  
Architects Inc.

30 Great Gulf Drive, Unit 20 | Concord ON | L4K 0K7  
T. 905.660.0722 | [www.baldassarra.ca](http://www.baldassarra.ca)

OWNERS INFORMATION:



**Vaughan Block 64**  
**Highway 50 Investment LP**

Vaughan, ON

**SITE PLAN**  
**PHASE 2**  
CITY FILE NO.: Z.21.054 & DA.21.063

DATE:	LOADING	DRAWN BY:	CHECKED:	SCALE:
OCT. 2021	HP			1:600
PROJECT No.		DRAWING No.		

21-45

A-1.0



SCHEDULE B:

COMMENTS FROM AGENCIES, BUILDING STANDARDS & DEVELOPMENT PLANNING

Department / Agency *Comments Received	Conditions Required		Nature of Comments
Building Standards (Zoning) *See Schedule B	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
Development Planning	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	Recommend Approval w/Conditions

External Agencies *Comments Received	Conditions Required		Nature of Comments *See Schedule B for full comments
Alectra	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
TRCA	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
Region of York	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
Region of Peel	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments

**To:** Committee of Adjustment

**From:** Sarah Scauzillo, Building Standards Department

**Date:** April 24, 2025

**Applicant:** Weston Consulting

**Location:** 30 Labourers Way  
PLAN 65R31303 Part of Lot 12 CONC 10

**File No.(s):** A051/25

**Zoning Classification:**

The subject lands are zoned EM1 and subject to the provisions of Exception 14.1005 and EM2 and subject to the provisions of Exception 14.1005 under Zoning By-law 001-2021, as amended.

#	Zoning By-law 01-2021	Variance requested
1	A minimum of <b>1.0 parking spaces per 100 m<sup>2</sup> of gross floor area</b> is required for a Manufacturing or Processing Facility. [Section 6.3.5, Table 6-2]	To permit a minimum of <b>0.5 parking spaces per 100m<sup>2</sup> of gross floor area</b> for a Manufacturing or Processing Facility.

**Staff Comments:**

General Comments	
1	The applicant shall be advised that additional variances may be required upon review of detailed drawing for building permit/site plan approval.

**Conditions of Approval:**

If the committee finds merit in the application, the following conditions of approval are recommended.

\* Comments are based on the review of documentation supplied with this application.

**To:** Christine Vigneault, Committee of Adjustment Secretary Treasurer

**From:** Nancy Tuckett, Director of Development and Parks Planning

**Date:** May 27, 2025

**Name of Owner:** Alison Kimmell, Highway 50 Nominee Inc.

**Location:** 30 Labourers Way

**File No.(s):** A051/25

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**Proposed Variance(s):**

1. To permit a minimum of **0.5 parking spaces per 100 m<sup>2</sup>** of gross floor area for a Manufacturing or Processing Facility.

**By-Law 001-2021 Requirement(s):**

1. A minimum of **1.0 parking spaces per 100 m<sup>2</sup>** of gross floor area is required for a Manufacturing or Processing Facility.

**Official Plan:**

Vaughan Official Plan 2010 ('VOP 2010'): "Prestige Employment"

**Comments:**

The Owner is seeking relief to permit an increase in Gross Floor Area ("GFA") and change in use to an approved low-rise industrial building with the above noted variance.

The Subject Lands previously obtained site plan approval for a single-storey industrial building with a total GFA of 17,881.10 m<sup>2</sup> to be used for distribution and warehousing as well as accessory office uses. A revised proposal has been brought forward by the Owner to increase the GFA to 18,336.39 m<sup>2</sup> by way of a mezzanine within the approved building envelop. The proposal also changes the primary use of the building from distribution and warehousing to manufacturing or processing.

The change in use and increase in GFA will result in a total minimum parking requirement of 211 spaces, whereas 157 spaces are proposed. Development and Transportation Engineering staff have reviewed the Parking Justification Study provided by NexTrans Consulting Engineers and are of the opinion that the proposed parking configuration remains suitable and sufficient.

Accordingly, the Development and Parks Planning Department supports the requested variances and is of the opinion that the proposal is minor in nature, maintains the general intent and purpose of the Official Plan and Zoning By-law, and is desirable for the appropriate development of the land.

**Recommendation:**

The Development and Parks Planning Department recommends approval of the application.

**Conditions of Approval:**

If the Committee finds merit in the application, the following condition of approval is recommended:

1. That the Owner obtains site plan approval from the Development and Parks Planning Department by way of a note to file DA.21.063 to the satisfaction of Development and Parks Planning staff.

**Comments Prepared by:**

Harry Zhao, Planner

Janany Nagulan, Senior Planner



**Date:** May 1<sup>st</sup> 2025

**Attention:** **Christine Vigneault**

**RE:** Request for Comments

**File No.:**

**Related Files:** **A051-25**

**Applicant:** Weston Consulting

**Location** 30 Labourers Way

**COMMENTS:**

- ☐ We have reviewed the proposed Variance Application and have no comments or objections to its approval.
- ☒ We have reviewed the proposed Variance Application and have no objections to its approval, subject to the following comments (attached below).
- ☐ We have reviewed the proposed Variance Application and have the following concerns (attached below).

Alectra Utilities (formerly PowerStream) has received and reviewed the proposed Variance Application. This review, however, does not imply any approval of the project or plan.

All proposed billboards, signs, and other structures associated with the project or plan must maintain minimum clearances to the existing overhead or underground electrical distribution system as specified by the applicable standards, codes and acts referenced.

In the event that construction commences, and the clearance between any component of the work/structure and the adjacent existing overhead and underground electrical distribution system violates the Occupational Health and Safety Act, the customer will be responsible for 100% of the costs associated with Alectra making the work area safe. All construction work will be required to stop until the safe limits of approach can be established.

In the event construction is completed, and the clearance between the constructed structure and the adjacent existing overhead and underground electrical distribution system violates the any of applicable standards, acts or codes referenced, the customer will be responsible for 100% of Alectra's cost for any relocation work.

**References:**

- Ontario Electrical Safety Code, latest edition (Clearance of Conductors from Buildings)
- Ontario Health and Safety Act, latest edition (Construction Protection)
- Ontario Building Code, latest edition (Clearance to Buildings)
- PowerStream (Construction Standard 03-1, 03-4), attached
- Canadian Standards Association, latest edition (Basic Clearances)

If more information is required, please contact either of the following:

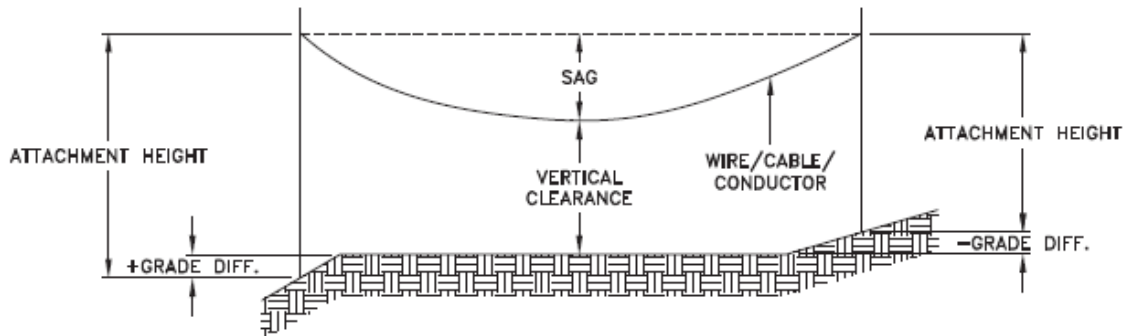
Mr. Stephen Cranley, C.E.T  
Supervisor, Distribution Design, ICI & Layouts (North)  
**Phone:** 1-877-963-6900 ext. 31297

**E-mail:** [stephen.cranley@alectrautilities.com](mailto:stephen.cranley@alectrautilities.com)

Mitchell Penner  
Supervisor, Distribution Design-Subdivisions  
**Phone:** 416-302-6215

**Email:** [Mitchell.Penner@alectrautilities.com](mailto:Mitchell.Penner@alectrautilities.com)

LOCATION OF WIRES, CABLES OR CONDUCTORS	SYSTEM VOLTAGE			
	SPAN GUYS AND COMMUNICATIONS WIRES	UP TO 600V AND NEUTRAL	4.16/2.4kV TO 27.6/16kV (SEE NOTE 1)	44kV
	MINIMUM VERTICAL CLEARANCES (SEE NOTE 2)			
OVER OR ALONGSIDE ROADS, DRIVEWAYS OR LANDS ACCESSIBLE TO <u>VEHICLES</u>	442cm	442cm	480cm	520cm
OVER GROUND ACCESSIBLE TO <u>PEDESTRIANS AND BICYCLES ONLY</u>	250cm	310cm	340cm	370cm
ABOVE TOP OF RAIL AT <u>RAILWAY CROSSINGS</u>	730cm	730cm	760cm	810cm



MINIMUM ATTACHMENT HEIGHT = MAXIMUM SAG  
 + MINIMUM VERTICAL CLEARANCE (FROM ABOVE TABLE)  
 + GRADE DIFFERENCE  
 + 0.3m (VEHICLE OR RAILWAY LOCATION)  
 + SNOW DEPTH (PEDESTRIAN LOCATION, SEE NOTE 3)

**NOTES:**

1. THE MULTIGROUNDED SYSTEM NEUTRAL HAS THE SAME CLEARANCE AS THE 600V SYSTEM.
2. THE VERTICAL CLEARANCES IN THE ABOVE TABLE ARE UNDER MAXIMUM SAG CONDITIONS.
3. REFER TO CSA STANDARD C22.3 No.1, ANNEX D FOR LOCAL SNOW DEPTH VALUES.
4. ALL CLEARANCES ARE IN ACCORDANCE TO CSA STANDARD C22.3.

CONVERSION TABLE	
METRIC	IMPERIAL (APPROX)
810cm	27'-0"
760cm	25'-4"
730cm	24'-4"
520cm	17'-4"
480cm	16'-0"
442cm	15'-5"
370cm	12'-4"
340cm	11'-4"
310cm	10'-4"
250cm	8'-4"

**REFERENCES**

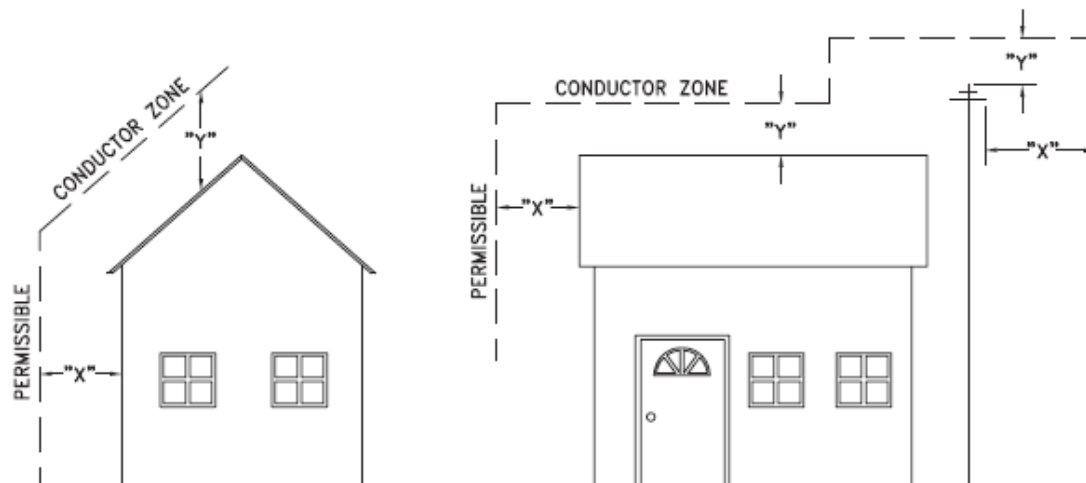
SAGS AND TENSIONS | SECTION 02

**MINIMUM VERTICAL CLEARANCES OF WIRES, CABLES AND CONDUCTORS ABOVE GROUND OR RAILS**

ORIGINAL ISSUE DATE: 2010-DEC-24 REVISION NO: R1 REVISION DATE: 2012-JAN-09

Certificate of Approval	
This construction Standard meets the safety requirements of Section 4 of Regulation 22/04	
Joe Crozier, P.Eng.	2012-JAN-09
Name	Date
P.Eng. Approval By:	Joe Crozier





VOLTAGE	MINIMUM HORIZONTAL CLEARANCE UNDER MAXIMUM SWING CONDITIONS DIMENSION "X" (SEE NOTES 1, 3 & 4)	MINIMUM VERTICAL CLEARANCE UNDER MAXIMUM DESIGN SAG CONDITIONS DIMENSION "Y" (SEE NOTES 1, 2, 4 & 5)
0-600V AND NEUTRAL	100cm	250cm
4.16/2.4 TO 44kV	300cm	480cm

### NOTES

1. UNDER NO CIRCUMSTANCES SHALL A CONDUCTOR BE PERMITTED TO PENETRATE THE ENVELOPE SHOWN BY THE DOTTED LINE.
2. THE VERTICAL CLEARANCES ARE UNDER CONDITIONS OF MAXIMUM DESIGN SAG.
3. THE HORIZONTAL CLEARANCES ARE UNDER CONDITIONS OF MAXIMUM SWING. WHERE THE CONDUCTOR SWING IS NOT KNOWN A HORIZONTAL CLEARANCE OF 480CM SHALL BE USED.
4. BUILDINGS THAT EXCEED 3 STOREYS OR 15M IN HEIGHT, THE MINIMUM HORIZONTAL CLEARANCE OF THE SECONDARY CONDUCTORS SHOULD BE INCREASED TO 300cm WHERE IT IS NECESSARY TO ALLOW FOR THE RAISING OF LADDERS BY LOCAL FIRE DEPARTMENTS.
5. IN SITUATIONS SUCH AS MULTI-LEVEL GARAGES, WHERE ROOFS ARE NORMALLY USED BY PERSONS AND VEHICLES, THE VERTICAL CLEARANCES OF POWERSTREAM STANDARD 03-1 SHALL APPLY.
6. DISTRIBUTION LINES CONSTRUCTED NEAR BUILDINGS SHALL BE BUILT TO AVOID OVERHANG WHEREVER POSSIBLE. WHERE LINES MUST BE CONSTRUCTED OVER OR ADJACENT TO BUILDINGS THE APPLICABLE HORIZONTAL AND VERTICAL CLEARANCES SHALL BE AT CONDITIONS OF MAXIMUM CONDUCTOR SWING AND MAXIMUM SAG. THE ABOVE CLEARANCES ARE DESIGNED TO PREVENT PERSONS ON OR IN BUILDINGS AS WELL AS EXTERNAL MACHINERY USED IN CONJUNCTION WITH A BUILDING TO COME IN CONTACT WITH CONDUCTORS. EFFORTS SHOULD BE MADE TO INCREASE THESE CLEARANCES WHERE POSSIBLE.
7. ALL CLEARANCES ARE IN ACCORDANCE TO CSA C22.3 NO.1-06 (TABLE-9).

CONVERSION TABLE	
METRIC	IMPERIAL (APPROX)
480cm	16'-0"
300cm	10'-0"
250cm	8'-4"
100cm	3'-4"

### MINIMUM VERTICAL & HORIZONTAL CLEARANCES OF CONDUCTORS FROM BUILDINGS OR OTHER PERMANENT STRUCTURES (CONDUCTORS NOT ATTACHED TO BUILDINGS)

ORIGINAL ISSUE DATE: 2010-MAY-05 REVISION NO: REVISION DATE:

PS:\System Planning and Standards\Standard Design\PowerStream Standards\PowerStream Standards working folder\Section 3\3-4\DWG 03-4 R0 May 5, 2010.dwg, 5/5/2010 8:22:02 AM, Adobe PDF

**Certificate of Approval**  
This construction Standard meets the safety requirements of Section 4 of Regulation 22/04  
Debbie Dadwani, P.Eng. 2010-MAY-05  
Name Date  
P.Eng. Approval By: D. Dadwani

**From:** [Cameron McDonald](#)  
**To:** [Committee of Adjustment Mailbox](#)  
**Subject:** [External] RE: A051/25 - 30 LABOURERS WAY - REQUEST FOR COMMENTS, CITY OF VAUGHAN  
**Date:** Wednesday, April 30, 2025 11:23:32 AM

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**CAUTION!** This is an external email. Verify the sender's email address and carefully examine any links or attachments before clicking. If you believe this may be a phishing email, please use the Phish Alert Button.

Hello,

Based on a review of our screening mapping, I can confirm that the subject property is not located within TRCA's Regulated Area. As such, any site alteration or development on the property would not require a permit from the TRCA.

Based on the above, we have no comments/requirements.

Regards,

**Cameron McDonald**

Planner I

Development Planning and Permits | Development and Engineering Services

T: [\(437\) 880-1925](tel:(437)880-1925)

E: [cameron.mcdonald@trca.ca](mailto:cameron.mcdonald@trca.ca)

A: [5 Shoreham Drive, Toronto, ON, M3N 1S4](#) | [trca.ca](http://trca.ca)



**From:** [Development Services](#)  
**To:** [Committee of Adjustment Mailbox](#)  
**Subject:** [External] RE: A051/25 - 30 LABOURERS WAY - REQUEST FOR COMMENTS, CITY OF VAUGHAN  
**Date:** Wednesday, April 30, 2025 9:28:22 AM

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**CAUTION!** This is an external email. Verify the sender's email address and carefully examine any links or attachments before clicking. If you believe this may be a phishing email, please use the Phish Alert Button.

Good morning,

The Regional Municipality of York has completed its review of the above minor variance and has no comment.

Regards,

*Gabrielle*

**Gabrielle Hurst MCIP, RPP** | Associate Planner, Development Planning, Economic and Development Services Branch | The Regional Municipality of York | 1-877 464 9675 ext 71538 | [gabrielle.hurst@york.ca](mailto:gabrielle.hurst@york.ca) | [www.york.ca](http://www.york.ca)



<b>SCHEDULE C: PUBLIC &amp; APPLICANT CORRESPONDENCE</b>
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Correspondence Type	Name	Address	Date Received (mm/dd/yyyy)	Summary
Applicant			04/23/2025	Application Cover Letter
Applicant			05/02/2025	Parking Study

Development Planning Department  
City of Vaughan  
2141 Major Mackenzie Drive  
Vaughan, Ontario L6A 1T1

April 21, 2025  
File No. 10237

**Attn: Committee of Adjustment**  
**Christine Vigneault, Manager & Secretary Treasurer**

**RE: Minor Variance Application**  
**Part of Lot 11 and 12, Concession 10 (Highway 50)**  
**Related Files (DA.21.063, DA.21.028, and Z.21.029)**

Weston Consulting is the planning representative for Highway 50 Nominee Inc., the registered owner of the lands legally known as Part 1 of Lot 11 and 12, Concession 10 in the City of Vaughan (the 'subject lands' or the 'site'). This letter has been prepared in support of a Minor Variance application to request a reduced parking ratio for the approved and built industrial warehouse building identified as Building B on the enclosed Site Plan.

## Context

The site is known as Block H within the Block 64 South Block Plan, located north of Langstaff Road and south of Trade Valley Drive, and is currently occupied by two industrial warehouse buildings. It is approximately 8.09 hectares (69,775.32 square metres) in size with approximately 118 metres of frontage along Highway 50. The site is occupied by two industrial warehouse buildings: Building A, located along Hunter's Valley Road, and Building B, located along Highway 50.

The site is designated *Prestige Employment* and *General Employment* according to Schedule 13 (Land Use) of the Vaughan Official Plan (VOP). The City of Vaughan is currently developing a New Vaughn Official Plan 2025, which proposes to designate the site *Prestige Employment* and *General Employment* according to Schedule 13 (Land Use). According to Zoning By-law 001-2021, Building A is zoned *EM2 (General Employment Area)*, and Building B is zoned *EM1 (Prestige Employment Area)*.

The Provincial Planning Statement (PPS) defines settlement areas as urban areas within municipalities that are built-up and where development is concentrated through a mix of land uses. Policy 2.3.1 of the PPS states that settlement areas shall be the focus of growth and development. The subject lands are located within a settlement area and are considered an appropriate location for development. Section 2.8.2 of the PPS also provides policy direction to protect and preserve employment areas for current and future uses and ensure that the necessary infrastructure is provided to support current and projected needs.

## Background

Development of the subject lands occurred in two phases. Phase 1 consisted of a Site Development Application for Building A (DA.21.028) to permit the development of a single-storey *Warehousing and Distribution Facility* located on the eastern portion of the subject lands. Building A was approved in August 2023 and consisted of 20,908.79 square metres of Gross Floor Area (GFA) and 210 parking spaces. In October 2023, a Minor Variance application (A150/23) was approved by the Committee of Adjustment permit a reduced parking rate of 0.5 spaces/100m<sup>2</sup> of GFA for the *Warehousing and Distribution Facility* use and a reduced parking ratio of 3.0 spaces/100m<sup>2</sup> of GFA for the *Accessory Office* use. The variance sought relief from Zoning By-law 1-88 to implement the standards established in Zoning By-law 1-21 for Building A.

Phase 2 consisted of a Site Development Application for Building B (DA.21.063) to permit the development of a single-storey *Warehousing and Distribution Facility* located on the western portion of the site. Building B was approved in July 2024 and consisted of 17,881.10 square metres of GFA and 157 parking spaces.

Building A and Building B were designed and approved without an end use identified. The owner now intends to lease Building B to a tenant that does not meet the definition of *Warehousing and Distribution Facility* under Zoning By-law 1-21. The future tenant is a window manufacturing company, which meets the definition of *Manufacturing or Processing Facility*. The tenant has requested minor design changes to the building to accommodate their operations, specifically the construction of an office mezzanine. The mezzanine will increase the overall GFA of the building. The *Manufacturing or Processing Facility* GFA will increase from 16,987.05 square metres to 17,001.93 square metres and the office GFA component will increase from 894.05 square metres to 1,364.46 square metres. We are currently working with City Staff to appropriately amend the existing Site Plan Agreement and associated drawings for Building B to address the minor design modifications. These changes are limited to the inside of Building B and will not change the building footprint.

As a result of the technical change in use from *Warehousing and Distribution Facility* to *Manufacturing or Processing Facility*, a Minor Variance is being requested to permit a reduced parking ratio of 0.5 parking spaces per 100m<sup>2</sup> of GFA. The requested variance will maintain the parking ratio that was originally approved for Building B and reflects the existing parking conditions of the site.

Table 1 – Site Statistics of Building A and Building B

Site Statistics			
Building	Use	GFA (m <sup>2</sup> )	Approved Parking Spaces
A	Office	479.04	139
	Warehouse	20,429.75	
	<i>Total Building A</i>	<i>20,908.79</i>	
B	Office	1,364.46	157
	Warehouse	17,001.93	
	<i>Total Building B</i>	<i>18,366.39</i>	
<b>Total GFA</b>		<b>39,275.18</b>	<b>296</b>

## Purpose of Application

The enclosed Minor Variance application seeks relief from Zoning By-law 1-21 to permit a reduced parking rate for Building B. The Site Development Application for Building B was approved based on the definition of *Warehousing and Distribution Facility*:

*“Warehousing and Distribution Facility means a building or part of a building used for the indoor storage of goods, wares, merchandise, materials or articles as a principal use, but does not include a retail store.”*

The required parking rate for a *Warehousing and Distribution Facility* is 0.5 spaces per 100 m<sup>2</sup> of GFA. The Site Development Application for Building B was approved with this rate and Building B currently provides 157 parking spaces. However, due to the technical change in use from *Warehousing and Distribution Facility* to *Manufacturing or Processing Facility*, a variance is required to permit a reduced parking rate.

The future tenant of Building B will be a window production company, which meets the definition of a *Manufacturing or Processing Facility*:

*“Manufacturing or Processing Facility means a building part of a building for the manufacturing, assembly, processing altering, treating, repair of products, materials or goods.”*

The required parking rate for a *Manufacturing or Processing Facility* is 1 space per 100 m<sup>2</sup> of GFA. A Minor Variance is being requested permit a reduced parking rate for Building B, which would maintain the rate of 0.5 spaces per 100 m<sup>2</sup> of GFA that was originally approved. The intent is to maintain the number of parking spaces provided despite the technical change in use from *Warehousing and Distribution Facility* to *Manufacturing or Processing Facility*. It should be noted that Building B is constructed and the parking spaces originally approved exist on the site.

Due to the technical change in use, Building B is required to provide 211 parking spaces at a rate of 1 space per 100m<sup>2</sup> for the *Manufacturing or Processing Facility* component. This results in a deficiency of 54 spaces. However, as shown in Table 2, the parking requirement for Building B is satisfied when the originally approved parking ratio of 0.5 spaces per 100 m<sup>2</sup> is maintained despite the increased GFA.

Table 2 – Parking Requirement for Building B

Building B (Warehousing and Distribution Facility)			
Use	Spaces Required	Spaces Provided	Variance
Office 3 spaces/100 m <sup>2</sup>	41	157	+31
Warehousing and Distribution Facility 0.5 spaces/100 m <sup>2</sup>	85		
Sub Total	126		
<b>Total for Building A + B</b>	<b>245</b>	<b>296</b>	<b>+51</b>

As shown in in Table 3, the parking requirement for Building B is deficient by 54 spaces when the *Manufacturing or Processing Facility* rate is applied.

Table 3 – Parking Requirement for Building B

Building B (Manufacturing or Processing Facility)			
Use	Spaces Required	Spaces Provided	Variance
Office 3 spaces/100 m <sup>2</sup>	41	157	-54
Manufacturing or Processing Facility 1 space/100 m <sup>2</sup>	170		
Sub Total	211		
<b>Total for Building A + B</b>	<b>330</b>	<b>296</b>	<b>-35</b>

It should be noted Building A is over supplied by 19 spaces as shown in Table 4. Given the overall site operates as one, the parking deficiency for Building B is reduced to 35 spaces. A Parking Justification Study is currently underway and will be submitted in support of the application once completed.

Table 4 – Parking Requirements for Building A

Parking Requirements			
Use	Spaces Required	Spaces Provided	Variance
<b>Building A</b>			
Office 3 spaces/100 m <sup>2</sup>	17	139	+19
Warehousing and Distribution Facility 0.5 spaces/100 m <sup>2</sup>	102		
<i>Sub Total</i>	120		

## Policy Analysis and Planning Justification

The Planning Act, 1990 sets out the legal framework that governs the consideration for Minor Variance applications and Section 45 of the Planning Act authorizes the Committee of Adjustment to make decisions regarding minor variances applications. Pursuant to Section 45(1) of the Planning Act, a Minor Variance must satisfy the following tests:

- The requested variances maintain the general intent and purpose of the Official Plan;
- The requested variances maintain the general intent and purpose of the Zoning By-law;
- The requested variances are minor in nature; and,
- The variances are desirable for the appropriate development or use of the lands.

The following section provides the planning rationale that the proposed development and associated requested variance satisfies each test under Section 45 (1) of the *Planning Act*.

### 1. Maintains the General Intent and Purpose of the Official Plan

The subject lands are designated *Prestige Employment* and *General Employment* according to the VOP. The intent of employment areas is to protect lands for employment uses and ensure the long-term viability and economic well-being of the city. Permitted uses within the *Prestige Employment* and *General Employment* designations include a full range of industrial uses. This includes warehousing, manufacturing, and accessory office. Although the future tenant for Building B represents a technical change in use from *Warehousing and Distribution Facility* to *Manufacturing or Processing Facility*, the use is still permitted. In our opinion, the variance maintains the general intent and purpose of the Official Plan.

### 2. Maintains the General Intent and Purpose of the Zoning By-Law

The subject lands are split-zoned within the Employment Zones category of Zoning By-law 1-21. Building A is zoned *EM2 (General Employment Area)*, and Building B is zoned *EM1 (Prestige Employment Area)*. The intent of the Employment Area Zones is to provide for a wide range of employment uses, consistent with the VOP, including a *Manufacturing or Processing Facility*, *Warehousing and Distribution Facility*, and office uses. The future use of Building B as a window manufacturing facility is permitted within the EM1 zone.



The Zoning By-law sets out minimum parking requirements to ensure there is a sufficient parking supply to meet the anticipated demands of various permitted uses. At the time Building B was approved, the end user was not known and the parking ratio was based on a *Warehousing and Distribution Facility* use, which requires 0.5 spaces/100 m<sup>2</sup> of GFA. The owner has identified a tenant for Building B, however the tenant does not meet the definition of *Warehousing and Distribution Facility*. Although the use is permitted, the tenant meets the definition of a *Manufacturing or Processing Facility*. The Zoning By-law establishes a parking rate of 1 space/100 m<sup>2</sup> of GFA for this use. The Minor Variance application seeks relief from this parking requirement to permit a reduction of 0.5 spaces/100 m<sup>2</sup>, which is the rate originally approved. The tenant of Building B is aware of the provided parking spaces and has confirmed it is sufficient for their operations. Furthermore, although the tenant will be defined as a *Manufacturing or Processing Facility* under the Zoning By-law, it is our understanding that their operation will maintain a warehousing component and will not reflect a traditional manufacturing facility. It should also be noted that the 157 parking spaces originally approved have been constructed.

The intent of the Zoning By-law to provide sufficient parking to meet the demand of the permitted use is maintained. It is our opinion that the requested variance meets the general intent and purpose of the Zoning By-law.

### 3. Desirable for the Appropriate Use of the Land

The desirability and appropriateness of the requested variance can be addressed by determining the compatibility of the proposed development within the existing site context, the VOP and the Zoning By-law. The site is located within an Employment Area and is zoned General Employment EM2. The site is a logical and appropriate place for employment type uses given the surrounding context and the tenant's use is permitted within the VOP and Zoning By-law. Building B was originally approved based on a rate of 0.5 parking spaces/100m<sup>2</sup>. The request for a reduction in parking will maintain the approved rate and is due to a technical change in use under the Zoning By-law. The 157 parking spaces originally approved are constructed and the variance will enable the continued operation of Building B for employment use. It is our understanding that the number of parking spaces is sufficient to meet their anticipated parking demand. In our opinion, the requested variance is desirable and appropriate for the use of the subject lands.

### 4. Minor in Nature

The requested variance is required to address a technical change in use of the approved Building B to meet the future tenant's needs. The reduced parking is technical in nature, will maintain the originally approved parking rate, and is required allow Building B to be leased as the 157 parking spaces are constructed. The surplus of parking for Building A contributes to a smaller deficiency of parking spaces for Building B. The requested parking rate for Building B results in an appropriate supply of parking and is not anticipated to adversely impact the surrounding area. In our opinion, the requested variance for the reduced parking is minor in nature and will not negatively impact the surrounding area.

### Conclusion

Based on the above analysis, it is our opinion that the requested variance satisfies the four tests for a minor variance approval pursuant to Section 45 (1) of the *Planning Act*. The reduction in parking is required to address the needs of a new tenant for Building B and is desirable and appropriate for the development of the subject lands, and minor in nature.

It is our opinion that the requested variance has merit, represents good planning and should be considered for approval by the Committee of Adjustment. In support of the enclosed Minor Variance application, we are pleased to submit the following materials in digital format:

1. Requested Variances Form;
2. Buildings and Structure Setbacks Form;
3. Buildings and Structure Dimensions Form;
4. Authorization Form;
5. Authorizing Statements Form;
6. Sworn Declaration Form;
7. Tree Declaration Form; and,
8. Site Plan Phase 2 prepared by Baldassarra Architects Inc. dated March 12, 2025.

As noted above, a Parking Justification Study will be submitted under separate cover. Payment of the Minor Variance application fee will be provided under separate cover.

We kindly request receipt of the enclosed application. We request that the application be circulated as soon as possible so that the Committee of Adjustment may consider this application at its earliest available hearing date. Should you have any questions or require additional materials, please contact the undersigned at ext. 329 or Sarah Burjaw at ext. 374.

Yours truly,

**Weston Consulting**

Per:



Darrin Cohen MCIP, RPP  
Senior Planner



Sarah Burjaw, BA, MPL, MCIP, RPP  
Planner

Copy: Highway 50 Nominee Inc.  
Casandra Krysko, City of Vaughan

April 25, 2025

Highway 50 Nominee Inc.

**Re: Parking Study  
Proposed Industrial Development  
Block 64 – Parcel H – Phase 2  
Our Project No. NT-21-028**

---

NexTrans Consulting Engineers (a Division of NextEng Consulting Group Inc.) was retained by Highway 50 Investment LP. (the 'Client') to undertake a Parking Justification Study in support of a Minor Variance application to request a reduced parking ratio for the approved and built industrial warehouse building identified as Building B in the site plan.

The subject property is known as Block H within the Block 64 South Block Plan, located north of Langstaff Road and south of Trade Valley Drive, and is currently occupied by two industrial warehouse buildings: Building A is located along Hunter's Valley Road, and Building B is located along Highway 50. Building A and Building B were designed and permitted for Warehousing and Distribution Facility. The owner now intends to lease Building B to a tenant that does not meet the definition of Warehousing and Distribution Facility under the Zoning Bylaw 01-2021. The future tenant is a window manufacturing company, which meets the definition of a Manufacturing or Processing Facility. The future tenant is currently located at 3350 Langstaff Road.

## **1.0 PURPOSE OF THIS STUDY**

The purpose of this Parking Justification Study is to use the following factors to confirm that there is sufficient capacity to accommodate the new tenant based on the existing number of parking spaces. These factors are:

- Parking utilization study results for proxy sites with similar characteristics
- Existing land uses in the area
- Recommended mitigation measures, such as Transportation Management to support non-auto modes of transportation.

## **2.0 DEVELOPMENT OVERVIEW**

### **2.1. Proposed Development**

Building B has been constructed and is intended to be used as a single industrial/manufacturing (including office use) building with a gross floor area of approximately 18,366.39 m<sup>2</sup> and a total of 157 parking spaces. Building A on the site is already fully built, has a total of 139 parking spaces, and is occupied by a warehouse facility. **Figure 1** illustrates the subject site location. **Figure 2** illustrates the site plan.

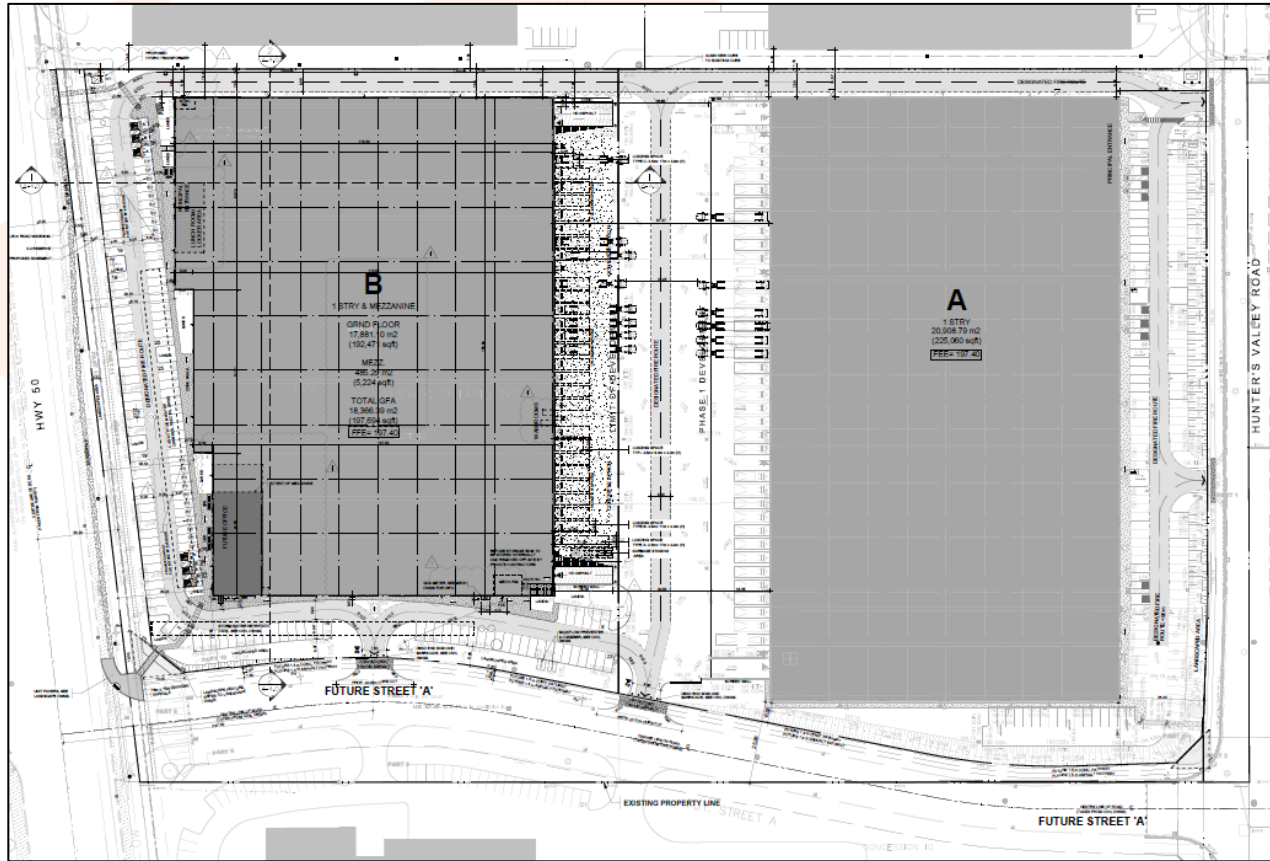


Figure 1 – Proposed Development Location



Source: Google Maps

**Figure 2 – Proposed Development Site Plan**



## 2.2. Zoning By-law Vehicle Parking Requirement

The City of Vaughan Comprehensive Zoning By-Law 01-2021 was created to implement the City's Official Plan. The parking requirement and supply for the Building B are summarized in **Table 1**.

**Table 1 – City of Vaughan Comprehensive Zoning By-Law Vehicle Parking Requirement**

Type	GFA (m²)	Parking Rates (Industrial Use)	Parking Requirement	Parking Provided	Different
Manufacturing or Processing facility	17,001.93 m²	1 space per 100 m²	170 spaces	157 spaces	54 spaces
Office Use	1,364.46 m²	3 spaces per 100 m²	41 spaces		

Based on the City Comprehensive Zoning By-Law, a total of 211 parking spaces are required for the proposed development. It is our understanding that the proposed development provides 157 vehicle parking spaces, which presents a technical shortfall of 54 parking spaces or 25.6% reduction.

## 2.3. Recommended Parking Rate for The Proposed Development

The following is the recommended parking rate (Table 2) for the proposed development, based on the parking justification provided in subsequent sections of this Study.



**Table 2 – Recommended Vehicle Parking Rates for the Proposed Development**

Type	GFA (m <sup>2</sup> )	Parking Rate	Parking Provided
Manufacturing or Processing Facility, including Office Use	18,366.39	0.85 spaces per 100m <sup>2</sup>	157 spaces

Based on the recommended vehicle parking rate of 0.85 spaces per 100 m<sup>2</sup>, the proposed development will provide a total of 157 parking spaces.

### 3.0 Vehicle Parking Justification Based Surveys

Parking surveys were conducted at Building A to determine if there is surplus parking available for future use. Additionally, a parking survey was carried out for the future tenant of Building B, who is currently operating at 3350 Langstaff Road, to establish the actual parking demand rate associated with their operations.

#### 3.1. Vehicle Parking Utilization Survey Results

The parking surveys were conducted over two typical business days, covering 12 hours each day.

Building A – 350 Hunter Valley Road has a GFA of 20908.79 m<sup>2</sup> and a total of 139 parking spaces.

- Tuesday 15<sup>th</sup>, 2025: from 6:30 am to 6:30 pm
- Wednesday 16<sup>th</sup>, 2025: from 6:30 am to 6:30 pm

Future tenant, which is currently located at 3350 Langstaff Road, has a GFA of 8361.27 m<sup>2</sup>, a total of 96 parking spaces, and 140 employees.

- Tuesday 15<sup>th</sup>, 2025: from 6:30 am to 6:30 pm
- Wednesday 16<sup>th</sup>, 2025: from 6:30 am to 6:30 pm

For analysis purposes, NexTrans has utilized two main indicators to analyze the survey results:

- Parking utilization percentage – is the percentage calculated based on the total number of parked vehicles divided by the total number of available spaces.
- Parking utilization rates – is the ratio calculated based on the total number of parked vehicles divided by the total GFA.
- Parking utilization rates per employee—this is the ratio calculated based on the parking demand divided by the total number of staff on site; it will determine the parking demand for future employees.

The survey results and detailed calculations are included in **Appendix A**.

#### 3.2. Parking Utilization Percentage

For this assessment, three percentage categories have been established: average, lowest, and highest utilization percentage. **Table 3** summarizes the survey results.

**Table 3 – Parking Utilization Percentage**

Site	Category	Date	
		Tuesday, April 15 <sup>th</sup> , 2025	Wednesday, April 16 <sup>th</sup> , 2025
350 Hunter Valley Drive	Average	9%	10%
	Lowest	1%	1%
	Highest	13%	14%
3350 Langstaff Road	Average	48%	50%
	Lowest	11%	16%
	Highest	65%	64%

Based on the assessment noted above, the average parking demand percentage ranges are the following:

- 350 Hunter Valley Drive: the lowest demand is 1%, the highest demand is 13%, and the average demand is 9%.
- 3350 Langstaff Road: the lowest demand is 10%, the highest demand is 55%, and the average demand is 37%.

### 3.3. Parking Utilization Rates per 100m<sup>2</sup>

For this assessment, three percentage categories have been established: average, lowest, and highest utilization rate per 100 m<sup>2</sup>. **Table 4** summarizes the survey results.

**Table 4 – Parking Utilization Rates**

Site	Category	Date	
		Tuesday, April 15 <sup>th</sup> , 2025	Wednesday, April 16 <sup>th</sup> , 2025
350 Hunter Valley Drive	Average	0.06 spaces per 100 m <sup>2</sup>	0.06 spaces per 100 m <sup>2</sup>
	Lowest	0.01 spaces per 100 m <sup>2</sup>	0.01 spaces per 100 m <sup>2</sup>
	Highest	0.09 spaces per 100 m <sup>2</sup>	0.09 spaces per 100 m <sup>2</sup>
3350 Langstaff Road	Average	0.56 spaces per 100 m <sup>2</sup>	0.57 spaces per 100 m <sup>2</sup>
	Lowest	0.13 spaces per 100 m <sup>2</sup>	0.18 spaces per 100 m <sup>2</sup>
	Highest	0.74 spaces per 100 m <sup>2</sup>	0.73 spaces per 100 m <sup>2</sup>

Based on the assessment summarized above, the observed average parking demand percentages for the surveyed sites are as follows:

- **350 Hunter Valley Drive:** Parking demand ranged from a low of 0.01 spaces per 100 m<sup>2</sup> to a high of 0.09 spaces per 100 m<sup>2</sup>, with an average demand of 0.06 spaces per 100 m<sup>2</sup>.
- **3350 Langstaff Road:** Parking demand ranged from a low of 0.01 spaces per 100 m<sup>2</sup> to a high of 0.09 spaces per 100 m<sup>2</sup>, with an average demand of 0.06 spaces per 100 m<sup>2</sup>.

These results demonstrate that there is surplus parking (more than 54 spaces) available at 350 Hunter Valley Drive, which can be utilized if Building B requires additional spaces in the future. Furthermore, the survey confirms that the future tenant, who currently operates at 3350 Langstaff Road, will have sufficient parking to meet their projected demand at the proposed site. Overall, these findings support and justify the proposed 25.6% reduction or the parking rate of 0.85 spaces per 100 m<sup>2</sup> for the subject development.

### 3.4. Parking Utilization Rates per Employee

For this assessment, the parking rate per employee has been calculated based on the highest number of parking demand divided by the number of existing employees. **Table 4** summarizes the survey results.

Table 4 – Parking Utilization Rates

Site	GFA	Number of Employees	Number of Parking Spaces	Parking Rate per employee
Existing Facility- 3350 Langstaff	8361.27 m <sup>2</sup>	140 employees	96 spaces	0.44 spaces per employee

Upon relocation to Building B, the future tenant's workforce may increase to up to 190 employees. Based on the parking survey findings, a total of 84 parking spaces (190 employees x 0.44 spaces per employee) would be required to accommodate this projected staff level. The proposed development will provide 157 parking spaces, which is sufficient to meet the anticipated parking demand of the future tenant and provides additional capacity for flexibility.

### 3.5. Transportation Demand Management

Transportation Demand Management (TDM) is a coordinated series of actions aimed at maximizing the people-moving capability of the transportation system. It is intended to help reduce single-occupant auto use. Potential TDM measures may include but are not limited to: TDM-supportive land use, bicycle and pedestrian programs and facilities, public transit improvements, preferential treatments for buses and high occupancy vehicles (if applicable), ridesharing, and employee incentives.

- Reduce parking rates to reduce the number of single-occupant vehicle trips to and from the proposed development.
- Provide bicycle parking spaces at convenient and secure locations.
- Encourage employees to take transit, carpool, or other modes of transportation, such as walking and cycling, to and from work. The owner can also consider providing incentives for employees, such as subsidized monthly transit passes.

### 4.0 Conclusion

Based on the comprehensive justifications provided above, it is concluded that a reduction to the proposed parking rate is justified, desirable. **The proxy parking utilization survey indicates that 0.85 spaces per 100 m<sup>2</sup> for manufacturing (including office use) development is reasonable and justified.**

We trust the enclosed sufficiently addresses your needs. Should you have any questions, please do not hesitate to contact the undersigned.

Yours truly,

#### Nextrans Consulting Engineers

A Division of NextEng Consulting Group Inc.

Prepared by:



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Transportation Analyst

Approved by:



Richard Pernicky, MITE  
Principal



## **Appendix A – Parking Survey Result**

# Parking Survey

350 Hunter Valley Drive

GFA

20908.79

Total Parking

139

Time	Parking Demand April 15	Parking Rate	Parking %	Parking Demand April 16	Parking Rate	Parking %
6:30:00 AM	2	0.01	1%	2	0.01	1%
7:00:00 AM	6	0.03	4%	7	0.03	5%
7:30:00 AM	6	0.03	4%	7	0.03	5%
8:00:00 AM	5	0.02	4%	7	0.03	5%
8:30:00 AM	7	0.03	5%	7	0.03	5%
9:00:00 AM	17	0.08	12%	18	0.09	13%
9:30:00 AM	17	0.08	12%	19	0.09	14%
10:00:00 AM	18	0.09	13%	19	0.09	14%
10:30:00 AM	18	0.09	13%	18	0.09	13%
11:00:00 AM	18	0.09	13%	17	0.08	12%
11:30:00 AM	17	0.08	12%	17	0.08	12%
12:00:00 PM	15	0.07	11%	16	0.08	12%
12:30:00 PM	17	0.08	12%	17	0.08	12%
1:00:00 PM	17	0.08	12%	17	0.08	12%
1:30:00 PM	17	0.08	12%	17	0.08	12%
2:00:00 PM	17	0.08	12%	17	0.08	12%
2:30:00 PM	18	0.09	13%	17	0.08	12%
3:00:00 PM	17	0.08	12%	16	0.08	12%
3:30:00 PM	17	0.08	12%	16	0.08	12%
4:00:00 PM	17	0.08	12%	16	0.08	12%
4:30:00 PM	17	0.08	12%	16	0.08	12%
5:00:00 PM	7	0.03	5%	11	0.05	8%
5:30:00 PM	7	0.03	5%	10	0.05	7%
6:00:00 PM	4	0.02	3%	6	0.03	4%
6:30:00 PM	2	0.01	1%	3	0.01	2%
average	13	0.06	9%	13	0.06	10%



Parking Survey

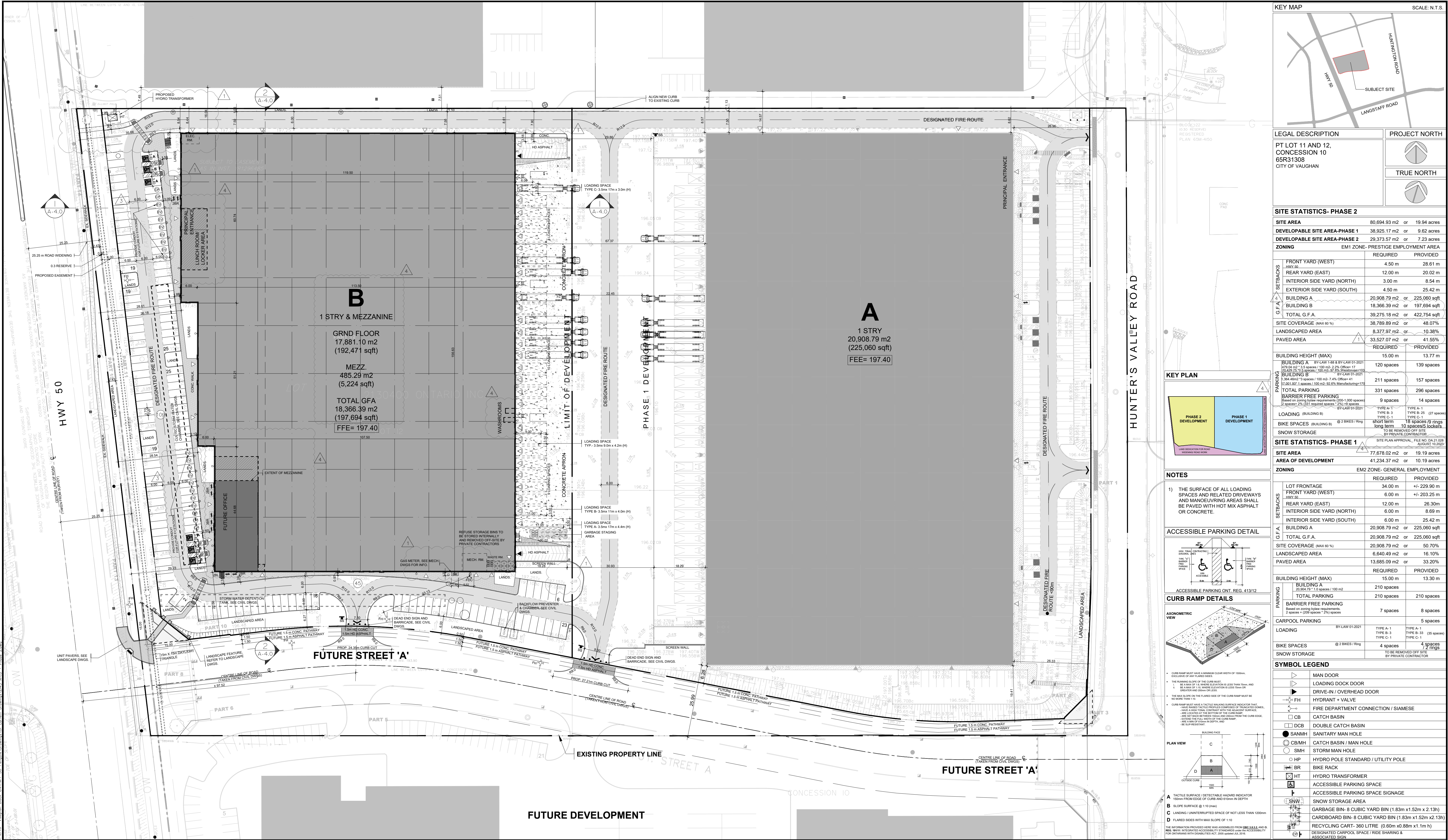
3350 Langstaff Road  
GFA 8361.27  
Total Parking 96  
total employee 140

Time	Parking Demand April 15	Parking Rate	Parking %	Rate per employee	Parking Demand April 16	Parking Rate	Parking %
6:30:00 AM	11	0.13	11%	0.08	15	0.18	16%
7:00:00 AM	46	0.55	48%	0.33	42	0.50	44%
7:30:00 AM	53	0.63	55%	0.38	55	0.66	57%
8:00:00 AM	59	0.71	61%	0.42	59	0.71	61%
8:30:00 AM	61	0.73	64%	0.44	60	0.72	63%
9:00:00 AM	61	0.73	64%	0.44	61	0.73	64%
9:30:00 AM	60	0.72	63%	0.43	58	0.69	60%
10:00:00 AM	62	0.74	65%	0.44	60	0.72	63%
10:30:00 AM	60	0.72	63%	0.43	61	0.73	64%
11:00:00 AM	57	0.68	59%	0.41	60	0.72	63%
11:30:00 AM	53	0.63	55%	0.38	56	0.67	58%
12:00:00 PM	45	0.54	47%	0.32	48	0.57	50%
12:30:00 PM	48	0.57	50%	0.34	50	0.60	52%
1:00:00 PM	48	0.57	50%	0.34	53	0.63	55%
1:30:00 PM	20	0.24	21%	0.14	53	0.63	55%
2:00:00 PM	50	0.60	52%	0.36	52	0.62	54%
2:30:00 PM	46	0.55	48%	0.33	44	0.53	46%
3:00:00 PM	44	0.53	46%	0.31	42	0.50	44%
3:30:00 PM	43	0.51	45%	0.31	40	0.48	42%
4:00:00 PM	40	0.48	42%	0.29	39	0.47	41%
4:30:00 PM	40	0.48	42%	0.29	39	0.47	41%
5:00:00 PM	41	0.49	43%	0.29	39	0.47	41%
5:30:00 PM	39	0.47	41%	0.28	36	0.43	38%
6:00:00 PM	39	0.47	41%	0.28	35	0.42	36%
6:30:00 PM	36	0.43	38%	0.26	35	0.42	36%
average	46	0.56	48%	0.33	48	0.57	50%

If move to new building	157 spaces
gfa	18366.93
39% %	
0.34 rate: spaces per 100m2	
84.14 spaces for 190 employee	

190 staff





KEY MAP

SCALE: N.T.S.

LEGAL DESCRIPTION

PT LOT 11 AND 12,  
CONCESSION 10  
65R31308  
CITY OF VAUGHAN

PROJECT NORTH

TRUE NORTH

SITE STATISTICS- PHASE 2

SITE AREA	80,694.93 m <sup>2</sup>	or	19.94 acres
DEVELOPABLE SITE AREA-PHASE 1	38,925.17 m <sup>2</sup>	or	9.62 acres
DEVELOPABLE SITE AREA-PHASE 2	29,373.57 m <sup>2</sup>	or	7.23 acres
ZONING	EMI ZONE- PRESTIGE EMPLOYMENT AREA		

REQUIRED

PROVIDED

FRONT YARD (WEST)

4.50 m

28.61 m

REAR YARD (EAST)

12.00 m

20.02 m

INTERIOR SIDE YARD (NORTH)

3.00 m

8.54 m

EXTERIOR SIDE YARD (SOUTH)

4.50 m

25.42 m

G.F.A.

BUILDING A

20,908.79 m<sup>2</sup>

or

225,060 sqft

BUILDING B

18,366.39 m<sup>2</sup>

or

197,694 sqft

TOTAL G.F.A.

39,275.18 m<sup>2</sup>

or

422,754 sqft

SITE COVERAGE (MAX 60 %)

38,789.89 m<sup>2</sup>

or

48.07 %

LANDSCAPED AREA

8,377.97 m<sup>2</sup>

or

10.38 %

PAVED AREA

33,527.07 m<sup>2</sup>

or

41.55 %

BUILDING HEIGHT (MAX)

15.00 m

13.77 m

PARKING

BUILDING A

120 spaces

139 spaces

BUILDING B

211 spaces

157 spaces

TOTAL PARKING

331 spaces

296 spaces

BARRIER FREE PARKING

9 spaces

14 spaces

LOADING (BUILDING B)

2 spaces + 2% (331 required spaces + 2%) = 49 spaces

BY-LAW 01-2021

BIKE SPACES (BUILDING B)

@ 2 BIKES / Ring

18 spaces/8 rings

10 spaces/5 lockers

SNOW STORAGE

TO BE REMOVED OFF SITE

BY PRIVATE CONTRACTOR

SITE STATISTICS- PHASE 1

SITE PLAN APPROVAL: FILE NO. DA-21-028  
AUGUST 16, 2023

SITE AREA

77,678.02 m<sup>2</sup>

or

19.19 acres

AREA OF DEVELOPMENT

41,234.37 m<sup>2</sup>

or

10.19 acres

ZONING

EM2 ZONE- GENERAL EMPLOYMENT

REQUIRED

PROVIDED

LOT FRONTAGE

34.00 m

+/- 229.90 m

FRONT YARD (WEST)

6.00 m

+/- 203.25 m

REAR YARD (EAST)

12.00 m

26.30 m

INTERIOR SIDE YARD (NORTH)

6.00 m

8.69 m

INTERIOR SIDE YARD (SOUTH)

6.00 m

25.42 m

G.F.A.

BUILDING A

20,908.79 m<sup>2</sup>

or

225,060 sqft

TOTAL G.F.A.

20,908.79 m<sup>2</sup>

or

225,060 sqft

SITE COVERAGE (MAX 60 %)

20,908.79 m<sup>2</sup>

or

50.70 %

LANDSCAPED AREA

6,640.49 m<sup>2</sup>

or

16.10 %

PAVED AREA

13,085.09 m<sup>2</sup>

or

33.20 %

BUILDING HEIGHT (MAX)

15.00 m

13.30 m

PARKING

BUILDING A

210 spaces

210 spaces

TOTAL PARKING

210 spaces

210 spaces

BARRIER FREE PARKING

7 spaces

8 spaces

CARPOOL PARKING

BY-LAW 01-2021

5 spaces

LOADING

BY-LAW 01-2021

TYPE A-1

TYPE B-3

TYPE C-1

(35 spaces)

BIKE SPACES

@ 2 BIKES / Ring

4 spaces

4 spaces

SNOW STORAGE

TO BE REMOVED OFF SITE

BY PRIVATE CONTRACTOR

SYMBOL LEGEND

MAN DOOR

LOADING DOCK DOOR

DRIVE-IN / OVERHEAD DOOR

HYDRANT + VALVE

FIRE DEPARTMENT CONNECTION / SIAMESE

CATCH BASIN

DOUBLE CATCH BASIN

SANITARY MAN HOLE

CATCH BASIN / MAN HOLE

STORM MAN HOLE

HYDRO POLE STANDARD / UTILITY POLE

BIKE RACK

HYDRO TRANSFORMER

ACCESSIBLE PARKING SPACE

ACCESSIBLE PARKING SPACE SIGNAGE

SNOW STORAGE AREA

GARBAGE BIN- 8 CUBIC YARD BIN (1.83m x 1.52m x 2.13m)

CARDBOARD BIN- 8 CUBIC YARD BIN (1.83m x 1.52m x 2.13m)

RECYCLING CART- 360 LITRE (0.60m x 0.88m x 1.1m h)

DESIGNATED CARPOOL SPACE / TRIDE SHARING & ASSOCIATED SIGN

No.	ISSUED	DATE
6	RE-ISSUED FOR SPA NO. 5	OCT. 20, 2023
7	RE-ISSUED FOR SPA NO. 6	DEC. 12, 2023
8	RE-ISSUED FOR SPA NO. 7	JAN. 08, 2024
9	RE-ISSUED FOR SPA NO. 8	MAR. 8, 2024
10	ISSUED FOR CONDITIONAL PERMIT	APRIL 11, 2024

11	ISSUED FOR PERMIT NO. 23 131450 000 01 C	JUNE 13, 2024
12	ISSUED FOR MINOR VARIANCE	MAR. 12, 2025

No.	REVISION	DATE
1	REVISED AS NOTED	FEB. 21, 2024
2	REVISED AS NOTED	JUNE 13, 2023
3	REVISED AS NOTED	AUG. 23, 2024
4	REVISED AS NOTED	MAR. 12, 2025

BALDASSARRA

Architects Inc.

30 Great Gulf Drive, Unit 20 | Concord ON | L4K 0K7  
T. 905.660.0722 | [www.baldassarra.ca](http://www.baldassarra.ca)

OWNERS INFORMATION:



Vaughan Block 64  
Highway 50 Investment LP

Vaughan, ON

SITE PLAN  
PHASE 2

CITY FILE NO.: Z.21.054 & DA.21.063

DATE: OCT. 2021  
PROJECT No.

DRAWN BY: HP  
DRAWING No.

CHECKED: SCALE: 1:600

21-45

A-1.0



SCHEDULE D: BACKGROUND

Application No. (City File)	Application Description (i.e. Minor Variance Application; Approved by COA / OLT)
N/A	N/A