

ITEM: 6.6	REPORT SUMMARY MINOR VARIANCE APPLICATION FILE NUMBER: A019/25
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Report Date: March 14, 2025

THIS REPORT CONTAINS COMMENTS FROM THE FOLLOWING  
DEPARTMENTS & AGENCIES (SEE SCHEDULE B):

Additional comments from departments and agencies received after the publication of the report will be made available on the City’s [website](#).

Internal Departments *Comments Received	Conditions Required		Nature of Comments
Committee of Adjustment	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
Building Standards (Zoning)	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
Development Planning	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	Recommend Approval/No Conditions
Development Engineering	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	Recommend Approval/No Conditions
Forestry	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	General Comments w/Conditions
Development Finance	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments

External Agencies *Comments Received	Conditions Required		Nature of Comments *See Schedule B for full comments
Alectra	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
Region of York	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
TRCA	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments

PUBLIC & APPLICANT CORRESPONDENCE (SEE SCHEDULE C)				
All personal information collected because of this public meeting (including both written and oral submissions) is collected under the authority of the Municipal Act, the Municipal Freedom of Information and Protection of Privacy Act (MFIPPA), the Planning Act and all other relevant legislation, and will be used to assist in deciding on this matter. All personal information (as defined by MFIPPA), including (but not limited to) names, addresses, opinions and comments collected will become property of the City of Vaughan, will be made available for public disclosure (including being posted on the internet) and will be used to assist the Committee of Adjustment and staff to process this application.				
Correspondence Type	Name	Address	Date Received (mm/dd/yyyy)	Summary
N/A				

BACKGROUND (SCHEDULE D, IF REQUIRED)	
* Background Information contains historical development approvals considered to be related to this file. This information should not be considered comprehensive.	
Application No. (City File)	Application Description (i.e. Minor Variance Application; Approved by COA / OLT)
N/A	N/A

ADJOURNMENT HISTORY	
* Previous hearing dates where this application was adjourned by the Committee and public notice issued.	
Hearing Date	Reason for Adjournment (to be obtained from NOD_ADJ)
N/A	N/A

SCHEDULES	
Schedule A	Drawings & Plans Submitted with the Application
Schedule B	Comments from Agencies, Building Standards & Development Planning
Schedule C (if required)	Public & Applicant Correspondence
Schedule D (if required)	Background

	<b>MINOR VARIANCE APPLICATION</b> <b>FILE NUMBER A019/25</b>
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CITY WARD #:	4
APPLICANT:	Lisa Colt
AGENT:	Hickory Dickory Decks Vaughan-King
PROPERTY:	46 Shemer Drive, Thornhill
ZONING DESIGNATION:	See Below
VAUGHAN OFFICIAL PLAN (2010) DESIGNATION:	Vaughan Official Plan 2010 ('VOP 2010'): "Low Rise Residential"
RELATED DEVELOPMENT APPLICATIONS:	N/A
PURPOSE OF APPLICATION:	Relief from the Zoning By-law is being requested to permit the construction of a proposed deck in the rear yard.

The following variances have been requested from the City’s Zoning By-law:

The subject lands are zoned R4A(EN), Fourth Density Residential Zone (Established Neighbourhood) and subject to the provisions of Exception 14.729 under Zoning By-law 001-2021, as amended.

#	Zoning By-law 001-2021	Variance requested
1	No portion of the required rear yard shall be encroached or otherwise obstructed by an uncovered platform exceeding 1.2 metres above grade by any more than 2.4 metres. [4.13]	To permit an uncovered platform to encroach into the rear yard a maximum of 4.28 metres.

HEARING INFORMATION
<b>DATE OF MEETING:</b> Thursday, March 20, 2025 <b>TIME:</b> 6:00 p.m. <b>MEETING LOCATION:</b> Vaughan City Hall, Woodbridge Room (2 <sup>nd</sup> Floor), 2141 Major Mackenzie Drive <b>LIVE STREAM LINK:</b> <a href="http://Vaughan.ca/LiveCouncil">Vaughan.ca/LiveCouncil</a>
PUBLIC PARTICIPATION
<p>If you would like to speak to the Committee of Adjustment at the meeting, either remotely or in person, please complete the <a href="#">Request to Speak Form</a> and submit to <a href="mailto:cofa@vaughan.ca">cofa@vaughan.ca</a></p> <p>If you would like to submit written comments, please quote file number above and submit by mail or email to:</p> <p><b>Email:</b> <a href="mailto:cofa@vaughan.ca">cofa@vaughan.ca</a></p> <p><b>Mail:</b> City of Vaughan, Office of the City Clerk, Committee of Adjustment, 2141 Major Mackenzie Drive, Vaughan, ON, L6A 1T1</p> <p>To speak electronically, pre-registration is required by completing the <a href="#">Request to Speak Form</a> on-line and submitting it to <a href="mailto:cofa@vaughan.ca">cofa@vaughan.ca</a> no later than NOON on the last business day before the meeting.</p> <p><b>THE DEADLINE TO REGISTER TO SPEAK ELECTRONICALLY OR SUBMIT WRITTEN COMMENTS ON THE ABOVE NOTED FILE(S) IS NOON ON THE LAST BUSINESS DAY BEFORE THE MEETING.</b></p>

INTRODUCTION
<p>Staff and Agencies act as advisory bodies to the Committee of Adjustment. The comments contained in this report are presented as recommendations to the Committee.</p> <p>Section 45(1) of the Planning Act sets the criteria for authorizing minor variances to the City of Vaughan’s Zoning By-law. Accordingly, review of the application may consider the following:</p>

INTRODUCTION	
<p>That the general intent and purpose of the by-law will be maintained.</p> <p>That the general intent and purpose of the official plan will be maintained.</p> <p>That the requested variance(s) is/are acceptable for the appropriate development of the subject lands.</p> <p>That the requested variance(s) is/are minor in nature.</p> <p>Public written and oral submissions relating to this application are taken into consideration by the Committee of Adjustment as part of its deliberations and final decision on this matter.</p>	

COMMITTEE OF ADJUSTMENT	
Date Public Notice Mailed:	March 6, 2025
Date Applicant Confirmed Posting of Sign:	March 1, 2025
Applicant Justification for Variances: <small>*As provided in Application Form</small>	The signed contract with my client has stipulated that we maintain the suggested encroachment to the setback.
Was a Zoning Review Waiver (ZRW) Form submitted by Applicant: <small>*ZRW Form may be used by applicant in instances where a revised submission is made, and zoning staff do not have an opportunity to review and confirm variances prior to the issuance of public notice.</small>	Yes <input type="checkbox"/> No <input checked="" type="checkbox"/>
COMMENTS:	
N/A	
Committee of Adjustment Recommended Conditions of Approval:	None

BUILDING STANDARDS (ZONING)	
**See Schedule B for Building Standards (Zoning) Comments	
Building Standards Recommended Conditions of Approval:	None

DEVELOPMENT PLANNING	
**See Schedule B for Development Planning Comments.	
Development Planning Recommended Conditions of Approval:	None

DEVELOPMENT ENGINEERING	
<a href="#">Link to Grading Permit</a> <a href="#">Link to Pool Permit</a> <a href="#">Link to Curb Curt Permit</a> <a href="#">Link Culvert Installation</a>	
<p>The proposed work by the Owner/Applicant is increasing the lot coverage on the subject property. The added hardscape may have impacts on the City's Storm Water management system. Development Engineering strongly encourages the Owner / Applicant introduce Low-Impact Development (LID) measures (e.g., bioswales, permeable pavers, rain gardens, rain barrels etc.) to reduce the impacts to the stormwater system. Should further information be required, please contact the Development Engineering COA reviewer.</p>	
Development Engineering Recommended Conditions of Approval:	None

PARKS, FORESTRY & HORTICULTURE (PFH)	
No comments, conditions below	
PFH Recommended Conditions of Approval:	A Private Property Tree Removal & Protection Permit (Construction/Infill) is required for the removal/injury to trees (20cm DBH and over) on the subject property and trees located within 6 metres of the subject property, as per By-Law 052-2018. Please apply through the online portal.

DEVELOPMENT FINANCE	
No comment no concerns	
Development Finance Recommended Conditions of Approval:	None

BY-LAW AND COMPLIANCE, LICENSING AND PERMIT SERVICES	
No comments received to date.	
<b>BCLPS Recommended Conditions of Approval:</b>	None

BUILDING INSPECTION (SEPTIC)	
No comments received to date.	
<b>Building Inspection Recommended Conditions of Approval:</b>	None

FIRE DEPARTMENT	
No comments received to date.	
<b>Fire Department Recommended Conditions of Approval:</b>	None

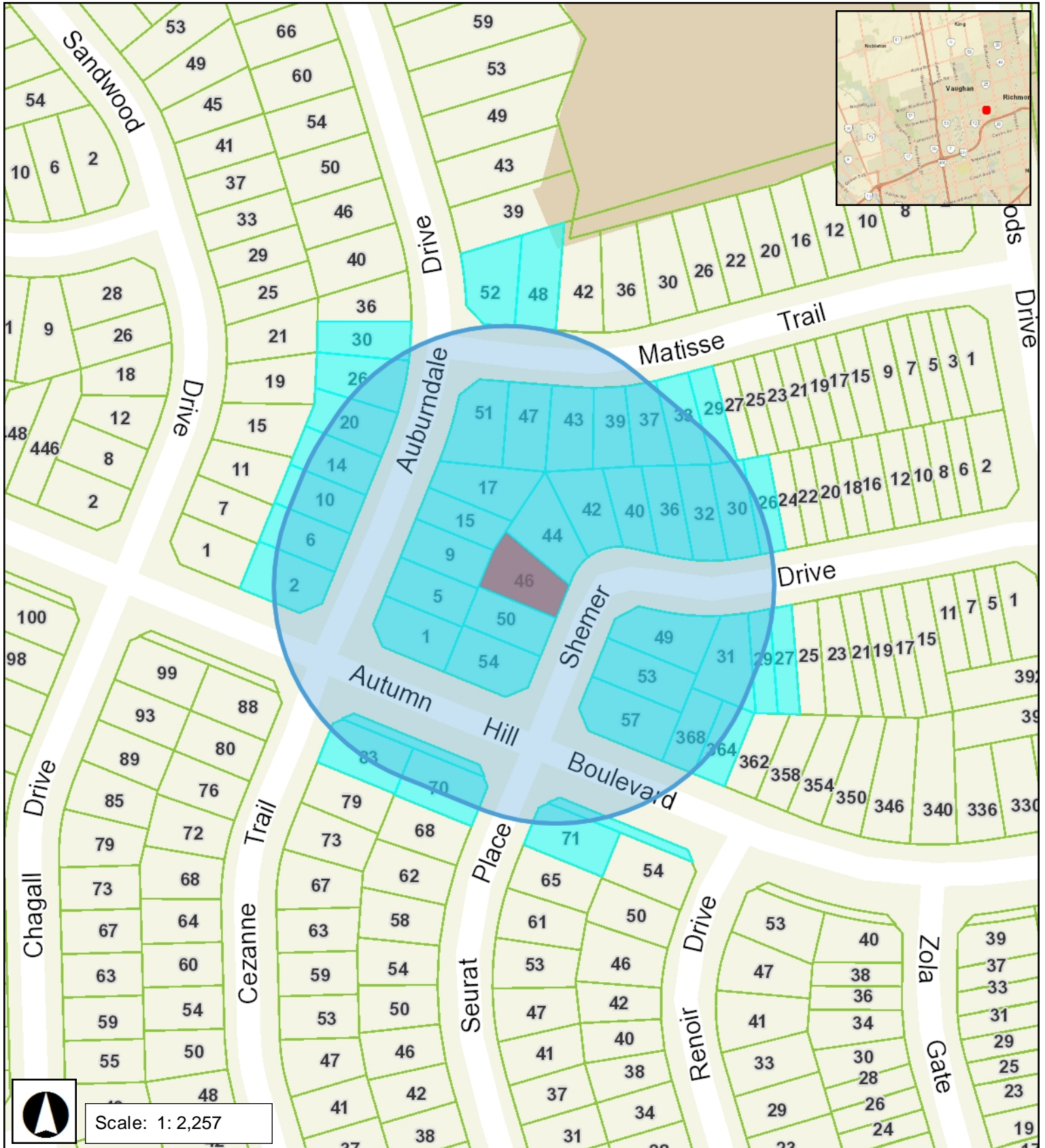
RECOMMENDED CONDITIONS OF APPROVAL SUMMARY		
Should the Committee find it appropriate to approve this application in accordance with request and the sketch submitted with the application, as required by Ontario Regulation 200/96, the following conditions have been recommended:		
#	DEPARTMENT / AGENCY	CONDITION
1	Parks, Forestry and Horticulture Operations <a href="mailto:kari.sthyrhansen@vaughan.ca">kari.sthyrhansen@vaughan.ca</a>	A Private Property Tree Removal & Protection Permit (Construction/Infill) is required for the removal/injury to trees (20cm DBH and over) on the subject property and trees located within 6 metres of the subject property, as per By-Law 052-2018. Please apply through the online portal.
<i>All conditions of approval, unless otherwise stated, are considered to be incorporated into the approval “if required”. If a condition is no longer required after an approval is final and binding, the condition may be waived by the respective department or agency requesting conditional approval. A condition cannot be waived without written consent from the respective department or agency.</i>		

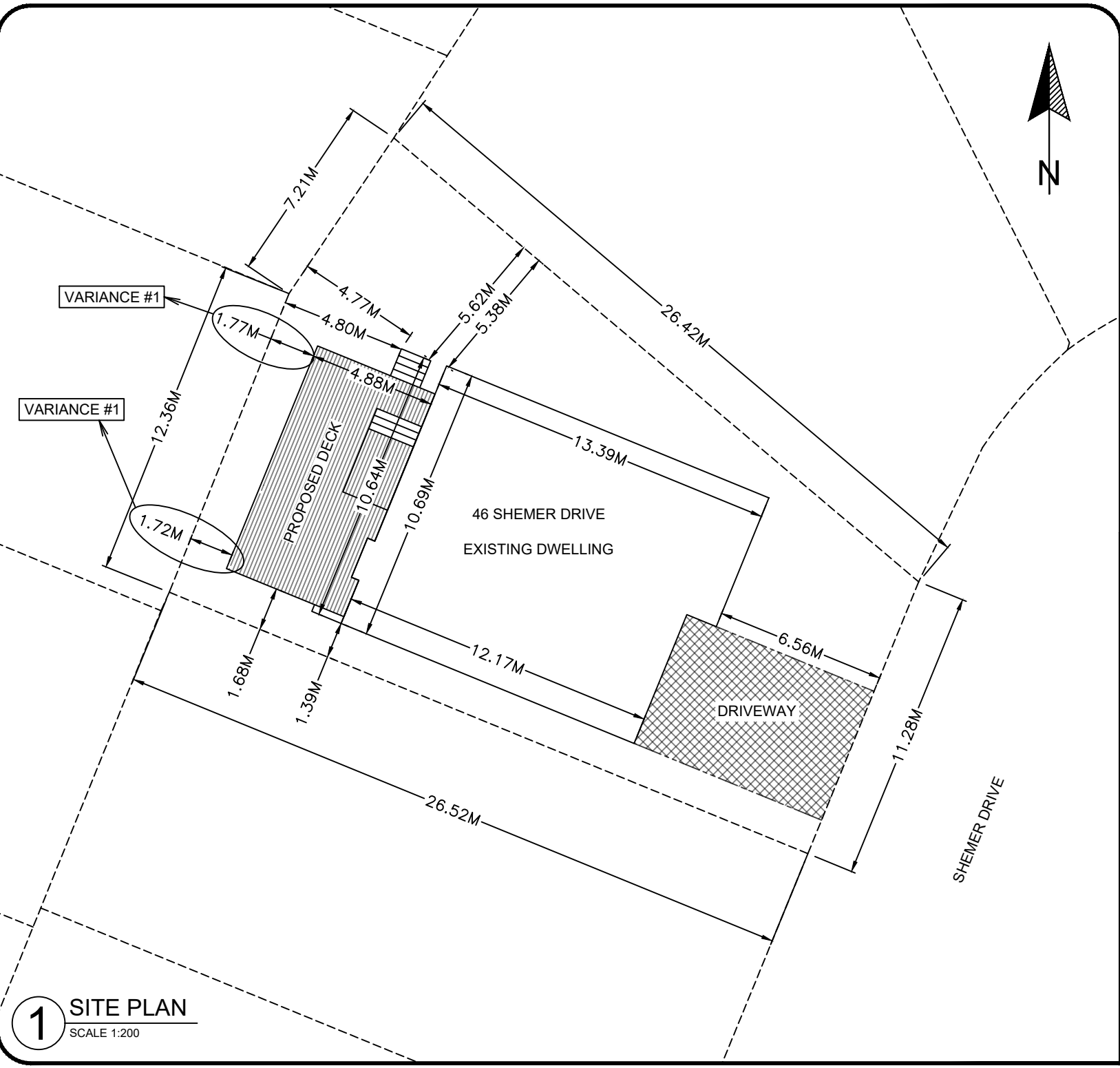
IMPORTANT INFORMATION
<b>CONDITIONS:</b> It is the responsibility of the owner/applicant and/or authorized agent to obtain and provide a clearance letter from respective department and/or agency ( <b>see condition chart above for contact</b> ). This letter must be provided to the Secretary-Treasurer to be finalized. All conditions must be cleared prior to the issuance of a Building Permit.
<b>APPROVALS:</b> Making any changes to your proposal after a decision has been made may impact the validity of the Committee’s decision.  An approval obtained from the Committee of Adjustment, where applicable, is tied to the building envelope shown on the plans and drawings submitted with the application and subject to the variance approval.  A building envelope is defined by the setbacks of the buildings and/or structures shown on the plans and drawings submitted with the application, as required by Ontario Regulation 200/96. Future development outside of an approved building envelope, where a minor variance was obtained, must comply with the provisions of the City’s Zoning By-law.  Elevation drawings are provided to reflect the style of roof (i.e. flat, mansard, gable etc.) to which a building height variance has been applied. Where a height variance is approved, building height is applied to the style of roof (as defined in the City’s Zoning By-law) shown on the elevation plans submitted with the application.  Architectural design features that are not regulated by the City’s Zoning By-law are not to be considered part of an approval unless specified in the Committee’s decision.
<b>DEVELOPMENT CHARGES:</b> That the payment of the Regional Development Charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the Regional Development Charges By-law in effect at the time of payment.  That the payment of the City Development Charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the City's Development Charges By-law in effect at the time of payment.

IMPORTANT INFORMATION
That the payment of the Education Development Charge if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the Boards of Education By-laws in effect at the time of payment
That the payment of Special Area Development charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and The City's Development Charge By-law in effect at the time of Building permit issuance to the satisfaction of the Reserves/Capital Department.
<b>NOTICE OF DECISION:</b> If you wish to be notified of the decision in respect to this application or a related Ontario Land Tribunal (OLT) hearing you must complete a Request for Decision form and submit to the Secretary Treasurer (ask staff for details). In the absence of a written request to be notified of the Committee’s decision you will <b>not</b> receive notice.


**SCHEDULE A: DRAWINGS & PLANS**








**1** SITE PLAN  
SCALE 1:200



**Hickory Dickory Decks**  
Head Office  
115 Dundas St W  
Dundas, On L9H 7L6  
905-689-4774  
hddoffice@gmail.com

Project Name:  
**ANDREW KOTLER**

Project Address:  
**46 SHEMER DRIVE  
THORNHILL, ON  
L4J 9C1**

I have reviewed and take responsibility for the design and am qualified in the appropriate category as a designer under subsection 3.2.5 of Division C of the Ontario Building Code.  
Name: Brad Arnold  
BCIN #: 24758  
Signature: 

Drawn By:  
**STEPHANIE ALLEN**

Date:  
**AUGUST 19,  
2024**

**SP101**



# GENERAL NOTES

- ① A MINIMUM LIVE LOAD OF 1.9 kPA APPLIED IN ALL LOCATIONS
  - ② THE PRESCRIBED SNOW LOAD FOR SELECTED ONTARIO LOCATIONS IS INDICATED IN TABLE 1.2 IN SUPPLEMENTARY GUIDELINE SB-1 OF THE ONTARIO BUILDING CODE. THE SNOW LOAD SHALL BE APPLIED AS THE MINIMUM LIVE LOAD WHERE IT IS GREATER THAN 1.9 kPA
  - ③ LUMBER NO. 2 SPF OR BETTER TO BE USED ALONG WITH CORROSION RESISTANT SPIRAL NAILS OR SCREWS
  - ④ CANTILEVERED JOISTS AND BEAMS ARE LIMITED TO 1/6 THE MEMBERS LENGTH
  - ⑤ CONCRETE PIERS SHALL BEAR ON UNDISTURBED SOIL. THE BEARING CAPACITY OF THE SOIL SHALL BE DETERMINED PRIOR TO CONSTRUCTION
  - ⑥ HELICAL PILES SHALL CONFORM TO MANUFACTURER'S SPECIFICATIONS AND BE INSTALLED BY CERTIFIED INSTALLER. HELICAL CONFORMITY REPORT TO BE RECEIVED FROM INSTALLER/ENGINEER.
  - ⑦ JOISTS SPANNING MORE THAN 6'-0" TO HAVE BRIDGING AT LEAST EVERY 6'-0"
  - ⑧ MAXIMUM BEAM & JOIST CANTILEVERS:
    - 2"X6" MEMBERS = NOT PERMITTED
    - 2"X8" MEMBERS = 1'-4"
    - 2"X10" MEMBERS = 2'-0"
    - 2"X12" MEMBERS = 2'-0"
  - ⑨ MINIMUM BEARING FOR BEAMS & JOISTS:
    - MIN. BEAM BEARING =  $3\frac{1}{2}"$
    - MIN. JOIST BEARING =  $1\frac{1}{2}"$
  - ⑩ INSTALLATION OF MANUFACTURED LOW MAINTENANCE DECK BOARDS TO BE INSTALLED AS PER MANUFACTURER'S SPECIFICATIONS
  - ⑪ INSTALLATION OF PRE-FABRICATED COMPONENTS (RAILING SYSTEMS, ETC.) TO BE INSTALLED AS PER MANUFACTURER'S SPECIFICATIONS
  - ⑫ EXTERIOR STAIRS:
    - MIN. RISE =  $4\frac{7}{8}"$
    - MAX. RISE =  $7\frac{7}{8}"$
    - MIN. RUN = 10"
    - MAX. RUN = 14"
    - MIN. WIDTH = 34"
    - MIN. HEADROOM = 6'-5"
  - ⑬ EXTERIOR GUARDS:
    - DECK TO GRADE > 5'-11" = 3'-6" HEIGHT
    - DECK TO GRADE < 5'-11" = 3'-0" HEIGHT
    - STAIRS = 3'-0" HEIGHT
    - MAX. BETWEEN PICKETS = 4"
- NO MEMBER OR ATTACHMENT BETWEEN  $5\frac{1}{2}"$  - 3'-0" HIGH SHALL FACILITATE CLIMBING

## Hickory Dickory Decks



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Project Name:


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L4J 9C1**

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BCIN #: 24758

Signature: 

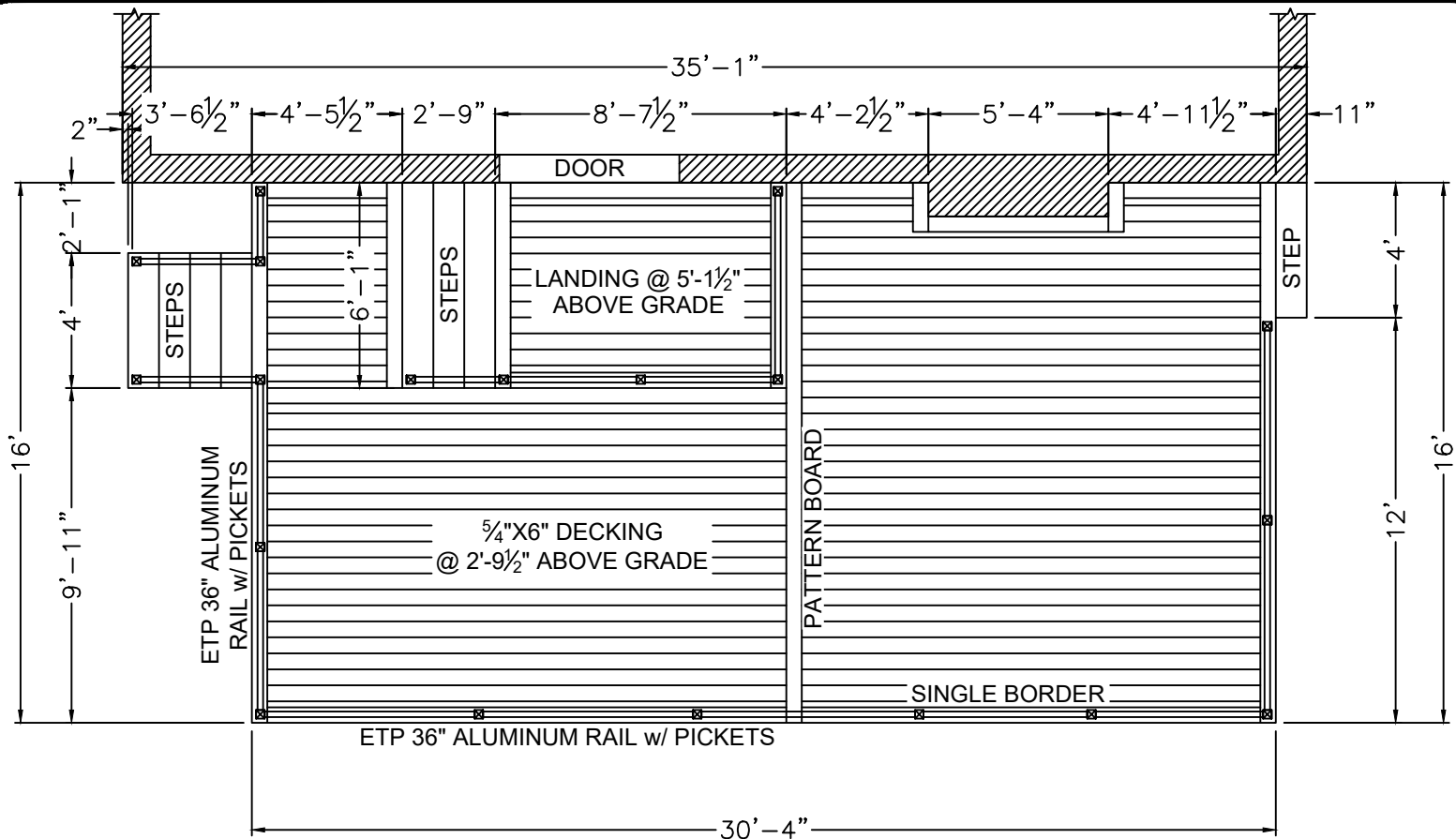
Drawn By:

**STEPHANIE  
ALLEN**

Date:

**AUGUST 19,  
2024**

**A001**



**1** DECK FLOOR PLAN  
SCALE: 3/16" = 1' - 0"

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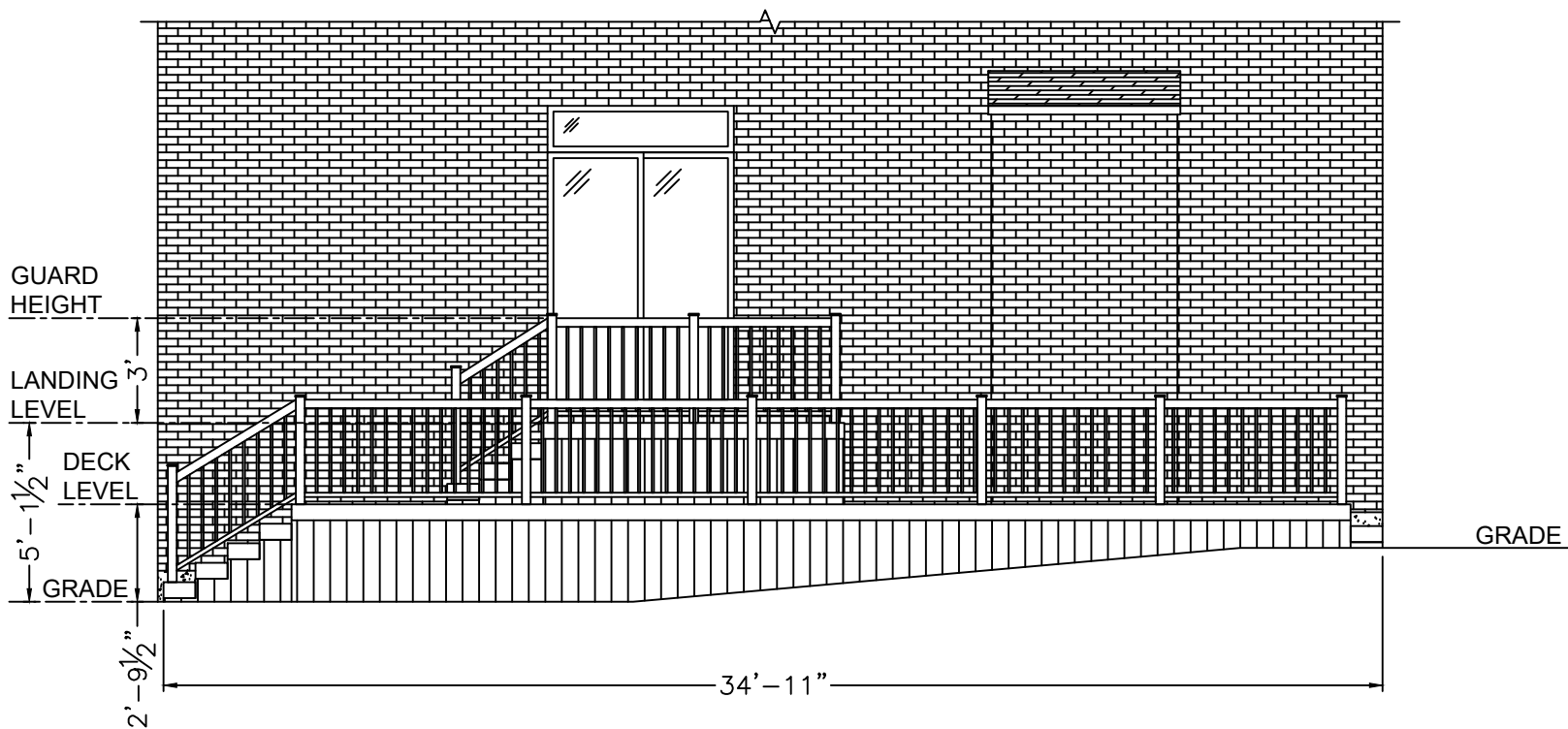
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Name: Brad Arnold  
BCIN #: 24758  
Signature:

Drawn By:  
**STEPHANIE ALLEN**

Date:  
**AUGUST 19,  
2024**

**A101**



**1** DECK FRONT ELEVATION  
SCALE:  $\frac{3}{16}" = 1' - 0"$

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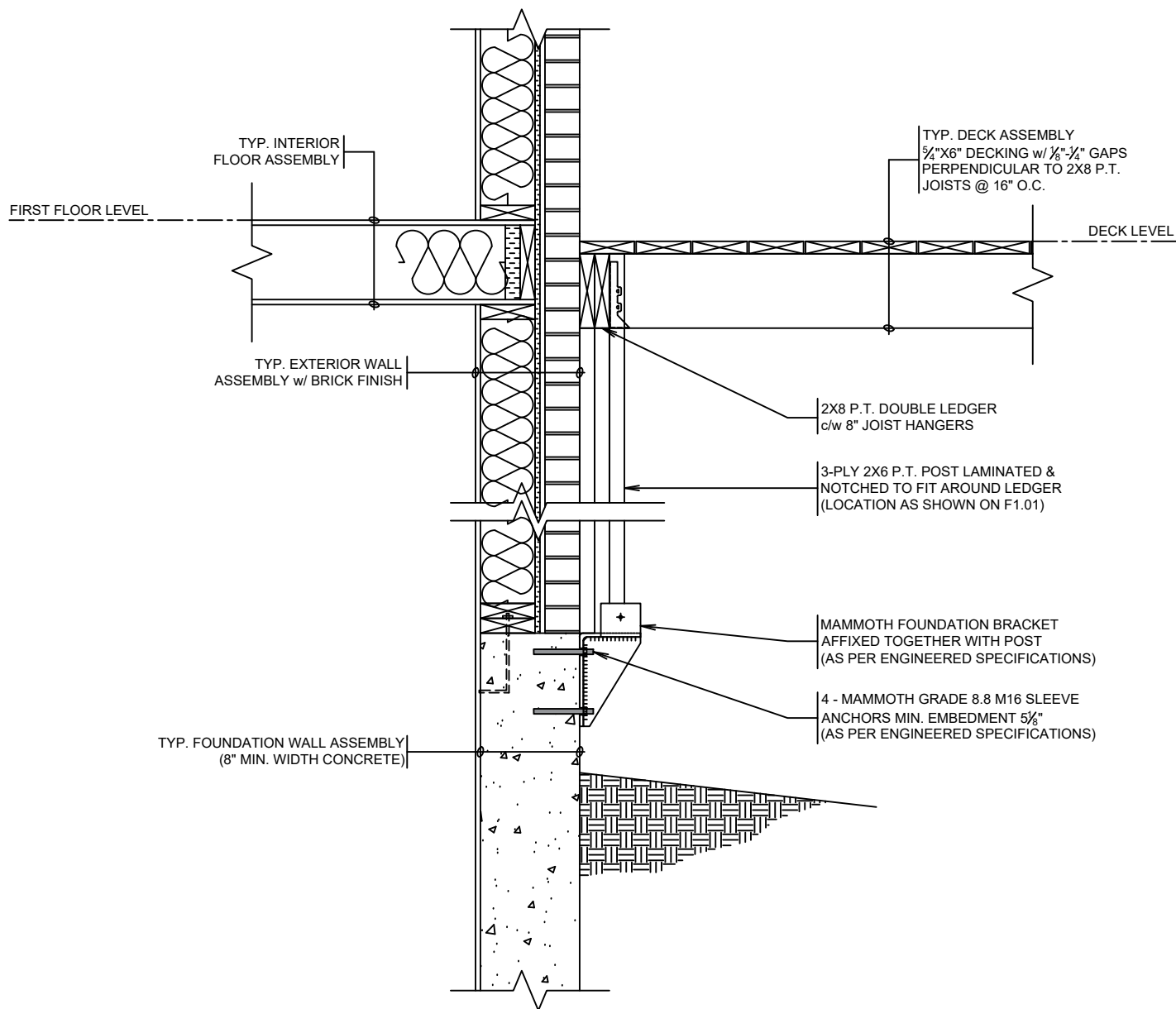
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Name: Brad Arnold  
BCIN #: 24758  
Signature: *AP*

Drawn By:  
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Date:  
**AUGUST 19, 2024**

**A201**

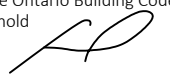


**1** DECK STRUCTURAL ATTACHMENT  
SCALE:  $\frac{3}{4}" = 1' - 0"$

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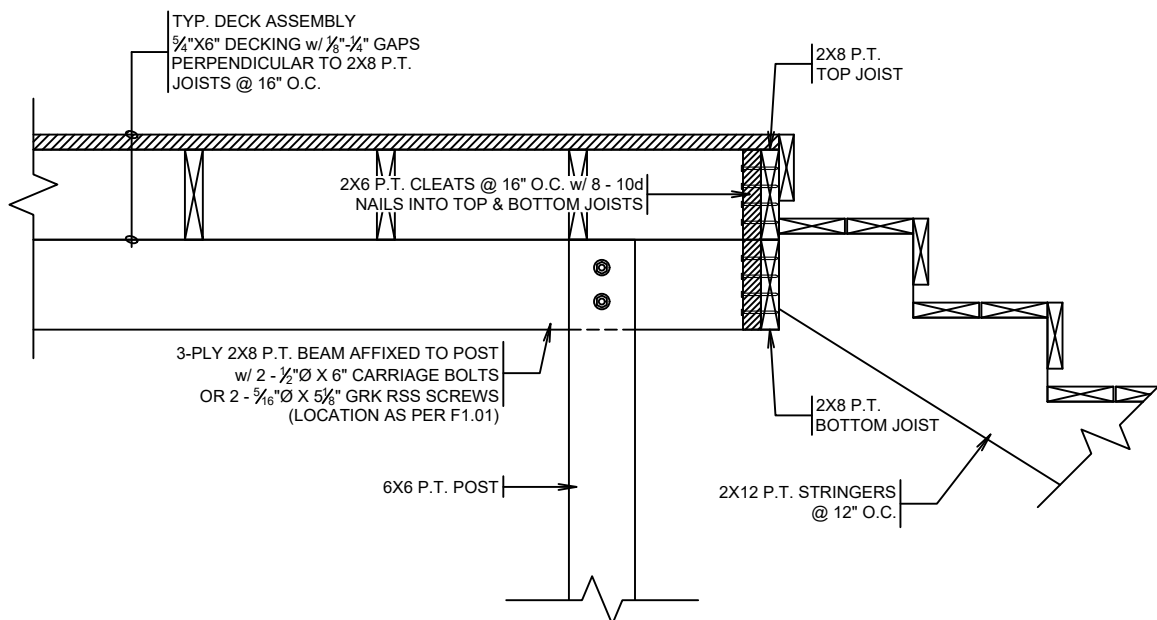
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Signature: 

Drawn By:  
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Date:  
**AUGUST 19,  
2024**

**A3 01**



**1** DECK STAIR ATTACHMENT


SCALE: 3/4" = 1' - 0"



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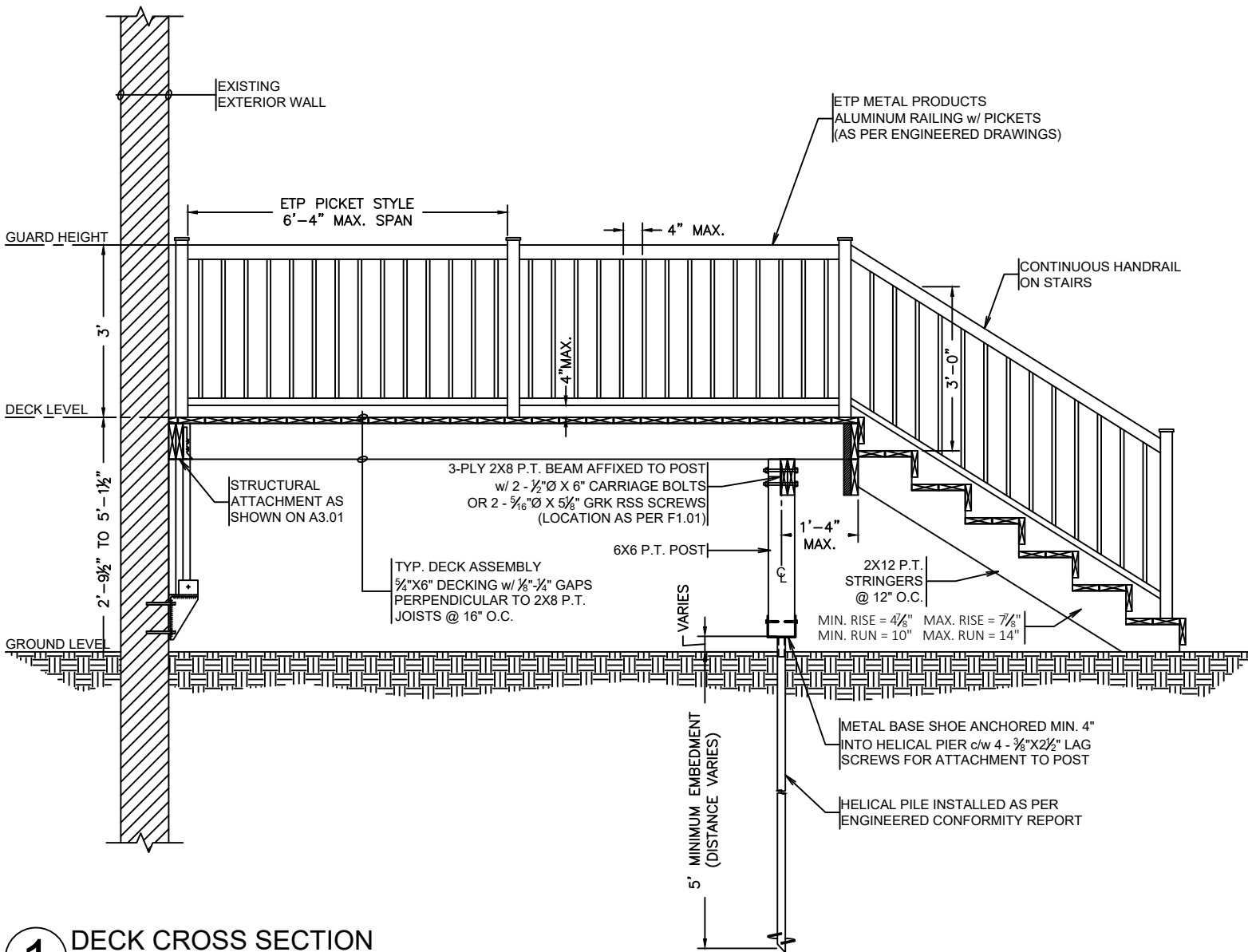
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Signature: 

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Date:  
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**A3 02**



## 1 DECK CROSS SECTION

SCALE:  $\frac{3}{8}" = 1' - 0"$


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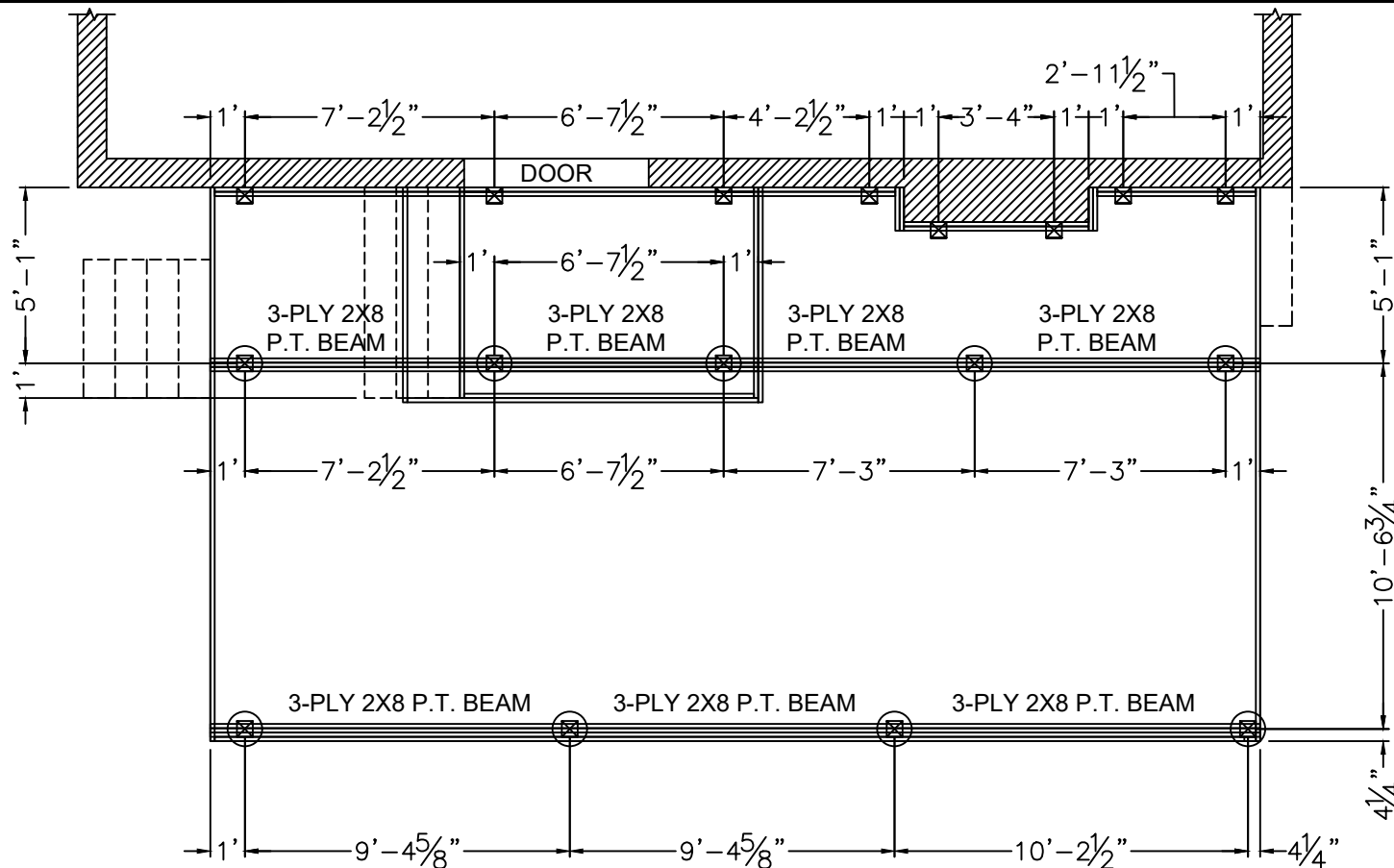
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Name: Brad Arnold  
BCIN #: 24758  
Signature: 

Drawn By:  
**STEPHANIE  
ALLEN**

Date: AUGUST 19, 2024


A4|01





**1** DECK FOUNDATION PLAN  
SCALE: 3/16" = 1' - 0"


- ☒ 3-PLY 2X6 P.T. POSTS ON FOUNDATION BRACKETS (AS PER ENGINEERING)
- ☒ 6X6 P.T. POSTS ON HELICAL PILES (AS PER ENGINEERING)



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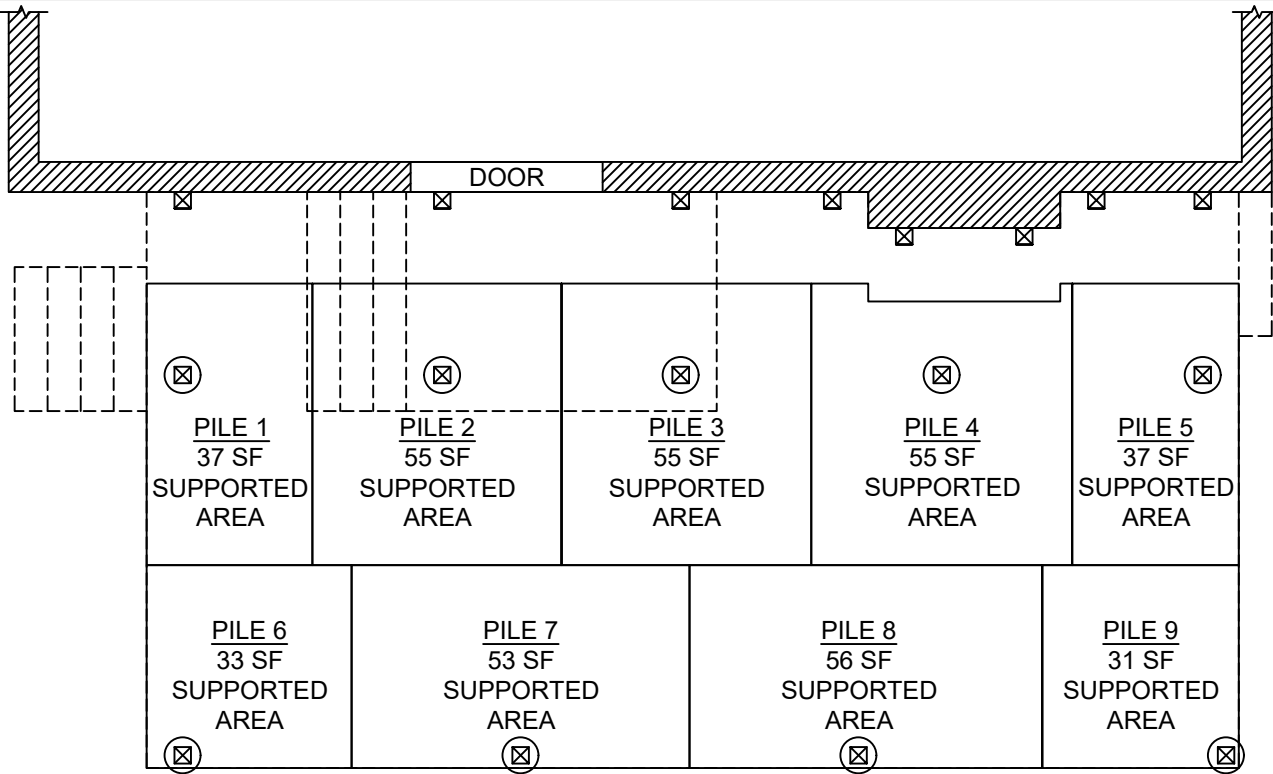
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Signature: 

Drawn By:  
**STEPHANIE ALLEN**

Date:  
**AUGUST 19, 2024**

**10  
E101**



**1** DECK FOOTING PLAN  
SCALE: 3/16" = 1' - 0"

- 3-PLY 2X6 P.T. POSTS ON  
☒ FOUNDATION BRACKETS  
(AS PER ENGINEERING)
- ☒ 6X6 P.T. POSTS ON HELICAL  
PILES (AS PER ENGINEERING)

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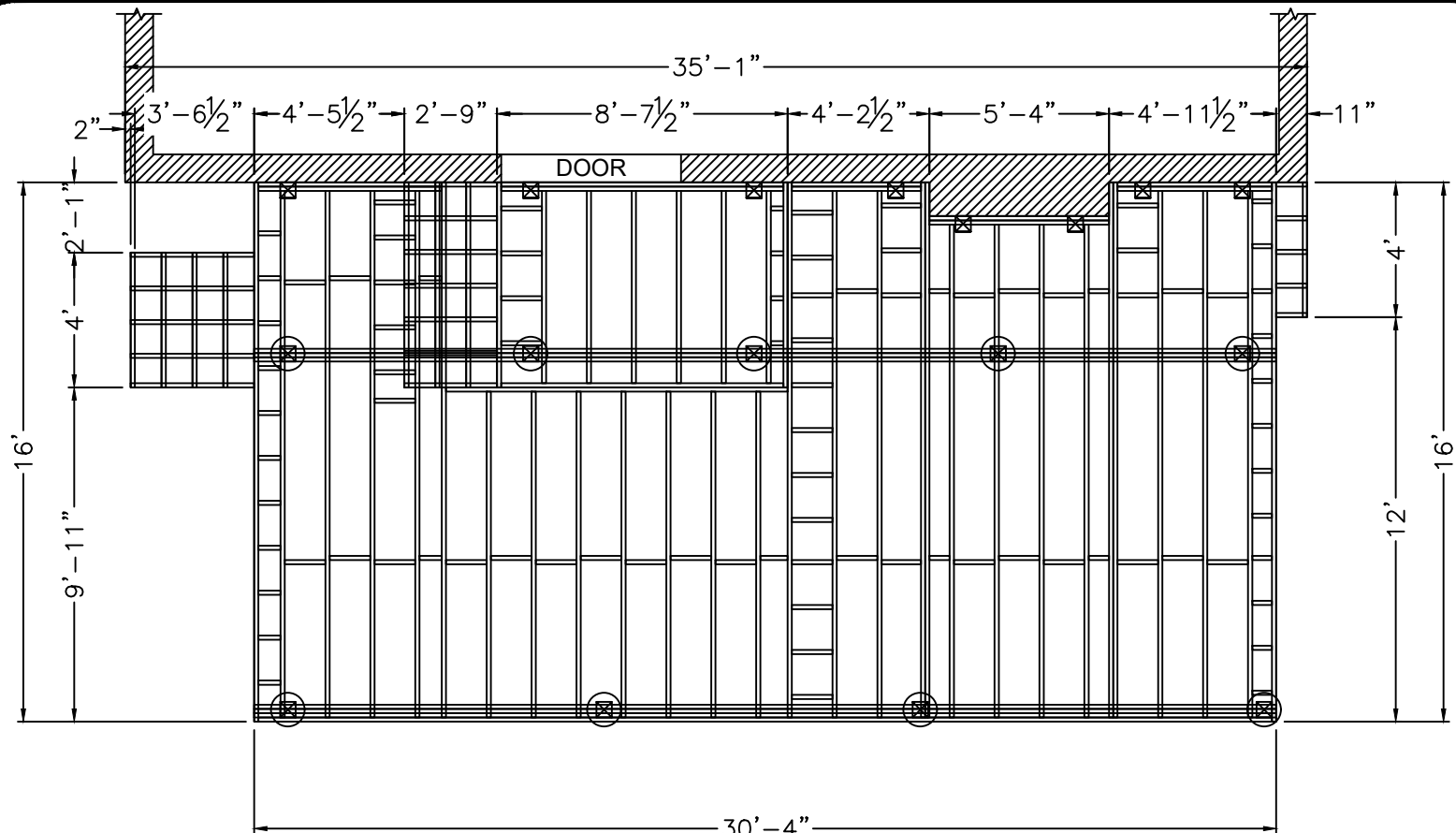
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**STEPHANIE ALLEN**

Date:  
**AUGUST 19, 2024**


**10  
F201**



2x8 P.T. JOISTS @ 16" O.C.  
2x8 P.T. MID-SPAN BLOCKING  
2x12 P.T. STRINGERS @ 12" O.C.

**1** DECK SUBSTRUCTURE PLAN  
SCALE: 3/16" = 1' - 0"

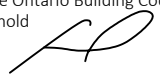
- 3-PLY 2X6 P.T. POSTS ON  
☒ FOUNDATION BRACKETS  
(AS PER ENGINEERING)
- ☒ 6X6 P.T. POSTS ON HELICAL  
PILES (AS PER ENGINEERING)



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Drawn By: <b>STEPHANIE ALLEN</b>	<b>101</b> <b>S101</b>
Date: <b>AUGUST 19, 2024</b>	

SCHEDULE B:  
COMMENTS FROM AGENCIES, BUILDING STANDARDS &  
DEVELOPMENT PLANNING

Department / Agency *Comments Received	Conditions Required		Nature of Comments
Building Standards (Zoning) *See Schedule B	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
Development Planning	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	Recommend Approval/No Conditions

External Agencies *Comments Received	Conditions Required		Nature of Comments *See Schedule B for full comments
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Region of York	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments
TRCA	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>	General Comments

**Date:** February 20<sup>th</sup> 2025

**Attention:** **Christine Vigneault**

**RE:** Request for Comments

**File No.:**

**Related Files:** **A019-25**

**Applicant:** Hickory Dickory Decks Vaughan

**Location** 46 Shemer Drive

**COMMENTS:**

☐

We have reviewed the proposed Variance Application and have no comments or objections to its approval.

☒

We have reviewed the proposed Variance Application and have no objections to its approval, subject to the following comments (attached below).

☐

We have reviewed the proposed Variance Application and have the following concerns (attached below).

Alectra Utilities (formerly PowerStream) has received and reviewed the proposed Variance Application. This review, however, does not imply any approval of the project or plan.

All proposed billboards, signs, and other structures associated with the project or plan must maintain minimum clearances to the existing overhead or underground electrical distribution system as specified by the applicable standards, codes and acts referenced.

In the event that construction commences, and the clearance between any component of the work/structure and the adjacent existing overhead and underground electrical distribution system violates the Occupational Health and Safety Act, the customer will be responsible for 100% of the costs associated with Alectra making the work area safe. All construction work will be required to stop until the safe limits of approach can be established.

In the event construction is completed, and the clearance between the constructed structure and the adjacent existing overhead and underground electrical distribution system violates the any of applicable standards, acts or codes referenced, the customer will be responsible for 100% of Alectra's cost for any relocation work.

**References:**

- Ontario Electrical Safety Code, latest edition (Clearance of Conductors from Buildings)
- Ontario Health and Safety Act, latest edition (Construction Protection)
- Ontario Building Code, latest edition (Clearance to Buildings)
- PowerStream (Construction Standard 03-1, 03-4), attached
- Canadian Standards Association, latest edition (Basic Clearances)

If more information is required, please contact either of the following:

Mr. Stephen Cranley, C.E.T  
Supervisor, Distribution Design, ICI & Layouts (North)  
**Phone:** 1-877-963-6900 ext. 31297

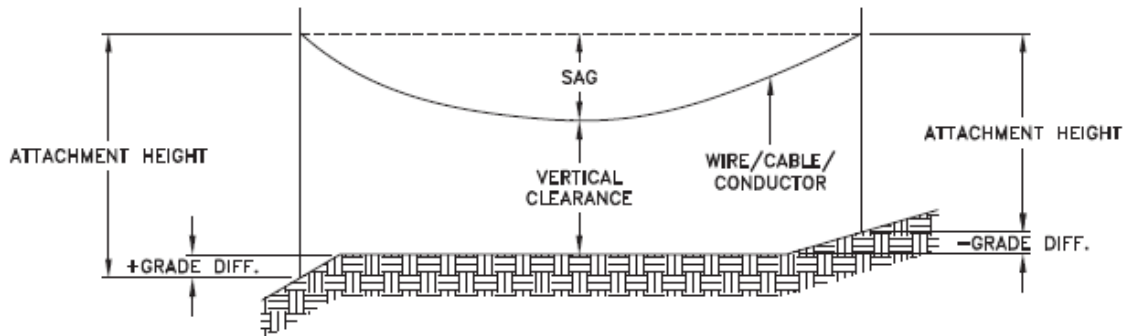
**E-mail:** [stephen.cranley@alectrautilities.com](mailto:stephen.cranley@alectrautilities.com)

Mitchell Penner  
Supervisor, Distribution Design-Subdivisions  
**Phone:** 416-302-6215

**Email:** [Mitchell.Penner@alectrautilities.com](mailto:Mitchell.Penner@alectrautilities.com)



LOCATION OF WIRES, CABLES OR CONDUCTORS	SYSTEM VOLTAGE			
	SPAN GUYS AND COMMUNICATIONS WIRES	UP TO 600V AND NEUTRAL	4.16/2.4kV TO 27.6/16kV (SEE NOTE 1)	44kV
	MINIMUM VERTICAL CLEARANCES (SEE NOTE 2)			
OVER OR ALONGSIDE ROADS, DRIVEWAYS OR LANDS ACCESSIBLE TO <u>VEHICLES</u>	442cm	442cm	480cm	520cm
OVER GROUND ACCESSIBLE TO <u>PEDESTRIANS AND BICYCLES ONLY</u>	250cm	310cm	340cm	370cm
ABOVE TOP OF RAIL AT <u>RAILWAY CROSSINGS</u>	730cm	730cm	760cm	810cm



MINIMUM ATTACHMENT HEIGHT = MAXIMUM SAG  
 + MINIMUM VERTICAL CLEARANCE (FROM ABOVE TABLE)  
 + GRADE DIFFERENCE  
 + 0.3m (VEHICLE OR RAILWAY LOCATION)  
 + SNOW DEPTH (PEDESTRIAN LOCATION, SEE NOTE 3)

**NOTES:**

1. THE MULTIGROUNDED SYSTEM NEUTRAL HAS THE SAME CLEARANCE AS THE 600V SYSTEM.
2. THE VERTICAL CLEARANCES IN THE ABOVE TABLE ARE UNDER MAXIMUM SAG CONDITIONS.
3. REFER TO CSA STANDARD C22.3 No.1, ANNEX D FOR LOCAL SNOW DEPTH VALUES.
4. ALL CLEARANCES ARE IN ACCORDANCE TO CSA STANDARD C22.3.

CONVERSION TABLE	
METRIC	IMPERIAL (APPROX)
810cm	27'-0"
760cm	25'-4"
730cm	24'-4"
520cm	17'-4"
480cm	16'-0"
442cm	15'-5"
370cm	12'-4"
340cm	11'-4"
310cm	10'-4"
250cm	8'-4"

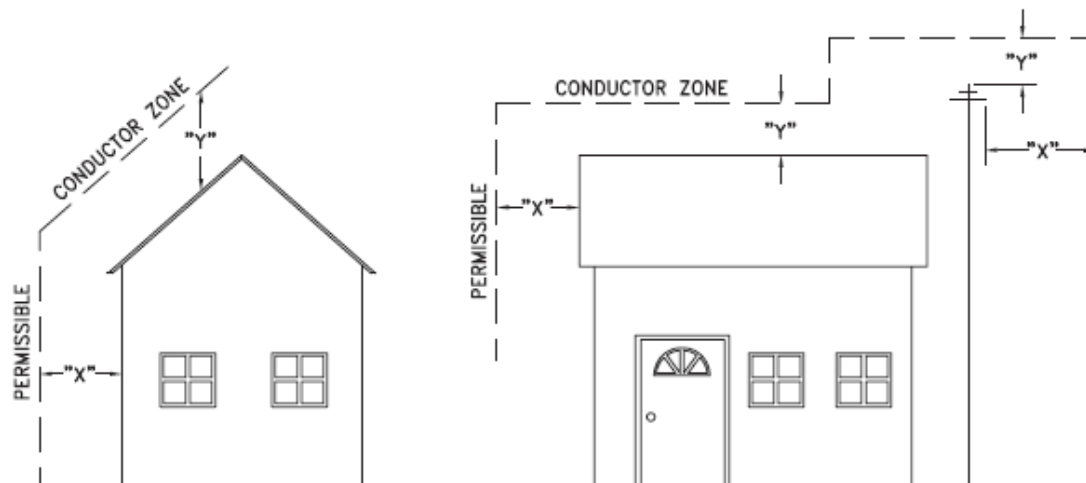
**REFERENCES**

SAGS AND TENSIONS | SECTION 02

**MINIMUM VERTICAL CLEARANCES OF WIRES, CABLES AND CONDUCTORS ABOVE GROUND OR RAILS**

ORIGINAL ISSUE DATE: 2010-DEC-24 REVISION NO: R1 REVISION DATE: 2012-JAN-09

Certificate of Approval	
This construction Standard meets the safety requirements of Section 4 of Regulation 22/04	
Joe Crozier, P.Eng.	2012-JAN-09
Name	Date
P.Eng. Approval By:	Joe Crozier



VOLTAGE	MINIMUM HORIZONTAL CLEARANCE UNDER MAXIMUM SWING CONDITIONS DIMENSION "X" (SEE NOTES 1, 3 & 4)	MINIMUM VERTICAL CLEARANCE UNDER MAXIMUM DESIGN SAG CONDITIONS DIMENSION "Y" (SEE NOTES 1, 2, 4 & 5)
0-600V AND NEUTRAL	100cm	250cm
4.16/2.4 TO 44kV	300cm	480cm

### NOTES

1. UNDER NO CIRCUMSTANCES SHALL A CONDUCTOR BE PERMITTED TO PENETRATE THE ENVELOPE SHOWN BY THE DOTTED LINE.
2. THE VERTICAL CLEARANCES ARE UNDER CONDITIONS OF MAXIMUM DESIGN SAG.
3. THE HORIZONTAL CLEARANCES ARE UNDER CONDITIONS OF MAXIMUM SWING. WHERE THE CONDUCTOR SWING IS NOT KNOWN A HORIZONTAL CLEARANCE OF 480CM SHALL BE USED.
4. BUILDINGS THAT EXCEED 3 STOREYS OR 15M IN HEIGHT, THE MINIMUM HORIZONTAL CLEARANCE OF THE SECONDARY CONDUCTORS SHOULD BE INCREASED TO 300cm WHERE IT IS NECESSARY TO ALLOW FOR THE RAISING OF LADDERS BY LOCAL FIRE DEPARTMENTS.
5. IN SITUATIONS SUCH AS MULTI-LEVEL GARAGES, WHERE ROOFS ARE NORMALLY USED BY PERSONS AND VEHICLES, THE VERTICAL CLEARANCES OF POWERSTREAM STANDARD 03-1 SHALL APPLY.
6. DISTRIBUTION LINES CONSTRUCTED NEAR BUILDINGS SHALL BE BUILT TO AVOID OVERHANG WHEREVER POSSIBLE. WHERE LINES MUST BE CONSTRUCTED OVER OR ADJACENT TO BUILDINGS THE APPLICABLE HORIZONTAL AND VERTICAL CLEARANCES SHALL BE AT CONDITIONS OF MAXIMUM CONDUCTOR SWING AND MAXIMUM SAG. THE ABOVE CLEARANCES ARE DESIGNED TO PREVENT PERSONS ON OR IN BUILDINGS AS WELL AS EXTERNAL MACHINERY USED IN CONJUNCTION WITH A BUILDING TO COME IN CONTACT WITH CONDUCTORS. EFFORTS SHOULD BE MADE TO INCREASE THESE CLEARANCES WHERE POSSIBLE.
7. ALL CLEARANCES ARE IN ACCORDANCE TO CSA C22.3 NO.1-06 (TABLE-9).

CONVERSION TABLE	
METRIC	IMPERIAL (APPROX)
480cm	16'-0"
300cm	10'-0"
250cm	8'-4"
100cm	3'-4"

### MINIMUM VERTICAL & HORIZONTAL CLEARANCES OF CONDUCTORS FROM BUILDINGS OR OTHER PERMANENT STRUCTURES (CONDUCTORS NOT ATTACHED TO BUILDINGS)

ORIGINAL ISSUE DATE: 2010-MAY-05 REVISION NO: REVISION DATE:

PS:\System Planning and Standards\Standard Design\PowerStream Standards\PowerStream Standards working folder\Section 3\3-4\DWG 03-4 R0 May 5, 2010.dwg, 5/5/2010 8:22:02 AM, Adobe PDF

**Certificate of Approval**  
This construction Standard meets the safety requirements of Section 4 of Regulation 22/04  
Debbie Dadwani, P.Eng. 2010-MAY-05  
Name Date  
P.Eng. Approval By: D. Dadwani

**To:** Committee of Adjustment

**From:** Sean Fitzpatrick, Building Standards Department

**Date:** February 19, 2025

**Applicant:** Hickory Dickory Decks Vaughan

**Location:** 46 Shemer Drive  
PLAN 65M3673 Lot 340

**File No.(s):** A019/25

**Zoning Classification:**

The subject lands are zoned R4A(EN), Fourth Density Residential Zone (Established Neighbourhood) and subject to the provisions of Exception 14.729 under Zoning By-law 001-2021, as amended.

#	Zoning By-law 001-2021	Variance requested
1	No portion of the required rear yard shall be encroached or otherwise obstructed by an uncovered platform exceeding 1.2 metres above grade by any more than 2.4 metres. [4.13]	To permit an uncovered platform to encroach into the rear yard a maximum of 4.28 metres.

**Staff Comments:**

**Stop Work Order(s) and Order(s) to Comply:**

There are no outstanding Orders on file

**Building Permit(s) Issued:**

Building Permit No. 25-100033 for Single Detached Dwelling - Deck, Issue Date: (Not Yet Issued)

**Other Comments:**

General Comments	
1	The applicant shall be advised that additional variances may be required upon review of detailed drawing for building permit/site plan approval.

**Conditions of Approval:**

If the committee finds merit in the application, the following conditions of approval are recommended.

\* Comments are based on the review of documentation supplied with this application.

**To:** Christine Vigneault, Committee of Adjustment Secretary Treasurer

**From:** Nancy Tuckett, Director of Development and Parks Planning

**Date:** March 12, 2025

**Name of Owner:** Lisa Colt

**Location:** 46 Shemer Drive, Thornhill

**File No.(s):** A019/25

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**Proposed Variance(s) (By-law 001-2021):**

1. To permit an uncovered platform to encroach into the rear yard a maximum of 4.28 m.

**By-Law Requirement(s) (By-law 001-2021):**

1. No portion of the required rear yard shall be encroached or otherwise obstructed by an uncovered platform exceeding 1.2 m above grade by any more than 2.4 m.

**Official Plan:**

City of Vaughan Official Plan 2010 ('VOP 2010'): "Low-Rise Residential"

**Comments:**

The Owner is seeking relief to permit a deck, with the above noted variance.

The Development and Parks Planning Department has no objection to Variance 1 to permit a deck encroachment of 4.28 m into the rear yard, whereas the Zoning By-law permits 2.4 m. The proposed deck is to be located in the rear yard of an existing two (2)-storey dwelling. The lower portion of the deck is proposed to be approximately 0.64 m above grade. A landing platform is proposed on top of the lower portion of the deck, with steps to facilitate access from the dwelling's rear door. The proposed height for the landing platform is approximately 1.6 m above grade. The proposed 2.4 m encroachment does not hinder the functionality of the rear yard as an outdoor amenity space, as the deck does not span the entire length of the rear yard. The modest size and design of the proposed deck is not anticipated to cause any adverse massing impacts to the surrounding properties.

Accordingly, the Development and Parks Planning Department can support the requested variance and is of the opinion that the proposal is minor in nature, maintains the general intent and purpose of the Official Plan and Zoning By-law, and is desirable for the appropriate development of the land.

**Recommendation:**

The Development and Parks Planning Department recommends approval of the application.

**Conditions of Approval:**

If the Committee finds merit in the application, the following conditions of approval are recommended:

None.

**Comments Prepared by:**

Alyssa Pangilinan, Planner 1  
Janany Nagulan, Senior Planner

**From:** [Cameron McDonald](#)  
**To:** [Committee of Adjustment Mailbox](#)  
**Subject:** [External] RE: A019/25 - 46 Shemer Drive - REQUEST FOR COMMENTS, CITY OF VAUGHAN  
**Date:** Thursday, February 20, 2025 3:50:37 PM

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Hello,

Based on a review of our screening mapping, I can confirm that the subject property is not located within TRCA's Regulated Area. As such, any site alteration or development on the property would not require a permit from the TRCA.

Based on the above, we have no comments/requirements.

Regards,

**Cameron McDonald**

Planner I

Development Planning and Permits | Development and Engineering Services

T: [\(437\) 880-1925](tel:(437)880-1925)

E: [cameron.mcdonald@trca.ca](mailto:cameron.mcdonald@trca.ca)

A: [5 Shoreham Drive, Toronto, ON, M3N 1S4](#) | [trca.ca](http://trca.ca)



**From:** [Development Services](#)  
**To:** [Committee of Adjustment Mailbox](#)  
**Subject:** [External] RE: A019/25 - 46 Shemer Drive - REQUEST FOR COMMENTS, CITY OF VAUGHAN  
**Date:** Friday, February 21, 2025 10:52:20 AM

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Good morning,

The Regional Municipality of York has completed its review of the above minor variance application and has no comment.

Regards,

*Gabrielle*

**Gabrielle Hurst MCIP, RPP** | Associate Planner, Development Planning, Economic and Development Services Branch | The Regional Municipality of York | 1-877 464 9675 ext 71538 | [gabrielle.hurst@york.ca](mailto:gabrielle.hurst@york.ca) | [www.york.ca](http://www.york.ca)



**SCHEDULE C: PUBLIC & APPLICANT CORRESPONDENCE**

Correspondence Type	Name	Address	Date Received (mm/dd/yyyy)	Summary
N/A				

SCHEDULE D: BACKGROUND

Application No. (City File)	Application Description (i.e. Minor Variance Application; Approved by COA / OLT)
N/A	N/A