

**COUNCIL MEETING – NOVEMBER 19, 2024
COMMUNICATIONS**

		<u>Rpt. No.</u>	<u>Item(s) No.</u>	<u>Committee</u>
<u>Distributed November 15, 2024</u>				
C1.	Marisa Parise, dated November 4, 2024.	37	16	Committee of the Whole
C2.	Cosimo Napoli, Councillor, Ward 6, Town of Caledon, dated November 4, 2024.	37	16	Committee of the Whole
C3.	Salvina Greco, Bolton, dated November 4, 2024.	37	16	Committee of the Whole
C4.	Darlene Gray, dated November 4, 2024.	37	16	Committee of the Whole
C5.	Loredana Abramovitch, dated November 4, 2024.	37	16	Committee of the Whole
C6.	Konstantina, dated November 4, 2024.	37	16	Committee of the Whole
C7.	Simar Walia, dated November 4, 2024.	37	16	Committee of the Whole
C8.	Adwoa, dated November 4, 2024.	37	16	Committee of the Whole
C9.	Carmelo Galluccio, dated November 4, 2024.	37	16	Committee of the Whole
C10.	Wendy Ficociello, dated November 4, 2024.	37	16	Committee of the Whole
C11.	Concerned resident and mother of two, dated November 4, 2024.	37	16	Committee of the Whole
C12.	Nina Colucci, dated November 4, 2024.	37	16	Committee of the Whole
C13.	Carmelo Galluccio, dated November 4, 2024.	37	16	Committee of the Whole
C14.	melissa, dated November 4, 2024.	37	16	Committee of the Whole
C15.	Elizabeth & Eugenio Ardito, Sant Farm Drive, Bolton, dated November 4, 2024.	37	16	Committee of the Whole
C16.	Susan Scott, dated November 4, 2024.	37	16	Committee of the Whole
C17.	Nicole Dumanski, dated November 4, 2024.	37	16	Committee of the Whole
C18.	Dev Gosal, dated November 5, 2024.	37	16	Committee of the Whole
C19.	Cassandra and Marc-Anthony Simonetta, dated November 5, 2024.	37	16	Committee of the Whole
C20.	Luigi Di Palma, dated November 5, 2024.	38	5	Committee of the Whole (Public Meeting)

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**COUNCIL MEETING – NOVEMBER 19, 2024
COMMUNICATIONS**

	<u>Rpt. No.</u>	<u>Item(s) No.</u>	<u>Committee</u>
C21. Irene, dated November 7, 2024.	38	5	Committee of the Whole (Public Meeting)
C22. Weicheng Zhang, Sibella Way, Vaughan, dated November 5, 2024	38	6	Committee of the Whole (Public Meeting)
C23. Farzad Moradveisi, dated November 6, 2024	38	6	Committee of the Whole (Public Meeting)
C24. Fabiola Becerril, dated November 7, 2024.	38	5	Committee of the Whole (Public Meeting)
C25. Zeynep Asci, dated November 8, 2024	38	6	Committee of the Whole (Public Meeting)
C26. Daniel Shafro, dated November 11, 2024	39	9	Committee of the Whole
C27. Milana Shur, dated November 11, 2024	39	9	Committee of the Whole
C28. Arwa Aziz Dungarpurwala., dated November 11, 2024	39	9	Committee of the Whole
C29. Memorandum from the Deputy City Manager, Planning, Growth Management and Housing Delivery, dated November 15, 2024.	37	2	Committee of the Whole
<u>Distributed November 18, 2024</u>			
C30. Nick Pinto, The WWHA, Inc., dated November 18, 2024	38	4	Committee of the Whole (Public Meeting)
C31. Lisa Martino, Strawberry Hill Crt., Bolton, dated November 16, 2024.	37	16	Committee of the Whole
C32. Amanda Corbett, Bolton, dated November 18, 2024.	37	16	Committee of the Whole
C33. Memorandum from the Deputy City Manager, City Treasurer and Chief Financial Officer, dated November 18, 2024.	39	22	Committee of the Whole
C34. Confidential memorandum from the City Manager and the Deputy City Manager, City Treasurer and Chief Financial Officer, dated November 18, 2024.	39	22	Committee of the Whole

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Please note there may be further Communications.

**C1
Communication
Council – November 19, 2024
CW(1) – Report No. 37 Item No. 16**

From: [Adelina Bellisario](#)
To: [Adelina Bellisario](#)
Subject: FW: [External] Nov. 5th Delegation agenda 5.2
Date: November-04-24 2:59:50 PM

From: marisa parise <[REDACTED]>
Sent: Monday, November 4, 2024 1:29 PM
To: Clerks@vaughan.ca
Subject: [External] Nov. 5th Delegation agenda 5.2

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Good afternoon,

As the safety issues increase exponentially for Vaughan/Bolton especially along Albion Vaughan Townline road safety continues to be a concern with transport trucks, drivers and illegal truck yards.

We are concerned residents delegating this matter, please add my name to the file of complaints.

Thank you,

Marisa Parise

Mortgage Broker

Ph: (416)-305-SAVE...(7283)

Email: save@marisamortgage.com

www.marisamortgage.com

Mobile Mortgage Calculator: <http://m.mortgageweb.ca/marisaparis>

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Please only send to domain email address: save@marisamortgage.com

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From: [Adelina Bellisario](mailto:Adelina.Bellisario@vaughan.ca)
To: [Adelina Bellisario](mailto:Adelina.Bellisario@vaughan.ca)
Subject: FW: [External] RE: Nov. 5, 2024 - Carmela Palowski - Illegal truck yards in Vaughan bordering on Caledon backyards (12151 Albion Vaughan Rd.)
Date: November-04-24 3:00:25 PM

From: Cosimo Napoli <Cosimo.Napoli@caledon.ca>
Sent: Monday, November 4, 2024 1:14 PM
To: Franca Pisani [REDACTED] <Clerks@vaughan.ca>
Cc: ccrsa.group@gmail.com; Annette Groves <Annette.Groves@caledon.ca>; Mario Russo <Mario.Russo@caledon.ca>; Tony Rosa <Tony.Rosa@caledon.ca>; mayor@vaughan.ca
Subject: [External] RE: Nov. 5, 2024 - Carmela Palowski - Illegal truck yards in Vaughan bordering on Caledon backyards (12151 Albion Vaughan Rd.)

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Dear Franca,

Thank you for your email and for sharing your concerns. We appreciate your commitment to the safety of our community and your recognition of our collaborative efforts.

Please rest assured that we will continue to work together to address these issues and seek effective solutions for the residents of Vaughan, Caledon, and Brampton.

Best regards,
Cosimo Napoli
Councillor, Ward 6
Town of Caledon

Cell: 416-371-5454

Email: cosimo.napoli@caledon.ca

Town of Caledon | www.caledon.ca | www.visitcaledon.ca | Follow us @TownOfCaledon



From: Franca Pisani <[REDACTED]>
Sent: Friday, November 1, 2024 10:40 PM
To: clerks@vaughan.ca
Cc: ccrsa.group@gmail.com; Annette Groves <Annette.Groves@caledon.ca>; Mario Russo <Mario.Russo@caledon.ca>; Tony Rosa <Tony.Rosa@caledon.ca>; Tony Rosa <Tony.Rosa@caledon.ca>; Mayor@vaughan.ca; Cosimo Napoli <Cosimo.Napoli@caledon.ca>
Subject: Nov. 5, 2024 - Carmela Palowski - Illegal truck yards in Vaughan bordering on Caledon backyards (12151 Albion Vaughan Rd.)

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the contents to be safe.

Dear Mayor Del Duca and Council Members,

I would like to thank Mayor Del Duca for collaborating with Mayor Groves and Mayor Brown in this very urgent matter before the residents of Vaughan, Caledon and Brampton. All Mayor's and Council members are showing the residents that you are all united in this urgent matter for the safety of their residents.

I am a long-time resident of Bolton, and I am writing to express my deep concerns regarding the illegal truck yards that are threatening our communities safety. The situation we are facing is beyond what any resident should have to endure. We are currently surrounded by these illegal operations. I have already lost two friends in accidents involving large commercial trucks. So you can understand my extreme concerns.

A particular soon to be truck yard at 12151 Albion Vaughan Road is set to open soon, and I urge The City of Vaughan to make sure that it does not open to commercial vehicles or containers. This address is zoned for agriculture.

I recently had a close call while driving north on Cold Creek Road on October 31st. I managed to avoid an accident when a truck turned right from Kirby Road to go southbound onto Cold Creek Road (picture attached from my dash cam). As I was approaching, and without regard that I had the right of way, he took both northbound and southbound lanes to make his turn. Luckily, I was able to slow down enough and avoid a head on collision. This truck was going to park at one of the many illegal truck lots on Cold Creek Road. I have also attached a picture of a property in that area that has an application in for a zoning change that should not be passed.

Albion Vaughan Road is of similar width to Cold Creek Road but experiencing significantly heavier traffic with vehicles and trucks exceeding speeds of 80 km/h, and during rush hour there is barely a gap between vehicles traveling northbound and southbound. There are residents backyards just across from this soon to be truck yard. It would be devastating to see an accident that pushes a truck into someone's backyard.

The thought of hundreds of trucks entering and exiting from the yard on Albion Vaughan Road, merging onto a two-lane road that is already heavy with traffic, is terrifying.

I also worry that many of the drivers may be unqualified and that many of the trucks may be unfit for the road.

I urge you to take immediate action to ensure that the property at 12151 Albion Vaughan Road does not open for commercial vehicle operations or container storage as this poses a significant risk to all resident's safety.

The fill being brought into these illegal truck yards poses a serious risk of soil contamination, particularly if it's not properly regulated. This, combined with potential diesel fuel leaks, could render the land unsuitable for future farming and impose high cleanup costs on developers, assuming it can even be restored. The long-term impacts could be both environmental and financial.

Additionally, Provincial permission needs to be granted to all Mayors to barricade the entrances and exits of all illegal truck yards. The fines imposed for these illegal operations are often viewed as merely a "cost of doing business" or ignored altogether.

I want to extend my heartfelt thank you to our Mayor Annette Groves, Councillor Tony Rosa, Councillor Cosimo Napoli and Councillor Mario Russo, for their unwavering dedication they have shown to this matter. At first, I didn't fully grasp or appreciate just how much work Mayor Groves and her council have put in over the years, but it's become clear how deeply committed they have been. Mayor Groves has been tirelessly hard at work and I am truly grateful for her efforts and those of her Council.

It is now wonderful to see three dedicated and influential Mayors collaborating to prioritize the safety and well-being of their communities. My heartfelt gratitude to each of you for your hard work, commitment and teamwork.

Franca Pisani

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C3

Communication

Council – November 19, 2024

CW(1) – Report No. 37 Item No. 16

From: [Adelina Bellisario](mailto:Adelina.Bellisario@osc.gov.on.ca)
To: [Adelina Bellisario](mailto:Adelina.Bellisario@osc.gov.on.ca)
Subject: FW: [External] Agenda 5.2 - November 5, 2024
Date: November-04-24 3:00:45 PM

From: Salvina Greco <SGreco@osc.gov.on.ca>
Sent: Monday, November 4, 2024 1:03 PM
To: Clerks@vaughan.ca; Marilyn lafrate <Marilyn.lafrate@vaughan.ca>; mayor@vaughan.ca
Subject: [External] Agenda 5.2 - November 5, 2024

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To Whom It May Concern,

Subject: Illegal Trucking/Container Yard Development and Safety Concerns on Albion Vaughan Road

I am writing as a concerned citizen of Bolton, Ontario, to raise urgent concerns about the illegal trucking and container yards multiplying in our community. In particular, I want to bring your immediate attention to two new trucking yards currently being developed on **Albion Vaughan Road**, just **100 feet south of Dovaston Gate** (the entrance to our subdivision) on the east side, within York Region (the green belt). The second one, 100 feet after this yard, (at the train tracks), a previous interlocking company grading the land for a truck/container lot.

It appears that the landowners are proceeding with grading the land under the assumption that it can be converted into a trucking/container yard without proper authorization or community consultation. This development is alarming and unacceptable for several reasons:

??? **Safety Risks on Hwy 50 and Albion Vaughan Road**

Hwy 50 and surrounding roads have already become extremely dangerous due to the overwhelming number of trucks, leading to numerous fatal accidents. With new trucking yards at the entrance of our subdivision, the risk to residents, especially young drivers like my children, will increase exponentially. Albion Vaughan Road serves as the only entrance and exit for our community, and adding heavy truck traffic will further endanger residents.

??? **Impact on Property Values and Quality of Life**

The presence of these trucking and container yards is driving down property values in the area. The peace and safety of our residential neighborhood are being compromised, which is unfair to families who chose Bolton for its livability and community atmosphere.

??? **Violation of Local Zoning and Bylaws**

It is my understanding that this site is not zoned for industrial use, yet the land is already being

graded, and containers have appeared on the property. It seems that some of these operations exploit the boundary between Vaughan and Peel Region, affecting residents in Bolton and Caledon without appropriate oversight or consideration.

I respectfully request that the appropriate authorities **visit the site immediately to investigate and enforce zoning bylaws**. Giving out fines is just not sufficient. These trucking yard development need to be shut down to protect the safety and well-being of our community.

Thank you for your attention to this matter.

Sincerely,
Salvina Greco
Bolton, Ontario

Salvina Greco | Ontario Securities Commission | Investment Management | Review Officer
20 Queen Street West, 22nd Floor | Toronto ON M5H 3S8

Tel: 416-593-8181
sgreco@osc.gov.on.ca

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[Ontario Securities Commission](#)

From: [Adelina Bellisario](#)
To: [Adelina Bellisario](#)
Subject: FW: [External] Agenda 5.2 Nov 5 2024 please help us!
Date: November-04-24 3:01:05 PM

From: Dee G [REDACTED]
Sent: Monday, November 04, 2024 12:50 PM
To: Clerks@vaughan.ca
Subject: [External] Agenda 5.2 Nov 5 2024 please help us!

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Hello!

As you may be aware, there are several ILLEGAL truck yards opening up along Albion Vaughan Road, between Hwy 50 and King on the East side, which is technically Vaughan so they fall under your jurisdiction.

These ILLEGAL truck yards are causing **SIGNIFICANT safety risks due to the high volume of transport truck traffic they bring to our residential roads**, health hazards and hardships to the residents of Bolton, particularly the ones that border that road. Not to mention that they will be affecting our property value.

On behalf of all of us Bolton residents, we implore your town council to PLEASE SHUT THESE ILLEGAL TRUCK YARDS DOWN! **YOU WILL SAVE LIVES!**

These illegal truck yards need to move somewhere else that is zoned for such use and located in a commercial area, not right beside our homes!!

Thank you for your time and consideration.

Darlene Gray

**C5
Communication
Council – November 19, 2024
CW(1) – Report No. 37 Item No. 16**

From: [Adelina Bellisario](#)
To: [Adelina Bellisario](#)
Subject: FW: [External] Road safety and illegal truck yards
Date: November-04-24 3:11:37 PM

From: BERNIE ABRAMOVITCH [REDACTED]
Sent: Monday, November 04, 2024 12:50 PM
To: Clerks@vaughan.ca
Subject: [External] Road safety and illegal truck yards

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[Sent from Rogers Yahoo Mail on Android](#)

Hello

I'm writing because there is a great concern about the above mentioned. It's a growing problem that keeps getting bigger and bigger and seems out of control and our communities of Caledon, vaughan, brampton, well basically all ontraio are trying to come together to try and stop the non sence. Our lives,our kids lives and our communities are at steak. We have lost lives and land to the illegal acts of the so called bad actors that are infiltrating our lives and community. I ask that you listen very seriously to our protest and take all that is being put in front of you to a high degree of consideration.

Regards

Loredana Abramovitch

Hide trimmed content

C6

Communication

Council – November 19, 2024

CW(1) – Report No. 37 Item No. 16

From: [Adelina Bellisario](#)
To: [Adelina Bellisario](#)
Subject: FW: [External] Illegal truck yards Bolton
Date: November-04-24 3:11:50 PM

From: Konstantina Tsitsias [REDACTED]
Sent: Monday, November 04, 2024 12:24 PM
To: Clerks@vaughan.ca
Subject: [External] Illegal truck yards Bolton

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Good Afternoon,

I want to bring to your attention my concerns about all the illegal truck yards in my town of Bolton. I live in the area back up to Albion Vaughan Rd and it has become a death trap. Actually all of Bolton has become a death trap. This poor community is surrounded by these illegal container yards which pose such a safety hazard to our community. Most of these drivers do not have proper training nor licenses and are causing major accidents as they are blowing through red lights, not following the laws of the road.

I moved to Bolton 12 years ago and it has changed so much. It is covered in trucks and containers

I drive everyday praying that nothing happens. I have an 18 year old and I fear for her safety.

Please do something about this!!

Konstantina

From: [Adelina Bellisario](#)
To: [Adelina Bellisario](#)
Subject: FW: [External] Illegal truck yards border Caledon Vaughan: Agenda 5.2 Nov 5 delegation
Date: November-04-24 3:12:26 PM

From: Harsimarjit Singh [REDACTED]
Sent: Monday, November 04, 2024 12:35 PM
To: Clerks@vaughan.ca
Subject: [External] Illegal truck yards border Caledon Vaughan: Agenda 5.2 Nov 5 delegation

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Hi

I am writing this email to make the city aware of the illegal truck yards popping up on the border of Caledon and Vaughan. These truck yards operates illegally and the trucks parked in these truck yards are safety issue to the general public due to their reckless driving and blatant disregard to the rules. Please look into the situation. Thanks.

Regards
Simar Walia

From: [Adelina Bellisario](#)
To: [Adelina Bellisario](#)
Subject: FW: [External] Re: Agenda 5.2 November 5th 2024
Date: November-04-24 3:12:50 PM

From: Adwoa Kesewa Appiah [REDACTED]
Sent: Monday, November 04, 2024 12:38 PM
To: Clerks@vaughan.ca
Subject: [External] Re: Agenda 5.2 November 5th 2024

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Hello,

I live in the Bolton gateway area and the trucks on Albion Vaughan road go at such a high speed that residents getting out of our neighborhood and the Dovaston road neighborhood are always scared.

School buses ply this road so imagine parents concern and worry each time we hear the trucks.

These heavy trucks speed on the Albion-Vaughan road day and night at such high speed we can barely sleep with our windows opened.

Besides the noise pollution, there is the constant trucks fuel pollution for resident living besides these roads.

Also because there are many trucks plying this road illegal truck depots are popping out on farm lands near this road and these truck yards can serve as a place for keeping cars stolen from our neighborhood- my neighbour had teenagers break into her house last year demanding their car keys. These yards are breeding car theft in our neighborhood.

I moved here almost 6 years ago and OPP/YRP used to patrol this road but not anymore. We need some calmness on this road especially where there are houses and the illegal truck depots gone.

Thank you for listening.

Kind regards,
Adwoa

[Yahoo Mail: Search, Organize, Conquer](#)

C9
Communication
Council – November 19, 2024
CW(1) – Report No. 37 Item No. 16

From: [Adelina Bellisario](#)
To: [Adelina Bellisario](#)
Subject: FW: [External] Agenda. 5.2
Date: November-04-24 3:12:51 PM

From: carmelo galluccio [REDACTED]
Sent: Monday, November 04, 2024 12:30 PM
To: Clerks@vaughan.ca
Subject: [External] Agenda. 5.2

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We are having a serious issue with these truck yards opening up directly behind our house and with zero help from anyone. We cant sleep anymore, cant go in our back yards, Nothing. I counted 12 in a 1 km stretch of road and have 2 more opening up right at my bedroom window. We cant do this anymore!!! We are tired from the noise, and scared. Please see attached photos from my bedroom. I have 13 more pictures from the 1km stretch of road but I can't email them all at once. I will send a second email . PLEASE HELP US!!!! WE ARE BEGGING.

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C10

Communication

Council – November 19, 2024

CW(1) – Report No. 37 Item No. 16

From: [Adelina Bellisario](mailto:Adelina.Bellisario)
To: [Adelina Bellisario](mailto:Adelina.Bellisario)
Subject: FW: [External] ILLEGAL TRUCKING YARDS - CALEDON/BOLTON
Date: November-04-24 3:13:02 PM

From: Wendy Ficociello <wficociello@lanterracon.com>
Sent: Monday, November 04, 2024 12:48 PM
To: Clerks@vaughan.ca
Subject: [External] ILLEGAL TRUCKING YARDS - CALEDON/BOLTON

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To whom it May Concern

There is a never ending stream of illegal trucking yard enveloping our town along Albion Vaughan, Cold Creek Road, Mayfield just to name a few

We in Caledon are frustrated every week a new yard opens up.

These are causing families to loose loved ones, not to mention the ill affects of environmental and infrastructure implications

Our lives are being adversely affected not to mention our property values being diminished. This is the City of Vaughan's fault

We expect action from our neighbor and would expect that our neighbor would look out for our best interest

These truck yards need to be closed immediately. Has anyone checked if the lands are even zoned for this use. Probably note.

Who keeps issuing permits for this. How many more lives in Caledon need to be lost at the hands of the City of Vaughan

Shame on your Vaughan for allowing this happy on our doors step. You want these yards, put them elsewhere on the abundance of Vacant lands available in your municipality

Regards

Wendy Ficociello

C11

Communication

Council – November 19, 2024

CW(1) – Report No. 37 Item No. 16

From: [Adelina Bellisario](#)
To: [Adelina Bellisario](#)
Subject: FW: [External] PLEASE STOP Illegal Truck Yards and Help our Town Safety
Date: November-04-24 3:13:17 PM

From: Darren and Sonia Cool [REDACTED]
Sent: Monday, November 4, 2024 2:24 PM
To: Clerks@vaughan.ca
Subject: [External] PLEASE STOP Illegal Truck Yards and Help our Town Safety

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To Whom it May Concern,

Vaughan has allowed (or not allowed) numerous trucking yards to operate on the border of Vaughan/Caledon. This has turned our town, one of which I have been living in for the last 20 plus years into a dumpster town with nothing but c-cans and contaminated land for certain.

Home properties along Kirby, Huntington are parking lots for 18 wheelers. They run red lights on a daily, no intention of stopping or slowing down for miles. This is what we deal with everyday. You take a chance on the roads, a big risk when driving and especially for those who have kids and licences, it's scary.

Please, please help stop this madness. We are inundated with trucks and truck yards, many of which are not proper licenced behind the wheel nor are they legal yards.

Thank you for your time.

Concerned resident and mother of two.

From: [Adelina Bellisario](#)
To: [Adelina Bellisario](#)
Subject: FW: [External] RE: 12151 Albion Vaughan Line - City of Vaughan
Date: November-04-24 3:13:30 PM

From: Toni Colucci [REDACTED]
Sent: Monday, November 4, 2024 2:23 PM
To: annette.groves@caledon.ca; doug.fordco@pc.ola.org; Clerks@vaughan.ca
Subject: Fwd: [External] RE: 12151 Albion Vaughan Line - City of Vaughan

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Hello,

My name is Mrs. Colucci, and I've sent many emails including many calls regarding the illegal truck lands, especially the one behind my house.

I would like to express my concerns regarding this. Not sure how people are allowed to commit illegal acts and our towns/cities are letting them.

Yesterday, Sunday is one of my days off and to enjoy being home and quiet. All I can hear is the bobcat going back and forth. I have videos. I did call the Vaughan bylaw and left a message. Nothing was done about it. No one returned my call. So basically we are allowing people to commit crimes and the people who live nearby have to just accept this. This is absolutely absurd. Why don't our politicians care about us?? There needs to be an environmental assessment done to make sure that the noise levels and the air pollution levels will not interfere with our health! Why are we allowing them to set up shop in a residential area??

I would appreciate someone calling me or emailing me back with the status of this illegal truck depot.

[REDACTED]

Nina Colucci
Have a great day!
Sent from my iPhone

Begin forwarded message:

From: Toni Colucci <TColucci@varconconstruction.com>
Date: November 4, 2024 at 11:02:34 AM EST
To: Toni Colucci [REDACTED]
Subject: FW: [External] RE: 12151 Albion Vaughan Line - City of Vaughan

From: Toni Colucci <tcolucci@Varconconstruction.com>
Sent: May 31, 2024 3:16 PM
To: Toni Colucci <tcolucci@Varconconstruction.com>
Subject: FW: [External] RE: 12151 Albion Vaughan Line - City of Vaughan

From: Miriam Anania <Miriam.Anania@vaughan.ca>
Sent: Thursday, February 1, 2024 9:07 AM
To: Toni Colucci <tcolucci@Varconconstruction.com>
Subject: FW: [External] RE: 12151 Albion Vaughan Line - City of Vaughan

You don't often get email from miriam.anania@vaughan.ca. [Learn why this is important](#)

[EXTERNAL]

Good morning, Mr. Colucci,

Please contact me at the phone number below regarding your concerns about this property.

Thank you,

Miriam Anania, M.L.E.O., C.P.S.O.

By-Law Enforcement Officer

Tel. - 905-832-8505, ext. 8151 / miriam.anania@vaughan.ca

Fax - 905-832-8549

City of Vaughan / By-Law and Compliance, Licensing & Permit Services

2141 MacKenzie Dr., Vaughan, ON L6A 1T1

www.vaughan.ca

From: Toni Colucci <tcolucci@Varconconstruction.com>
Sent: Wednesday, January 31, 2024 2:02 PM
To: bylaw.licensing <ByLaw.Licensing@vaughan.ca>
Cc: Toni Colucci [REDACTED]
Subject: [External] RE: 12151 Albion Vaughan Line - City of Vaughan

CAUTION! This is an external email. Verify the sender's email address and carefully examine any links or attachments before clicking. If you believe this may be a phishing email, please use the Phish Alert Button.

My name is Toni Colucci, and I am writing to you to ask for your assistance in an urgent

matter.

As a resident of Bolton and neighbor to City of Vaughan, Kleinburg; I have to say we are very concerned.

We moved to Bolton 32 years ago and we were very fortunate to be able to purchase a home on Waterbury Street 25 years later.

We have loved living where we are because we had this beautiful view on the City of Vaughan, Kleingburg field. We had been told that eventually the City of Vaughan was going to build homes on this land and we were happy about it. As residents of Bolton/Caledon; we have seen many changes and some have been good and some not so great.

But overall, it has been a really nice place to live and raise my family here. I am sure that you are aware that there has been many illegal areas that have been set up for Containers, Trucks or other.

On January the 12th through January 14th (Friday, Saturday, and Sunday) the neighbour across the street from our home, 12151 Albion Vaughan Line, started excavating the lot next to their home.

They have dugged up the soil and they were moving the dirt around the area and making big piles especially in front of Albion Vaughan. They made the pile so high that when driving by you cannot see behind hit. Why??

This land is Zoned as Agriculture and only allows a maximum of 2 dwellings. Also, this land is part of the Oak Ridges Moraine which has very stringent regulations for any municipal processes.

I have attached pictures, and you can see they have a Digger, Loader, and Excavator on the property. They also worked on the 19th of January as well. I have attached a picture I took on the 21st of January so that you can see the barrier they have built.

My concern is that they might have intentions to make this some sort of yard for trucks, containers, or landscaping companies. Any of the above will impact the residents of Bolton, especially the ones who drive back on to Albion/Vaughan Road.

The Road is already overloaded by heavy trucks that the road passing the train tracks are constantly needing repair and very difficult to drive on with your vehicle. My concern is if they decide to park large trucks or containers, there will be a noise issue along with the air quality. How will we be able to sit in our back yards?

Can you please explain why the trucks and workers are on this site and what is their plan and have there been or will there be any formal applications for re-zoning.

Your attention to this matter is greatly appreciated and the by-law department for the City of Vaughan I am hoping that you can help the residents of Bolton.

Sincerely,

Toni Colucci

This e-mail, including any attachment(s), may be confidential and is intended solely for the attention and information of the named addressee(s). If you are not the intended recipient or have received this message in error, please notify me immediately by return e-mail and permanently delete the original transmission from your computer, including any attachment(s). Any unauthorized distribution, disclosure or copying of this message and attachment(s) by anyone other than the recipient is strictly prohibited.





C13

Communication

Council – November 19, 2024

CW(1) – Report No. 37 Item No. 16

From: [Adelina Bellisario](#)
To: [Adelina Bellisario](#)
Subject: FW: [External] Agenda 5.2
Date: November-04-24 3:13:58 PM

From: carmelo galluccio <[REDACTED]>
Sent: Monday, November 04, 2024 12:32 PM
To: Clerks@vaughan.ca
Subject: [External] Agenda 5.2

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Please see attached more photos of the illegal truck yards that are directly in a residential area. We need help. We are tired from the noise, stressed because city of vaughan won't listen or do anything besides just "discuss "

Get [Outlook for Android](#)



**NO
TRESPASSING**





V69TAS004
P52177









WELLINGTON
MOTOR FREIGHT

Wolf Pack
LOGISTICS



C14
Communication
Council – November 19, 2024
CW(1) – Report No. 37 Item No. 16

From: [Adelina Bellisario](#)
To: [Adelina Bellisario](#)
Subject: FW: [External] TRUCK YARDS ON ALBION VAUGHAN
Date: November-05-24 10:05:03 AM

From: Melissa Jesso [REDACTED]
Sent: Monday, November 4, 2024 6:36 PM
To: Clerks@vaughan.ca; mayor@vaughan.ca; Marilyn lafrate <Marilyn.lafrate@vaughan.ca>
Subject: [External] TRUCK YARDS ON ALBION VAUGHAN

CAUTION! This is an external email. Verify the sender's email address and carefully examine any links or attachments before clicking. If you believe this may be a phishing email, please use the Phish Alert Button.

Good evening,
Please investigate these truck yards on Albion-Vaughan, these trucks are making the area dangerous for our families.

Thank you,
melissa

C15

Communication

Council – November 19, 2024

CW(1) – Report No. 37 Item No. 16

From: [Adelina Bellisario](#)
To: [Adelina Bellisario](#)
Subject: FW: [External] Agenda 5.2 November 5th delegation
Date: November-05-24 10:05:05 AM

From: Elizabeth Ardito <[REDACTED]>
Sent: Monday, November 4, 2024 7:07 PM
To: Clerks@vaughan.ca
Cc: Elizabeth Ardito <[REDACTED]>
Subject: [External] Agenda 5.2 November 5th delegation

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Good evening,

I would to express our concerns about road safety and the Illegal truck yards bordering Caledon/Vaughan.

I'm referring to the two new truck yards on Albion Vaughan Road north of the railroad tracks and south of the King/Vaughan Road. The one address is 12001 Albion Vaughan Road. Both locations have driveways on Albion Vaughan Road.

The added trucks on this road, pulling in and out of these driveways will add additional dangers to the community of Bolton. I drive this road on a daily basis and remember the incidents that have occurred at the intersection of King/Vaughan Road and Dovaston Gate. One was the loss of a older gentleman to a bicycle vs dump truck crash and the other one was two teenagers walking and being hit by a truck.

Please stop these truck yards before they begin. Our community and children depend on it.

Thank you in advance for your consideration.

Kind regards,
Elizabeth & Eugenio Ardito
[REDACTED] Sant Farm Drive
Bolton, ON

[REDACTED]
[REDACTED]

C16
Communication
Council – November 19, 2024
CW(1) – Report No. 37 Item No. 16

From: [Adelina Bellisario](#)
To: [Adelina Bellisario](#)
Subject: FW: [External] Illegal Trucking Yards.
Date: November-05-24 10:05:22 AM

From: Susan Scott [REDACTED]
Sent: Monday, November 4, 2024 9:24 PM
To: Clerks@vaughan.ca
Subject: [External] Illegal Trucking Yards.

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Good evening,

I am writing this email with regards to the illegal tracking yards in Vaughan on the borderline of the town of Bolton.

My concern is trucks, which continue to stage along Cold Creek drive. This area is clearly marked as no stopping, but trucks continue to disregard this rule. Along the sides of these roads we are finding trucks that have laid down in the ditch because they can't make the turns. These roads and shoulders cannot support these transports

My next concern is that I'm a frequent traveler of these roads surrounding town. Now at the side of the road in the ditches I am finding on the regular bottles filled with urine. I also hike the conservation lands surrounding town and constantly find truckers who are pulling over to defecate in the woods and leave big piles of toilet paper. Obviously these properties are not providing proper washroom facilities. I have driven by the yard that is currently at the southwest corner of Cold Creek and Kirby and witnessed men urinating on the road with their penis in full view.

Traffic is a nightmare. Trucks cannot make the turn from Albion Vaughan to Kirby and vice versa.

Please care about the far corners of your jurisdiction. These lots are operating at all hours of the night. My understanding is they are fronts for criminal activity. The drivers of these vehicles are not following road rules.

Safety of Boltons residents should be taken into consideration.

Thank you for your time,

Susan Scott



[Yahoo Mail: Search, Organize, Conquer](#)

C17
Communication
Council – November 19, 2024
CW(1) – Report No. 37 Item No. 16

From: [Adelina Bellisario](#)
To: [Adelina Bellisario](#)
Subject: FW: [External] Road safety
Date: November-05-24 10:06:54 AM

From: Nicole Dumanski <n[REDACTED]>
Sent: Monday, November 4, 2024 11:07 PM
To: Clerks@vaughan.ca
Subject: [External] Road safety

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Hello, please take this email into consideration for the safety of our Bolton community. I hate feeling like my life and my family's is at risk when driving in and out of this town everyday for work.

Please, please make some progress for change, and prevent any future traumas for members of our community.

From: [Adelina Bellisario](#)
To: [Adelina Bellisario](#)
Subject: FW: [External] Agenda Delegation Nov 5th, 2024: Carmela Palkowaski
Date: November-05-24 10:06:54 AM

From: Dev Gosal [REDACTED]
Sent: Tuesday, November 5, 2024 8:34 AM
To: Clerks@vaughan.ca
Subject: [External] Agenda Delegation Nov 5th, 2024: Carmela Palkowaski

CAUTION! This is an external email. Verify the sender's email address and carefully examine any links or attachments before clicking. If you believe this may be a phishing email, please use the Phish Alert Button.

Good Morning Mayor of Vaughan and City Councillors,

I am writing this email on behalf of myself and the frustrated residents of Bolton who are living in the close proximity of Albion Vaughan Rd. We do want to bring to your immediate attention about an illegal truck yard that is not wasting any time to establish itself right behind the homes of Bolton Community. This email is in part to an agenda item presentation by **Carmela Palkowaski on the 5th of Nov 2024**. Similar to other illegal truck yards in the surrounding areas of Bolton, Brampton and Vaughan, this development will too have a significant safety, environmental, and quality-of-life issues for our community, and we are requesting immediate intervention to prevent its further development.

The properties in question are as follows:

12152 Albion Vaughan Rd
12001 Albion Vaughan RD Interlock depot .

Over the recent weeks, the residents have witnessed an increase in development activity in these yards eg. bulldozing and flattening the land, bringing landfill which I assume is for parking trucks and erecting metal fencing. My understanding is that these properties are zoned agricultural and will be used as future truck yards which is inconsistent with zoning by laws, and as such it raises serious concerns regarding:

Increase in Traffic and Safety Risks: The increased truck traffic has already led to

congestion and dangerous road conditions due to illegal yards on Mayfield rd, Highway 50 and Albion King Rd (located opposite to our subdivision) and cold creek Rd due to existing 11 illegal yards on Albion King Rd.

Environmental Impact: The unregulated truck yard lacks appropriate infrastructure to handle issues such as waste disposal, drainage, and noise management. We are worried about potential long-term environmental damage, including contamination of local water sources and impact on air quality due to emissions.

Quality of Life for residents: The continuous noise, pollution, and visual intrusion from this illegal truck yard severely disrupt our neighborhood's peace and could potentially lower property values.

Our request is that the City of Vaughan Council take immediate action to address this issue, which is critical to preserving the safety and well-being of our community.

Specifically, we urge you to:

Investigate the activity at these locations to verify its compliance with zoning bylaws.

Enforce any zoning and environmental regulations that prevent unauthorized truck yard developments.

Provide regular updates on the progress of this issue and any actions taken by the city to stop unauthorized truck activities.

We, as residents, are committed to ensuring our neighborhood remains a safe and pleasant place to live, and we trust that the Council shares this commitment. Please let us know how we can support your efforts in resolving this matter.

Thank you for your immediate attention to this critical issue. We look forward to your prompt response and to seeing decisive action to protect our community.

Best Regards

Dev Gosal (she/her)

██████████

"If you are working on something that you really care about, you don't have to be pushed. The vision pulls you." - Steve Jobs

C19

Communication

Council – November 19, 2024

CW(1) – Report No. 37 Item No. 16

From: Clerks@vaughan.ca
To: [Adelina Bellisario](mailto:Adelina.Bellisario)
Subject: FW: [External] STOP THE TRUCKS
Date: November-05-24 1:35:32 PM
Attachments: [To Vaughan.pdf](#)

From: Cassandra Antenucci [REDACTED]
Sent: Tuesday, November 05, 2024 12:55 PM
To: Clerks@vaughan.ca
Subject: [External] STOP THE TRUCKS

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Hello,

Busy mom here. I apologize and understand that I am past the deadline, however, I hope you can read this letter.

Thank you!

Hello,

I am writing this on behalf of my husband and I. We are natives of Bolton, Ontario, having lived here for over 20 years, attending elementary and secondary school here, and we now have a 10 month old daughter. We are currently living on the South Hill of Bolton and have experienced the roads getting busier, trucks infiltrating our roads, accidents becoming more apparent and the overall small-town-feel disappearing. We both fear for our future, and more importantly, the future of our daughter here in our town.

Four months ago, one of my friends passed away in an accident on Highway 50 involving a truck and I am in a constant state of fear driving down Highway 50. My sister also works as an Emergency Room Nurse at Cortellucci Hospital, fifteen minutes away, and there are numerous accidents she tells our family about, involving trucks nearby. A man in his 60's that she was caring for last night is about to pass away from the injuries he suffered from their car cutting stuck under a truck. I worry when I am driving with my family, and I would not want my daughter to drive on these roads in the future.

If we happen to get another trucking lot in Bolton, we would have to seriously consider moving our family out of the town we grew up in and love.

We truly hope something can be done, as we dream to live in a town that is safe and flourishing- not one that puts our family and the rest of our community at risk every day.

Kindly,
Cassandra and Marc-Anthony Simonetta

From: [Assunta Ferrante](#)
To: [Adelina Bellisario](#)
Subject: FW: [External] Zoning By-law amendment File Z.24.031 (5850 Langstaff Rd.) / MZO.24.001 / OP.24.014
Date: November-07-24 9:23:58 AM

From: Lou Di Palma <[REDACTED]>
Sent: Tuesday, November 5, 2024 6:51 PM
To: Clerks@vaughan.ca
Cc: Adriano Volpentesta <Adriano.Volpentesta@vaughan.ca>; Lucy Cardile <Lucy.Cardile@vaughan.ca>
Subject: [External] Zoning By-law amendment File Z.24.031 (5850 Langstaff Rd.) / MZO.24.001 / OP.24.014

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Dear Office of the City Clerk - I am emailing to express my concern about the proposed development at 5850 Langstaff Road. This development is proposed to have 2,043 residential units with buildings ranging in height from 22-34 storeys. I am a constituent and live at 41 Umbria Crescent. I use the nearby local roads every day, while the 427 extension has relieved some traffic from highway 27, this area is still heavily travelled.

This level of density and height belongs in a Major Transit Station Area (MTSA). The Committee of the Whole meeting from May 30, 2024 "Official Plan Amendment (Protected Major Transit Station Areas)", File 26.18 adopted 20 MTSA locations in Vaughan, Highway 27 and Langstaff Road is not one of them. This development belongs near those MTSA stations. The area which is heavily travelled by various transport trucks because of the Costco distribution centre and the FedEx distribution centre **cannot handle another 2,000 vehicles from this development!!**

As a concerned citizen I ask the City of Vaughan to reconsider the size and scale of this development **it does not fit in with the existing scale.** For information I am also copying Councillor Volpentesta.

Thank you
Luigi Di Palma
[REDACTED]

C21

Communication

Council – November 19, 2024

CW(PM) – Report No. 38 Item No. 5

From: [Assunta Ferrante](#)
To: [Adelina Bellisario](#)
Subject: FW: [External] Insanity
Date: November-07-24 9:59:27 AM

From: Irene Ford <[REDACTED]>
Sent: Wednesday, November 6, 2024 9:50 PM
To: Haiqing Xu <Haiqing.Xu@vaughan.ca>
Cc: Council@vaughan.ca <Council@vaughan.ca>; Todd Coles <Todd.Coles@vaughan.ca>
Subject: [External] Insanity

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How could you possibly think this is reasonable? To announce at this at public meeting and not update the staff report?

Why would you not pull the agenda item and bring it back at the next public meeting. The public should not be asked to review this staff report.

As the Mayor indicated planning is complicated and we shouldn't be confusing people.

Irene

From: [Assunta Ferrante](#)
To: [Adelina Bellisario](#)
Subject: FW: [External] Letter to City Council -- zoning amendment (Z.21.002)
Date: November-07-24 9:28:18 AM

From: weicheng zhang [REDACTED] >
Sent: Tuesday, November 5, 2024 4:18 PM
To: Clerks@vaughan.ca
Subject: [External] Letter to City Council -- zoning amendment (Z.21.002)

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Dear City Council,

I live at [REDACTED] Sibella way .

I am aligned in the opposition to the proposed zoning amendment (Z.21.002) by Q Towers Limited Partnership for a 10-story and a 5-story building at the southwest corner of Major Mackenzie Drive and Fossil Hill Road.

Main concerns as below:

1. Overwhelmed Infrastructure and Traffic Congestion: Our neighborhood is already experiencing significant traffic congestion, will be severely impacted by this high-density development. The 11 elementary schools and 2 secondary schools within a 2 km radius, including Tommy Douglas Secondary School directly across from the proposed site. Traffic is often at a standstill from 7:30 AM to 11:00 AM and again from 2:00 PM to 6:00 PM. Adding hundreds of new residents and vehicles will only exacerbate the safety risks for students and residents.

2. Parking and Road Safety Issues: The influx of new residents and commercial visitors will drastically increase parking demands, spilling over to nearby residential streets. This would congest residential roads and create safety issues, particularly for pedestrians and children.

3. Strain on Community Infrastructure: The current infrastructure is not designed to support a large-scale development and density. The addition of hundreds of new residents and commercial spaces will place a strain on our local streets and public services, impacting the

accessibility and safety of our neighborhood. OPPOSITION TO ZONING AMENDMENT Z.21.002

4. Deviation from Original Neighborhood Plan: When purchasing our homes, we were informed that this vacant lot would be developed as a low-rise commercial plaza, aligning with the aesthetics of other local plazas. The proposed high-rise structures are inconsistent with this original plan.

5. Visual Disruption and Loss of Privacy: The proposed 10-story building will be visually dominate the neighborhood, reducing privacy for surrounding homeowners and disrupting the neighborhood's character. The high-rise structure will stand in stark contrast to our residential homes, severely impacting natural sunlight, the aesthetic appeal and quality of life in the area.

I urge the City Council to reject this amendment to preserve the safety, accessibility, and character of our neighborhood. We respectfully request that the development be reconsidered as a low-rise commercial plaza, ensuring it aligns with the neighborhood's original plan and infrastructure capabilities.

Thank you for considering.

Weicheng Zhang

From: [Assunta Ferrante](#)
To: [Adelina Bellisario](#)
Subject: FW: [External] About Proposed Mixed-Use Development Permit
Date: November-07-24 9:28:38 AM

From: F. M. [REDACTED]
Sent: Wednesday, November 6, 2024 10:40 AM
To: Clerks@vaughan.ca
Subject: [External] About Proposed Mixed-Use Development Permit

CAUTION! This is an external email. Verify the sender's email address and carefully examine any links or attachments before clicking. If you believe this may be a phishing email, please use the Phish Alert Button.

Hi there,

I am writing to formally dispute the permit for the proposed mixed-use development that includes a 10-story building with 336 residential units and 451 square meters of commercial use, and a 5-story building with 91 residential units. While we understand the need for urban growth, there are significant concerns regarding the suitability of this development in our area. These concerns include:

1. Disruption of Area Consistency and Character:

Introducing a 10-story building consisting primarily of low-rise structures is unprecedented in our neighborhood. Such a development would disrupt the architectural cohesion and visual harmony of the area, altering the community's character and aesthetic. A building of this height would stand out dramatically and change the area's skyline in a manner that many residents find undesirable.

2. Impact on Student Safety:

Our neighborhood is home to approximately four schools, making it a hub for students commuting on foot and by bike. The proposed development is likely to significantly increase population density and traffic congestion. This raises safety concerns for students who must navigate more crowded streets and potentially hazardous conditions during their daily commutes. We believe that adding such a high-density development near schools compromises the safety of our young residents.

3. Privacy Concerns for Students:

The proposed 10-story building would allow residents to overlook school grounds and areas where students gather, raising privacy concerns. Parents and community members have expressed discomfort with the idea that residents of such a tall building could observe student activities, whether during school hours or after. We feel that this could have unintended negative impacts on the well-being and privacy of our children. In light of these concerns, we respectfully request that the City Planning Department reconsider the scale and scope of this project and engage in further consultation with the local community to ensure that any new development aligns with the area's character and prioritizes the safety and privacy of its residents.

4. Strain on local infrastructure and Resources:

Adding a high-density residential and commercial structure will put increased strain on the already limited local infrastructure, such as roads, public transit, water supply, sewage systems, and emergency services. The current infrastructure may not support a sudden surge in population, leading to congestion, delays, and potential overuse that would degrade the quality of life for current residents.

5. Parking and Traffic Congestion Issues:

The proposed development's size suggests a sharp increase in vehicle traffic, yet the provided parking facilities may be insufficient, resulting in spillover into surrounding streets. This could intensify competition for parking spaces and worsen congestion, especially during peak hours. The influx of new residents and visitors to the commercial spaces will likely add strain to local roadways, making everyday commutes more challenging for current residents.

6. Potential for Increased Noise and Light Pollution:

With the development's proposed density, both noise and light pollution are expected to increase, particularly in the evenings and early mornings. This would disrupt the neighborhood's current quiet ambiance and could negatively impact residents' quality of life, especially those in proximity to the site.

There is a perception that the process leading to this permit lacked adequate consultation with residents, leaving many community members feeling unheard. A project of this magnitude requires transparency and community input to address local concerns effectively and ensure that developments align with the neighborhood's vision.

Thank you for your attention to this matter. We look forward to a thorough consideration of our concerns.

Sincerely,

Farzad Moradveisi

C24

Communication

Council – November 19, 2024

CW(PM) – Report No. 38 Item No. 5

From: Clerks@vaughan.ca
To: [Adelina Bellisario](mailto:Adelina.Bellisario)
Subject: FW: [External] Proposed Condo Development at Hwy 27 & Langstaff
Date: November-08-24 8:21:35 AM

From: fabiolabecerril@gmail.com [REDACTED]
Sent: Thursday, November 7, 2024 9:40 PM
To: mayor@vaughan.ca; Linda Jackson <Linda.Jackson@vaughan.ca>; Clerks@vaughan.ca; Council@vaughan.ca; Gila Martow <Gila.Martow@vaughan.ca>; Chris Ainsworth <Chris.Ainsworth@vaughan.ca>; Rosanna DeFrancesca <Rosanna.DeFrancesca@vaughan.ca>; Adriano Volpentesta <Adriano.Volpentesta@vaughan.ca>; Marilyn Iafrate <Marilyn.Iafrate@vaughan.ca>; Mario G. Racco <MarioG.Racco@vaughan.ca>; Gino Rosati <Gino.Rosati@vaughan.ca>; Mario Ferri <Mario.Ferri@vaughan.ca>
Subject: [External] Proposed Condo Development at Hwy 27 & Langstaff

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To whom it may concern,

I'm writing to voice my strong opposition to the three condo towers proposed for [5850 Langstaff Road](#), Part of Lot 11, Concession 8, and Part 1 on Reference Plan 65R-27642 (vicinity of Langstaff Road and Highway 27).

As a resident of the area, I strongly believe this type of development is inappropriate for our existing neighbourhood. With 2,034 residential units proposed for this area, and assuming at least 2 people live in each unit - that's over 4,000 people slated for this tiny section. The required services and infrastructure would be overwhelming for an already established community. Traffic on Highway 27 and adjacent roads is bad enough, and simply put - this area of the City can't handle that many more residents.

Condo towers of this height will block out the sun, reduce privacy, and further intensify an already built-out neighbourhood. High-density development such as this belongs in other areas of the City where high-rise development already exist, such as the Vaughan Metropolitan Centre, the surrounding area, and along major corridors such as Highway 7, where rapid bus transit and access to the subway exist today. Approving this proposal would accomplish the

opposite of what councillors are aiming to do - reduce traffic congestion and manage growth responsibly - particularly adjacent to an Environmental Protection Zone.

Approving this development will negatively impact the character of our neighbourhood - the reason I chose this area of the City.

I ask for your support not to approve these condo towers, and voice your opposition to the provincial government as well not to issue a Minister's Zoning Order (MZO).

Sincerely,

Fabiola Becerril

C25

Communication

Council – November 19, 2024

CW(PM) – Report No. 38 Item No. 6

From: Clerks@vaughan.ca
To: [Adelina Bellisario](mailto:Adelina.Bellisario)
Subject: FW: [External] About Q Towers
Date: November-11-24 8:27:06 AM

From: [REDACTED]
Sent: Friday, November 8, 2024 4:28 PM
To: Rosanna DeFrancesca <Rosanna.DeFrancesca@vaughan.ca>; Clerks@vaughan.ca; Judy Jeffers <Judy.Jeffers@vaughan.ca>; Andrea Buchanan <Andrea.Buchanan@vaughan.ca>
Cc: canemreasci@hotmail.ca
Subject: [External] About Q Towers

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Dear City Council,

Please find attached a community petition opposing the proposed zoning amendment Z.21.002 by Q Towers Limited Partnership to construct a high-density condominium development at the southwest corner of Major Mackenzie Drive and Fossil Hill Road. This document includes signatures from 115 residents (additional signatures being collected) expressing significant concerns regarding traffic congestion, safety, and neighborhood character.

In addition to the petition, we are submitting video footage that illustrates the severe congestion and safety risks currently present in our neighborhood, especially near the local high school during peak hours. These videos provide firsthand evidence of the traffic issues that would only worsen with the proposed development.

We urge the council to consider these concerns and prioritize the integrity and safety of our neighborhood in your decision-making process.

Thank you for your attention to this matter.

Sincerely,

Zeynep Asci

C26
Communication
Council – November 19, 2024
CW(2) – Report No. 39 Item No. 9

From: [Assunta Ferrante](#)
To: [Adelina Bellisario](#)
Subject: FW: [External] Dog park in Carville Mills, Ward 4
Date: November-12-24 3:40:00 PM
Attachments: [image001.png](#)

From: Anna Commisso <Anna.Commisso@vaughan.ca>
Sent: Monday, November 11, 2024 12:49 PM
To: Clerks@vaughan.ca
Cc: Daniel Shafro <[REDACTED]>; Chris Ainsworth <Chris.Ainsworth@vaughan.ca>
Subject: RE: [External] Dog park in Carville Mills, Ward 4

Please see below email to be added to the communications for the November 12 CW1 meeting, item 9, [OFF-LEASH DOG AREA PROGRAM UPDATE NOVEMBER 2024](#).

Thank you,

Anna Commisso
Executive Assistant
905-832-8585 x8198 | anna.commisso@vaughan.ca

City of Vaughan | Ward 4, Councillor Chris Ainsworth
2141 Major Mackenzie Drive., Vaughan, ON L6A 1T1



To subscribe to the Ward 4 E-Newsletter click [here](#)

From: Daniel Shafro [REDACTED]
Sent: Monday, November 11, 2024 10:06 AM
To: Anna Commisso <Anna.Commisso@vaughan.ca>; Chris Ainsworth <Chris.Ainsworth@vaughan.ca>
Subject: [External] Dog park in Carville Mills, Ward 4

CAUTION! This is an external email. Verify the sender's email address and carefully examine any links or attachments before clicking. If you believe this may be a phishing email, please use the Phish Alert Button.

Hi Anna,

I am writing this to you and Chris to bring your attention to the need for a dog park in Carville Mills. There is a suitable space in Carville Mills Park and I hope the site could be considered for a future dog park.

Thank you very much.

--

Daniel Shafro, Broker
RE/MAX Realtron Realty Inc.
Direct: 647-290-9082
Office: 905-764-6000
www.DanielShafro.com



From: [Assunta Ferrante](#)
To: [Adelina Bellisario](#)
Subject: FW: [External] Re: Urgent Need for Dog Park in Carville Mills (Block 11)
Date: November-12-24 3:40:19 PM
Attachments: [image001.png](#)

From: Anna Commisso <Anna.Commisso@vaughan.ca>
Sent: Monday, November 11, 2024 12:49 PM
To: Clerks@vaughan.ca
Cc: [REDACTED] Chris Ainsworth <Chris.Ainsworth@vaughan.ca>
Subject: FW: [External] Re: Urgent Need for Dog Park in Carville Mills (Block 11)

Please see below email to be added to the communications for the November 12 CW1 meeting, item 9, [OFF-LEASH DOG AREA PROGRAM UPDATE NOVEMBER 2024](#).

Thank you,

Anna Commisso
Executive Assistant
905-832-8585 x8198 | anna.commisso@vaughan.ca

City of Vaughan | Ward 4, Councillor Chris Ainsworth
2141 Major Mackenzie Drive., Vaughan, ON L6A 1T1



To subscribe to the Ward 4 E-Newsletter click [here](#)

From: Milana [REDACTED]
Sent: Monday, November 11, 2024 10:17 AM
To: Anna Commisso <Anna.Commisso@vaughan.ca>
Cc: Chris Ainsworth <Chris.Ainsworth@vaughan.ca>; Len Abelman <LAbelman@wzmh.com>; [REDACTED] N Merchant [REDACTED]
Subject: [External] Re: Urgent Need for Dog Park in Carville Mills (Block 11)

CAUTION! This is an external email. Verify the sender's email address and carefully examine any links or attachments before clicking. If you believe this may be a phishing email, please use the Phish Alert Button.

Hi Anna,

I hope this message finds you well. My name is Milana Shur and I am a dog owner and a resident of Ward 4.

I am reaching out to express my strong support for the proposal put forth by my fellow ward members to establish a designated dog park in our community.

A dog park would provide a safe, city-approved space for dogs to run off-leash, socialize, and enjoy the outdoors. This would benefit not only our pets but also enhance the sense of community among pet owners and other residents. With a designated area, we can ensure that dogs are exercised responsibly while minimizing their impact on other shared spaces.

Thank you for considering this initiative.

Best,

Milana Shur

On Sun, Nov 10, 2024 at 10:07 PM N Merchant <niral.merchant@gmail.com> wrote:

To: Anna Commisso, Executive Assistant to Councillor Chris Ainsworth

Subject: Urgent Need for Dog Park in Carrville Mills (Block 11)

Hello Anna,

I am reaching out as a proud resident of Ward 4 and Block 11 and a member of the Carrville Mills Ratepayers Association regarding the upcoming meeting on Dog Parks in Vaughan scheduled for November 12, 2024.

As a dog owner who walks my dog each and every day, and meets countless other members of our RatePayers Association in Block 11, we all wish to let you know that we desperately need a Dog park in our community. The other options are too far away or are already overpacked when we visit them.

The increase in dogs during COVID has caused this ripple effect, and our dogs deserve the right to be able to run freely in self contained off leash areas.

We immediately propose the funding of a Dog Park to be placed in the park

on Marc Santi known as Carrville Mills Park. The park has ample playground, soccer field, skateboard park and walking trails but there are portions of the park that are under-utilized which could easily be turned into off leash Dog Park areas, without interfering with the aforementioned uses.

We would be grateful if you could advocate for this proposal and prioritize it to address the growing needs of dog owners and their pets in our area.

Thank you very much for your consideration and support.

Warm regards,
Niral Merchant, Director
Carrville Mills RatePayers Association

N V Merchant

[REDACTED]
[REDACTED]

From: [Assunta Ferrante](#)
To: [Adelina Bellisario](#)
Subject: FW: [External] Dog Park request at Marc Santi park/ crimson common ground:
Date: November-12-24 3:40:39 PM

-----Original Message-----

From: Anna Commisso <Anna.Commisso@vaughan.ca>
Sent: Monday, November 11, 2024 12:50 PM
To: Clerks@vaughan.ca
Cc: Chris Ainsworth <Chris.Ainsworth@vaughan.ca>; Arwa Aziz [REDACTED]
Subject: RE: [External] Dog Park request at Marc Santi park/ crimson common ground:

Please see below email to be added to the communications for the November 12 CW1 meeting, item 9, OFF-LEASH DOG AREA PROGRAM UPDATE NOVEMBER 2024.

Thank you,

Anna Commisso
Executive Assistant
905-832-8585 x8198 | anna.commisso@vaughan.ca

City of Vaughan | Ward 4, Councillor Chris Ainsworth
2141 Major Mackenzie Drive., Vaughan, ON L6A 1T1

To subscribe to the Ward 4 E-Newsletter click [here](#)

-----Original Message-----

From: Arwa Aziz [REDACTED]
Sent: Monday, November 11, 2024 10:44 AM
To: Anna Commisso <Anna.Commisso@vaughan.ca>
Subject: [External] Dog Park request at Marc Santi park/ crimson common ground:

CAUTION! This is an external email. Verify the sender's email address and carefully examine any links or attachments before clicking. If you believe this may be a phishing email, please use the Phish Alert Button.

Dear Anna Commisso,

I m Arwa Aziz Dungarpurwala resident of the Carville Mills area of ward 4.
Origin Way Crimson Forest/ Marc Santi junction.

We have a super cute golden retriever who is lovable and friendly and with high energy n spirits.
We wish for him to explore and use his high energy to play with his friends and connect with other dog pals of same nature.
There is a plentiful area for human children to play that can be shared with our four legged animals, who don't have and many options for their emotional and physical wellbeing as our children do.

Our dogs are living beings too. Enforcing boundaries and and limiting them to parks that need us to drive 4 n 5 miles distance is not practical for working parents and doesn't help our dogs need to burn their energy and give them the full physical exercise that is their need. Effecting both their physical and emotional health and making it more difficult for dog owner to action their need to care for their pets.

This may be the one reason that dog owners may neglect or abandon their dogs. Our dogs are such wonderful beings who only know how to give love. And law enforcement restricts their needs or make it difficult to care for them.

Please help us make their lives better and give us some relief so we can manage from our busy schedules, the wellbeing of our children as well as our four legged animals with equal love n care.

They don't have a voice,

we are their only voice,

They need more physical activity than the children even.

The children don't care anymore to go out.

But dogs are always waiting all day waiting at our mercy to take them out.

That 20mins they deserve to run freely.

Their life span is limited to 10 or 12 years.

Pls I request you think compassionately toward our animals needs n give us a dog park in the area.

The Carville Mills park is close by and in the back triangular area behind the soccer field is plentiful area to become a dedicated dog park that can be actioned with ease and efficiently.

Regards

Arwa Aziz Dungarpurwala.





C29

Communication

Council – November 19, 2024

CW(1) – Report No. 37 Item No. 2

DATE: November 15, 2024

TO: Mayor and Members of Council

FROM: Haiqing Xu, Deputy City Manager, Planning, Growth Management and Housing Delivery

RE: COMMUNICATION – COUNCIL, November 19, 2024

Item # 2, Report # 37

**NOTICE OF OBJECTION TO THE NOTICE OF INTENT TO
DESIGNATE KIRBY HOUSE LOCATED AT 2480 KIRBY ROAD
UNDER PART IV OF THE ONTARIO HERITAGE ACT**

Recommendation

1. THAT Council consider the Notice of Objection dated August 6, 2024, in conjunction with the Building Condition Assessment dated November 14, 2024 and withdraw its decision of May 22, 2024, to designate the subject property at 2480 Kirby Road under Part IV of *Ontario Heritage Act*;
2. THAT Council require the owner to enter into a letter of undertaking as a condition of Heritage Clearance for demolition to erect a commemorative display, material salvage and reuse, in a manner that recognizes and carries forward the legacy of the Kirby House and its significance to the community to the satisfaction of the City.

Background

2480 Kirby Road has been a recognized significant heritage building since 2005 as a listed property under Section 27 of the *Ontario Heritage Act*. City staff research on the subject property has confirmed that the cultural heritage value of 2480 Kirby Road meets eight (8) out of nine (9) criteria set out under OHA Regulation 9/06 for physical, associative and contextual cultural heritage value. A complete designation report that outlines these values was presented to Heritage Vaughan Committee on April 24, 2024, recommended to Committee of the Whole on May 7, 2024, and approved by City Council on May 22, 2024.

A Notice of Objection to the Notice of Intent was submitted by the Owner's representative that stated the condition of the building being "very poor" and "beyond reasonable and practical repair" (Attachment 1). A report from staff was presented to Committee of the Whole on November 5, 2024, in response to the Notice of Objection. The Committee deferred the report back to staff in light of additional information that will

be provided by the Owner and asked of the owner and representative(s) that access be granted onto the property for staff inspection of the site. Staff visited the site on November 04, 2024, and provided photos (Attachment 2), which show brick spalling on the kitchen wing of the building.

A new Building Condition Assessment Report authored by Sandro Soscia, a licensed Professional Engineer specialized in building structure, was submitted to the City on November 14, 2024 (Attachment 3).

Analysis and Options

The Building Condition Assessment Report states that *“This structure has undergone significant deterioration and does not meet the structural requirements of a dwelling”* and *“To make the building habitable, a complete reconstruction is necessary”*.

The report concludes that *“The building does not meet the minimum acceptable standards for public health and public safety, structural sufficiency, environmental integrity and energy conservation. We recommend demolition of 2480 Kirby Road, City of Vaughan.”*

In addition to the Conditions Assessment Report the Owner’s representative had also submitted a communication to the Committee of Whole that was prepared by heritage consultants from LHC (Attachment 4), questioning the Notice of Intent to Designate (NOID) and the Statement of Cultural Heritage Values but did not dispute the overall heritage value of the property.

Based on the findings of the Building Condition Assessment Report, staff recommend that the Notice of Intent to Designate the Kirby House located at 2480 Kirby Road be withdrawn, and that Council direct staff to require the owner to enter into a letter of undertaking as a condition of Heritage Clearance for demolition to erect a commemorative display, material salvage and reuse in a manner that celebrates the legacy of the Kirby House and its significance to the community.

For more information, please contact Shahrzad Davoudi-Strike, ext. 8653.

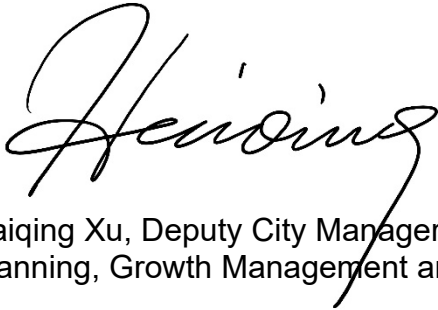
Attachments

1. Notice of Objection Letter
2. Site Photos package
3. Building Condition Assessment Report
4. Communication memo by LHC

Prepared by

Shahrzad Davoudi-Strike, Senior Manager of Development Planning, ext. 8653.
Nancy Tuckett, Director of Development Planning, ext. 8529.

Respectfully submitted by

A handwritten signature in black ink, appearing to read "Haiqing Xu". The signature is written in a cursive style with a long, sweeping tail that extends downwards and to the right.

Haiqing Xu, Deputy City Manager
Planning, Growth Management and Housing Delivery

HUMPHRIES PLANNING GROUP INC.

FOUNDED IN 2003

August 6, 2024
HPGI File: LI24L

SUBMITTED VIA EMAIL: clerks@vaughan.ca

City of Vaughan
2141 Major Mackenzie Drive
Vaughan, ON
L6A 1T1

Attn: Todd Coles, City Clerk

**Re: Intent to Designate under the *Ontario Heritage Act*
2480 Kirby Road, Vaughan**

Humphries Planning Group Inc (HPGI) represents 1411069 Ontario Inc., the owner of the property located at 2480 Kirby Road in the City of Vaughan (the “**Subject Property**”). It is our understanding that the Council of the Corporation of the City of Vaughan intends to designate the Subject Property for reasons of cultural heritage value or interests, pursuant to Part IV, Section 29 of the Ontario Heritage Act (the “**Act**”). Under subsections 29(5)-(6) of the Act, any person may object to the designation of the property within thirty days of the publication of the notice of intention to designate in the newspaper by serving the Clerk a notice of objection, including any information relevant to their rationale for objection. On behalf of our client, we are filing this objection in response to the Notice of Intention to Designate the Subject Property under Part IV, Section 29 of the Ontario Heritage Act.

The Property is currently occupied by a 2-storey brick building that has, in recent years, fallen into a state of significant disrepair. The building has been vacant for approximately 5 years, and currently is disconnected from gas, water, and electricity services. As set out in the April 11, 2024 report from Heritage Planning, Staff assert that the Property has cultural heritage value and meets 4 of the Ontario Regulation 9/06 criteria prescribed for municipal designation under Part IV, Section 29 of the Ontario Heritage Act. Our client disagrees with this conclusion and submits that the building, in its current condition, is not physically or functionally appropriate to warrant designation. The building is unfit for any form of residential occupancy.

While the Statement of Cultural Heritage Value states that the condition of the building is “fair”, this is not truly representative of the existing conditions on the ground. The so-called “brick house” is, in fact, a vacant building that is in very poor condition. We understand that years of being unoccupied has seriously damaged the structural integrity of the brick house including a partially collapsed roof, damaged floors, and the presence of mold all of which render the

building uninhabitable and beyond reasonable or practical repair. In its current status the building poses a serious risk to human health and safety should it be retained. It is unreasonable and unfair to subject our client to the additional obligations that flow from a property being designated under the Ontario Heritage Act in light of the current state of the building.

For the reasons stated above together with additional reasons which may be shared in future correspondence, our client formally objects to the Notice of Intent to Designate the Subject Property. We request a meeting with Staff to discuss the matter as soon as possible and ask that this correspondence be included on the public record and as part of any subsequent consideration of this matter by Vaughan City Council. We also request notice of any subsequent decisions made in respect of this matter

Yours truly,
HUMPHRIES PLANNING GROUP INC.

Rosemarie L. Humphries B.A. MCIP RPP
Principal

cc. *1411069 Ontario Inc*

2480 KIRBY ROAD – PHOTOS

Staff Photos from 2001-2005

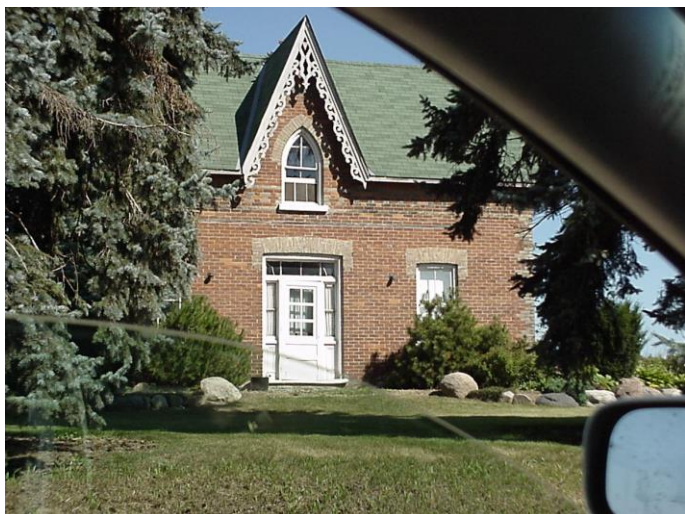


Photo provided by By-law Enforcement, November 04, 2024



with cedar posts and railing. The ends of the logs are sawed off square, and the tops and bottoms dressed off straight; they are then halved into one another and pinned. They project about 18 inches from the corners.

The second illustration we give is—

This building can be constructed with stone, brick, or timber. It is a dwelling suitable for a small family, the main building having a hall six feet wide running through the centre and entering the kitchen; on the left side of this hall is a large living room or parlour, 11 x 13, and store room, 14 x 7; on the right are two bedrooms, one 13 x 14 and the other 11 x 14; in the rear addition is a large kitchen, a pantry and bed-room. From the kitchen is a stair leading to the dairy and cellar, which should either have a brick or concrete floor, laid with a fall to the drain. If the kitchen floor is brought down to the cellar, it will materially assist in its ventilation. The walls of the cellar to the ground surface should be built with stone laid in water-lime mortar, and be at least 18 inches thick. None of the ceilings of the rooms should be under 10 feet high. The manner of laying out the interior may be as in the following figure:—



A SMALL GOTHIC COTTAGE.

The second figure shows the front elevation of the cottage which is situated on a raised terrace, and built with red brick, with white brick corners. The window-sills and the drips over the front door and windows, could be of dry pine, painted and sanded, but stone would be better. A small gable is raised over the front door, surmounted with a turned pinnacle, and having a simple piece of tracery fastened to the under side of the cornice, and in the centre of this gable is a small trefoil window to give light and ventilation to the garret. The roof should project at least two feet over the walls, having tin eaves-gutters and down pipes to carry the rain water to the cisterns. The shingles should be of dry pine, laid on four inches to the weather, and bedded in good hair mortar. The walls, if built hollow, a brick and a half thick, would be very dry and warm, and require no lathing on the interior surface. Round flues of glazed tile are much better than square brick flues, as they are safer from fire, and do not require sweeping.

The cost of a cottage of the above description would not exceed \$1,000. If built of timber, and boarded perpendicularly, or rough-casted on the outside, the cost would be about \$750. It must be borne in mind, however, that estimates are governed by the style of finish and the price of material in a given locality.

Size of main building, 36 x 28. Kitchen extension, 21 x 22. Scale, 12 feet to the inch.



IMPROVED FARMING IN ALBANY COUNTY, STATE OF NEW YORK.—A friend of ours, in this county, began farming some 20 years ago. His farm would then produce 20 tons of hay for sale, and eight for home consumption. This present season he has sold forty-three tons, and has enough left to winter 675 sheep, with his farm horses and other stock. The size of the farm, we believe, is about 100 acres. Its productiveness has been increased by the system of stock feeding we have so long advocated, together with draining where most necessary. The number of sheep mentioned above, are fattening for the late winter or early spring market. Notwithstanding the high price of Indian corn and all feeding material, and the tempting inducement to sell more hay, when it commands \$22 or \$23 a ton, the sheep will be fed as freely as ever. There is no success in half-way work, as our friend understands; and the conse-

quence is, he is the most successful sheep feeder within our acquaintance in this or any other State.—Country Gentleman

CLIPPING SHEEP.—This has grown into a great abuse in Great Britain, as well as, to some extent, in this country. At the meeting of the Agricultural Society, Dec. 9th, the following resolutions were adopted: 1. That the prizes must have been really and fairly earned. 2. That the year of the exhibition should be the year of the shearing for part two inspectors be named the sheep yard, with instructions to report cases in which the sheep have not been really and fairly shorn bare.

The Breeder and Grazier.

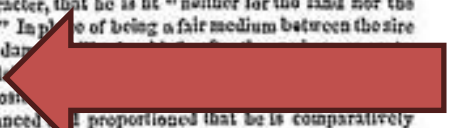
The Horse.

THE introduction of pure blood, when judiciously managed, will be productive of good not only to the carriage, but also to the farm horse. But judgment and great caution are necessary in considering the points and qualities of a blood stallion before he is admitted to mares whose progeny is intended for the saddle, the carriage, or for the heavier work of the farm. Let his shape and qualities be good, bad or indifferent, the stallion which possesses a sprinkling of blood is almost sure to be the animal which will be selected. His light action, showy appearance, and high sounding pedigree, will be a sufficient recommendation to those who are ignorant of the various points and qualities which are connected with the strength, substance, and ability and disposition of the animal. Besides, the same horse is certain of being put to a great number of mares of all sorts, shapes, and sizes, without the slightest regard to that exact adaptation of form, in sire and dam, which is absolutely necessary in order to secure a satisfactory result. Breeding from blood stallions is highly commendable, provided it is done with judgment and discretion; but the practice of violent crossing is decidedly objectionable, and cannot be too strongly condemned. "Extremes in crossing," observes a writer in the *New Sporting Magazine*, "are very rarely successful; and it is really astonishing to see farmers so constantly putting their complete cart-mares to thorough-bred horses, expecting to have foals of a class fit for hunters, whereas nine times out of ten, they are fit neither for draught nor hunting; for though they may be up to weight, which is what they expect, there is always a lamentable deficiency of pluck; and the same mare, with a three-parts bred horse, would be likely to produce a more valuable class of foals." Here refer to the combination of blood and substance, frequently the result of a character, that he is fit "neither for the hand nor the sea." In place of being a fair medium between the sire and dam, while the blood is in excess, the foal will be opposed to a balanced proportion that he is comparatively useless. His temper, moreover, is frequently characterized by a want of conformity to the purpose for which his owner might think him in other respects best adapted. At the last Provincial Exhibition in Kingston, there were some half dozen specimens of pure blood, which, with judicious management in crossing, might be made subservient to the improvement both of our carriage and farm horses.

The Suffolk Punch appears to be a good type for improving our ordinary race of agricultural horses. He is a decided favorite in several of the Eastern Counties of England. Being of medium size, compact, thick, and "punchy" in appearance, good step, and exceedingly muscular and enduring, he could not fail, when judiciously used, to get a progeny possessing many desirable qualities, and adapted to the wants of farmers in this country. A stallion of this breed has been in use for several years in Guelph, Woodstock, and subsequently in some places west of the latter, that has produced stock, we have been informed, of a very desirable character. It is often observed that short-legged, firm, compact horses, do their work better, and last longer than larger ones, particularly if they have a clean, flat bone and plenty of muscle. It often happens that cart horses of great height and weight have round bones; but round-boned horses, of any breed, are often gummy, and are apt to get gray; besides which, it indicates softness. For these, among other reasons, the Suffolk is deserving an extensive trial in this country.

Red Bay may be classed among the breeds of draught horses, and forms a distinct breed. Their colour and general points are very uniform; of a large size, 16 hands and upwards; colour bright bay, as the name indicates, with black

VIEW SHOWN IN PHOTO PROVIDED FROM BY-LAW ENFORCEMENT



FRONT VIEW SHOWN IN STAFF PHOTOS





SITE PHOTOS, MAY 2024



SITE PHOTOS, MAY 2024



Photo provided by By-law Enforcement, November 04, 2024



10376 Yonge Street • Suite 307 • Richmond Hill • Ontario • L4C 3B8 • T: 905•237•5410, F: 905•237•5413, E:ssoscia@sosciaeng.ca

BUILDING CONDITION ASSESSMENT

2480 Kirby Road

Vaughan, Ontario

SOSCIA ENGINEERING LTD.

Project number 24 – 136

Executive Summary

Soscia Professional Engineers Inc. visited Kirby Road in the City of Vaughan, Ontario for the purpose of determining whether the existing dwelling is structurally stable and whether the dwelling is suitable for habitation.

The study was limited to a visual inspection of the building components and as found conditions. Destructive testing was not performed. The Ontario Building Code and the Occupation Health and Safety Act (OHSA) are used in assessing the building condition.

The subject building is a 2-storey structure and appears to have been abandoned for many years. The building sits on a stone foundation wall and is not in a condition conducive to preservation. The building and roof were not properly sealed which allowed water to infiltrate in the building, where water damage is apparent throughout the ceiling assembly of the second floor (**Figure 1**).

The existing exterior brick (above grade) is of a non-load bearing type with interior wood framing transferring loads to a stone load bearing wall foundation. The exterior brick is experiencing severe spalling throughout. Exterior brick connections to the sheathing has been compromised and are no longer adequate or safe to laterally support the existing brick conditions. Due to this failure, large openings have developed leading to water infiltration subjecting the interior wood framing to rotting conditions (**Figure 2**). In addition, the brick is in very poor condition from weathering where significant section loss is occurring systematically throughout the exterior walls (**Figure 3**). Mortar between the joints is eroding and porous leading to freezing and thawing further undermining the structural integrity of the exterior brick wall system leading to possible structural collapse.

Foundation walls are of stone rubble and have experienced water infiltration as seen from inside the basement (**Figure 4**). A strong aroma due to mold was apparent possessing a health and safety hazard alongside sustained water exposure being imposed on the structural elements of the building. The combination of water damage and mold growth can be seen on the main structural elements in the basement such as posts and joists. This water exposure to these main structural elements subject them to rot and threatens the building's stability.

This structure has undergone significant deterioration and does not meet the structural requirements of a dwelling as defined in the Ontario Building Code. Furthermore, we are of the firm opinion that the structure will not be capable of withstanding centrifugal forces during the transportation of the building. Transportation of this building will pose a safety hazard to the general public

The exterior walls are a face sealed envelope assembly. They do not provide the required resistance for vapor diffusion; they do not provide the necessary resistance to air transfer and do not provide the required resistance to heat transfer. In consequence of no air barrier, no vapor barrier and no thermal insulation the building assemblies and materials have deteriorated. The deterioration has led to the development of mold, rot and corrosion, all of which are detrimental to an individual's health and is in strict contravention of both the Ontario Building Code and the Occupation Health and Safety Act.

The interior wood structure is constructed of rough-sawn beams and joists that is original to the structure and constructed using traditional techniques. Main structural beam supporting second floor as seen from the ground floor kitchen has undergone flexural failure splitting the beam longitudinally from end-to-end (**Figure 5**). Several other structural members display similar failure patterns (**Figure 6**) subjecting the building to possible internal collapse. Further, due to bending failure, the deflection has been magnified where the interior floors are out of plumb on both ground floor and second floor (**Figure 7**). Deflection is significant enough to where interior walls are cracking due to the floor sinking (**Figure 8**).

To make the building habitable, a complete reconstruction is necessary, starting with excavation and progressing through foundations, above-grade framing, and finishes. Excavation is required to facilitate foundation repairs and the preparation for a new slab on grade. The foundations need to be entirely rebuilt, including new footings, foundation walls, and a new slab on grade, all adhering to the Ontario Building Code (OBC) requirements. Above-grade framing will involve constructing new exterior walls, lintels, and solid load bearing brick, along with an engineered floor joist system for both the ground and second floors. The roof will need to be reconstructed with new trusses, sheathing, and shingles. Finally, the finishes must be redone to include new insulation, vapor barriers, drywall, painting, and all other finishing touches in accordance with OBC standards. Overall, the repairs needed to make the house habitable are extensive.

In addition, based on the structural condition identified in this report, we are in the opinion that transporting the structure poses a significant health and safety hazards to the public. This is due to an internally comprised structure with main structural beams having undergone failure. Exterior bricking is falling apart and the structure in its entirety will not be able to withstand the dynamic movement and centrifugal forces from transportation.

Based on our findings we are of the opinion that this building is not habitable. The building does not meet the minimum acceptable standards for public health and public safety, structural sufficiency, environmental integrity and energy conservation. We recommend demolition of 2480 Kirby Road, City of Vaughan.

We further conclude that the non-compliance with the Ontario Building Codes, and the Ontario Occupational Health and Safety Acts overrides any historical and cultural value that this dwelling is said to contain. We recommend, that this house undergo demolition because of its inhabitable condition.





BUILDING CONDITION ASSESSMENT

2480 Kirby Road

Vaughn, Ontario

1.0 INTRODUCTION

1.1 Terms of References

Soscia Engineering Ltd. was authorized by Mr. Marc Bozzo, to conduct a building condition survey of the building and property located at 2480 Kirby Road. Soscia Professional Engineers personnel were to carry out a visual walk-through survey of the building and property to review various elements and services of the building. The purpose of the building survey was to determine whether the existing dwelling is structurally stable and whether the dwelling is suitable for habitation.

1.2 Scope of Work

Our scope of work was to include visual assessment and review of:

- Review of the roof and building envelope (visual only),
- Review of the building structural components,

The work was to be conducted in accordance with Soscia Professional Engineers verbal agreement with Mr. Marc Bozzo. The objective of the survey was to review the condition of the various building elements and components to assess their present condition in reference to compliance with the latest edition of the Ontario Building Code and Occupation Health and Safety Act.



837 Princess Street, Suite 400
Kingston, Ontario
Canada K7L 1G8
Phone: 613-507-7817
Toll free: 833-210-7817
info@lhcheritage.com
www.lhcheritage.com

MEMORANDUM

C 42

Communication

CW(1) – November 5, 2024

Item No. 2

TO: 1411069 Ontario Inc.
Tony Gugleitti
President
1 & 2 Bradwick Drive
Concord, ON

CC: Rosemarie Humphries
Humphries Planning Group Inc.

FROM: LHC Heritage Planning & Archaeology Inc.

DATE: 23 October 2024

**RE: REVIEW OF THE NOTICE OF INTENTION TO DESIGNATE DOCUMENTATION FOR
2480 KIRBY ROAD IN THE CITY OF VAUGHAN, ONTARIO**

LHC Heritage Planning & Archaeology Inc. (LHC) was retained in August 2024 by 1411069 Ontario Inc. (the Owner) of 2480 Kirby Road (the Property) to review a Notice of Intention to Designate (NOID) issued by the City of Vaughan (the City) and supporting documentation for the NOID.

On 30 July 2024, the Office of the City Clerk issued a NOID under the *Ontario Heritage Act (OHA)* to designate the Property under Part IV Section 29 of the *OHA*. On 06 August 2024, Humphries Planning Group Inc. sent a letter of objection to the City on behalf of the Owner.

According to Section 29(6) of the *OHA*, municipal Council has 90 days after the end of the 30-day period (30 days from NOID is 29 August 2024 plus 90 days is 27 November 2024) to decide regarding whether or not to withdraw the NOID.

1 NOID REVIEW

The *OHA* includes specific requirements for a NOID. Section 29(4) of the *OHA* includes three content requirements for a NOID served on an owner, and states:

- Notice of intention to designate property that is served on the owner of property and on the Trust under clause (3) (a) shall contain,
- (a) an adequate description of the property so that it may be readily ascertained;
 - (b) a statement explaining the cultural heritage value or interest of the property and a description of the heritage attributes of the property; and

(c) a statement that notice of objection to the notice of intention to designate the property may be served on the clerk within 30 days after the date of publication of the notice of intention in a newspaper of general circulation in the municipality under clause (3) (b). 2005, c. 6, s. 17 (2); 2019, c. 9, Sched. 11, s. 7 (4).

LHC's review of the NOID finds that it does not include an adequate description of the property [Section 29(4a)]. Since the *OHA* does not define what an adequate description of the property includes, guidance from the *Ontario Heritage Tool Kit Designating Heritage Properties (Tool Kit)* informs LHC's understanding of what should be included in the NOID. The *Tool Kit* states that:

The *Description of Property* describes the general character of the property and identifies those aspects of the property to which the designation applies. In addition to providing information so that the location of the property can be identified (i.e. municipal address and neighbourhood if appropriate), it should outline the principal resources that form part of the designation (i.e. buildings, structures, landscapes, remains, etc.) and identify an discernable boundaries.¹

The NOID includes the municipal address but does not include a description of its general character or specifically identify those aspects of the property to which the designation applies. It also does not specify the principal resources, although it can be inferred that the house on the property is the focus of the designation. The NOID does not describe discernable boundaries.

The NOID includes a statement explaining the cultural heritage value or interest of the property [Section 29(4b)] in the section titled "Reasons for Proposed Designation". However, the NOID itself does not include a description of heritage attributes of the property. It references an 11 April 2024 Heritage Vaughan report with a statement of cultural heritage value, but all the required information is not included in the NOID itself. The NOID is not clear about what details a heritage designation by-law would contain and what heritage attributes must be conserved to conserve the heritage value of the property.

2 11 APRIL 2024 HERITAGE VAUGHAN COMMITTEE REPORT - REVIEW

The Heritage Vaughan Committee Report proposes and recommends the designation of the Property. It includes the municipal address and legal description of the Property, and an evaluation against the criteria for determining cultural heritage value or interest from *Ontario Regulation 9/06 (O. Reg. 9/06)*. The evaluation indicates that the property meets 8 of the 9 criteria. In LHC's professional opinion, it is highly unusual for a property like this one to meet so many of the criteria.

¹ Province of Ontario, Ontario Heritage Tool Kit, Designating Heritage Properties, 2006, 15.

The evaluation utilizes the common approach of grouping the criteria into three main categories: design or physical value, historical or associative value, and contextual value.

It includes a brief discussion or explanation following each group of criteria. Additional detail is included in a Statement of Cultural Heritage Value included as Attachment 2 to the report. The evaluation for physical value or design value indicates that the City believes the Property meets two of the three criteria, criteria 1 and 2. It makes a case that the property is “an excellent representative and surviving example of the Gothic Cottage style”, which may support the property in meeting criterion 1. However, the evaluation finds that the Property demonstrates a high degree of craftsmanship or artistic merit (criterion 2) but there is no discussion of how it meets the criterion. In LHC’s professional experience, a visit to the building—usually including interior access—and detailed photographic documentation of the entire building(s) and potential heritage attributes would be required to determine if the Property demonstrates craftsmanship. Attachment 3 includes two windshield survey images and a Google Streetview image; however, these images only depict the façade from a distance and are insufficient for determining potential for craftsmanship. Review of recent site photographs and an on site review of the exterior on 12 September 2024 suggests that while the building does exhibit a number of decorative elements including buff on red brickwork (*i.e.*, headers, quoins and banding) and decorative wooden bargeboard along the central gable peak, these elements and the patterns are relatively standard in examples of dichromatic brick Gothic Cottage architecture across Ontario. Further, the application of these simple and widely used patterns did not require a higher than usual application of skill on the part of the bricklayer.²

The evaluation indicates that the City believes the Property meets all three of the historical value or associative value criteria, criteria 4 through 6. However, the discussion following the historical value or associative value criteria only makes a case for criterion 4. There is information that identifies William and Joseph Kirby as historically significant and connected to the Property which could support it meeting criterion 4. The description includes supporting information about the potential significance of William Kirby. There is some confusion in the reference to Joseph Kirby, as LHC’s preliminary research suggests that James H. Kirby was a township councillor and reeve, not Joseph. It is unknown if J.H. Kirby went by both James and Joseph, or if there may be conflicting historical references. The report only states that he was a multi-term councillor in Vaughan Township. It does not **directly** connect him to the Property while serving on Township Council or confirm that he lived at the Property during his time in office. There is no discussion on how the Property yields or has the potential to yield information that contributes to an understanding of a community or culture (criterion 5). There is no discussion on how the Property reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community (criterion 6). An architect, artist, builder,

² T. Ritchie. “Notes on dichromatic brickwork in Ontario,” *Association for Preservation Technology Bulletin*, 11, 2, pp. 60-75, 1979.

designer, or theorist were not identified for this Property. It is unusual – although not impossible – for a specific architect, artist, builder, designer, or theorist to be associated with a rural farmhouse since the intent of the *Canadian Farmer* and other architectural design booklets was to provide broad access to these styles and allow anyone to construct one for themselves.

The discussion of contextual value criteria indicates that the City believes the Property meets all three of the contextual value criteria, criteria 7 through 9. The discussion does not specify how the Property meets these criteria except to describe it as a longstanding landmark (criterion 9). The report mentions that the Property is the “namesake property” of Kirby Road, without expanding on what that means and mentions that it is one of the remaining residential properties associated with the Hamlet of Hope, without explaining how this demonstrates that it meets any of the *O. Reg. 9/06* criteria or how the property supports, maintains or defines the character of the area (criterion 7). Limited information about the context of the Property is presented but there is no analysis describing how the Property supports, defines, or maintains the character of the area or how it is physically, functionally, visually, or historically linked to its surroundings.

In LHC’s professional opinion this report does not provide adequate support for the conclusions.

3 STATEMENT OF CULTURAL HERITAGE VALUE - REVIEW

A Statement of Cultural Heritage Value is attached to the Heritage Vaughan report. This statement includes the address; legal description of the Property; a brief overview of the Property and discussion for architectural/physical value, historical/associative value, and contextual value; a Summary of Cultural Heritage Value that appears to be a list of heritage attributes and associations; and a bibliography/resources.

The overview describes the Property as located in the community of the Hamlet of Hope. However, it appears that this was a rural property outside of the hamlet and it is understood that the hamlet no longer exists. The statement does not describe the property sufficiently for the reader to understand where it is, what it includes, or what the boundaries may be. Furthermore, the overview describes the property condition as fair; however, there is no supporting information or images to verify this assessment. It is unclear how condition was determined or if whomever prepared this document visited the Property for this assessment.

The discussions of architectural/physical value, historical/associative value, and contextual value are more detailed than the information included in the main Heritage Vaughan report. The discussion includes a description of the house and limited historical information but does not include citations linking the information to the bibliography or list of resources consulted. It is unclear exactly where the information came from, its relevance, or the quality of the source material.

The document states “He [William Kirby] was recognized as a significant local pioneer. His son Joseph H would go on to not only inherit the Kirby Farm, but also served on the Vaughan Township Councils for several terms.” This is an example of a statement that is not referenced

and does not include objective analysis to confirm William's significance to the community. The statement also does not include details on the significance Joseph Kirby (James Kirby) had while on Township Council, or if he lived at or farmed the Property while also serving on Council. This information may be relevant to the background and evaluation of the Property, but the documents do not effectively link information to the evaluation.

The discussion of contextual value describes the disappearance of the hamlet of Hope and asserts that this Property is a remaining piece of the hamlet. The statement also describes how the buildings in the hamlet disappeared over time. From this description, it appears that this Property was rural, outside of the hamlet, and increasingly isolated from any historic context associated with the hamlet as tangible evidence of it disappeared over time. Therefore, without much more information, context and landscape analysis the report does not make an effective case that the Property is historically linked to the hamlet of Hope.

The discussion of contextual value also asserts that the Property is a landmark because it is the only property with significant structures along the road. Review of Kirby Road between Dufferin Street and Jane Street (focussing on the area around Keele Street and Kirby Road, which was formerly the hamlet of Hope) did not identify a definable character supported or maintained by the former farmhouse at 2480 Kirby Road. Along Kirby Road is a mix of trees, berms, residential subdivisions, cultivated fields without notable agricultural structures or features (e.g., barns, silos). The farmhouse itself is partially shielded from view along the road by large coniferous trees. From Kirby Road, other aspects of the golf course on the property are equally prominent. There is no information indicating that this property is or was used in wayfinding. The discussion of the Property as a landmark is not supported by sources.

The summary of cultural heritage value includes a bullet list of physical features and historic facts about the Property. A list of heritage attributes should be physical features of the Property that are key to understanding its cultural heritage value or interest. The list of physical features appears to be used as a list of potential heritage attributes. However, this list does not explain how they connect to each of the relevant criteria from *O. Reg. 9/06* as required by *Ontario Regulation 385/21 (O. Reg. 9/06)* if they are to be used in a designation By-law. This list also includes speculation regarding the originality of wood frame windows. Since heritage attributes are supposed to be the key physical features that must be retained to conserve cultural heritage value or interest, it is inappropriate to speculate about originality. This kind of discussion is better suited for research summaries. Furthermore, many of the features listed are imprecise. For example, it is unknown if "the stone laid foundation" is rubble, coursed, cut or if it is a crawlspace or full basement. There is concern that the vague nature of how features of the house are described introduces potential for different interpretations around conservation which make planning for change, maintenance, or rehabilitation challenging.

The summary of contextual value states "the property is a longstanding landmark in the area and is the namesake property of Kirby Road, and its location and orientation in relation to Kirby Road is important in establishing that connection". This description as a landmark contains

multiple ideas. It is unclear if only the house, the entire Property, or any natural or landscape elements are considered part of the landmark. The vague description raises questions such as: How does the orientation and location of the Property or house on Kirby Road establish a connection? Was the road named for the Property or –as seems likely—for one or more of the Kirby family members? How is this property any more of a landmark than any other older agricultural property in the rural areas of the City? In LHC’s professional opinion, more analysis is required to evaluate the historical significance of the context around the Property.

Furthermore, the Statement of Cultural Heritage Value would benefit from a map or site plan illustrating where the City finds that cultural heritage value is located on the Property. It would also benefit from a section that clearly states which parts of the Property the City finds do not have cultural heritage value or interest.

4 CONCLUSION

In LHC’s professional opinion, the NOID and supporting information in the report to Heritage Vaughan and attached Statement of Cultural Heritage Value is incomplete and includes insufficient analysis and supporting materials. It is unclear exactly how many of the criteria from *O. Reg. 9/06* are met by the Property. LHC would recommend a much more detailed research and evaluation report be prepared before the City considers designation of this Property. Furthermore, the information in the 11 April 2024 report to Heritage Vaughan, the Statement of Cultural Heritage Value, and the NOID are different and it is unclear what details a heritage designation by-law would contain and how heritage attributes connect to each of the criteria from *O. Reg. 9/06*. This creates significant uncertainty around management of the Property.

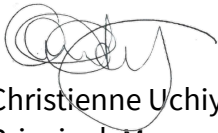
As described in this Statement of Cultural Heritage Value, the list of what appears to be heritage attributes would be difficult to work with from a heritage planning standpoint and does not enable the owner to clearly understand what needs to be conserved to conserve cultural heritage value on this Property.

LHC recommends additional research and analysis of the Property be completed –with clearly referenced sources—to clearly identify which criteria from *O. Reg 9/06* it may meet. If evaluation finds that the Property still meets criteria for designation, a NOID should be prepared that includes all information required by the *OHA* for NOIDs and also includes all information required by *O. Reg. 385/21* for designation By-laws so that the NOID can serve as a draft By-law and the owner can very clearly understand the relevant implications.

SIGNATURES



Benjamin Holthof, MPI, MMA, RPP, MCIP, CAHP
Senior Heritage Planner



Christienne Uchiyama, MA, CAHP
Principal, Manager of Heritage Consulting Services



1.3 Brief Description of Building

The building at 2480 Kirby Road is a 2-storey structure and appears to have been abandoned for many years. The structure is a wood framed building with wood floor joist, wood planking and conventional wood framed roof members. The walls are of brick. Basement walls are loadbearing stone rubble with stud framing supporting wooden roof structure and floors.

The exterior walls are a face sealed envelope assembly and does not provide the required resistance for vapor diffusion, does not provide the necessary resistance to air transfer nor provide the required resistance to heat transfer.

The building utilities have been decommissioned.

2.0 METHODOLOGY

The survey of the building components was carried out on November 5th, 2024. Soscia Engineering Ltd. personnel were on-site to review the components outlined in the Scope of Work (report Section 1.2). Access was provided throughout the building. Our general approach to the project consisted of the following:

- Discussions with the client.
- Visual examination of accessible components.
- Preparation of a report summarizing our findings.

The observations of exterior cladding and structural framing were made from floor level by unaided visual observation. The visual review was conducted to evaluate each item specified in the report format outline, in an effort to determine obvious areas of concern with respect to the general characteristics of the building.

The Structural Assessment in part 3 will be broken down into the following:

- Exterior
- Roof
- Basement

- Ground Floor
- Second Floor

For each observation item under review (listed above), the report describes:

- Description,
- observations of existing conditions
- Compliance with OBC and OHSA of Ontario.

Representative photographs were taken of typical deficiencies.

3.0 STRUCTURAL ASESMENT

3.1 Exterior

3.1.1 Description

The exterior of the building is of a brick veneer non-load bearing façade with interior load bearing exterior stud walls. The foundation walls are of stone rubble. The building has entrances on the south face, 2 entrances on the east and an entrance on the north face. Brick is seen to be spalling and decaying due to weathering. In addition, the exterior brick with mortar is both porous.

3.1.2 Observations

The exterior brick has spalled at the base of the wall and has been structurally comprised. This pattern is systematic and occurs throughout the perimeter of the property at varying locations. Exterior brick connections to the sheathing have been compromised and are no longer adequate or safe to laterally support the existing brick conditions. Openings are seen due to bricks being detached from the house and exposing the interior of the structure to outside elements. Interior wood structural elements as seen from the exterior appears to be rotting in some locations.

The brick veneer exterior is significantly spalling throughout. The veneer does not meet the requirements of the OBC.

At high stress point locations such as window openings, the brick appears to be further stressed where diagonal cracking is exhibited throughout the exterior structure (Figure 9).

The exterior walls appear not to be plumb which may be a result of the structure being compromised (Figure 10). The chimney on the west face of the building appears to have been added after the building was built and is not interlocked with the structure, posing an additional hazard.

3.1.3 Compliance

As the lateral connection of exterior brick to sheathing is inadequate to restrain the brick, the brick system is experiencing failure in several locations. This includes crumbling of the brick at the base which leads to potential structural collapse of the brick. Freezing and thawing of the porous brick leads to water permeating throughout the brick system subjecting it to brittle cracking and instability of the brick. In addition, the progressive inadequacy of the exterior brick façade leads to openings subjecting the internal structural elements to termites, water infiltration and hazardous conditions.

In general, the exterior of the structure is in a very poor condition and is in **non compliance** of both the Ontario Building Code and the Occupational Health and Safety Act of Ontario.

3.2 Roof

3.2.1 Description

The roof consists of a black/green shingles on an A-framed style structure that is conventionally framed. A chimney is located on the west elevation of the building. Half of the roof is occupied by the second floor where ceilings joists can be seen from inside. Access to the roof space was not accessible at the time of visit.

3.2.2 Observations

The soffits on the roof appear to be damaged with the fascia boards falling apart in several locations (**Figure 11**). These damaged soffits create openings that allow for water infiltration and moisture build-up in the attic space.

Water damage on the second-floor ceiling (**Figure 12**) suggests a faulty roof with ongoing water infiltration. Ceiling paint can be seen flaking off with discoloured molded ceilings indicating water infiltration/exposure from faulty roof (**Figure 13**). Roof rafters, sheathing and collar ties may also be experiencing water damage.

Given the amount of rework (patching) (**Figure 14**) of the second-floor ceiling, water discolouration of ceiling, an inspection of the roof structure for decay is recommended, as it was not accessible during the time of visit.

Ceiling in locations throughout appear to have long splits parallel to collar ties indicating potential structural deflection laterally due to roof wanting to displace outward (**Figure 15**). This is further evidenced by large structural induced tears at the ceiling and wall interface as a result of the wall being displaced laterally outwards which may explain why exterior walls are not plumb. Contributing factors would be the water infiltration leading to potential decay of roof supporting elements and loadbearing end walls not being able to withstand the outward force transmitted from the rafter bearing ends.

3.2.3 Compliance

Water leakage of the roof is apparent with the condensation and discolouring of the ceiling finishes. This water damage subjects the roof structural framing to a loss of integrity and instability. In addition, cracks that are long and parallel to roof members demonstrates lateral deflection from structural inadequacy to confine the movement. Structural exterior wall that are not plumb and is evidenced by large cracks at ceiling

and wall interface is an example of the roof deviating laterally and pushing the wall outwards.

In general, the roof of the structure is in a very poor condition and is in **non compliance** of both the Ontario Building Code and the Occupational Health and Safety Act of Ontario.

3.3 Basement

3.3.1 Description

The basement is approximately 8 foot in height with stone exterior walls. Floor joists are encased in stone in some areas and ledger boards in others. The finished floor is of a concrete slab on grade.

3.3.2 Observations

Basement walls are not consistent throughout where alterations have been made to sections of the wall introducing masonry block and brick (**Figure 17**). These locations create instability in the wall with introduction of cold joints and inconsistent material and can become unpredictable under the lateral soil bearing pressure it is retaining.

Large openings in the stone bearing walls were seen to make room for mechanical systems added later on (**Figure 18**). Due to the large openings, there are several floor joists and flooring systems that have no direct bearing, compromising the floor system and removing the required top lateral support of the retaining load bearing walls.

Water infiltration is apparent as large areas of slab on grade and around the perimeter edge of walls there is discolouring due to water absorption (**Figure 19**). The moisture buildup led to a strong musty smell and presence and can be seen with the peeling of the stone cover where moisture is trapped (**Figure 20**). In addition, the heavy presence of moisture in the basement is seen absorbed through the main structural wood posts (**Figure 21**) and floor joists (**Figure 22**). The wood posts in addition are bearing on the slab on grade without a spread footing which is structurally inadequate for any live loading of the structure.

Basement walls are not waterproofed, does not contain weeping tile and contains no drainage board. This allows water into the basement.

3.3.3 Compliance

Load bearing stone wall subjected to lateral soil pressures and gravity loading are inconsistent in material and is unpredictable. Large holes at the top of stone wall removes critical bearing of floor joists.

Water infiltration through the exterior stone wall and from the underside of slab on grade is evident with the absorptive discolouring of the structural joists, posts and slab. The structural integrity of these prolonged exposure to water has led to structural weaknesses.

Freezing and thawing of the water will further weaken the structure and may contribute to a fatal collapse. Load bearing walls with large openings are not structurally adequate to carry the loads.

In general, the basement of the structure is in a very poor condition and is in **non compliance** of both the Ontario Building Code and the Occupational Health and Safety Act of Ontario.

3.4 Ground Floor

3.4.1 Description

The ground floor bears on a conventional floor system with true dimensional lumber supported on foundation stone wall and timber beams.

3.4.2 Observations

Upon entry from the east elevation of the building it was observed that the main timber beam supporting second floor joists had undergone structural failure. The timber beam has undergone flexural bending failure as indicate by the end-to-end splitting (Figure 5). This is further seen on other main structural timber supporting beams (Figure 6).

Joists in the kitchen location are not plumb and deflecting with the beam as seen with gaps being formed to underside of ceiling (Figure 23).

White mold is observed spread throughout on the exposed timber (Figure 24). Peeling of ceiling and walls is apparent throughout indicating moisture intrusion (Figure 25).

3.4.3 Compliance

Main structural supporting elements as observed in the kitchen carrying joists has lost half its capacity due to the split along mid-depth of beam from end-to-end. This is a contributing factor to deflection in the flooring system and is structurally unstable subjected to collapse. White mold is observed throughout the wood members and over time can weaken wood fibers, compromising the wood's structural integrity. This weakening can lead to wood rot if the mold persists, potentially causing beams to warp, crack and progressively advances the already unstable beam. Furthermore, rampant molding possesses a health risk and contributes to a toxic indoor air pollution.

In general, the ground floor of the structure is in a very poor condition and is in **non compliance** of both the Ontario Building Code and the Occupational Health and Safety Act of Ontario.

3.5 Second Floor

3.5.1 Description

The second floor is conventionally framed with wood joists and wood flooring original to the structure. The stairway is located at the entrance from the south elevation of the building.

3.5.2 Observations

On-going mold and water damage effects are seen throughout the second floor. Rework to ceilings is consistent throughout entire second floor ceiling indicating water damage from roof.

Deflection appears to occur both in the downward gravity and lateral direction. Downward gravity cracking is observed by the cracking of the interior walls due to floor settlement (**Figure 8**). Lateral deflection is observed at the ceilings throughout where there are long structural tears. As half the second floor is located within the bottom half of the roof assembly structure, the rafters bear at the exterior walls. This means that there is a lateral force due to gravity loading of the A-framed roof structure where the ceiling cracking and large structural cracks at the ceiling wall interface (**Figure 16**) demonstrates the exterior wall is unable to confine these lateral loads adequately resulting in a bending out of plane (not plumb) exterior walls. At the same time, the ceiling is splitting as a result of lateral deflection (Figure 26). Diagonal cracking at a 45

degree angle displayed on interior walls indicates shear failure. This could be the result of one side of the structure settling while the other side is not, developing shear cracks (**Figure 27**).

Droppings are seen throughout the second floor (**Figure 28**) indicating the Presence of rodent or wildlife infestation.

3.5.3 Compliance

The second floor is a concern due to evident deflection in the downward and lateral directions. These are indications of a structural instability of the structures inability to confine these movements in a manner that is safe. Additional loading of the second floor will lead to a collapse through the failed members supporting the floor or a combination of roof loading from snow, earthquake or winds adding additional stress on the exterior walls.

Additionally, to this is the water infiltration from the roof onto the ceiling and exposing roof members to moisture further advances structural decline. Wildlife infiltration with rodent droppings throughout the floor further stresses the structure and indoor air quality.

In general, the second floor of the structure is in a very poor condition and is in **non compliance** of both the Ontario Building Code and the Occupational Health and Safety Act of Ontario.

3.6 Extent of Repairs

We are in the opinion that, to make the building habitable, the dwelling will need to be reconstructed. The order of reconstruction starts with the excavation, foundations, above grade framing and finishes.

- **Excavation:** Excavation is necessary to facilitate foundation repair work of a new slab on grade and preparation work for foundations.
- **Foundations:** The foundations need to be completely reconstructed which includes new footings, foundation walls and new slab on grade. All foundations are to adhere to the requirements of the OBC. Foundations are to be waterproofed and comes with drainage board and weeping tiles.
- **Above-Grade Framing:** The above-grade framing will require new exterior walls, lintels, and load bearing solid brick. A new engineered floor joist system for ground and second floor. The roof will be required to be reconstructed with new trusses, sheathing and shingles.
- **Finishes:** The finishes must be reconstructed to include new insulation, vapor barriers, drywall, painting and finishing, all in accordance with the OBC requirements.

3.7 House Lift Condition for Transportation

Based on the structural condition as identified on this report, we are in the opinion that the dwelling would have to be shored and braced in its totality. This is necessary to withstand the centrifugal forces that will be applied to the structure during transportation. Furthermore, the roof structure is lacking lateral support.

The house has multiple safety concerns, including a stressed roof, deteriorating brick façade and a failed internal floor framing system supporting second floor. This house is susceptible to immediate collapse.

- **Roof Structure:** The roof structure has been exposed to water infiltration and the state of these members in these prolonged conditions are unknown. The exterior walls supporting the roof is out of plumb with evident cracking throughout is a concern when transporting the structure.
- **Brick Facade:** The brick façade does not have adequate lateral connection to the sheathing. Exterior brick is seen failing off the structure in addition to the lack of grout and brick fullness is susceptible to falling off due to centrifugal and vibrational forces when transporting.
- **Floor Framing:** The second-floor framing has main timber supporting members that have failed with visible end-to-end splitting. The uncertainty of these inadequate members is an additional concern of the floors collapsing into itself during transport due to the additional centrifugal and vibrational forces due to transportation.

Transportation of the structure possess a hazard to the health and safety of the public. The extent of reconstruction and repair to bring the structure to a safe standard for transportation would be costly and may still pose a safety issue to the public.



10376 Yonge Street • Suite 307 • Richmond Hill • Ontario • L4C 3B8 • T: 905•237•5410, F: 905•237•5413, E:ssoscia@sosciaeng.ca

4. Conclusion

The building structure at 2480 Kirby Road does not provide an adequate envelope that meets OBC and OHSa standards. Due to the gaps in the load bearing stone walls, the inconsistent material of stone and block in the walls. Water infiltration through the roof, stone basement walls and above grade exterior walls. The exterior brick system is dilapidated and increases external exposure to the structure. The ground floor has deflected significantly where noticeable warping and uneven leveling is noticeable. The second-floor main structural timber members have failed. The roof is not adequately confined by the exterior walls and is cracking throughout the ceilings and partition walls due to downward gravity and lateral deflection. The roof structure is missing lateral support and has potential of immediate collapse.

The structure contains many structural unsafe conditions. The structure does not comply with the structural requirements of the Ontario Building Code. We are of the firm opinion that this structure is unsafe and not habitable.

The building envelope at 2480 Kirby Road does not provide the protection necessary to prevent the development of mold, rot and corrosion, all of which are detrimental to an individual's health and is in strict contravention of both the Ontario Building Code and the Occupation Health and Safety Act. On this basis we conclude that the building is also not habitable.

The dwelling is not suitable for transportation.

We further conclude that the non-compliance with the Ontario Building Codes, and the Ontario Occupational Health and Safety Acts overrides any historical and cultural value that this dwelling is said to contain. We recommend, that this house undergo demolition because of its inhabitable condition.

If you have any questions, please do not hesitate to contact us.

Yours truly,

A handwritten signature in blue ink, appearing to read "Sandro Soscia", is written over a light blue circular scribble.

Sandro Soscia, P. Eng.
SOSCIA Professional Engineers Inc.



Figures



Figure 1: Cracking shown throughout the second floor ceiling and discolouring due to water penetration through roof.



Figure 2: Exterior brick is crumbling throughout the building where openings as shown exposes the building to water infiltration and infestation.

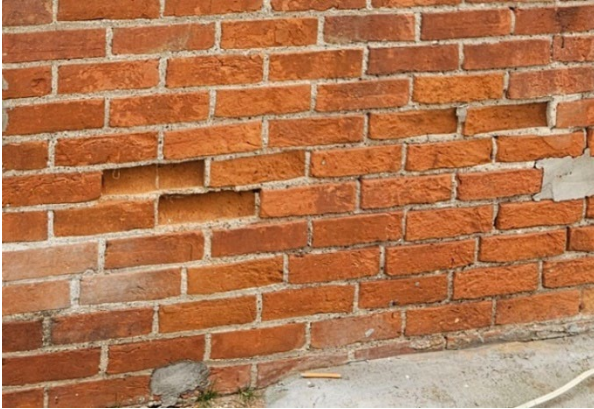


Figure 3: Section loss of brick systematic throughout exterior wall that are shown to be porous included with the mortar subjecting to freeze and thaw.



Figure 4: Water infiltration visible with discolouring of the slab due to water absorption throughout the basement.



Figure 5: Structural main supporting beam undergone flexural bending failure with splitting from end-to-end.



Figure 6: Main structural member undergoing flexural failure with splitting of member from end-to-end.



Figure 7: Leveler taken from top of stairwell on second floor.



Figure 8: Floor beneath is deflecting considerably where partition walls are sinking and cracking as shown in red.



Figure 9: Diagonal cracking at openings.



Figure 10: The east wall appears to not be plumb.



Figure 11: Roof Fascia.



Figure 12: Ceiling mold and discolouration due to water penetration from roof.



Figure 13: Paint peeling.



Figure 14: Ceiling work reworked (patched).



Figure 15: Parallel cracks to collar ties.



Figure 16: Ceiling tears at wall interface due to lateral displacement and walls pushing outwards out of plumb due to potential roof instability.



Figure 17: Introduction of new material.



Figure 18: Openings.



Figure 19: Discoloring of slab due to water absorption and infiltration.



Figure 20: Spalling.



Figure 21: Water penetration through slab and seen on posts without adequate bearing.



Figure 22: White mold and water discoloration due to water moisture and penetration.



Figure 23: Deflection between ceiling and joists.



Figure 24: White mold seen throughout on timber and wood framing.

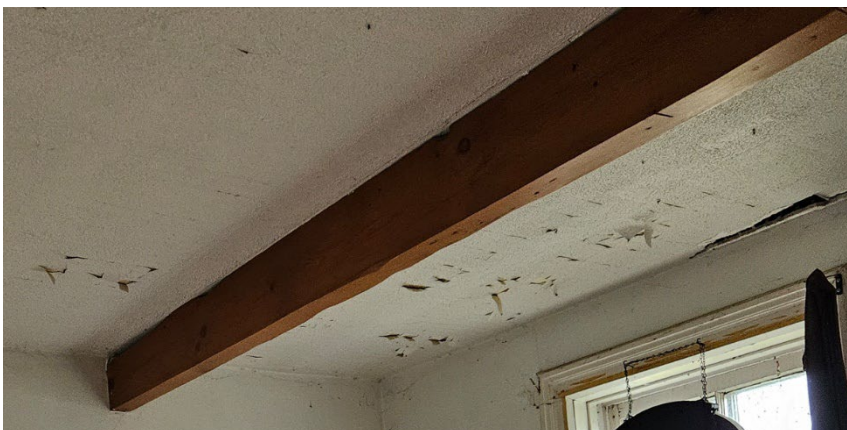


Figure 25: Paint peeling due to moisture.



Figure 26: Long ceiling splits due to tearing from lateral deflection.



Figure 27: Diagonal cracking seen and may be from one side of building settling more than other side.



Figure 28: Droppings seen throughout the structure indicating infestation of rodent or wildlife.



November 18, 2026

Sent via Email

City of Vaughan
2141 Major Mackenzie Drive
Vaughan, ON L6A 1T1

C30

Communication

Council – November 19, 2024

CW(PM) – Report No. 38 Item No. 4

Attention: Honourable Mayor and Members of Council
Office of City Clerk

Re: Frank Micoli c/o 1315955 Ontario Inc.
Official Plan Amendment File OP.24.007
Zoning By-Law Amendment File Z.24.019
132, 144, 154 AND 166 Woodstream Boulevard
Vicinity of Martin Grove Road and Highway 7

Dear Honourable Mayor, Members of Council and Development Planning Staff,

The owner has submitted applications for an Official Plan Amendment and Zoning By-Law Amendment to enable a two-phased development consisting of two high-rise mixed-use buildings, with four residential towers, each 35 storeys tall, and a total of 1,221 residential units.

Our goal is to ensure that the concerns and priorities of Vaughan residents, particularly those in West Woodbridge, are taken into account in this development.

The subject lands fall within the City's Urban Boundary and are governed by the Vaughan Official Plan (VOP 2010). According to Schedule 1 of the VOP 2010, these lands are primarily designated as "Employment Area." Employment Areas are considered stable zones intended for industrial, manufacturing, warehousing, and office uses.

We understand that Vaughan must intensify to accommodate a growing population and foster a healthy business environment. At The WWHA, Inc., we advocate for "compatible development" that integrates seamlessly with the existing environment and contributes to a livable, complete community.

After reviewing the available application materials on Vaughan PLANit Viewer, we have identified several concerns regarding the proposed development.

Concerns Regarding Land Use Designations and Employment Areas

The absence of clearly defined land use designations leads to speculation, with predictable consequences. The notion that redesignating "Employment Areas" to "High-Rise Mixed-Use" will resolve the housing crisis is flawed. This approach may not meet the long-term needs of the community. The core issue is the lack of clear land use designations, and the conversion of "Employment Areas" can only be effectively addressed through consistent adherence to well-defined land use policies.

As outlined in the June 25, 2024, Committee of the Whole report, the "Martin Grove Road and Highway 7 - Special Area Report" fails to account for the cumulative impact of all developments in the Martin Grove Road and Highway 7 area. Without a coordinated approach, the area risks becoming a fragmented and unsustainable urban space.

The special area is bordered by Highway 7 to the north, Highway 27 to the west, Kipling Avenue to the east, and the Vaughan Grove turf and baseball fields to the south. It includes a mix of commercial, industrial, office, retail, and automotive uses.

The boundary was established to encompass employment conversions and current development proposals in the Martin Grove Road and Highway 7 area.

We disagree with the Policy Planning and Special Programs Department's belief that, due to the current development pressures within the special area and the absence of a primary or local centre or Secondary Plan, the appropriate solution is to require applications to submit a joint Development Concept Report and Phasing Plan, informed by several studies.

A Call for a Comprehensive Land Use Study

We respectfully request a land use study to guide development in the Martin Grove Road and Highway 7 Special Area to create livable, complete communities. The City should not approve new developments on a site-by-site basis without a comprehensive plan that integrates all parcels, with predefined massing, density, built form, public spaces, and services.

This study should include direct community consultation to ensure that the priorities of residents guide the City in promoting sustainable, community-oriented development. The goal is to create a well-planned, harmonious community that aligns with residents' values and enhances their quality of life—key principles for effective urban planning.

Should you have any further questions or require additional information, please do not hesitate to contact us.

Sincerely,



Nick Pinto

President

The WWHA, Inc.

From: Clerks@vaughan.ca
To: Adelina Bellisario
Subject: FW: [External] Concerns Regarding Illegal Truck Yards and Safety Issues in Caledon
Date: November-18-24 8:39:04 AM

-----Original Message-----

From: Lisa Martino [REDACTED]
Sent: Saturday, November 16, 2024 10:42 AM
To: Clerks@vaughan.ca; mayor@vaughan.ca; Marilyn Iafrate <Marilyn.Iafrate@vaughan.ca>; Mayor@caledon.ca
Subject: [External] Concerns Regarding Illegal Truck Yards and Safety Issues in Caledon

CAUTION! This is an external email. Verify the sender's email address and carefully examine any links or attachments before clicking. If you believe this may be a phishing email, please use the Phish Alert Button.

Dear Municipality of Vaughan and Caledon representatives,

I am writing to express my deep concern regarding the growing number of illegal truck yards that have been appearing in the Caledon area, particularly near the Vaughan/Caledon border. This issue has become a serious problem in my community, especially in Bolton, where I reside.

The increasing presence of large trucks on roads that are designated as no-truck routes is creating significant safety hazards. In September, a tragic incident occurred when a young woman named Adriana lost her life due to the carelessness of a truck driver. This heartbreaking event highlights the urgent need to address the safety risks posed by these illegal truck operations.

Furthermore, these trucking yards are not only an eyesore but are also negatively impacting the beauty of the landscape in Caledon, as well as destroying agricultural land and animal habitats. What was once a serene and picturesque area is rapidly becoming overrun by these operations, diminishing the quality of life for local residents.

I urge you to take immediate action to address these issues, as the safety and well-being of the community are at risk. I hope that we can work together to find a solution that prevents further accidents and preserves the integrity of our town. I strongly believe that harsh penalties must be imposed on those operating these illegal truck yards, or they will continue to undermine the safety, beauty, and livability of our community. Without stronger enforcement, these operations will persist, and the problem will only worsen.

Thank you for your attention to this pressing matter. I look forward to your response and any steps that will be taken to resolve these issues.

Sincerely,
Lisa Martino

[REDACTED] Strawberry Hill Crt
Bolton, Ontario
[REDACTED]

From: Clerks@vaughan.ca
To: [Adelina Bellisario](mailto:Adelina.Bellisario)
Subject: FW: [External] todd.coles@vaughan.ca
Date: November-18-24 11:45:48 AM

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CW(1) – Report No. 37 Item No. 16

From: Amanda Corbett [REDACTED]
Sent: Monday, November 18, 2024 11:39 AM
To: Clerks@vaughan.ca
Subject: [External] todd.coles@vaughan.ca

CAUTION! This is an external email. Verify the sender's email address and carefully examine any links or attachments before clicking. If you believe this may be a phishing email, please use the Phish Alert Button.

Dear City of Vaughan Council,

I'm writing to you today regarding the urgent road safety and illegal land use issue that is ongoing in the City of Vaughan. My email is in support of Carmela Palkowski's presentation to Council on Nov 5th, 2024.

There are many issues on the roads and lands of the City of Vaughan, Town of Caledon, City of Brampton and other areas of the GTA. There is illegal land use going on right on the border of the City of Vaughan and the Town of Caledon - on City of Vaughan lands that are zoned agricultural and are being prepared to be used as illegal truck yards.

Something needs to be done urgently by the City of Vaughan about this! City of Vaughan by-law needs to be attending these properties (specifically 12151 Albion Vaughan Road, and 12001 Albion Vaughan Road, and any others in the area, like on Cold Creek) on a regular basis and fining the owners for the illegal use of land that is going on.

The owners of these properties need to be fined on a regular basis for:

- the illegal fill they are bringing in (which is likely contaminated)
- the fill that they are moving around to flatten the agricultural land and cover it in gravel
- the fences that they are illegally putting on the properties
- the illegal widening of the driveways to prepare the land for storage of trucks and intermodal containers
- the damage they are doing to the environment
- not having permits to do any of these activities
- any other offenses that are observed

I attended the presentation that Carmela gave in person, and was glad to hear from Mayor Del

Duca about the Mayors of several cities and towns working together to help address this issue. This is not new information - we are aware and happy that the Mayors are working together and trying to get more help from the Province. That said, this is not a complete answer to this issue. There are many things that the City of Vaughan can do right now and on a regular basis to help with this while they work with the other Mayors and try to get a response from the Province (see my list of items above). My point is, **the City of Vaughan needs to do more right now, and do it on a regular basis.**

Councillor lafrate also had town staff explain the process of dealing with these illegal land use issues - with the goal of informing us that these things take time. Again, this is not new information - we are aware that it takes a lot of time to deal with illegal land use through the courts. However, doing nothing while we wait for those things is not acceptable. The City of Vaughan has tools (mentioned above) to keep going back to these properties on a regular basis and continuing to fine them. I would like to know how many times these properties have been visited by Vaughan by-law, and how many times they have been fined. This should be happening on a weekly basis - and if it isn't, then that needs to change.

You need to make it uncomfortable for these people using the land illegal to continue what they are doing - that's the only way to get the message across to them while we wait for a Province that isn't acting quickly enough, and for the court system which is too bogged to deal with these issues in a timely manner.

Please - we are asking for your help to make your community and neighbouring communities a safer place to live. These properties cannot continue to operate unchecked. I would like a response as to what the City of Vaughan is currently doing to address these issues on these properties.

Looking forward to your response & some meaningful action,
Amanda Corbett
Bolton Resident

Thank you,
Amanda Corbett
Bolton ON resident

DATE: November 18, 2024

TO: Mayor and Members of Council

FROM: Michael Coroneos, Deputy City Manager, City Treasurer and Chief Financial Officer

RE: COMMUNICATION – Council – November 19, 2024

Committee of the Whole (2), November 12, 2024, Addendum Item #22

DEVELOPMENT CHARGES REDUCTION OPTIONS – RESPONSE TO MEMBER'S RESOLUTION

Below are the responses to the proposed amendments of Committee members presented and discussed at the Committee of the Whole meeting on November 12, 2024 for Item #22.

Recommendations

THAT Recommendations 2, 3, and 5 be revised to:

2. That staff be directed to revise the DC rates on all residential development applications to the rates in effect on September 21, 2018, until the later of November 19, 2029, or the passage of a new City-Wide DC Background Study and DC By-law, through the use of Section 27 agreements;

3. That staff be directed to revise the DC rates on low-rise residential development applications deemed complete or equivalent prior to September 21, 2018, to the rates in effect when those applications were made, until November 19, 2029, or the passage of a new City-Wide DC Background Study and DC By-law, whichever is later, through the use of Section 27 agreements;

5. Notwithstanding anything in the Development Charge Interest Policy – Under Sections 26.1, 26.2 And 26.3 of the Development Charges Act, 1997, Policy No. 12.C.17, that staff be directed to cease charging of interest on residential developments pursuant to sections 26.1 and 26.2 of the Development Charges Act until November 19, 2029, or the passage of a new DC Background Study and DC Bylaw.

In response to amendments passed by the Committee of the Whole, draft Policy 12.C.22 found at "Attachment 1" was updated. Some of the key revisions include :

- Section 1: "Development Approval" definition was revised to enable all types of Residential Development applications. Consent applications were added to the

definition to capture residential intensification which does not require a re-zoning, site plan or subdivision approval.

- Section 2: Revised parameters upon which a DC rate reduction or DC deferral will be provided by the City pursuant to a S. 27 Agreement. The policy is available for the later of five-year period or the enactment of a new DC Bylaw and a DC deferral can be made for two years. A developer shall obtain building permit issuance within 18 months of entering into a S. 27 Agreement. Should no building permits be issued in accordance with the terms of the s.27 Rate-Setting Reduction Agreement, the applicant/owner may elect to pre-pay DCs under the conditions of a separate 2-year Pre-payment agreement.
- Section 5: the owner shall provide a Letter of Credit, or other form of security to the satisfaction of the City Solicitor and Chief Financial Officer to secure the DC deferral.

For more information, contact Michael Marchetti, Director, Financial Planning & Development Finance, ext. 8271 or Nelson Pereira, Manager, Development Finance, ext. 8393

Respectfully submitted,

Michael Coroneos, Deputy City Manager, City Treasurer and Chief Financial Officer



THE CORPORATION OF THE CITY OF VAUGHAN

CORPORATE POLICY

POLICY TITLE: DEVELOPMENT CHARGES RATE REDUCTION AND DEFERRAL FOR RESIDENTIAL DEVELOPMENTS

POLICY NO.: 12.C.22

Section:	Finance & Budgets		
Effective Date:	November 19, 2024	Date of Last Review:	Click or tap to enter a date.
Approval Authority:	Policy Owner:		
Council	DCM, Corporate Services, City Treasurer & Chief Financial Officer		

POLICY STATEMENT

A policy governing the reduction of Development Charge (DC) rates and DC deferrals until the later of November 19, 2029, or the enactment of a new City-Wide Development Charges By-law.

PURPOSE

The purpose of this policy is to establish the rules and practices for a DC rate reduction and deferring DC for residential development. This policy is in response to the Mayor's Resolution from September 17, 2024 that was ratified on September 24, 2024.

SCOPE

This policy applies to Low-Rise Residential, Residential Buildings and Mixed-use Buildings, subject to the terms and conditions as set out in this policy. To be eligible, such residential development must:

- have an Application approved and have not been issued a building permit, or;
- in the case of Low-Rise Residential Development, have an Application that has been deemed complete prior to September 21, 2018, and have not been issued a building permit; and
- enter into a Section 27 Agreement(s) prior to the later of November 19, 2029, or the enactment of a new City-Wide Development Charges By-Law.

POLICY TITLE: DEVELOPMENT CHARGES RATE REDUCTION AND DEFERRAL FOR RESIDENTIAL DEVELOPMENTS

POLICY NO.: 12.C.22

LEGISLATIVE REQUIREMENTS

The City is permitted to enter into deferral agreements pursuant to section 27(1) of the *Development Charges Act, 1997* which states: "A municipality may enter into an agreement with a person who is required to pay a development charge providing for all or any part of a development charge to be paid before or after it would otherwise be payable."

The City is permitted to enter into agreements pursuant to section 27(2) of the *Development Charges Act, 1997* which states: "The total amount of a Development Charge payable under an agreement under this section is the amount of the Development Charge that would be determined under the by-law on the day specified in the agreement or, if no such day is specified, at the earlier of,

(a) the time the Development Charge or any part of it is payable under the agreement;

(b) the time the Development Charge would have been payable in the absence of the agreement. 1997, c. 27, s. 27 (2)."

This policy was prepared in accordance with the *Development Charges Act, 1997*, as amended.

DEFINITIONS

- 1) **Act:** The *Development Charges Act, 1997*, S.O. 1997, c. 27, as amended, revised, re-enacted or consolidated from time to time, and any successor statute.
- 2) **Area Specific Development Charges (ASDC):** A fee or charge imposed with respect to growth related net capital costs against a defined land area or per unit for specified services under the applicable By-law.
- 3) **Application:** Means a development application submitted to the City for zoning, plan of subdivision, site plan or consent (severance) approval in respect of a Low-Rise Residential Building, Residential Building or Mixed-Use Residential Building.
- 4) **Building Permit:** Means a permit or conditional permit issued under the *Building Code Act, 1992*, S.O. 1992, c. 23, which permits the construction of a building or structure, or which permits the construction of the foundation of a building or structure.
- 5) **Development Approval:** Final approval of any zoning, plan of subdivision, site plan application, or consent for land severance.

POLICY TITLE: DEVELOPMENT CHARGES RATE REDUCTION AND DEFERRAL FOR RESIDENTIAL DEVELOPMENTS

POLICY NO.: 12.C.22

- 6) Development Charges (DC):** Fees collected from developers to help to pay for the cost of infrastructure required to pay for municipal services to new development as prescribed by the Act.
- 7) Gross Floor Area:** Means the aggregate of the areas of each floor, whether above or below grade, measured between the exterior faces of the exterior walls of the building or structure or from the centre line of a common wall separating a nonresidential and residential use and,
- a) Includes the floor area of a mezzanine and the space occupied by interior walls and partitions;
 - b) Excludes, in the case of a building or structure containing an atrium, the sum of the areas of the atrium at the level of each floor surrounding the atrium above the floor level of the atrium;
 - c) Excludes the area of any self-contained structural shelf and rack storage facility approved by the Building Materials Evaluation Commission under the *Building Code Act, 1992*, S.O. 1992, c. 23, as amended;
 - d) Excludes the sum of the areas of each floor used, or designed or intended for use for the parking of motor vehicles unless the building or structure, or any part thereof, is a retail motor vehicle establishment or a standalone motor vehicle storage facility or a commercial public parking structure;
 - e) Excludes the surface area of swimming pools or the playing surfaces of indoor sport fields including but not limited to hockey arenas, and basketball courts; and
 - f) For the purposes of this definition, notwithstanding any other section of this this policy, the non-residential portion of a Mixed-use Building is deemed to include one-half of any area common to the residential and non-residential portions of such Mixed-use Building or structure.
- 8) Letter of Credit:** A form of financial security issued by a financial institution that guarantees payment or performance by one or more counterparties to a beneficiary (the City), in accordance with the City's Letter of Credit Policy 12.C.04, as may be amended from time to time.
- 9) Low-Rise Residential:** Means single-detached homes, semi-detached homes and townhouses, but excludes stacked townhouses, and small and large apartments.

POLICY TITLE: DEVELOPMENT CHARGES RATE REDUCTION AND DEFERRAL FOR RESIDENTIAL DEVELOPMENTS

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10) Residential Building: A residential use building, or the residential use portion of a Mixed-Use Building, and includes Low-Rise Residential.

11) Mixed-use Building: A building or structure containing residential and non-residential uses other than a home occupation, where the residential use is at least 51% of the Gross Floor Area of the building.

12) Other Security: Means any other form of security that has been approved by the City Solicitor and Chief Financial Officer to secure the DC deferral.

13) Section 27 Agreement: Means an agreement entered into between a developer and the City pursuant to Section 27 of the Act.

POLICY

1) DEVELOPMENT CHARGES SECTION 27 AGREEMENT FOR RATE REDUCTION AND DC DEFERRAL

- a) Upon obtaining Development Approval and prior to the issuance of the first Building Permit, a developer of a Residential Building or Mix-use Building may request a DC deferral and/or DC rate reduction, in accordance with this policy.
- b) Where an Application qualifies for a DC rate reduction, the rate shall be the DC rates:
 - (i) in effect on September 21, 2018 in accordance with By-Law 083-2018, or
 - (ii) in the case of a Low-Rise Residential Application that was deemed complete prior to September 21, 2018, the DC rates in effect on the date the Application was deemed complete.
- c) To obtain a DC rate reduction and/or DC deferral, prior to the issuance of the first Building Permit the developer shall:
 - (i) enter into a Section 27 Agreement with the City;
 - (ii) provide the City with a Letter of Credit or Other Security for the DCs being deferred; and
 - (iii) pay the appropriate fees pursuant to the City's Fees and Charges By-law.
- d) The DC rate reduction and DC deferral will only apply to the City's portion of DCs and is not applicable to ASDCs.
- e) Non-residential developments are not eligible under this policy.

POLICY TITLE: DEVELOPMENT CHARGES RATE REDUCTION AND DEFERRAL FOR RESIDENTIAL DEVELOPMENTS

POLICY NO.: 12.C.22

- f) This policy shall be in effect until the later of November 19, 2029 or the enactment of a new City-Wide DC By-law.
- g) The developer shall obtain Building Permit issuance for the Development Approval within 18 months of the Section 27 Agreement being fully executed after which time the Section 27 Agreement shall terminate and the DC rate deferral and/or DC deferral set out therein shall no longer be available or applicable.
- h) If prior to the expiry of the Section 27 Agreement a Development Approval has not achieved Building Permit issuance, a developer may request to receive the DC rate reduction set out herein by agreement to pre-pay the DCs on the remaining portion of units set out in the Section 27 Agreement. If Building Permits for units are not obtained within 24 months from the date of the Section 27 Agreement for pre-payment, then those units will be calculated at prevailing DC rates, and the outstanding DC portion will be collected at issuance of Building Permit.

2) DEVELOPMENT CHARGES RATE

- a) For greater certainty, the DC rates in effect on September 21, 2018 applicable to an Application that has qualified for a DC rate reduction, shall be:

BY-LAW 083-2018	RESIDENTIAL (\$/Unit)			
	Single/ Semi	Multiples	Large Apt (≥700 sq. ft.)	Small Apt (< 700 sq. ft.)
City-wide Engineering	\$32,104	\$26,481	\$19,579	\$14,112
City -wide General	\$18,089	\$14,920	\$11,031	\$7,952
Total	\$50,193	\$41,401	\$30,610	\$22,064

- b) In the event that a Development Approval requires multiple Building Permits, any Building Permit issued after the time period identified in Section 1(g) shall no longer be eligible for the DC rate reduction or DC deferral and the Section 27 Agreement shall terminate, unless a Section 27 Agreement for pre-payment has been entered into in accordance with this policy.
- c) The DC rate reduction may only be applied to any portion of DCs owed and which remain unpaid as of the date of this policy. The policy may not be applied retroactively to reduce previously paid DCs and no refund of DCs paid shall be made as a result of this policy.
- d) In the case of a plan of subdivision where the engineering portion of DCs has been paid at the execution of the subdivision agreement prior to this policy

POLICY TITLE: DEVELOPMENT CHARGES RATE REDUCTION AND DEFERRAL FOR RESIDENTIAL DEVELOPMENTS

POLICY NO.: 12.C.22

coming into effect, this policy may not be applied retroactively to the engineering portion of DCs and would only apply to the unpaid general services portion of DCs.

- e) A Section 27 Agreement will include the type of units and quantity of units that will be subject to the reduced rates.
- f) Any change to the type of units included within a Section 27 Agreement, will void the rate setting within the Section 27 Agreement, and the prevailing rates will be charged.
- g) The developer shall adhere to any administrative requirements set by the City to facilitate the execution and/or registration of a Section 27 Agreement and the developer acknowledges that entering into a Section 27 Agreement does not constitute approval of any other Application or issuance of a Building Permit.

3) DURATION OF DEFERRAL

- a) The DC deferral shall be available until the earlier of:
 - i) 24 months after the issuance of the first Building Permit; or
 - ii) the registration of a plan of condominium, if applicable.
- b) The DC deferral will only be applicable to units issued Building Permits within the 18-month time period from the execution of the Section 27 Agreement, determined under section 1 of this policy.
- c) The deferral period will begin the day the first Building Permit is issued by the City. Where multiple Building Permits are issued for a Development Approval, the deferral period begins on the day that each individual Building Permit is issued.

4) DEVELOPMENT CHARGES PAYABLE

- a) The amount of the DCs payable to the City, as required under the Act, will be based on the rates determined under Section 2 of this policy, multiplied by the number of dwelling units, as determined on the day that the developer enters into a Section 27 Agreement with the City and provides a Letter of Credit or Other Security in the case of a DC deferral.

POLICY TITLE: DEVELOPMENT CHARGES RATE REDUCTION AND DEFERRAL FOR RESIDENTIAL DEVELOPMENTS

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- b) Where a Letter of Credit is provided, payment of the outstanding DCs will be received via a draw upon the Letter of Credit on the expiry of the 24-month deferral period.

5) LETTER OF CREDIT AND OTHER SECURITY

- a) The developer shall provide a Letter of Credit or Other Security to secure the DC deferral. A Letter of Credit shall conform to the City's Letter of Credit Policy (policy 12.C.04), as amended from time to time. Any Other Security shall conform to the applicable Council policy, as may be approved from time to time.
- b) For Development Approvals with multiple permits (townhouse site plan applications or plans of subdivision) the full value of the DC payable for all units is to be secured with a Letter of Credit.
- c) If a Development Approval lapses or not all units within a Section 27 Agreement have been issued a Building Permit within the time period set out in Section 1 of this policy, then any remaining security or portion thereof shall be released back to the developer.

6) INTEREST WAIVER

- a) No interest will be charged for DCs payable under a Section 27 Agreement.

7) MIXED-USE BUILDINGS

- a) In the case of a Mixed-use Building, only the residential component of the City's DCs may be deferred.
- b) DCs for any non-residential uses within a Mixed-use Building will be charged the applicable prevailing non-residential use DC rate multiplied by the applicable non-residential Gross Floor Area.

8) FEES

- a) The developer shall pay any fees required to enter into the Section 27 Agreement, in accordance with the City's Fees and Charges By-law.

9) ROLES AND RESPONSIBILITIES

- a) Chief Financial Officer and City Treasurer
 - i) Signing of the Section 27 Agreements; and
 - ii) Maintains administrative authority and responsibility for this policy.

POLICY TITLE: DEVELOPMENT CHARGES RATE REDUCTION AND DEFERRAL FOR RESIDENTIAL DEVELOPMENTS

POLICY NO.: 12.C.22

- b) Director, Financial Planning & Development Finance and Deputy City Treasurer
 - i) Responsible for the implementation of, and adherence to, this policy.
- c) Legal Services
 - i) Drafts Section 27 Agreements, in accordance with Section 27 of the Act.
 - ii) Any other legal requirements with respect to or arising from the Section 27 Agreement or this Policy.
- d) Manager, Development Finance
 - i) Responsible for tracking of the rate setting expiration timeline;
 - ii) Responsible for the collection of all Development Charges when due;
 - iii) Processes the draw upon the Letter of Credit or Other Security at the point Development Charges are due, as may be applicable; and
 - iv) Responsible for maintaining compliance to this policy and drafting, reviewing and maintaining departmental operating procedures and processes under this policy.
- e) Development Planning staff
 - i) Responsible for deeming Applications complete;
 - ii) Responsible for verifying Development Approvals.

ADMINISTRATION

Administered by the Office of the City Clerk.

Review Schedule:	Other (specify) 2 years	Next Review Date:	September 30, 2026
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Related Policy(ies):	12.C.04 – Letter of Credit 12.C.17 - Development Charge Interest Policy Under Sections 26.1, 26.2 and 26.3 of the Development Charges Act, 1997
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Related By-Law(s):	109-2022 – City Wide Development Charges By-law
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Procedural Document:

Revision History

Date:	Description:
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Click or tap to enter a date.