

CITY OF VAUGHAN

EXTRACT FROM COUNCIL MEETING MINUTES OF JUNE 25, 2024

Item 3, Report No. 22, of the Committee of the Whole, which was adopted, as amended, by the Council of the City of Vaughan, via recorded vote, on June 25, 2024, as follows:

By approving the recommendation contained in Communication C19, memorandum from the Deputy City Manager, Planning and Growth Management, dated June 21, 2024, as follows:

That the Recommendations in the staff report (Report 22, Item 3) Committee of the Whole (1) June 4, 2024, for 171 MAPLECRETE LP AND 1930328 ONTARIO INC.: COMMUNITY INFRASTRUCTURE AND HOUSING ACCELERATOR FILE CIHA.23.003: 171 MAPLECRETE ROAD, 140-160 DOUGHTON ROAD AND BLOCK 2 ON REGISTERED PLAN 65M-4793, VICINITY OF MAPLECRETE ROAD AND DOUGHTON ROAD be deleted and replaced with the following:

- 1. THAT Council endorse the conversion of Community Infrastructure and Housing Accelerator File to a Minister's Zoning Order ('MZO') request;***
- 2. THAT Council endorse the Applicant to submit a request for a MZO to the Ministry of Municipal Housing and Affairs ('MMAH') with respect to the proposed Development; and***
- 3. THAT Council direct staff to work with the Applicant and the Ministry of Municipal Affairs and Housing to finalize a draft Minister's Zoning Order for Council's consideration that is in substantial conformity with Attachment 2 to this Communication; and***

By receiving the report of the Deputy City Manager, Planning and Growth Management, dated June 4, 2024.

- 3. 171 MAPLECRETE LP AND 1930328 ONTARIO INC. – COMMUNITY INFRASTRUCTURE AND HOUSING ACCELERATOR FILE CIHA.23.003: 171 MAPLECRETE ROAD, 140-160 DOUGHTON ROAD AND BLOCK 2 ON REGISTERED PLAN 65M-4793, VICINITY OF MAPLECRETE ROAD AND DOUGHTON ROAD**

The Committee of the Whole recommends:

- 1. That consideration of this matter be deferred to the June 25, 2024, Council meeting in accordance with the recommendation contained in Communication C1., Memorandum from the Deputy City Manager, Planning and Growth Management dated May 30, 2024.**

Recommendations

- 1. THAT Council request the Minister of Municipal Affairs and Housing ('MMAH') make an Order pursuant to Section 34.1 of the Planning Act for Community Infrastructure and Housing Accelerator ('CIHA') File CIHA.23.003 (171 Maplecrete LP and 1930328 Ontario Inc.)**

CITY OF VAUGHAN

EXTRACT FROM COUNCIL MEETING MINUTES OF JUNE 25, 2024

Item 3, CW Report 22 – Page 2

with respect to the lands municipally known as 171 Maplecrete Road, 140-160 Doughton Road and Block 2 on Plan 65M-4793, shown on Attachment 13, and staff be directed to submit a CIHA Order request package to the Minister of MMAH within 15 days of passing the Council Resolution. The request package shall include the following:

- a. A draft CIHA Order in substantially the same form as that shown in Attachment 13 of this report, which includes the following zoning provision as recommended by Staff:
 - 1) The minimum east side yard setback to the tower portion of any building on the Subject Lands shall be 12.5 m;
 - a) Notwithstanding the above, the minimum east side yard setback to the tower portion of any building may be reduced to 5 m, only if development applications are approved on the lands to the east and includes a 20 m road abutting the full length of the east property line of the Subject Lands, and that the application approvals shall be in full force and effect; or, the Owner enters into an Agreement with the adjacent landowner to the east agreeing to the reduced setback to the satisfaction of the City.
 - b. A request to the Minister of MMAH to attach conditions to the CIHA Order, as identified in Appendix D of Attachment 13, should the CIHA Order be made by the Minister of MMAH; and,
 - c. A request to the Minister of MMAH to exempt subsequent Site Plan and Draft Plan of Condominium Applications from consistency/conformity to provincial plans, the Provincial Policy Statement, A Place to Grow: Growth Plan for the Greater Golden Horseshoe, 2019 ('Growth Plan'), York Region Official Plan, 2022 ('YROP 2022'), Vaughan Official Plan 2010 ('VOP 2010') and the Vaughan Metropolitan Centre Secondary Plan ('VMCSP').
2. THAT for Zoning By-law 001-2021 the CIHA Order request impose a "V1 Vaughan Metropolitan Centre Station Zone" with site-specific zoning provisions for the Subject Lands municipally known as 171 Maplecrete Road, 140-160 Doughton Road, and Block 2 on Plan 65M-4793 as outlined in Appendix A of Attachment 13.

CITY OF VAUGHAN

EXTRACT FROM COUNCIL MEETING MINUTES OF JUNE 25, 2024

Item 3, CW Report 22 – Page 3

3. THAT for Zoning By-law 1-88, the CIHA Order request impose a “C9 Zone” with site-specific zoning provisions for the Subject Lands municipally known as 171 Maplecrete Road, 140-160 Doughton Road, and Block 2 on Plan 65M-4793 as outlined in Appendix B of Attachment 13.

Committee of the Whole (1) Report

DATE: Tuesday, June 4, 2024

WARD: 4

**TITLE: 171 MAPLECRETE LP AND 1930328 ONTARIO INC. –
COMMUNITY INFRASTRUCTURE AND HOUSING
ACCELERATOR FILE CIHA.23.003: 171 MAPLECRETE ROAD,
140-160 DOUGHTON ROAD AND BLOCK 2 ON REGISTERED
PLAN 65M-4793, VICINITY OF MAPLECRETE ROAD AND
DOUGHTON ROAD**

FROM:

Haiqing Xu, Deputy City Manager, Planning and Growth Management

ACTION: DECISION

Purpose

The Owner seeks endorsement from Committee of the Whole, requesting that Council make a resolution to the Minister of Municipal Affairs and Housing ('MMAH') to request the issuance of a Community Infrastructure and Housing Accelerator ('CIHA') Order which facilitates a high-rise mixed-use development with the following characteristics:

- Four (4) residential buildings and one (1) mixed-use building;
- A maximum height of 40-storeys and density of 8.0 times the area of the lot; or the height and density permitted by the in-effect Vaughan Metropolitan Centre Secondary Plan ('VMCSP');
- 1,565 apartment dwelling units (inclusive of 11.5% purpose-built rental); and,
- Minimum 11.5% non-residential uses, inclusive of 8,200 m² (11 floors) of hotel uses, 5,800 m² (4 floors) of office uses, 126 m² of ground-floor retail.

Report Highlights

- The Owner proposes Council consider facilitating the Development through use of the CIHA tool.
- The VMC Program proposes conditions as outlined in this report, should Council resolve to support use of the CIHA tool for the Development.
- The CIHA Order shall include the requirement for a minimum 12.5 m tower setback to the east property line to protect for an equitable 25 m minimum tower separation distance with lands to the immediate east.

Recommendations

1. THAT Council request the Minister of Municipal Affairs and Housing ('MMAH') make an Order pursuant to Section 34.1 of the *Planning Act* for Community Infrastructure and Housing Accelerator ('CIHA') File CIHA.23.003 (171 Maplecrete LP and 1930328 Ontario Inc.) with respect to the lands municipally known as 171 Maplecrete Road, 140-160 Doughton Road and Block 2 on Plan 65M-4793, shown on Attachment 13, and staff be directed to submit a CIHA Order request package to the Minister of MMAH within 15 days of passing the Council Resolution. The request package shall include the following:
 - a. A draft CIHA Order in substantially the same form as that shown in Attachment 13 of this report, which includes the following zoning provision as recommended by Staff:
 - 1) The minimum east side yard setback to the tower portion of any building on the Subject Lands shall be 12.5 m;
 - a) Notwithstanding the above, the minimum east side yard setback to the tower portion of any building may be reduced to 5 m, only if development applications are approved on the lands to the east and includes a 20 m road abutting the full length of the east property line of the Subject Lands, and that the application approvals shall be in full force and effect; or, the Owner enters into an Agreement with the adjacent landowner to the east agreeing to the reduced setback to the satisfaction of the City.
 - b. A request to the Minister of MMAH to attach conditions to the CIHA Order, as identified in Appendix D of Attachment 13, should the CIHA Order be made by the Minister of MMAH; and,
 - c. A request to the Minister of MMAH to exempt subsequent Site Plan and Draft Plan of Condominium Applications from consistency/conformity to provincial plans, the Provincial Policy Statement, A Place to Grow: Growth Plan for the Greater Golden Horseshoe, 2019 ('Growth Plan'), York

Region Official Plan, 2022 ('YROP 2022'), Vaughan Official Plan 2010 ('VOP 2010') and the Vaughan Metropolitan Centre Secondary Plan ('VMCSP').

2. THAT for Zoning By-law 001-2021 the CIHA Order request impose a "V1 Vaughan Metropolitan Centre Station Zone" with site-specific zoning provisions for the Subject Lands municipally known as 171 Maplecrete Road, 140-160 Doughton Road, and Block 2 on Plan 65M-4793 as outlined in Appendix A of Attachment 13.
3. THAT for Zoning By-law 1-88, the CIHA Order request impose a "C9 Zone" with site-specific zoning provisions for the Subject Lands municipally known as 171 Maplecrete Road, 140-160 Doughton Road, and Block 2 on Plan 65M-4793 as outlined in Appendix B of Attachment 13.

Background

Location: 171 Maplecrete Road, 140-160 Doughton Road, and Block 2 on Plan 65M-4793 ('Block 2') (collectively the 'Subject Lands'). The Subject Lands and the surrounding land uses are shown on Attachment 1.

The Subject Lands straddle the existing Vaughan Metropolitan Centre ('VMC') boundary (Attachment 2). The Block 2 lands are within the VMC, while the balance is being studied for incorporation into the VMC through the VMC Secondary Plan Update.

Date CIHA Application was deemed complete: January 29, 2024

Community Infrastructure and Housing Accelerator ('CIHA') Tool

Bill 109, More Homes for Everyone Act, 2022, received Royal Assent on April 14, 2022, and made several amendments to the Planning Act, R.S.O.1990, c.P.13 (the '*Planning Act*'). Amongst the series of amendments, the CIHA tool was created within section 34.1, which enables municipalities to submit requests to MMAH for the issuance of a zoning order ('CIHA Order') on a given site. Unlike traditional planning applications, a CIHA Order is not subject to provincial and municipal policy conformity/consistency analysis. Staff outlined the provisions of the CIHA tool in the February 28, 2023, Committee of the Whole Report, received by Council on March 21, 2023. The link to that report is provided within the Previous Reports/Authority section of this report.

Applications considered through the CIHA process are non-appealable. As such, a person or public body that intends on participating in the associated Public Meeting, either through an oral or written submission will not have provisions to appeal the final CIHA order as it is administered through the Minister of MMAH.

The Minister of MMAH may impose conditions on the approval of the CIHA Order at the request of the municipality, to ensure certain studies, assessments, consultations, and other necessary due diligence associated with any proposed development will be

addressed prior to construction activity. The imposition and the lifting of the CIHA Order conditions is at the sole discretion of the Minister of MMAH.

The Owner seeks that Council resolve to request that MMAH issues a CIHA Order for the Subject Lands to facilitate the Development

A CIHA application has been submitted to facilitate a high-rise mixed-use development on the Subject Lands. Architectural plans in support of the application present a concept (the 'Development') with the following characteristics as shown on Attachments 3 to 7:

- One (1) 30-storey mixed-use building and four (4) residential buildings being 24, 27, 27 and 29-storeys in height;
- A density of 8.0 times the area of the lot (total GFA of all uses is 112,451 m²);
- 1,565 dwelling units (inclusive of a minimum of 182 purpose-built rental units)
 - 27 bachelor units (1.7%);
 - 989 one-bedroom units (63%);
 - 541 two-bedroom units (34.6%);
 - 8 three-bedroom units (>1%);
- Non-residential uses
 - 8,243 m² of Hotel uses (11 floors);
 - 5,840 m² of Office uses (4 floors);
 - 126 m² of ground-floor Retail uses in the northwest building;
- 6,269 m² of amenity area (3,617 m² of indoor and 2,652 m² of outdoor);
- A north-south private street connecting White Elm Road to Doughton Road;
- 928 vehicular underground parking space and 1,618 bicycle parking spaces; and,
- A tower setback of 5 m to the east (shared) property line.

The draft CIHA Order submitted in support of the application accounts for factors beyond those conceptualized on the architectural plans, should greater permissions be realized through an update to the VMCSPP. These includes:

- The maximum height of any building on the lands shall be the greater of 40-storeys, or the height permitted by the in-effect VMCSPP;
- The maximum total GFA for all uses on the lands shall be the greater of 112,451m², or the density permitted by the in-effect VMCSPP; and,
- Minimums of 11.5% non-residential uses and 11.5% of the total dwelling units shall be purpose-built rental.

The Development is intended to be constructed in two (2) phases, with Phase 1 inclusive of the Hotel, Office, 182 purpose-built rental units, and up to 800 additional dwelling units' components. Phase 2 includes the balance of the dwelling units.

Exemptions from conformity/consistency to provincial plans and municipal official plans for future applications

Subsection 34.1(15) of the *Planning Act* allows the Minister of MMAH, at the request of Council via resolution, to exempt subsequent development approval processes from being required to be consistent with the Provincial Policy Statement ('PPS'), and from

conformity to provincial and municipal official plans. In this case, subsequent development approvals include Site Plan and Draft Plan of Condominium.

As a supplement to the Council resolution, the CIHA Order request must also include additional information including: the identification of any licenses, permits, approvals and permissions required to fully permit the proposed development, and a draft CIHA Order which permits the intended use on the Subject Lands.

The City is undertaking an update to the VMCS

The City's ongoing VMCS Update project aims to provide a renewed policy framework to support the build-out of a downtown and central business district as a complete community that is well supported by municipal services and social infrastructure to 2051 and beyond. The project is in Stage 4 of 5, whereby a recommended land use plan and updated Secondary Plan policies are being prepared.

The south portion of the Subject Lands are located within "Study Area A" (Attachment 2), one of the three contemplated expansion areas for the VMC. On February 28, 2024, a draft land-use framework was presented to VMC Sub-Committee. With respect to the Subject Lands, the draft framework conceptualizes:

- A "Mixed Use (Non-residential Uses Required)" designation;
- A maximum density range of 5.0 to 7.5 times the area of the lot;
- A maximum height range of 25 to 40-storeys;
- A non-residential requirement of 11.5%; and,
- A north-south ('N-S') public road along the full east limit of the Subject lands.

VMC Sub-Committee has since directed Staff to explore unlimited height and density provisions for the VMC Area.

Public Notice was provided in accordance with the Planning Act and Council's Notification Protocol

- Date the Notice (circulated 150m from the Subject Lands): February 8, 2024. Notice of Public Meeting was posted on the City's website at www.vaughan.ca and Notice Signs were installed along Maplecrete Road and Doughton Road, in accordance with the City's Notice Signs Procedures and Protocols.
- Date of Public Meeting: March 5, 2024, date ratified by Council March 26, 2024.
- Date of Committee of the Whole Courtesy Notice sent to those requested to be notified: May 27, 2024

Previous Reports/Authority

Previous reports related to the CIHA tool and the subject application are below:

Bill 109 More Homes For Everyone Act, 2022 – CIHA, Committee of the Whole Report [February 28, 2023, Committee of the Whole \(Item 1, Report 11\)](#)

171 Maplecrete LP and 1930328 Ontario Inc., Public Meeting Report

[March 5, 2024, Committee of the Whole Public Meeting \(Item 1, Report No. 8\) and adopted by Council on March 26, 2024](#)

Analysis and Options

Public Consultation and Indigenous Engagement is required in accordance with MMAH's CIHA Tool guidelines

Public Meeting

The March 5, 2024, Public Meeting generated one written and oral submission from KLM Planning, representatives of the landowner immediately east. The written submission is included as Attachment 11 to this report. A summary is provided below:

- The VMCS Update Study shows a conceptual N-S road between the two properties. Vaughan policy provides that the provision of a new road (where possible) should be split 50/50 amongst adjacent landowners;
- Each landowner should provide for a minimum 12.5 m tower setback from the shared property line to achieve a minimum 25 m tower separation distance. The CIHA application currently provides 5 m, which is not equitable.
- The proposed tower setback necessitates the potential N-S road to be located entirely on 130 Doughton Road. Without an agreement, the road should be split 50/50 or determined by the City to be not required at all;
- Landowners do not have any appeal rights upon issuance of a CIHA Order; and,
- The City shall consider the location and provision of the N-S road and appropriate setbacks prior to further advancement and Council endorsement of the CIHA application, to avoid prejudicial impacts to 130 Doughton Road's active development applications and development opportunities.

Indigenous Engagement

Prior to submitting the CIHA request to MMAH, the City is required to demonstrate that proper Indigenous consultation has occurred. The Owner has submitted records of outreach to a total of seven (7) Indigenous communities.

Response letters from the Mississaugas of the Credit First Nation and the Huron-Wendat Nation were received. There were no objections or significant concerns identified through the Indigenous engagement process.

VMC Planning is satisfied with the consultation process for the CIHA application. A summary of the Indigenous Engagement process along with the responses is provided in Attachment 14, which will form part of the submission materials to the MMAH should Council choose to endorse the CIHA Order request.

The CIHA Order is not required to be consistent with the PPS, or conform to the Growth Plan for the Greater Golden Horseshoe and the York Region Official Plan

While a CIHA Order is exempt from consistency/conformity to provincial plans, and the regional official plan, a high-level analysis is provided below for Council's consideration in determining merit for the proposal:

Provincial Policy Statement ('PPS')

The PPS provides direction on matters of provincial interest related to land use planning and development and includes building strong, healthy communities with an emphasis on efficient development and land use patterns, wise use and management of resources and protecting public health and safety.

The Subject Lands are within a Settlement Area and the Delineated Built-Up Area of the Built Boundary of York Region. The Proposed Development facilitates a compact urban form through the intensification of underutilized lands in the City's established Settlement Area where full municipal services exist. The compact urban form, the ability to utilize existing municipal infrastructure, and the opportunity to provide housing with varying unit sizes facilitate a higher density development that capitalizes on the transportation infrastructure investments, consistent with the PPS.

A Place to Grow: Growth Plan for the Greater Golden Horseshoe (the 'Growth Plan')

The Growth Plan provides a framework for implementing the Province's vision for building strong, prosperous communities within the Greater Golden Horseshoe to 2041. The premise of the Growth Plan is building compact, vibrant, and complete communities, developing a strong competitive economy, protecting, and wisely using natural resources, and optimizing the use of existing and new infrastructure to support growth in a compact and efficient form.

The Subject Lands are located in proximity to higher order transit facilities and within the Creditstone Bus Rapid Transit ('BRT') Protected Major Transit Station Area #56 ('PMTSA #56'). The Growth Plan defines a PMTSA as the area within an approximate 500 to 800m of a transit station.

York Region Official Plan ('YROP 2022')

The YROP 2022 designates the Subject Lands as "Community Area", which permits a wide range of urban uses, including residential, population-related employment and community services. An important policy direction for intensification areas is the creation of pedestrian oriented, 15-minute complete communities. The Development is a compact, urban built form with residential and employment uses. The Development is proximal to higher-order transit and provides housing options and tenures at a density that supports transit investments in the City of Vaughan.

The CIHA Order is not required to conform to the municipal Official Plan

While a CIHA Order is exempt from conformity to the municipal Official Plan, a high-level analysis is provided below for Council's consideration:

Vaughan Official Plan 2010 ('VOP 2010') and VMCSPP Context

VOP 2010 identifies Block 2 as within the VMC. The VMCSPP designates Block 2 as "Station Precinct" and identifies it as within "Site-Specific Policy Area F". This designation permits a broad mix of uses such as residential, retail, and service commercial, and encourages a mix of high and mid-rise buildings. Area F (implemented

by way of OPA #86) establishes a maximum building height of 7-storeys on Block 2. Office uses are permitted and retail, service commercial or public use frontages are recommended.

VOP 2010 designates the balance of the Subject Lands as “General Employment”. This designation identifies predominantly industrial areas characterized by low scale buildings with a variety of lot sizes to attract and accommodate a wide range of employment uses.

The Official Plan requires that the distance between the facing walls of two residential towers shall be a minimum of 25 m.

VOP 2010 and VMCSPP Analysis

The proposed Development does not conform to the VOP 2010 and VMCSPP. The VOP 2010 “General Employment” designation does not contemplate the proposed uses and urban built form. The VMCSPP “Station Precinct” designation permits the proposed uses, however, proposed height and density exceeds existing permissions.

The Development generally aligns with the land-use framework presented to VMC Sub-Committee as part of the VMCSPP Update. Heights of 24-30 storeys (or 40-storeys) and 12.6% non-residential component are in line with the heights and non-residential requirements. The proposed density of 8.0 slightly exceeds the earmarked 5.0 to 7.5 floor space index for the Subject Lands.

While the VMCSPP Update shows a conceptual N-S road along the east property line, it is not currently a requirement of the Official Plan. A 12.5 m tower setback to the east property line should be provided as further discussed later in this report.

A CIHA Order would override parts of Zoning By-laws 1-88 and 001-2021

In the case of a conflict with the Zoning By-laws, the zoning imposed by a CIHA Order would prevail to the extent of the conflict. In areas where there is no conflict, the applicable Zoning By-laws continue to apply.

Zoning By-laws 1-88 and 001-2021

Zoning By-law 1-88 zones the Subject Lands “EM2 General Employment Area Zone”, and “C9 Corporate Centre Zone” subject to site-specific exception 9(1473). These zones do not permit the proposed Development on the Subject Lands. The Owner is requesting a “C9 Corporate Centre Zone” with site-specific exceptions as identified in the draft CIHA Order in Attachment 13 of this report to permit the Development.

Zoning By-law 001-2021 zones the Subject Lands “EM2 General Employment Zone”, and “V1(H) Vaughan Metropolitan Centre Station Zone with a Holding Symbol” subject to site-specific exceptions 14.1031 and 14.1124. These zones do not permit the proposed Development on the Subject Lands. The Owner is requesting a “V1 Vaughan Metropolitan Centre Station Zone” with site-specific exceptions as identified in the draft CIHA Order in Attachment 13 of this report to permit the Development.

The Draft Order provides for phasing provisions. Phase 1 includes a minimum of 8,200 m² of Hotel uses, 5,800 m² of Office uses, and up to 982 dwelling units (182 of which are purpose-built rental). Phase 2 includes the balance of residential units. Staff support the phasing proposed, given the community benefit component is largely captured within Phase 1 as outlined later in this report.

The Owner requests consideration of a maximum residential tower floor plate of 850m² whereas 750 m² is permitted. Detailed tower design can be further refined at the Site Plan stage, and the proposed floor plate size is commensurate with approved towers within the VMC area. Staff are satisfied with this request.

The Owner requests consideration of site-specific setbacks relating to all yards. Staff generally support the proposed setbacks as presented in the plans and outlined in the Draft CIHA Order attached, save and except for the tower setback of 5 m from the east property line. Reasoning is outlined in the next section of this report.

Should Council endorse the CIHA Order request, the required zone provisions to permit the Development are identified in Attachment 13. Should the MMAH issue a CIHA Order in response to the Council resolution, only the MMAH may make further amendments to the CIHA Order.

A condition/provision shall be added to the CIHA Order which requires a 12.5m minimum tower setback from the east property line

The Owner has proposed an east side yard setback of 5m to the residential tower portions of Buildings “NE”, “E” and “SE” as shown on Attachment 3. Key design criteria consistently applied in the VMC and throughout the City is to provide a minimum 25m separation distance between towers. When there are abutting properties, a 12.5 m tower setback is applied equally on each property to achieve the 25m separation distance.

The Owner of lands to the immediate east has submitted Official Plan and Zoning Amendment Files OP.23.007 and Z.23.012 on September 7, 2023. The files are supported by a site plan shown on Attachment 8 that illustrates a 20 m road on the lands abutting the entire east property limit of the Subject Lands. The 20 m wide road when added to the 5m setback proposed for the towers on the Subject Lands would achieve a minimum 25m separation distance between towers on the two properties. However, these files are in circulation, have just gone to Public Meeting on June 4, 2024, have not been considered by Council and have no approval status.

The VMCS Update is underway wherein preliminary consideration has been given to a north/south road connection along the full east limit of the Subject lands and extending south, like that shown on the site plan for the lands to the east. However, at this time, the VMCS Update has not been approved by Council and has no status.

Accordingly, to ensure an equitable, minimum 25m tower separation distance is achieved with the lands to the east, the following provisions are included in the draft CIHA Order, as recommended by Staff:

- 1) The minimum east side yard setback to the tower portion of any building on the Subject Lands shall be 12.5 m;
 - a) Notwithstanding the above, the minimum east side yard setback to the tower portion of any building may be reduced to 5 m, only if development applications are approved on the lands to the east and includes a 20 m road abutting the full length of the east property line of the Subject Lands, and that the application approvals shall be in full force and effect; or, the Owner enters into an Agreement with the adjacent landowner to the east agreeing to the reduced setback to the satisfaction of the City.

The Planning Act allows for the CIHA Order to contain conditions that can only be cleared and/or removed by MMAH

Under Section 34.1(13) of the *Planning Act*, the MMAH May impose conditions on the CIHA Order. The conditions can only be cleared and/or removed by the Minister of MMAH. To ensure the orderly development of the Subject Lands, the City has proposed conditions, which may be referred to in Appendix D of Attachment 13, to be included with the CIHA order request. While the Minister of MMAH is solely responsible for lifting/removing conditions, it is anticipated that MMAH will liaise with appropriate parties requesting the conditions to ensure they are satisfactorily addressed before removing. Please refer to Attachment 12 for a summary outlining the rationale for each condition.

Community benefits above and beyond the Planning Act requirements are to be provided as part of the CIHA Order request, to the satisfaction of the City

The City considers contribution to community benefits for CIHA Order requests on a case-by-case basis. On March 21, 2023, Council adopted to receive the February 28, 2023, report which outlines key priorities intended to meet the City's objectives for creating complete communities. Key priorities in the form of Guiding Principles to be considered by Council when evaluating a CIHA application's merit are assessed below.

Geography: The geographical Guiding Principles identify preferred location criteria that contribute to the City's overall intensification objectives, which include:

1. Within the Urban Boundary but outside the Greenbelt Area
 - The Subject Lands are within the Urban Boundary and outside the Greenbelt.
2. Within Intensification Areas and Regional/Primary Intensification Corridors;
 - Block 2 is within the VMC boundary which is an intensification area, while the balance of the Subject Lands are contemplated to be within the VMC through the VMCS Update.
3. Areas where rapid transit service is in operation; and,

- The Subject Lands are located within PMTSA #56.
4. Areas where municipal water/ wastewater/ stormwater servicing capacity and regional wastewater servicing allocation are available.
- The Subject Lands are serviced by municipal services. Servicing allocation is a requirement for the development and is a condition imposed by the City.

Land Use: The Development should include significant contributions to the City beyond what is required for a standard Official Plan and Zoning By-law Amendment approval process.

- The Development consists of 182 purpose-built rental units (11.5%);
- Approximately 35% of the total units are family-sized (2-bedroom or greater) within a high-density development;
- 11.5% of non-residential uses are proposed, inclusive of a minimum of 5,800m² of office and 8,200m² of hotel uses; and,
- The Development contributes to sustainable transportation as it represents intensification of lands within a PMTSA.

Community Benefits Charge ('CBC') is applicable and will be collected at the building permit stage

The Development meets the criteria for CBC being 5 or more storeys and 10 or more units. The City passed the CBC By-law on September 14, 2022, which is therefore the applicable mechanism used to collect community benefits.

Operational Impact

VMC Planning

In advancing the Development forward, Staff will continue to work with the Owner, provided the CIHA Order is successful. Building footprints, massing and site layout are conceptual and will be subject to further updates as the Development progresses. The Development requires a detailed and strategic design that is transit supportive and pedestrian oriented. The provisions required to facilitate this vision will be incorporated into the Draft CIHA Order as conditions.

The CIHA Order is precedent setting for lands that are currently outside of the VMC. Issuance of an Order would proceed in advance of the development of broader long-term land use policies for the VMC and associated potential expansion areas.

Should the CIHA Order be successful, the Development will require a future Site Plan Application and multiple Draft Plan of Condominium applications.

Departmental and External Agency Comments

This Application was circulated to City Departments and External Agencies for comments. The following City Departments and External Agencies have provided their comments as included in Attachment 9.

- VMC Planning
- VMC Development Engineering
- VMC Urban Design and Parks Development
- Vaughan Building Standards (Zoning Division)
- Vaughan Real Estate Department
- Vaughan Fire and Rescue Services, Emergency Planning Division
- Vaughan Parks, Forestry and Horticulture Operations Department
- Vaughan Economic Development
- Canadian National Railway Company
- Bell Canada

The comments provided by the above-noted City Departments and External Agencies will be addressed through draft CIHA Order conditions shown in Attachments 12 and 13 of this report. Some condition requests are not included in the attachments as they deal with concerns that do not affect the principle of development and are best satisfied at the subsequent Development Application stage. CIHA Order conditions may only be imposed, managed, and cleared by MMAH.

The following City Departments and External Agencies responded with no comments or concerns:

- Vaughan Environmental Services Department
- Vaughan Cultural Heritage Division
- By-Law and Compliance, Licensing & Permit Services
- Bombardier
- Toronto and Region Conservation Authority
- Toronto Transit Commission
- York Catholic District School Board
- York Region District School Board
- Canada Post
- NavCanada
- Alectra Utilities
- Enbridge Gas

Financial Impact

There are no requirements for new funding associated with this report.

Broader Regional Impacts/Considerations

York Region has reviewed the application and provided their comments as shown in Attachment 10. Should Council elect to proceed with the resolution to forward the CIHA Order to MMAH, York region has provided conditions that are to be included in the Order as shown on Attachment 13 and further explained in Attachment 12.

Conclusion

VMC Planning Staff has reviewed the CIHA Application. Under Section 34.1 of the *Planning Act*, CIHA proposals are exempt from demonstrating consistency with the PPS, conformity to the Growth Plan, YROP and municipal Official Plan. Planning Staff believes in maintaining a comprehensive and orderly approach to planning.

Should Council choose to endorse CIHA application CIHA.23.003, VMC Planning Staff recommends Council include the Draft CIHA Order (Attachment 13) in its resolution and submission package to MMAH.

Should Council not endorse the CIHA Order request, file CIHA.23.003 shall be received for information and the Zoning By-laws remain the solely applicable zoning document for the Subject Lands. This decision would direct staff to cease the processing of the CIHA order request and close the file.

For more information, please contact Matthew Peverini, Senior Planner – VMC, at extension 3636.

Attachments

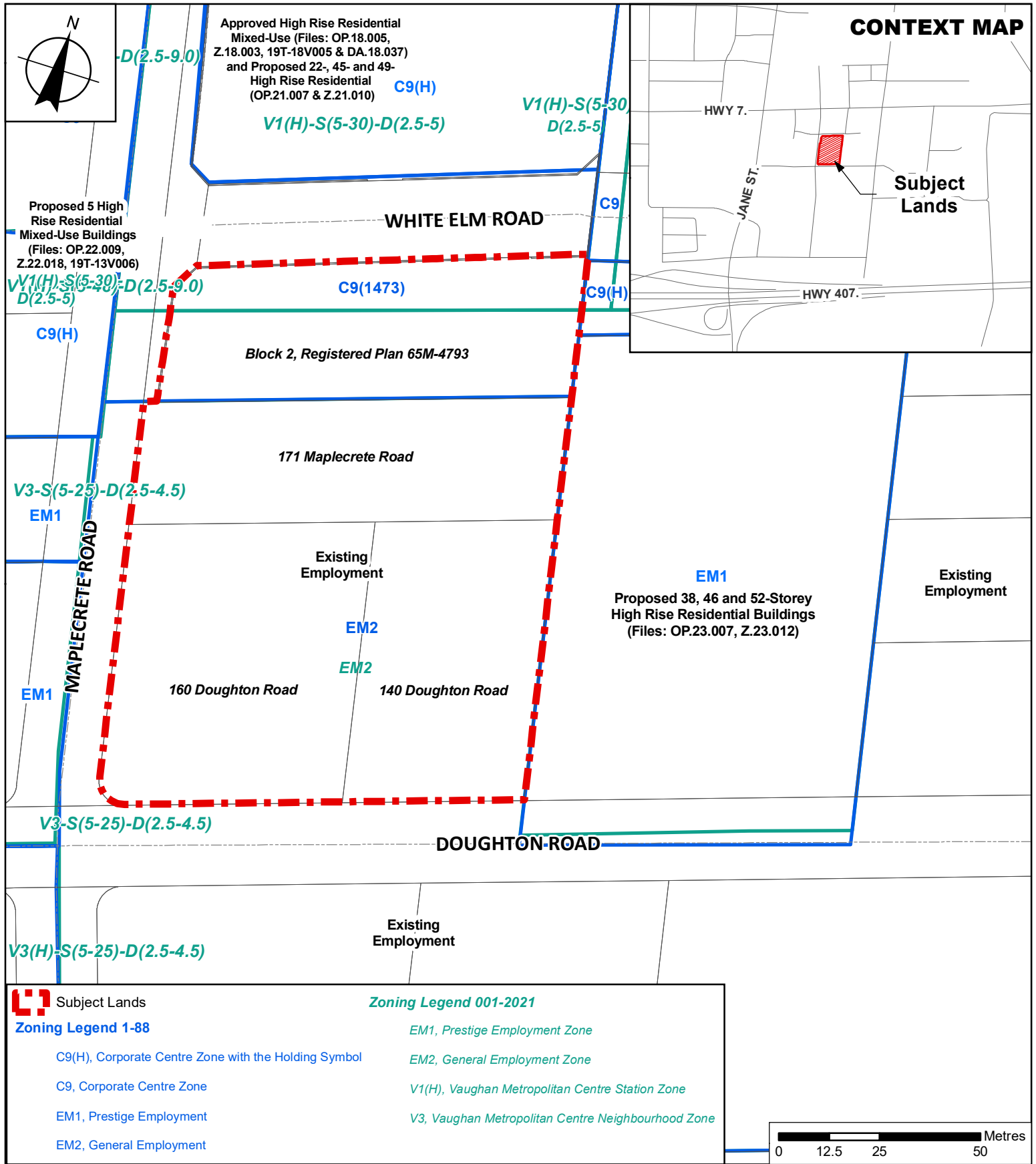
1. Context and Location Map
2. Potential VMC Boundary Expansion Areas
3. Conceptual Site Plan and Proposed Zoning
4. Conceptual Building Elevations (North and East)
5. Conceptual Building Elevations (South and West)
6. Conceptual Perspective Renderings
7. Conceptual Perspective Renderings (Street-view)
8. Conceptual Site Plan for Planning Files OP.23.007 and Z.23.012
9. City Departments and External Agencies Comments
10. York Region Comments
11. Public Meeting Communication
12. CIHA Order Conditions and Rationale
13. Draft CIHA Order
14. Indigenous Engagement Summary

Prepared by

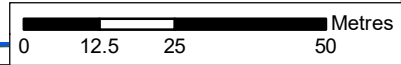
Matthew Peverini, Senior Planner – VMC, ext. 3636.

Gaston Soucy, Senior Manager of Planning and Urban Design – VMC, ext. 8266.

Christina Bruce, Director of Policy Planning and Special Programs, ext. 8231.



Subject Lands	Zoning Legend 001-2021
Zoning Legend 1-88	<i>EM1, Prestige Employment Zone</i>
C9(H), Corporate Centre Zone with the Holding Symbol	<i>EM2, General Employment Zone</i>
C9, Corporate Centre Zone	<i>V1(H), Vaughan Metropolitan Centre Station Zone</i>
EM1, Prestige Employment	<i>V3, Vaughan Metropolitan Centre Neighbourhood Zone</i>
EM2, General Employment	



Context and Location Map

LOCATION:
 171 Maplecrete Road, 140-160 Doughton Road
 and Block 2 of 65M-4793



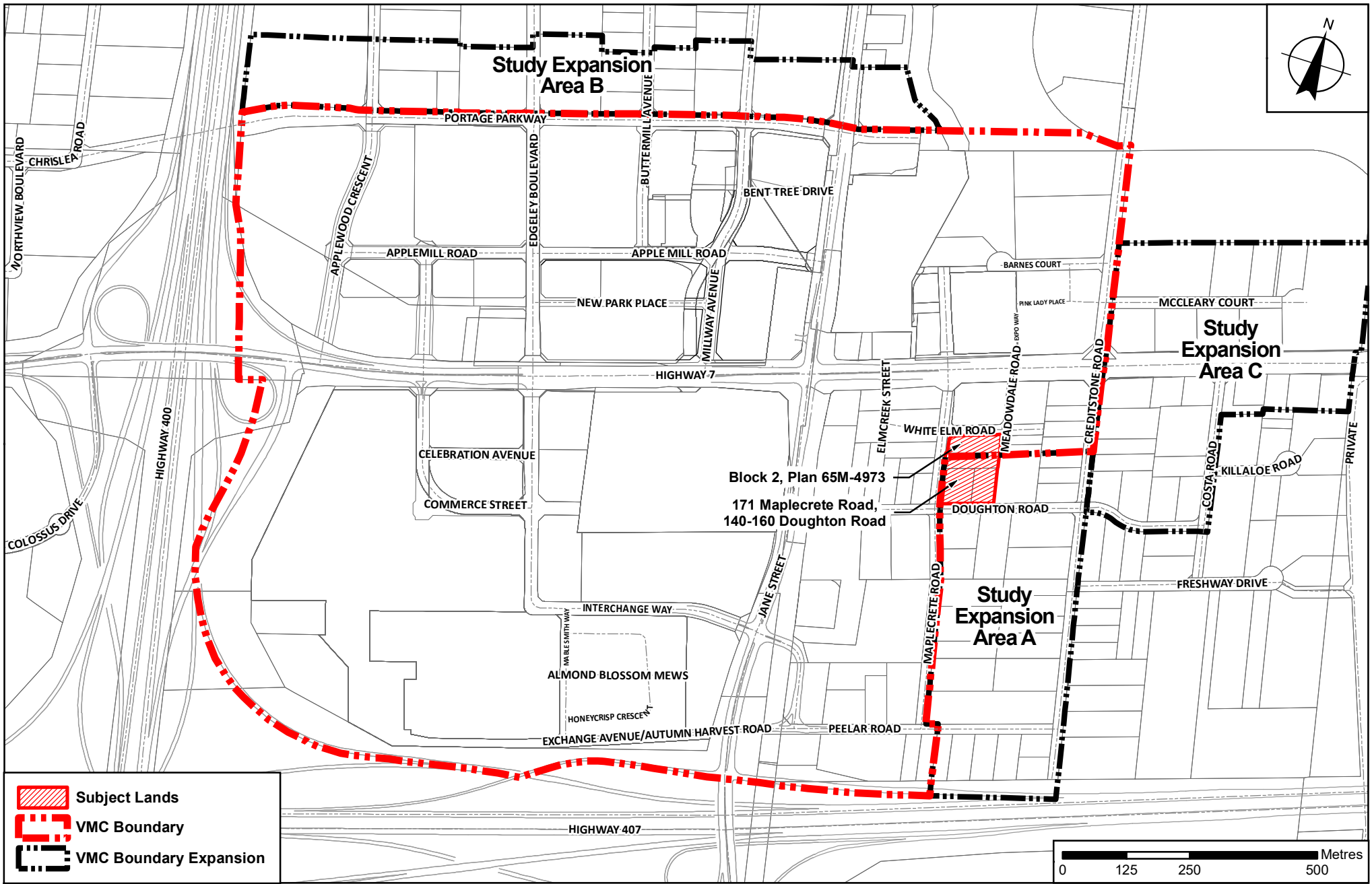
APPLICANT:
 171 Maplecrete LP & 1930328 Ontario Inc.

Attachment

FILE:
 CIHA.23.003

1

DATE:
 June 4, 2024



Potential VMC Boundary Expansion Areas

LOCATION: 171 Maplecrete Road, 140-160 Doughton Road, and Block 2 of 65M-4793

APPLICANT:
171 Maplecrete LP and
1930328 Ontario Inc.

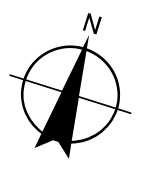
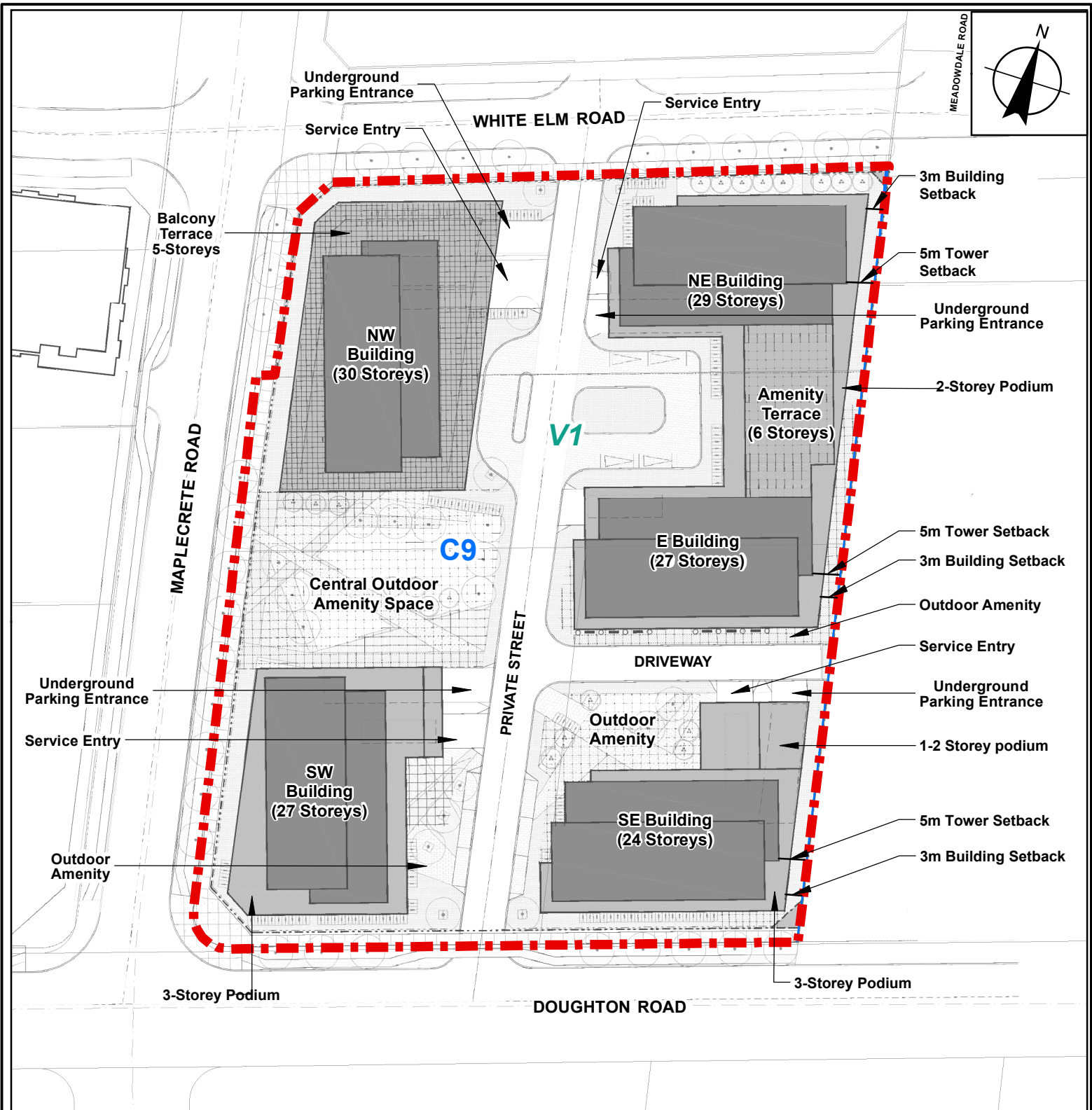


Attachment

FILE:
CIHA.23.003

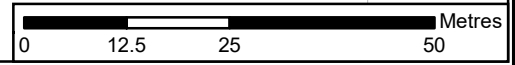
DATE:
June 4, 2024

2



 Subject Lands
Zoning By-Law 1-88
 C9, Corporate Centre Zone

Zoning By-Law 001-2021
 V1, Vaughan Metropolitan Centre
 Station Zone



Conceptual Site Plan and Proposed Zoning

LOCATION:
 171 Maplecrete Road, 140-160 Doughton Road
 and Block 2 of 65M-4793



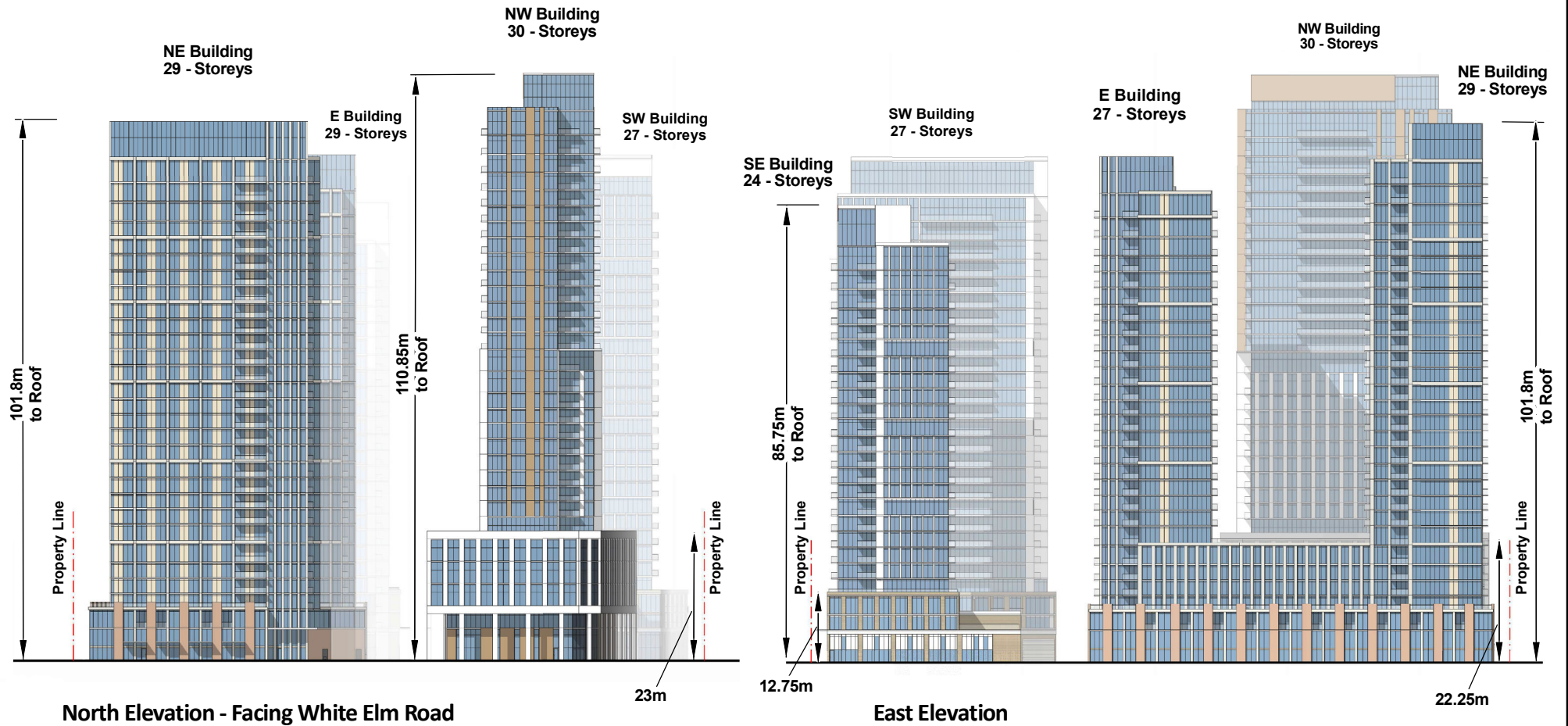
APPLICANT:
 171 Maplecrete LP & 1930328 Ontario Inc.

Attachment

FILE:
 CIHA.23.003

DATE:
 June 4, 2024

3



Conceptual Building Elevations (North and East)

LOCATION:
 171 Maplecrete Road, 140-160 Doughton
 Road and Block 2 of 65M-4793

APPLICANT:
 171 Maplecrete LP and 1930328 Ontario Inc.

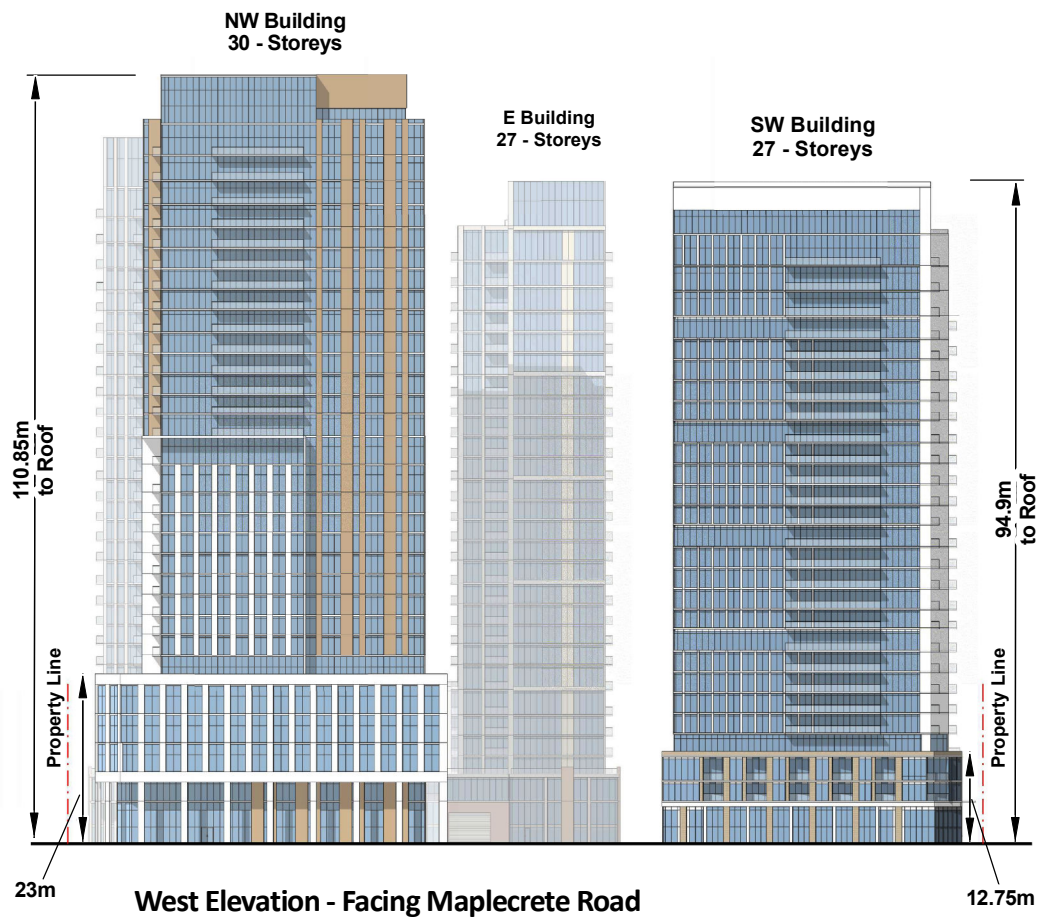
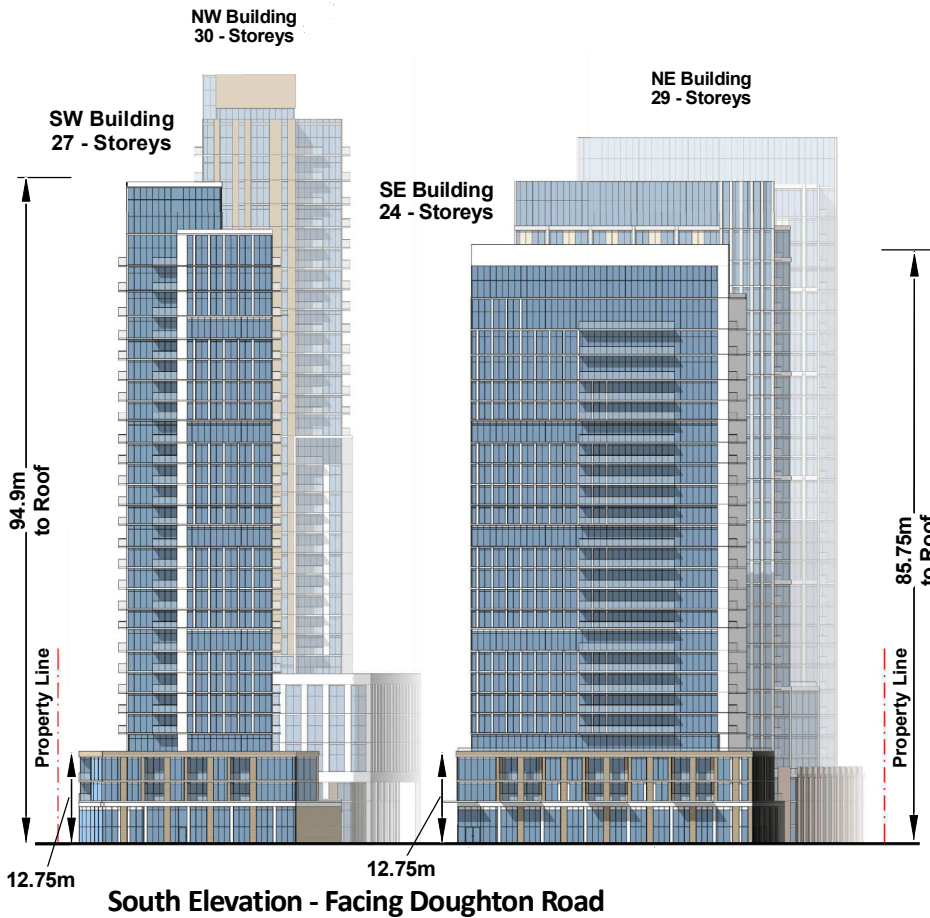


Attachment

FILE:
 CIHA.23.003

DATE:
 June 4, 2024

4



Building Elevations (South and West)

LOCATION:
171 Maplecrete Road, 140-160 Doughton Road and Block 2 of
65M-4793

APPLICANT:
171 Maplecrete LP and 1930328 Ontario Inc.



Attachment

FILE:
CIHA.23.003

DATE:
June 4, 2024

5



View Looking From the West



Conceptual Perspective Renderings

LOCATION:
171 Maplecreek Road, 140-160 Doughton Road and Block 2 of
65M-4793

APPLICANT:
171 Maplecreek LP and 1930328 Ontario Inc.



Attachment

FILE:
CIHA.23.003

DATE:
June 4, 2024

6



**Look From North West at Intersection of
White Elm Road and Maplecrete Road**



**Look From South West at Intersection of
Doughton Road and Maplecrete Road**

Conceptual Perspective Renderings (Street-view)

LOCATION:
171 Maplecrete Road, 140-160 Doughton Road and Block 2 of
65M-4793

APPLICANT:
171 Maplecrete LP and 1930328 Ontario Inc.

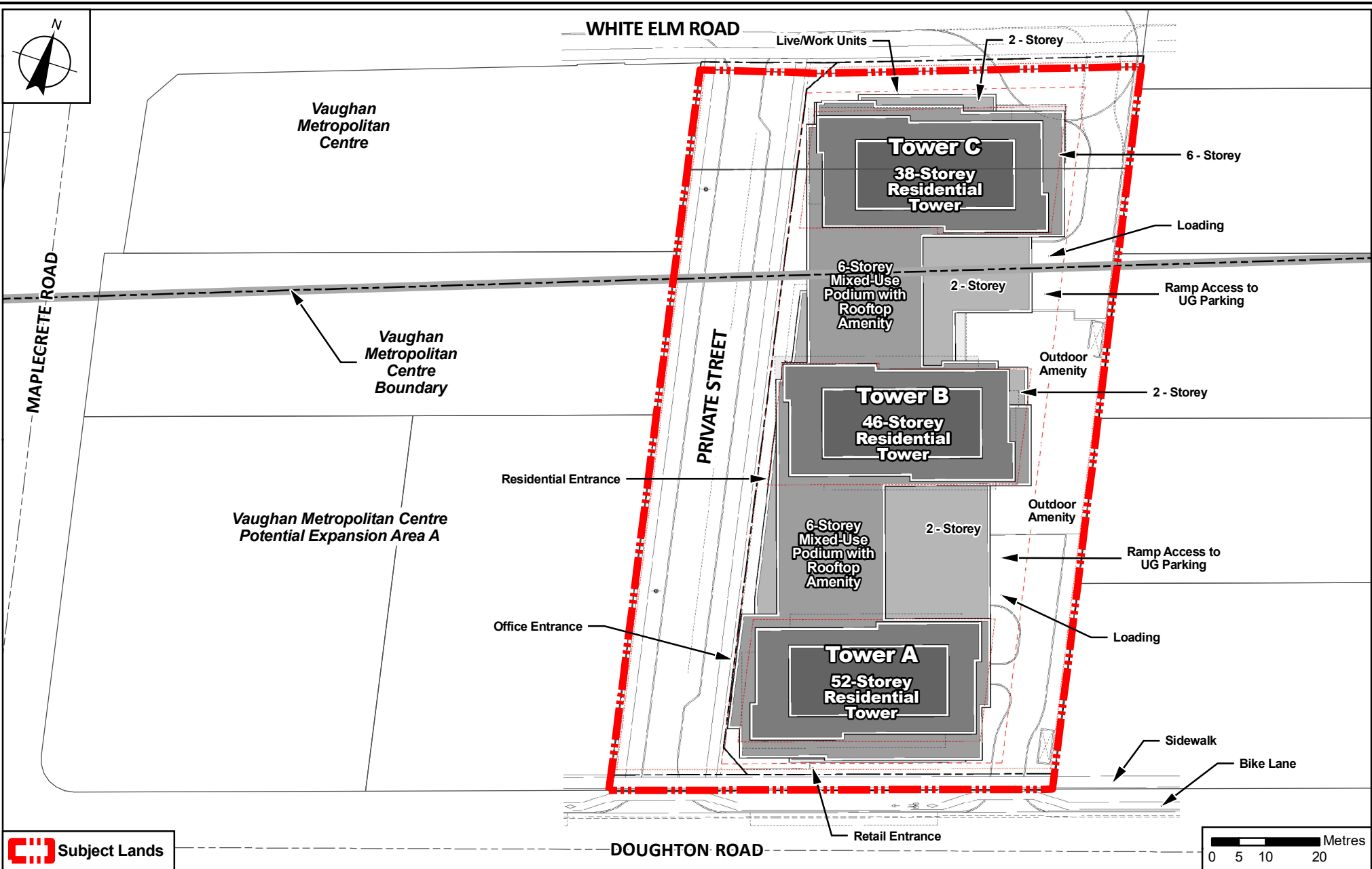


Attachment

FILE:
CIHA.23.003

DATE:
June 4, 2024

7



 Subject Lands

Proposed Site Plan for 130 Doughton Road (Files OP.23.007 and Z.23.012)

LOCATION:
171 Maplecrete Road, 140-160 Doughton Road and Block 2 of 65M-4793
APPLICANT:
171 Maplecrete LP & 1930328 Ontario Inc.



Attachment

File:
CIHA.23.003

Date:
June 4, 2024

8

ATTACHMENT 9

CITY DEPARTMENTS AND EXTERNAL AGENCIES COMMENTS

Date: March 6, 2024

To: Thanh Nguyen, Liberty Development Corporation

From: Matthew Peverini, Senior Planner - VMC

Cc: Gaston Soucy – Senior Manager, Planning and Urban Design – VMC
Christina Bruce – Director, Policy Planning and Special Programs

Re: **Planning Comments (1st Circulation) – File No. CIHA.23.003**

Applicant: 171 Maplecrete LP and 1930328 Ontario Inc.

Location: 171 Maplecrete Rd., 140-160 Doughton Road, and Block 2 on 65M-4793

VMC Planning Staff have reviewed the 1st Circulation of the Community Infrastructure and Housing Accelerator ('CIHA') Application on the above-mentioned site, circulated on January 22, 2024, and provide the following comments:

1. The Planning Justification Report ('PJR') acknowledges that a priority issue pertaining to the VMCSPP update is that the intensity of development trends are exceeding growth targets. The lands are forecasted to have a maximum density of 5.0 FSI to 7.5 FSI (dual) through the VMC Update process. The application contemplates 8.0 FSI which continues to exacerbate the priority issue of managing overdevelopment within the VMC.
2. Please coordinate with the neighbour immediately west respecting the forecasted n-s Local ROW along the east property line and tower separation distances. See VMCSPP policy 8.7.18, and VOP2010 policy 9.2.3.6 (d) which speaks to separation distances.
3. Staff support ~12.5% non-residential component of the project, as it achieves the minimum 11.5% "Mixed-Use (Non-Residential Requirement)" forecasted on the lands through the ongoing VMCSPP Update. Per comments from Economic Development, consider inclusion of additional ground-floor retail space. Note the VMCSPP update envisions Doughton Rd. to have a "required retail, service commercial, community facility or public use frontage".
 - A "Potential Minor Civic Facility" is forecasted in the vicinity of the Subject Lands through the VMCSPP update. Given this is a CIHA application, is there an opportunity to include "community infrastructure" component (or something along the same lines) on the ground floor along the Doughton Road frontage?
4. Staff encourage a more considerate effort to include a greater portion of family sized units. The proposed 2 and 3-BR currently represent less than 15% of the total unit yield. Additional family sized units within the Development were a point of discussion from Committee at the Public Meeting. Neighbouring approvals are composed of upwards of 30% of 2-BR and 3BR composition per project.

- 2951-2957 Hwy 7: approximately 34%
 - 216-220 Doughton: approximately 35%
 - 185 Doughton: approximately 33%
 - 2901 Hwy 7: over 50%
 - 2851 Hwy 7: approximately 40%
5. How are the 182 purpose-built rental units intended to be owned/operated? Any contemplation for “Affordable” purpose-built rental units (see definition of affordable in YROP 2022? Explore [Financial Incentives for Complete Communities | York Region](#) that may be applicable to the proposal.
 - For office, YR provides an 18 month deferral and purpose built rental has minimum 36 months DC deferral up to 20 years depending on scale of the project and if the units are affordable
 6. In the PJR, there are references to “in keeping with the intent of affordable housing policies by the region”. *Affordable* is defined by the YROP 2022, what component of this application is “in keeping with the intent” of the definition, and how is *affordable* being implemented?
 7. Consider VMCS Policy 8.7.17 regarding podiums. Generally podiums should be 4-stores. Consider a reduction with respect to the shared podium for the NE and E building.
 8. Please indicate all the tower floorplate sizes on the site plan and statistics. The Draft Order includes a blanket 850 m² permission for all towers, whereas 8.7.18 of the VMCS (for reference) permits 750 m². Variation in floor plate size is encouraged. Further, how does a larger floorplate contribute to “reducing negative impacts associated with shadows” as stated on page 21 of the PJR?
 9. Respecting Section 3.2 Transit-Oriented Development in the PJR, please clarify on how the forecasted population and jobs generated for the development were established. Assuming PPU is for population, but no basis for the office portion.
 10. Staff acknowledge Indigenous engagement sought as part of the initial submission. With the subsequent submission, please include a follow up (and response) from those communities that did not respond from the first round of consultation.
 11. Please clarify whether there is intent to connect to the lands east through the driveway, or whether the driveway is intended to terminate at the lot line. If there is ultimately a road along the east property line as forecasted through the VMCS update, consideration should be given to relocating the access point from Doughton Rd. onto the future road.
 12. Confirm dimensions of loading doors for waste collection. 7.5m minimum is required to facilitate Municipal Waste Collection.

13. Please clarify the strategy for the Condominium tenure for the build out of the subject lands. How will a phased tenure be addressed (matters like Zoning compliance, access etc.).

Draft Orders Comments

14. Review/ incorporate comments from Zoning to ensure appropriate relief to the Zoning By-law is recognized to ensure smooth implementation.
15. The Orders require significant improvement beyond what has been formally submitted. Several components of relief required from the ZBLs were not captured in the Draft Orders. Prior to resubmission, review and incorporate comments from Zoning and consult with staff as required. A Draft Order in the following link for Reference: [DRAFT CIHA Order](#)
16. Staff recommend that a phasing clause be included in the Order to ensure that the NW building is built as part of Phase 1 and before Phase 2. For example – “The approval of the Site Development Application for Phase 2 of the Subject Lands shall not proceed until a Building Permit has been issued for the minimum 8,200 m² of Hotel Uses and minimum 5,800 m² of Office Uses within the NW Building in Phase 1”.
17. Under **Definitions**, add “CIHA Order Conditions” means the requirements listed in Schedule ___ that shall be satisfied prior to the Minister of Municipal Affairs and Housing issuing the Order.
18. Under **Application**, remove Item 4 a). as lands are not brought into a secondary plan area through a Zoning Order. That is an Official Plan exercise.
19. Under **Permitted Uses** for V1 zone – need to add units on the ground floor as a permitted use for all towers (except NW). See Table 10-2 in By-law 001-2021.
20. Under **Zoning Requirements**, ensure definitions (i.e. height, lot, GFA, etc) and parking requirements (as a rate are included. Further, there is only one property line, unless you are proposing to carve up the subject lands. Please clarify.
21. There should be a Schedule (for reference) to the Order which labels the Towers and delineates the Phases.

Matthew Peverini, MCIP, RPP

Senior Planner, VMC

T. 905-832-8585 ext.3636

E. Matthew.Peverini@vaughan.ca

END OF MEMORANDUM

DATE: March 08, 2024

[sent via email only]

TO: Matthew Peverini, Senior Planner, VMC

RE: **Latif Fazel c/o 171 Maplecrete LP & 1930328 Ontario Inc.**
171 Maplecrete Road, 140-160 Doughton Road and Block 2 on 65M-4793
File No. CIHA.23.003 (Related File: PAC.22.079)
Engineering Review -1st Submission

The Policy Planning & Special Programs (VMC Development Engineering) staff have reviewed the submission in support of the above noted applications and provide the following comments:

The following materials were reviewed and formed the basis of our comments:

- Preliminary Functional Servicing Plan, Project No. 22151, Dwg. FSP-1, dated Jan. 19, 2024 prepared by Valdor Engineering Inc.
- Preliminary Functional Grading Plan, Project No. 11161, Dwg. FSG-1, dated Jan. 19, 2024 prepared by Valdor Engineering Inc.
- Functional Servicing and Stormwater Management Report – 171 Maplecrete Road, Project No. 22151, dated January 2024 prepared by Valdor Engineering Inc.
- Revised Report on Preliminary Geotechnical Investigation Proposed Highrise Buildings 171 Maplecrete Road, 140-160 Doughton Road & Block 2, Registered Plan 65M-4793, Vaughan, Ontario, Project No. 21-405-100R1, dated January 16, 2024 prepared by DS Consultants Ltd.
- Preliminary Hydrogeological Investigation Proposed Residential Development 171 Maplecrete Road, 140-160 Doughton Road & Block 2, Registered Plan 65M-4793, Vaughan, Ontario, Project No. 21-405-100, dated January 16, 2024 prepared by DC Consultants Ltd
- Architectural Drawings – 171 Maplecrete Rd, Project No. 140526, Dwg. A100, dated Jan. 19, 2024 prepared by Arcadis
- Landscape Plan – 171 Maplecrete Road, Project No. 23-118, Dwg. LS100, Rev. 01 dated Jan. 19, 2024 prepared by Studio tla
- Noise Impact Study – 171 Maplecrete Road, Vaughan, ON, Project No. GTR-22028965-A0, dated Jan. 19, 2024 prepared by EXP
- Transportation Impact Study - 171 Maplecrete Rd, Project No. 23198, dated January 2024 prepared by LEA

Site Description

The subject lands is located northeast corner of Doughton Road and Maplecrte Road. The development proposal is comprised of total 1.406 hectares of land:

- NE Tower (29 Storeys Residential)
- E Tower (29 Storeys Residential)
- NW Tower (30 Storeys Mixed-Use)
- SW Tower (27 Storeys Residential)
- SE Tower (24 Storeys Residential)
- Three (3) levels of underground parking

Preliminary Engineering Comments:

1. The Owner has submitted a report. “*Functional Servicing and Stormwater Management Report – 171 Maplecrete Road, Project No. 22151, dated January 2024 prepared by Valdor Engineering Inc.*”, the comments are as follows:

(Section 2.2) To maintain the water quality under the interim condition, please note that flushing program will be required for the dead-end watermain on White Elm Road.

(Section 2.3 & Fig. 2B) The report shall include the calculations pertaining to the size of the proposed water service connections. Complete the table below with supporting calculations to confirm the residual pressures at the building face is above the City’s requirements (min. 40 psi for Peak Hour Demand and min. 20 psi for Max. Day Demand + Fire Demand).

Service connections off White Elm Road, Maplecrete Road & Doughton Road	Flow (L/s)	Head Loss (psi)	Head Loss (kPa)	Residual pressure at main (psi/kPa)	Residual pressure at Building (psi/kPa)
100mm Domestic (Peak Hour)					
150mm Fire (Max. Day + Fire)					

(Fig. 2A) Please note that the existing Doughton watermain (300mm dia.) between Maplecrete Road and Jane Street will be upgraded to 400mm dia. by the City.

(Section 3.2 & Fig. 3A) Please note that the existing sanitary sewer that drains westerly on Doughton road will be upgraded to 600mm diameter prior to connecting to existing 900mm diameter trunk sanitary sewer on Jane Street.

(Fig. 3A) Please be advised that the design and construction of sanitary sewer upgrade on Maplecrete Road and Doughton Road to be delivered by the City. Please remove the reference to Croziers drawings and replace with “Accardi Scheaffers Prj. No. RFP22-093”

(Section 4.3) The report shall discuss the long-term dewatering requirement for Phase 1 & 2, see Hydrogeological Report – Section 4.5.2. Please note that the long-term groundwater discharge should be controlled as part of the allowable release rate for the site prior to discharging to the municipal storm sewer system. Please note, should there be any site related changes (relative to the hydrogeological predictions) that may result in higher groundwater discharge rates will also have to be managed to the allowable release rate. Confirmation on groundwater quality will also be required. Quality consideration need to be accounted for including any treatment and its associated backwash flows. It is encouraged to explore the options to re-use the groundwater on-site (such as irrigation, toilet flushing) to eliminate the requirement of permanent dewatering for this development.

Subject to the final recommendations in the Hydrogeological Report, a design brief for the long-term dewatering system shall be submitted to identify the layout of the pump setup, pre-treatment requirement and facilities, flow meter, sampling port, discharging point, emergency storage and space allocated for future proof the pre-treatment installation for the proposed development. Coordination between civil, architectural & geotechnical consultants will be required in order to complete the design of the permanent groundwater discharge system for the proposed development.

Please note the following requirements for the long-term dewatering discharge to municipal storm sewers:

- A long-term discharge approval is required for each connection to the City's storm sewer. Each groundwater discharge line will require a flow meter and sampling port. Sampling ports to be shown on Drawings.
- Upon transfer of ownership from developer to condominium corporation (s), if multiple condominiums exist, transfer of discharge approval ownership and responsibilities will need to be accounted for at transfer of ownership by developer/condo corporations.
- All discharge will need to be in compliance with the Sewer Use bylaw including water quality requirements.
- To future proof the discharge for compliance with the Storm Sewer bylaw limits:
 - i. Space is to be provided to accommodate treatment, to be supported with Mechanical Drawings showing space allotted for potential future treatment.
 - ii. Note should treatment be deemed necessary in the future, additional requirements will apply depending on the treatment specifics.
- For discharge approval requirements, please refer to: [Dewatering Discharge Approval \(vaughan.ca\)](#)

*Additional stormwater management comments prepared by Dana Khademi, CoV Storm Drainage Engineer:

Item	Subject	City Comments
1	Quantity Control	It is noted that the runoff from the subject site will be controlled to the target release rate (2 year post development with maximum 80% imperviousness) via a 250mm orifice and underground storage tank within P1 underground parking level. Please provide details of the SWM tank, orifice, outlet pipe and connection to the storm sewer within Mapcrete.
2	Storm Sewer Capacity	Redevelopment of the subject site may proceed prior the Mapcrete Road storm sewer upgrade. Therefore, please demonstrate with supporting calculations/storm sewer design sheet that the downstream storm sewer has capacity to convey the controlled flow from the subject site. Please consider the existing storm sewer network and the proposed storm sewer network as per the Liberty Cement submission.
3	HGL and On-Site SWM Measures	Please confirm if mitigation measures are required for the on-site SWM measure(s) based on the HGL, which was referenced to be established for

		the Liberty Cement/2901 Hwy. 7 development.
4	15mm Retention Volume	It is noted that the retention volume required for the 15mm on-site retention criterion will be achieved via rainwater harvesting and reuse for irrigation. At the site plan/detailed design stage, please provide supporting calculations demonstrating the retention volume will be fully utilized for irrigation. Another use for the harvested rainwater may be required to ensure the full retention volume is utilized in a timely manner.
5	Site Drainage Area	The existing and proposed drainage conditions figures (4 and 5, respectively) illustrate the pervious, impervious and roof areas within 171 Maplecrete, 160 & 140 Doughton Road. These land-uses are not shown for Block 2 of 2901 Hwy. 7. Please update these figures to include 2901 Hwy. 7 south of the proposed White Elm Road.
6	Maplecrete and Doughton Road Widening	Please provide design details for the widening of Maplecrete Road and Doughton Road adjacent to the subject site. Improvements/widening of RoWs/roads within the VMC southeast quadrant are required to provide quality control until a new SWM strategy for the southeast quadrant is established. With the subdivision/RoW submission, please ensure quality control is provided for the road runoff.

2. The Owner has submitted a noise report “*Noise Impact Study – 171 Maplecrete Road, Vaughan, ON, Project No. GTR-22028965-A0, dated Jan. 19, 2024 prepared by EXP*”, the comments are as follows:

(Section 3.0) What about air traffic? Is the subject land under any flight path?

(Section 3.0) The study shall include the assessment of ground borne vibration from Toronto-York Spadina Subway line which terminates at the VMC.

(Section 3.1.3, Table 7) The noise assessment shall include the Jane Street, Hwy 400, Hwy 407, White Elm Road. Future local road just east of the subject lands.

Apply the following **ultimate AADT** for the noise analysis and update the assessment accordingly:

Road	Ultimate AADT
Jane Street	45,000
Hwy 400	TBD – Consult with MTO
Hwy 407	TBD – Consult with 407ETR
White Elm Road (20m ROW)	*min. 10,000
Future Local Road (20m ROW) – at adjacent property (130 Doughton Road) between White Elm Road and Doughton Road	*min. 10,000

*Consult with traffic consultant, LEA, for the expected traffic volume and the ultimate AADT based on the Transportation Impact Study (Prj. # 23198) dated January 2024 prepared for the site. Supportive road traffic data to be included in the Appendix C.

(Section 3.1) The noise assessment shall clearly define the recommendations under the interim (Phase 1) and ultimate (Phase 2) conditions of the re-development.

(Table 9 - Column 1) - Clarify the Receptor ID with respective the Building (i.e. similar to Table 10)

(Section 3.1.4.1 & Fig. 3) For the outdoor amenity area, control measures (barriers) shall be designed to reduce the sound level as close to 55dBA. Otherwise, the proponent has to demonstrate and satisfy the City that it is not feasible to meet the Leq 55dBA sound level criterion. Provide a table to summarize the required sound barrier height to achieve 55dBA for OLA (in the format of 1dBA increments). Detail design of the barrier shall be shown on the site grading plan.

(Section 3.2.1) The noise assessment shall include the nearby Class III industrial facility (MacMillan Rail Yard, Toromont Industries Limited (Toromont) located at 3131 Highway 7)

(Section 3.2.3) The report shall include the assessment of the stationary noise from the nearby residential, commercial and industrial facilities/buildings within 300m of the site and the roof top equipment. Include detailed mapping showing the area subject to the proposed development and all industrial facilities, including zoning of vacant lands in the vicinity, and any other sources of adverse effects. The mapping should include radius's showing the minimum separation distances and influence areas of applicable Classes and the identification of facilities with the potential to cause adverse effects and their industrial classification.

(Section 3.2.7) Justify the potential impacts of emergency generators and mechanical roof top as part of the final building design. The noise assessment shall include the loading dock and garbage pick-up for the development.

3. The Owner has submitted a geotechnical report "*Revised Report on Preliminary Geotechnical Investigation Proposed Highrise Buildings 171 Maplecrete Road, 140-160 Doughton Road & Block 2, Registered Plan 65M-4793, Vaughan, Ontario, Project No. 21-405-100R1, dated January 16, 2024 prepared by DS Consultants Ltd.*", the comments are as follows:

(Section 4.8) The underground garage roof slab below the private road and driveway shall be designed to support the garbage truck & emergency vehicles such as fire truck. Table 4.8 shall recommend the pavement structure and the required fill materials between the pavement and P1 slab.

The Owner shall ensure that recommendations contained in the final report are being incorporated in the design and adequate field inspection is provided to validate the requirements during the construction.

4. The Owner has submitted a hydrogeological report “*Preliminary Hydrogeological Investigation Proposed Residential Development 171 Maplecrete Road, 140-160 Doughton Road & Block 2, Registered Plan 65M-4793, Vaughan, Ontario, Project No. 21-405-100, dated January 16, 2024 prepared by DC Consultants Ltd*”, the comments are as follows:

(Section 4.2) The final report shall discuss the feasibility of providing fully waterproofed system as an option to eliminate the long-term dewatering. If permanent dewatering is required, consultant team will be required to provide the clear scheme with respect to any groundwater pumping and discharging. Detail design shall include the layout of the pump setup, sampling port, discharging point and emergency storage and the treatment facilities based on the City’s Sewer Use By-law 087-2016, as amended.

5. At the **SITE PLAN** submission stage, the Owner shall also submit the following plans/reports for review and approval:

Servicing and Grading Plans

Please find the enclosed redlined plans (FSP1 & FSG1) for the preliminary comments.

External Lighting Plan for the Site

The plan to show lighting levels in units of lux. Please note, section 3.15 of By-law 1-88 states the following: “Any outdoor floodlighting associated with the use of any lot in any zone shall be directed inward and downward or in such a manner as to not illuminate any part of any adjacent lot.” The drawings shall bear the seal, date and signature of the Licensed Professional Engineer.

External Works – Road Widening on Maplecrete Road and Doughton Road (26m ROW)

a) Detailed design of road widening shall be part of the engineering submission. The drawing set shall include cover sheet, general notes sheet, grading plan, composite utility plan, plan and profile drawings, storm tributary area, standard detail plans, landscape and streetscape plan, streetlighting plan, erosion and sediment control plan, pavement marking and signage plan. Add the Director of Development Engineering approval block to the subdivision engineering submission drawings.

- White Elm Road (20M ROW) – Follow the cross-section previously approved for application 2901 Hwy 7. Details of the boulevard breakdown shall refer to the approved landscape drawings.
- Maplecrete Road (26M ROW) - Follow the cross-section previously approved for application 2901 Hwy 7. Details of the boulevard breakdown shall refer to the approved landscape drawings.
- Doughton Road (26m ROW) – Follow the Commerce St. cross-section (attached)

b) Street Lighting Design

The streetlighting plan including photometric analysis shall be submitted to the satisfaction of the VMC Program Department. Design drawings shall bear the seal, date and signature of the Licensed Professional Engineer.

(Design Parameters for Minor Collector – 26m ROW)

Luminaire type: LUMEC Capella LED (fitted with a 7-pin NEMA photocell receptacle conforming to ANSI C136.41 and prewired for 0-10V control)

Pole: straight, round in shape, base mounted

Material: Cast Aluminum

Colour & Finishes: Galvanized and then powder coated to texture (i.e. US2 fine iron glimmer)

Pole spacing: 25-35m

Overall pole height: max 7.62m (25 ft.)

Ave. illuminance: 10.0 Lux

Uniformity average/min: 3.0

Uniformity max/min: 6.0

Parallel/opposite pattern

(Design Parameters for Local Road – 20m ROW)

Luminaire type: LUMEC Capella LED (fitted with a 7-pin NEMA photocell receptacle conforming to ANSI C136.41 and prewired for 0-10V control)

Pole: straight, round in shape, base mounted

Material: Cast Aluminum

Colour & Finishes: Galvanized and then powder coated to texture (i.e. US2 fine iron glimmer)

Pole spacing: 25-35m

Overall pole height: max 7.62m (25 ft.)

Ave. illuminance: 7.0 Lux

Uniformity average/min: 6.0

Parallel/opposite pattern

NOTE:

- Design shall be based on medium pedestrian traffic
- Sidewalk Illuminance (Avg. 5 lux, Avg/Min. 4.0)
- Intersection Illuminance (refer to RP8-14 lighting guidelines for details)
- For White Elm Road, please refer to the approved street lighting plan for details (2901 Hwy 7, Photometric Analysis, Project No. 05628-00, Dwg. Photo-1, R7 dated Aug. 10, 2023 prepared by RTG Systems Inc.)

Development Agreement

The Owner shall enter into Development Agreement for the external works, to satisfy all conditions, financial or otherwise, of the City with regard to such matters the municipality may consider necessary including payment of the development levies, the provision of roads and municipal services landscaping and fencing.

The Owner shall convey sufficient property for the road widening at no cost and free of charge and encumbrance to the City. Details as follows:

- Convey sufficient property for the Maplecrete Road widening (26.0m R.O.W.). New property line shall be measured 13m from the centerline of the road.

- Convey sufficient property for the Doughton Road widening (26.0m R.O.W.). New property line shall be measured 13m from the centerline of the road.
- Maintain the 5.0m x 5.0m daylight triangle with 0.3m reserve at the south-east corner of White Elm Road and Maplecrete Road.
- Maintain the 8.0m x 8.0m daylight triangle and 3.0m return with 0.3m reserve at the north-east corner of Doughton Road and Maplecrete Road.
- Provide the 5.0m x 5.0m daylight triangle easement at the south-east corner of the subject land to protect the sight-line for the future north-south road on the adjacent property to the east that intersects with Doughton Road.
- Provide the 3.0m x 3.0m daylight triangle easement at the north-east corner of the subject land to protect the sight-line for the future north-south road on the adjacent property to the east that intersects with White Elm Road.

Dewatering Plan for Construction

The Owner shall coordinate with the civil and geotechnical consultants to provide a detail dewatering plan for the construction.

The submission shall include:

- The route of dewatering and the discharge point with erosion measures.
- The capacity of receiving system and any impacts on downstream flooding.
- The contingency plan during the course of construction and during heavy rainfall events.
- The detail of dewatering process for building foundation and underground parking structure.
- The bedding requirements for the installation of underground services.
- Supporting reports/plans/drawings detail but not limited to such as, discharge rate, duration of discharge, length of discharge operation, monitoring plan, contingency plan, sampling/testing method, location of site, discharge location and type of sewer system, type of flow meter and installation location, flow data logging, etc.

Shoring Excavation Plan

The Owner shall submit the shoring and tie-back design drawings for review and approval.

- No part of the shoring system shall be constructed on or within the City's lands or the public road allowance with the exception of the tiebacks.
- Tiebacks shall be constructed at a depth of no less than three (3) metres below the ground surface on City lands to provide adequate clearance for the maintenance of underground services/utilities.
- The Owner shall design the future structure so the tiebacks will become redundant once the permanent underground building structure is fully completed. The Owner shall provide the City with an Engineer's certificate to this effect to the satisfaction of the City.
- Refer to the Geotechnical Report for the site specific recommendations.
- Consent from adjacent landowner will be required for works on the adjacent property.

- The Owner shall enter into Encroachment Agreement as part of the Road Occupancy Permit, to satisfy all conditions, financial or otherwise, of the City with regard to construction/tieback encroachment.

Encroachment Agreement

Prior to commencement of construction, the Owner shall apply the Road Occupancy Permit for the construction and enter into Encroachment Agreement to satisfy all conditions, financial or otherwise, of the City (including shoring system, hoarding, construction access, crane swing and construction dewatering).

The following information shall be provided to prepare the Encroachment Agreement:

- OWNER: (The corporate name shall be consistent in the site plan application.)
- REPRESENTATIVE: (The Owner's representative who is charged with the administration of the terms of the agreement.)
- OWNER'S LANDS: (The description shall describe the relevant Lot, Concession, and designed Parts on a Reference Plans.)
- CITY LANDS: (The description shall describe the relevant Lot, Concession, and designed Parts on a Reference Plans.)
- COMMENCEMENT DATE/EXPIRY DATE:
- SIGNING OFFICIER: (Signing officers shall have the authority to bind the corporation.)
- EMERGENCY CONTACT INFORMATION OF OWNER: (name/phone no./email)
- CONSTRUCTION LOGISTICS PLAN: (Identify the following information on plan: property line, nearby driveways, covered/designated walkway, gates, limit of shoring, construction fence/hoarding, tower crane, crane reach, crane pickup area, lay down area, construction hoist, hoist platform, staging area, site offices, road traffic direction, construction traffic path)
- CRANE SWING PLAN: (The plan shall be prepared and certified by P.Eng.)

Construction Logistics Plan to clarify the construction traffic direction, staging area and signage installation.

Tentative Construction Schedule and Phasing Plan (including roads, underground parking, site servicing and all the above ground structure.)

1. OTHER

a) TRANSPORTATION REVIEW

- Please find the attached memorandum from Jillian Britto, Transportation Project Manager, dated March 8, 2024 for transportation comments.
- Refer to the attached redline drawing (LS100) for the active transportation comments

b) ENVIRONMENTAL SITE ASSESSMENT

The following materials were reviewed and formed the basis of the environmental comments:

- Site Screening Questionnaire, dated January 19, 2024

- Noise Impact Study, 171 Maplecrete Road, Vaughan, ON, prepared by EXP Services Inc. (EXP), dated January 19, 2024
- Phase One ESA, 140 & 160 Doughton Road, 171 Maplecrete Road, Vaughan, Ontario, prepared by DS Consultants Ltd. (DS), dated November 24, 2021
- Phase Two ESA, 140 & 160 Doughton Road, 171 Maplecrete Road, Vaughan, Ontario, prepared by DS, dated June 8, 2022
- Phase One ESA, 2901 Highway 7, Vaughan, Ontario, prepared by DS Consultants Ltd., dated April 8, 2022; and
- Phase Two ESA, Western and Southern Portions of 2901 Highway 7, Vaughan, Ontario, prepared by DS, dated April 26, 2022

Environmental comments are as follows:

1. An Air Quality/Land Use Compatibility study has not been provided with this submission. Given the commercial/industrial uses surrounding the lands that comprise this CIHA (collectively, the Subject Property), we would request that one be completed.
2. In the Noise Impact Study, EXP recommends the use of warning clauses due to a neighbouring stationary source and that further analysis "be performed when information of the proposed mechanical system is available." No comments are made relating to the current report; however, the City requests an updated noise report at the time of the site plan application that addresses EXP's recommendation for further analysis and to review the warning clauses with respect to a more complete design.
3. DS has provided several ESA reports, which as a whole, cover the Subject Property. In these reports, elevated pH levels were identified in surficial soils on the north portion of the Subject Property, a petroleum hydrocarbon (PHC) exceedance was identified in surficial soils on the southeast portion of the Subject Property, and various salt-related parameters in soil and groundwater were identified to exceed the applicable standards across the Subject Property. The salt-related exceedances were attributed to road salting for safety purposes and were discounted under the exemption within the regulation; however, DS recommended remedial excavations to address the pH levels and PHC exceedance. The Owner/DS is requested to submit a remedial action plan (RAP) that addresses these exceedances in accordance with the City's RAP checklist (attached).
4. DS is requested to provide the City with a Reliance Letter for their four reports listed above, in accordance with our template (attached). It is understood that further reports (remediation reports, as well as any Update report(s) which may be required for filing of a Record of Site Condition) will be issued in the future, a separate Reliance Letter will be requested for the additional reports once completed.
5. The Owner shall submit a copy of a Ministry of the Environment, Conservation, and Parks (MECP) Record of Site Condition filed on the environmental site registry covering the subject lands or implementation and confirmation of completion of the Remedial Action Plan to the satisfaction of the City, prior to applying for an above grade building permit.

c) WATER/SANITARY SERVICING ALLOCATION

For City Council to adopt a resolution allocating sewage and water supply capacity in accordance with the City's approved Servicing Capacity Distribution Policy assigning capacity to the Subject Lands.

d) The Owner/Owner is to obtain all necessary approvals from the Region of York, TRCA, and MTO, if applicable.

e) **SITE PLAN FEE (2024 rate)**

The Owner shall pay Development Engineering Site Plan Complex fee pursuant to the Fees and Charges By-law as amended. The fee amount is **\$670,095.22** ($\$657/\text{unit} \times 300 \text{ units} + \$300/\text{unit} \times 1,265 \text{ units} + \$6.58/\text{sqm} \times 14,209\text{sqm}$ – Site Statistics as per Drawing A001, 171 Maplecrete Rd dated Feb. 19, 2024 prepared by Aracadis) and shall be included with the next submission or prior to Development Engineering clearance. If the fee is not paid in the calendar year in which it is calculated, the fee will be subject to increase in the next calendar year. Payment of the Engineering Site Plan Fee shall be a condition of Final Site Plan Approval.

f) **GRADING INSPECTION FEE (2024 rate)**

The Owner shall pay Development Engineering Site Plan Grading Inspection fee pursuant to the Fees and Charges By-law as amended. The fee amount is **\$708.00** (1.41 ha) and shall be included with the next site plan submission. If the fee is not paid in the calendar year in which it is calculated, the fee will be subject to increase in the next calendar year. Payment of the Engineering Grading Inspection Fee shall be a condition of Final Site Plan Approval.

We will require that the above noted comments be addressed by the appropriate consultants and that all applicable reports be revised accordingly and resubmitted for review and approval. Please provide a Response Matrix with the next submission. Staff reserve the right to provide additional comments on road network/access, municipal services and environmental noise analysis as part of the future submissions.

Should you require further assistance, please contact me at extension 8567.

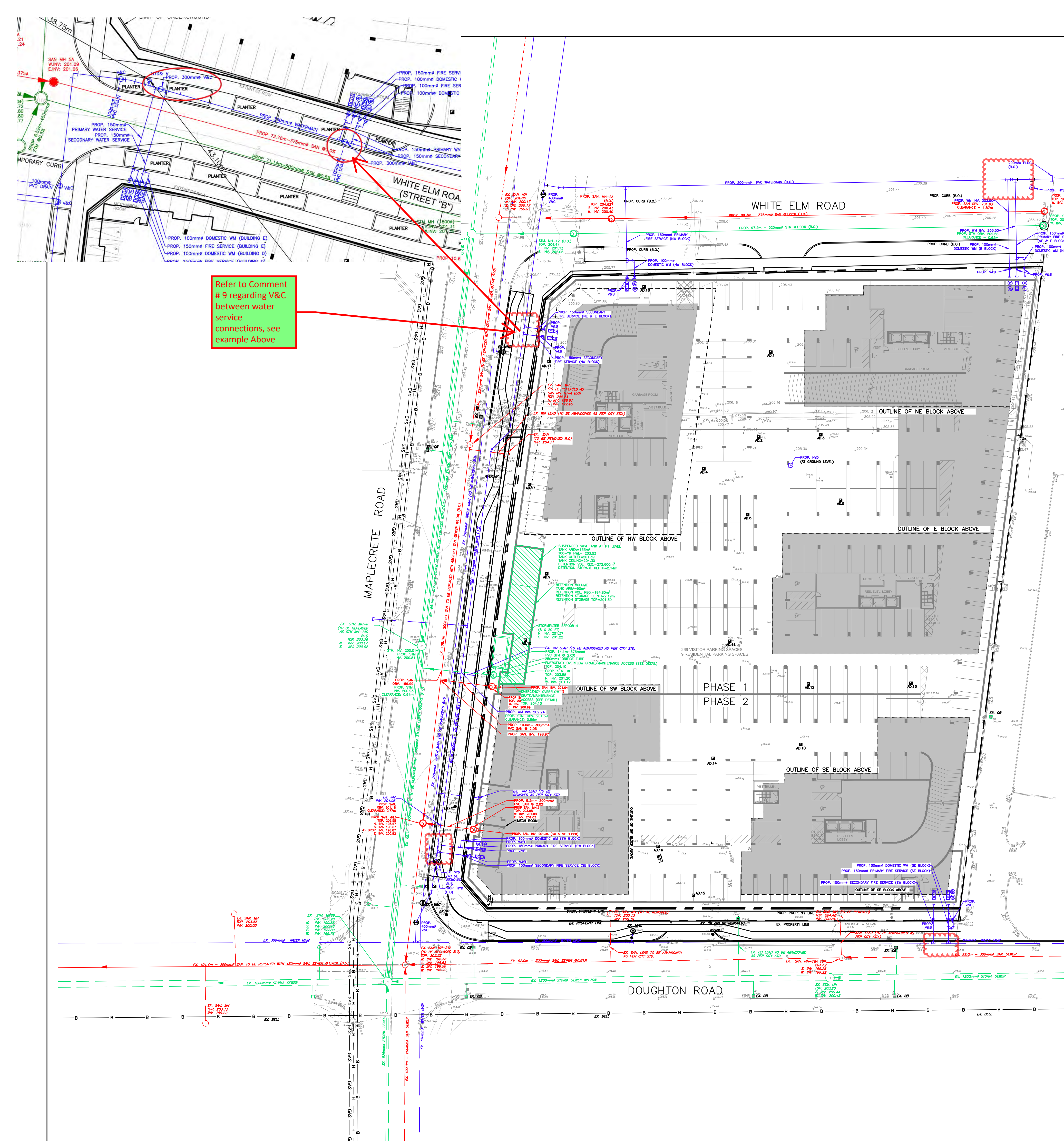
Sincerely,

Danny Woo, P.Eng.
Development Engineering Lead, VMC
Policy Planning & Special Program

Enclosures:

1. Redline Plans (Redlined_SUB1_Jan-19-2024 – 171 Maplecrete Rd_City comments.pdf)
2. Transportation Memo dated March 08, 2024 + Redlined Drawing LS100
3. Letter of Reliance – template
4. City's RAP checklist
5. Road cross-sections (White Elm Road, Maplecrete Road, Commerce Street)

Cc: Musa Deo, Manager of Development Engineering – VMC
Nelson Pereira, Manager of Development Finance



Refer to Comment # 9 regarding V&C between water service connections, see example Above

- Detailed Site Servicing Plan shall include the following:
1. Include the City file no. (DA.XX.XXX)
 2. Identify the dimension(s) of the proposed property line offset (13.0m) from the road centerline to confirm the road widening on Maplecrete Road and Doughton Road. Include the cross sections for road widening and verify any conflicts with existing utilities. The Owner will be responsible for making any adjustments or relocations and inform the affected utility company of the requirement to obtain approvals.
 3. For the road widening along the development frontage on Maplecrete Road and Doughton Road, submit the drawing set which shall include cover sheet, general notes sheet, general grading plan, composite utility plan, standard detail plans, landscape and streetscape plan, streetlighting plan, erosion and sediment control plan, pavement marking and signage plan. Include the Director of Development Engineering approval block to the submission drawings.
 4. Clearly identify the underground parking limit on plan.
 5. Provide cross-section(s) for the proposed service connections on White Elm Road, Maplecrete Road and Doughton Road. Maintain the vertical and horizontal clearance at the pipe crossing per MOE guidelines, where applicable. Identify inverts at the building for the service connections. The proposed lot line control MHS and sanitary/storm laterals shall conform with the City design standard C-102. The control manhole shall be independent from the parking structure (not attached) and place it close to property line. Ensure lot line control MHS are accessible and free of obstruction at all time.
 6. Sanitary service connection shall maintain with min. 2% slope and 2.6m frost cover.
 7. Provide the cross-section of the storm service connection including details such as inverts, frost depth (min. 1.2m) design head, 100YR HWL for the on-site control and the storage tank. To avoid flooding inside the parking garage, a positive outlet from the storage tank shall be designed for emergency overflow and drain directly to the road. Identify the 100 YR HGL for the road sewer and compare the water levels inside the downstream manholes to confirm the free flow condition provided in the design.
 8. Provide min. 2.1m frost cover below the road centerline for the water service connections.
 9. Provide V&C as isolation valve between the water services. (see example)
 10. With the multiple water service connections for the subject land, verify the proposed setup is fulfilling the building code requirement to effectively provide fire protection for the entire development (Ref: O.B.C. 3.2.9.7.)
 11. Identify the Siamese connection. Hydrant must be placed within 45m of any proposed Siamese connection.
 12. The private fire hydrant shall be protected by the internal fire BFP at the fire system riser in the P1 mechanical room. Under code and our Backflow By-law 177-2020, fire hydrants are to be protected by an approved BFP. Anti-tampering device shall be installed on private hydrant.
 13. Please note that one bulk meter per municipal address. The applicant shall clarify the condo. corp. arrangement for the subject development and determine the metering setup. City's Planning Dept. shall be engaged to apply for additional addresses where multiple meters are needed for billing purpose.
 14. Add "Note" to clarify the arrangement of the remote meter reading access on outside face of building that the water meter for each building can be read at the ground level without entering the mechanical room. Please make reference to Std. W-109 for the setup of meter remote wire.
 15. Include a table to summarize the clearance at the pipe crossing based on the MOE guidelines, if any.
 16. Remove existing service connections that will not be utilized by the new development. Identify the details of services decommissioning based on the City's requirements. Refer to City Design Criteria 1.4.8.1 for the details of decommissioning sewer service connections. Refer to City Design Criteria 1.5.7.1 for the details of decommissioning water service connections.
 17. Please note that White Elm Road will be extended on the west side of Maplecrete Road as part of the Liberty Maplecrete development (2951 Hwy 7). Refer to Valdor File: 11161
 18. The design of road widening, intersection alignment and servicing upgrade on Maplecrete Road shall be coordinated with Crozier Consulting Engineers (Project no. 1525-6298) and Accardi Schaeffers (Project no. RFP22-093) and Valdor Engineering (Project no. 11161)
 19. The road and servicing design at the eastern edge shall be coordinated with the adjacent properties shall be coordinated with adjacent development (130 Doughton Road – Civil consultant: Schaeffers Consulting Engineers, Project no. 2022-5268).
 20. If permanent dewatering is required for the development as per the Geotechnical/Hydrogeological Report, the consultant team shall complete the design of the permanent dewatering including the layout of the pump setup, treatment facilities, flow meter, sampling port, discharging point and emergency storage specifically designed for the proposed development. Supporting calculations for the design shall be included in the stormwater management report. Long-term discharge of groundwater to the municipal sewer system shall be designed based on the following requirements:
 - Explore the option to re-use the groundwater on-site instead of discharging to the municipal sewer system.
 - Discharge flows are to be discharged to the municipal storm system only. Discharge is not allowed to the sanitary system.
 - Average and maximum daily flow rates are to be provided as well as maximum daily volume.
 - Site drainage and collection/discharge flows are designed/operated in separate systems.
 - The combination of site and collection/discharge flows shall not exceed the allowable site discharge rate.
 - Discharge water shall meet the quality requirements limits as per the City's Sewer Use By-law 087-2016.
 - The permanent collection/discharge system shall include the layout of the pump setup, treatment facilities, flow meter, sampling port, discharging point and emergency storage and space allocated for future proof the pre-treatment installation specifically designed for the proposed development.
 - Identify the layout of permanent under floor drainage facilities on the Site Servicing Plan with the supporting calculations in the stormwater management report.
 - The applicant will be responsible for the ongoing water quality and quantity monitoring and reporting to the City on a yearly basis.
 - Permanent discharge permit will be required as part of the site plan approval.
 21. Provide the SWM summary (see attached template) to justify the breakdown of the actual site release rate in terms of controlled/uncontrolled flow and permanent dewatering to confirm the overall site release. If applicable, the SWM summary shall specify the designed max. long-term dewatering rate that the on-site SWM system can handle. Provide the detailed plan/storage in case you need to cease the dewatering discharge under emergency. It is encouraged to explore the options of having water-tight underground structure (i.e. bathtub design) or re-use the groundwater on-site (such as irrigation, toilet flushing) to eliminate the requirement for permanent dewatering for the development.
 22. To address the long-term dewatering requirements, identify the layout of Sampling Port (at ground surface) and Flow Meter (at P1 level) on the Site Servicing Plan. Please note that the long-term dewatering system shall include



- KEY PLAN
N.T.S.
- PROPOSED CATCH BASIN
 - STORM MANHOLE
 - SANITARY MANHOLE
 - VALVE AND BOX
 - HYDRANT
 - ▲ EXTERIOR DOOR LOCATION
 - FIRE DEPARTMENT CONNECTION
 - STAND ALONE FIRE DEPARTMENT CONNECTION
 - EXISTING AREA DRAIN
 - EXISTING CATCH BASIN
 - WATER METER
 - BACKFLOW PREVENTER
 - DOUBLE CHECK DETECTOR ASSEMBLY
 - EXISTING GAS MAIN
 - VALVE AND CHAMBER

VERSION NOTE
NOTES AND ELEVATIONS SHOWN ON THIS PLAN ARE IN METRES CAN BE CONVERTED TO FEET BY DIVIDING BY 0.3048.

RING NOTE (PLAN OF SURVEY)
INGS ARE GRID DERIVED FROM THE 6' UTM CO-ORDINATE SYSTEM ZONE 17, 83 (ORIGINAL) BASED ON REAL TIME NETWORK OBSERVATIONS.

VATION NOTE (PLAN OF TOPOGRAPHY)
ATIONS ARE GEODETIC AND ARE REFERRED TO THE CITY OF VAUGHAN H MARK No. 23-6, HAVING AN ELEVATION OF 202.852 METRES.

DATE	REVISIONS	BY
JAN 19/24	ISSUED FOR COORDINATION	D.G

DATE	REVISIONS	BY

PRELIMINARY

VALDOR ENGINEERING INC.
Consulting Engineers - Project Managers
571 CHERRYL ROAD, UNIT 4, 2ND FLOOR, WOODBINE, ONTARIO, L4B 5M7
TEL: (905) 884-0004 FAX: (905) 884-0007
E-MAIL: info@valdor-engineering.com www.valdor-engineering.com

PROPOSED RESIDENTIAL DEVELOPMENT
171 MAPLECRETE ROAD
CITY OF VAUGHAN

FUNCTIONAL SERVICING PLAN

SCALE 1:300	DATE OF DWG. MAR. 30/23	PROJECT NO. 22151
DESIGNED BY S.K	FSP-1	
CHECKED BY D.G.		

STORM WATER MANAGEMENT ANALYSIS SUMMARY

ALLOWABLE SITE RELEASE RATE (L/SEC): _____

SITE COMPOSITION

SITE CATCHMENT AREA (HA): _____
OFFICE COMPONENT ROOF AREA (HA): _____
WAREHOUSE ROOF AREA (HA): _____
CONTROLLED PAVEMENT AREA (HA): _____
UNCONTROLLED PAVEMENT AREA (HA): _____
CONTROLLED LANDSCAPE AREA (HA): _____
UNCONTROLLED LANDSCAPE AREA (HA): _____

ROOF STORM WATER MANAGEMENT SYSTEM

TOTAL NO. OF HOPPERS: _____
TOTAL NO. OF WEIRS: _____
WEIR RATING (L/SEC/CM): _____
100-YR STM PEAK RELEASE RATE (L/SEC): _____
MAX. DETENTION STORAGE REQUIREMENT (CU.M) _____
DESIGN ROOF SLOPE: _____
MAX. ROOF PONDING DEPTH AT HOPPER (CM) _____
AVE. ROOF PONDING DEPTH (MM): _____
AVE. DRAWDOWN TIME (HRS): _____

SITE ORIFICE CONTROLS

LOCATION: _____
ORIFICE DIA.: _____
INVERT ELEVATION: _____
CENTER-LINE ELEV: _____
DOWNSTREAM HGL: _____
DETENTION STORAGE TWL: _____
DESIGN HEAD: _____
DESIGN PEAK: _____
Q-REL (L/SEC): _____

STORM WATER MANAGEMENT ANALYSIS

PART A - DETENTION VOLUME REQUIREMENT

100-YR REQUIRED DETENTION STORAGE VOLUME: _____
ORIFICE RELEASE RATE AT (L/SEC) _____

MAXIMUM SITE DETENTION STORAGE AVAILABLE:

CATCHMENT AREAS: _____
DESIGN T.W.L: _____
SURFACE PONDING: _____
PIPE STORAGE: _____
STRUCTURE STORAGE: _____

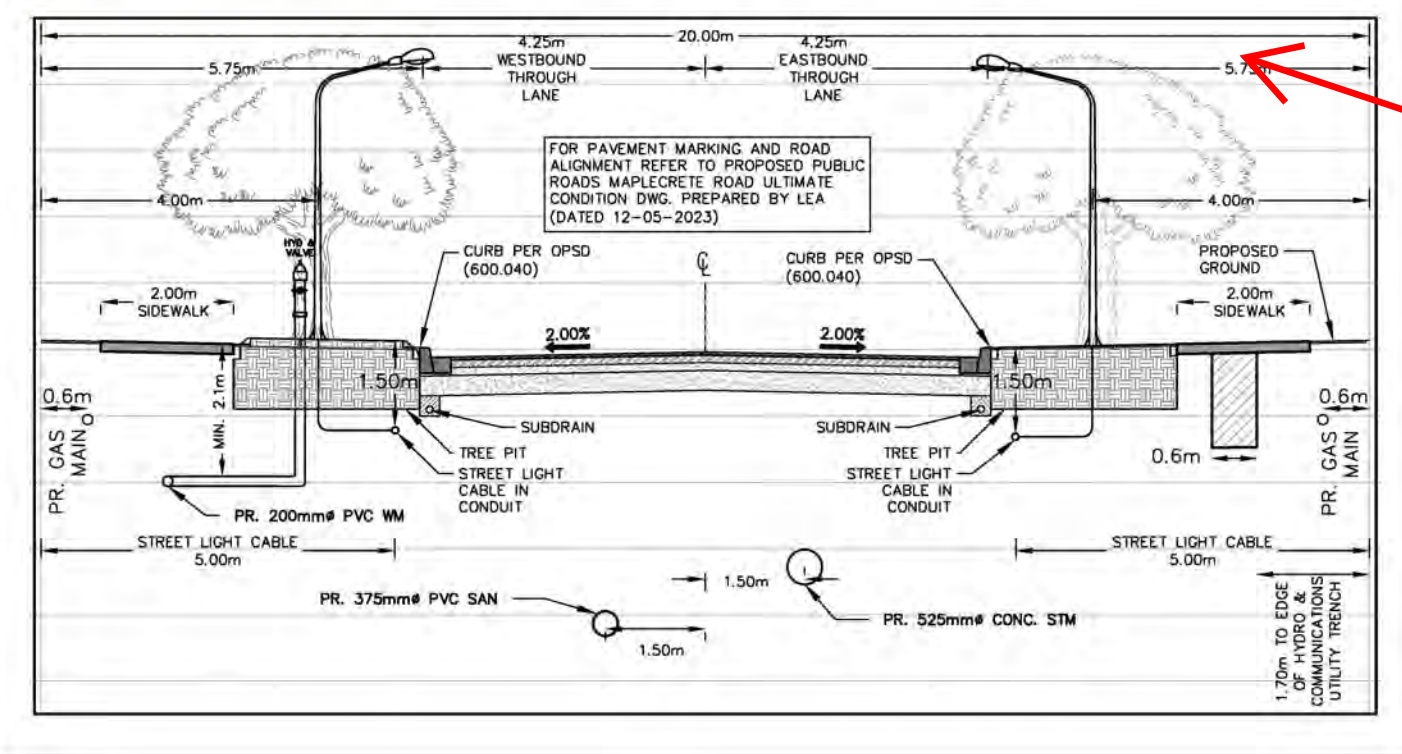
TOTAL _____

PART B-SITE CATCHMENT AREA RELEASE RATE

ACTUAL SITE RELEASE RATE: ORIFICE (L/SEC): _____
UNCONTROLLED (L/SEC) _____
TOTAL(L/SEC): _____

ALLOWABLE SITE RELEASE RATE (L/SEC): _____

INFORMATION ONLY TYPICAL CROSS SECTION - WHITE ELM ROAD
SCALE: N.T.S.



Details of the boulevard breakdown shall refer to the approved landscape drawings for 2901 Hwy 7

Maintain the 5.0m x 5.0m daylight triangle with 0.3m reserve at the south-east corner of White Elm Road and Maplecrete Road.

Maintain Provide the 3.0m x 3.0m daylight triangle easement at the north-east corner of the subject land to protect the sight-line for the future north-south road on the adjacent property to the east that intersects with White Elm Road.

REVIEWED
By Danny Woo at 8:56 am, Mar 08, 2024

NOTE:
THIS PLAN HAS BEEN PREPARED TO DEMONSTRATE FEASIBILITY OF THE PROPOSED DEVELOPMENT WITH RESPECT TO GRADING IN CONJUNCTION WITH THE OFFICIAL PLAN AMENDMENT AND ZONING BY-LAW AMENDMENT APPLICATIONS. DETAILED GRADING DESIGN WILL BE PREPARED AT THE SITE PLAN APPLICATION STAGE.

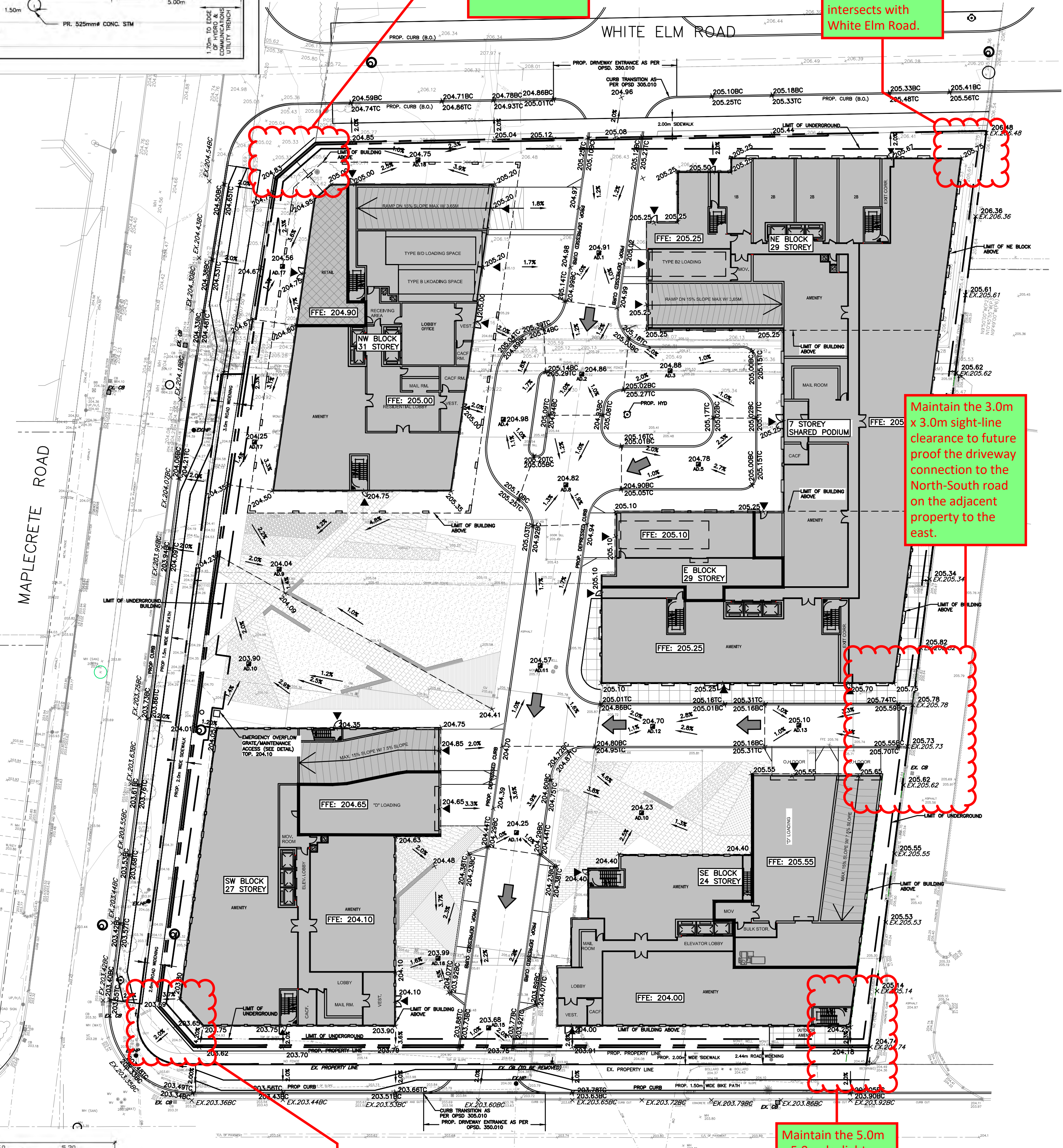
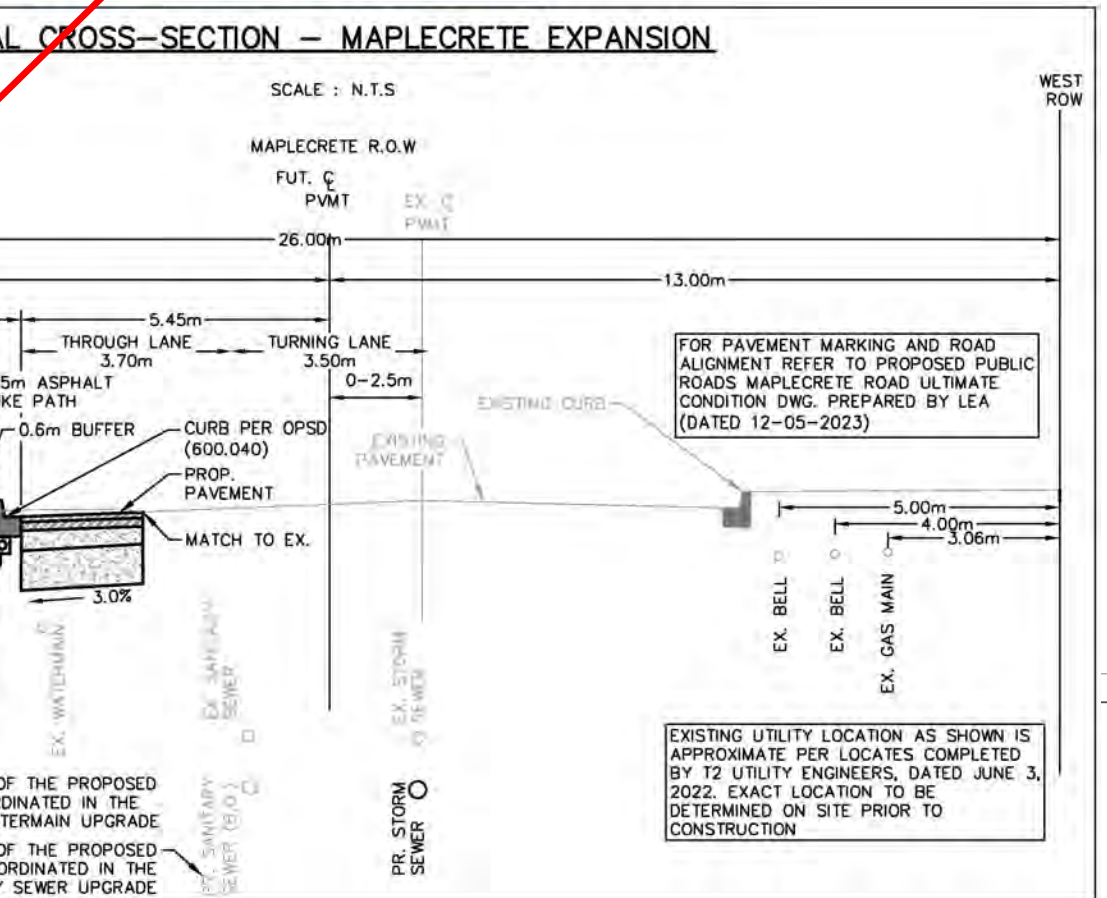


KEY PLAN
N.T.S.

*98.50 EX	EXISTING ELEVATION
2%	DRAINAGE FLOW DIRECTION AND SLOPE
x 98.35	PROPOSED ELEVATION
□	CATCH BASIN
○	STORM MANHOLE
●	SANITARY MANHOLE
⊕	VALVE AND BOX
⊙	VALVED HYDRANT
➔	OVERLAND FLOW ROUTE
● EX HP	EXISTING HYDRO POLE
○ GW	GUY WIRE
● SL	STREET LIGHT

Details of the boulevard breakdown shall refer to the approved landscape drawings for 2901 Hwy 7

INFORMATION ONLY



Maintain the 3.0m x 3.0m sight-line clearance to future proof the driveway connection to the North-South road on the adjacent property to the east.

Maintain the 8.0m x 8.0m daylight triangle and 3.0m return with 0.3m reserve at the north-east corner of Doughton Road and Maplecrete Road.

Maintain the 5.0m x 5.0m daylight triangle easement at the south-east corner of the subject land to protect the sight-line for the future north-south road on the adjacent property to the east that intersects with Doughton Road.

- Detailed Site Grading Plan shall include the following:
1. Include the City file no. (DA.XX.XXX)
 2. Identify the following on Grading Plan: Maintain the 5.0m x 5.0m daylight triangle with 0.3m reserve at the south-east corner of White Elm Road and Maplecrete Road.
Maintain the 8.0m x 8.0m daylight triangle and 3.0m return with 0.3m reserve at the north-east corner of Doughton Road and Maplecrete Road.
Maintain the 5.0m x 5.0m daylight triangle easement at the south-east corner of the subject land to protect the sight-line for the future north-south road on the adjacent property to the east that intersects with Doughton Road.
Maintain Provide the 3.0m x 3.0m daylight triangle easement at the north-east corner of the subject land to protect the sight-line for the future north-south road on the adjacent property to the east that intersects with White Elm Road.
Maintain the 3.0m x 3.0m sight-line clearance to future proof the driveway connection to the North-South road on the adjacent property to the east.
 3. The grading at the eastern edge shall be coordinated with the adjacent properties in order to complete the interim and ultimate road/driveway design. Provide grading details at the property limit under interim and ultimate conditions.
 4. Provide details for the interim and ultimate design of the N-S access driveway including the temporary vehicular turn-around at the south limit of Phase 1, if applicable.
 5. Show turning radii, indicate sufficient maneuvering area for access to loading area by semi-trailer truck or smaller shipping trucks, emergency vehicles as appropriate. Verify with CAD based software like Auto Turn or equivalent.
 6. Add note "Consent and agreement from adjacent landowner will be required for works on the private property." Please advise the City regarding the arrangement and coordination details.
 7. 0.3m reserves to be lifted for the legal access, if applicable.
 8. Show entrance driveway widths and radii. Driveway entrance shall have setback of a minimum of 1.0m clear from all aboveground services or other obstructions.
 9. Show cross-section(s) at the development frontages. For coordination purpose, please refer to the attached road cross-section(s) previously developed with other applications in the area. The following information shall be identified in your road cross-section to the satisfaction of the City:
 - Street Lighting
 - Hydrants
 - Gas main
 - Watermain/storm sewers/sanitary sewers (where applicable) with standard clearance
 - Tree trench to provide minimum growth medium volume
 - Filtration trench (LID) for drainage quality control
 - Hydro Transformer (where applicable)
 - Joint Utility Trench (to be reviewed with service providers)
 - Sidewalk/Bicycle Facility
 10. As part of the road widening design, provide the Utility Coordinate Plan which shall be prepared in consultation with all the utility providers.
 11. Municipal sidewalk through a driveway shall be constructed with a 200mm thick sidewalk, as per City Standard Drawing R-128.
 12. Tactile warning plates shall be installed at all depressed curbs leading to pedestrian facility, details per York Region standard.
 13. Identify the fire route on plan, verify the fire truck turning feasibility with CAD based software like Auto Turn or equivalent.
 14. Show snow storage area on plan.
 15. Identify the noise screen/barrier requirements as recommended in the Noise Report.
 16. Provide a georeferenced CAD file for the "Site Grading Plan"
 17. Provide an Erosion & Sediment Control Plan that identifies the erosion and sediment control measures to be in place to the satisfaction of the City and Region of York prior to commencement of any works, which shall be designed and conformed with the TRCA's Erosion & Sediment Control Guideline for Urban Construction dated December 2006 (or most recent version thereof).

CONVERSION NOTE
DISTANCES AND ELEVATIONS SHOWN ON THIS PLAN ARE IN METRES AND CAN BE CONVERTED TO FEET BY DIVIDING BY 0.3048.

BEARING NOTE (PLAN OF SURVEY)
BEARINGS ARE GRID DERIVED FROM THE 6° UTM CO-ORDINATE SYSTEM ZONE 17, NAD 83 (ORIGINAL) BASED ON REAL TIME NETWORK OBSERVATIONS.

ELEVATION NOTE (PLAN OF TOPOGRAPHY)
ELEVATIONS ARE GEODETIC AND ARE REFERRED TO THE CITY OF VAUGHAN BENCH MARK NO. 23-6, HAVING AN ELEVATION OF 202.852 METRES.

NO.	DATE	REVISIONS	BY
01.	JAN 19/24	ISSUED FOR COORDINATION	D.G

PRELIMINARY

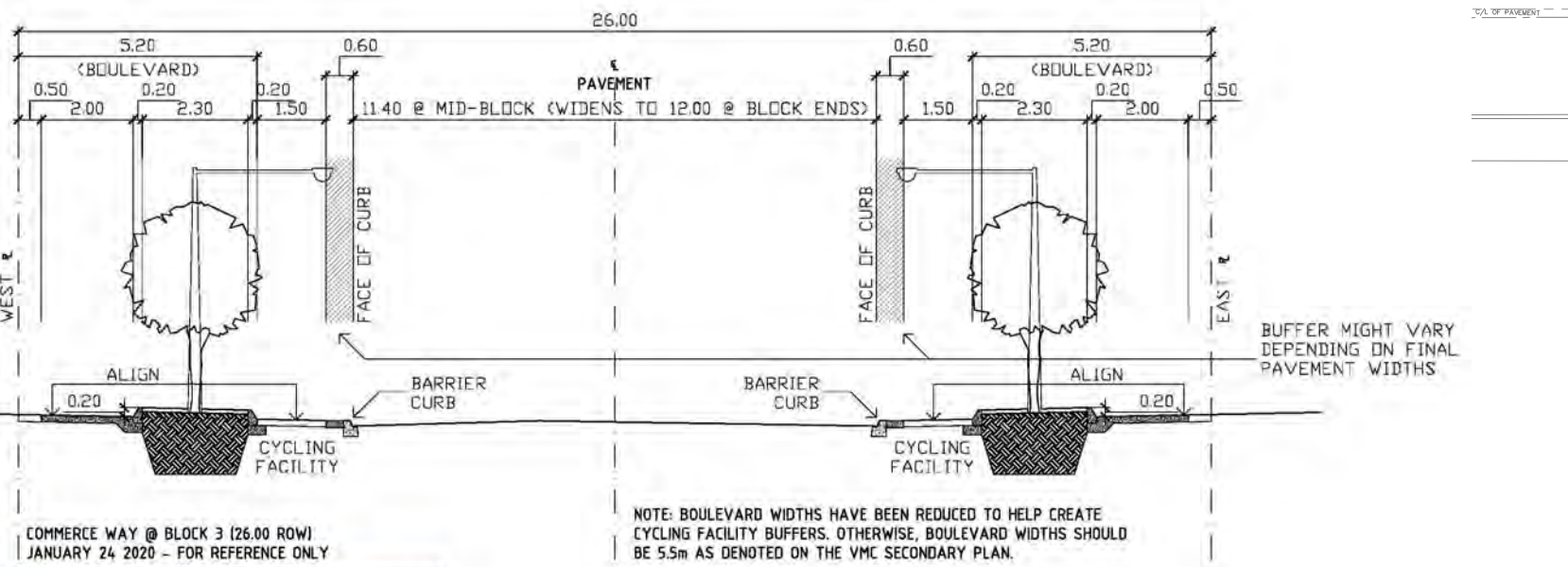
VALDOR ENGINEERING INC.
Consulting Engineers - Project Managers
571 CHERRYL ROAD, UNIT 4, 2ND FLOOR, WOODBINE, ONTARIO, L4B 3A9
TEL: (905) 884-0004 FAX: (905) 884-0007
EMAIL: info@valdor-engineering.com www.valdor-engineering.com

PROPOSED RESIDENTIAL DEVELOPMENT
171 MAPLECRETE ROAD
CITY OF VAUGHAN

FUNCTIONAL GRADING PLAN

SCALE 1:300	DATE OF DWG. MAR. 30/23	PROJECT NO. 22151
DRAWN BY S.K	DRAWING NO. FSG-1	
CHKD BY D.G.		

INFORMATION ONLY



memorandum

Submitted by email only

To: Danny Woo, Development Engineering Lead (VMC)

From: Jillian Britto, Transportation Project Manager, Development Engineering (VMC)
Sandy Yang, Active Transportation Infrastructure Specialist, Infrastructure Planning & Corporate Asset Management
Rosalie Shan, Project Engineer – Transportation & Road Safety, Transportation and Fleet Management Services – Public Works

Date: March 8, 2024

RE: **171 Maplecrete LP and 1930328 Ontario Inc.**
171 Maplecrete Rd., 140-160 Doughton Road, and Block 2 on 65M-4793
File No. CIHA.23.003
Transportation Engineering Review – 1st Submission dated
January 22, 2024

Transportation Engineering staff have reviewed the plans and reports submitted in support of the 1st submission for the above noted application, and provide the following comments:

General Comments

1. Staff discourage private driveway accesses onto minor collector roads; Staff request the applicant reconsider the vehicular connection of the private N/S driveway to Doughton Road for the following reasons:
 - a. Driveway accesses should be located on local roads, where possible, according to the VMC Secondary Plan.
 - b. The access will adversely impact the dedicated cycling facilities on Doughton Road whereas local roads have shared cycling facilities.
 - c. Proximity to the Maplecrete Road at Doughton Road intersection; the Transportation Impact Study (TIS) predicts queues along Doughton Road will extend beyond the access, which will adversely impact traffic flow.
 - d. In the future, this site will be connected to the N/S Local Road to the east, resulting in three driveway accesses, which is not necessary.

Staff request closing the access on Doughton Road and instead locate and protect for a future connection to the N-S local street, similar to the Liberty Cement development application to the north.

2. Please provide a Functional Road Plan illustrating how the proposed development will fit into the broader transportation road network in both the interim and ultimate conditions. This should include Maplecrete Road from White Elm Road to Doughton Road, Doughton Road from Maplecrete Road to the eastern terminus of the site, and the joint vision of how the site will connect to the future N-S local road and the neighbour to the east. The Functional Road Plan should include the following daylight triangle requirements:
 - a. Maplecrete at White Elm – 5 x 5m sight triangle for the northwest corner
 - b. Maplecrete at Doughton – 8 x 8m sight triangle for the southwest corner

- c. Private driveway (N/S) at White Elm – 3 x 3m sight triangles
- d. Private driveway (E/W) – 3 x 3m sightline protection at building corners for future connection to the N/S local road

Transportation Impact Study Comments

1. Please note that the TIS becomes public record upon submission and is available through the City's online PLANit application database, and it is expected that the applicant is relying on the information and recommendations of the report. As such, City Staff request removal of the Disclaimer page in the TIS Report.
2. Trip generation rates from the BA Group TIS Reports for 7800 Jane Street and 175 Millway Avenue were used to determine future trips from the proposed development. However, the context of the two BA Group sites are different to the proposed development in that this application is not adjacent the VMC Mobility Hub. Staff request that the trip generation rates be validated with surveys of sites within the VMC that are at a similar distance from the VMC Mobility Hub.
3. Figures 3-2 and 6-1 illustrate the future Maplecrete Road at White Elm Road intersection as an all-way stop-controlled intersection. Considering Maplecrete Road is a minor collector, it would be expected to be a two-way stop-controlled intersection with northbound and southbound being free flowing. Please confirm the intersection control required for this intersection under future conditions and provide adequate justification to support the recommendation.
4. Please confirm if left-turn lanes are warranted at the Maplecrete Road and White Elm Road intersection, and if so, confirm the impact on the on-street parking supply.
5. Tables 8-2 and 8-3 note the proposed supply of visitor and non-residential shared parking is 296 spaces whereas Table 8-1 notes a proposed supply of 269 spaces. Please confirm the correct number of shared visitor and non-residential parking spaces and revise the tables accordingly.
6. Please provide AutoTURN assessments for all on-street parking and/or passenger pick-up/drop-off (PPUDO) spaces, garage ramp accesses, loading spaces, emergency vehicles, etc.

Active Transportation Comments

Please see Active Transportation comments noted in the attached marked-up "*SUB1_Jan-19-2024 - Landscape Plan - 171 Maplecrete Rd_AT Comments*" drawings.

Site Plan Comments

1. Please remove the proposed lay-by parking on Maplecrete Road; only on-street parking in the curb lane should be illustrated on Maplecrete Road.
2. While the proposed site plan illustrates two jug handle-style PPUDO along the private N/S driveway, the design and the inclusion of temporary parking spaces may be problematic for fire and emergency access. Staff are in support of either a traffic circle with PPUDO spaces and/or parking along the outer lane (similar to the Expo towers) or the lay-by design proposed for the

southerly buildings; we are also open to other ideas that would safely accommodate vehicles, pedestrians crossing between the two buildings, and emergency vehicles.

3. The design of all lay-by parking areas should consider the locations of fire hydrants and any potential conflicts with access/operation for fire services.
4. The applicant should consolidate the number of parking garage entrances.

Conceptual Phasing Plan Comments

1. Please also provide the phasing plan for the proposed parking supply.

Please note that staff reserve the right to provide further comments upon receiving future submission materials. Please do not hesitate to contact the undersigned should you have any questions or require further clarification.

Sincerely,

Jillian Britto, P.Eng., PTOE

Transportation Project Manager, Development Engineering (VMC)

T: 905-832-8585 ext. 8410 | jillian.britto@vaughan.ca

Sandy Yang, RPP, MCIP

Active Transportation Infrastructure Specialist, Infrastructure Planning & Corporate Asset Management

T: 905-832-8585 ext. 8569 | sandy.yang@vaughan.ca

Rosalie Shan, P.Eng., MScE

Project Engineer – Transportation & Road Safety, Transportation and Fleet Management Services

T: 905-832-8577 ext. 6176 | rosalie.shan@vaughan.ca

Cc: Musa Deo, Manager, Development Engineering (VMC)

171 MAPLECRETE ROAD

171 Maplecrete Road
160 Doughton Road
140 Doughton Road
Block 2, Registered Plan 65M-4793
VAUGHAN, ON

Project Number
23-118

Client
171 MAPLECRETE LP

Consultant

Revisions

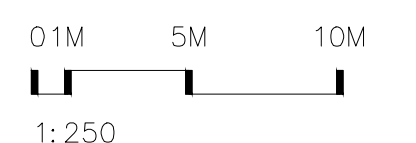
1 ISSUED FOR OHA APPLICATION 2024-01-19 ZW
ISSUED FOR XXX XXXX-MM-DD BY

Stamp



Key map

Scale



1:250

Phase
OPA PHASE

North



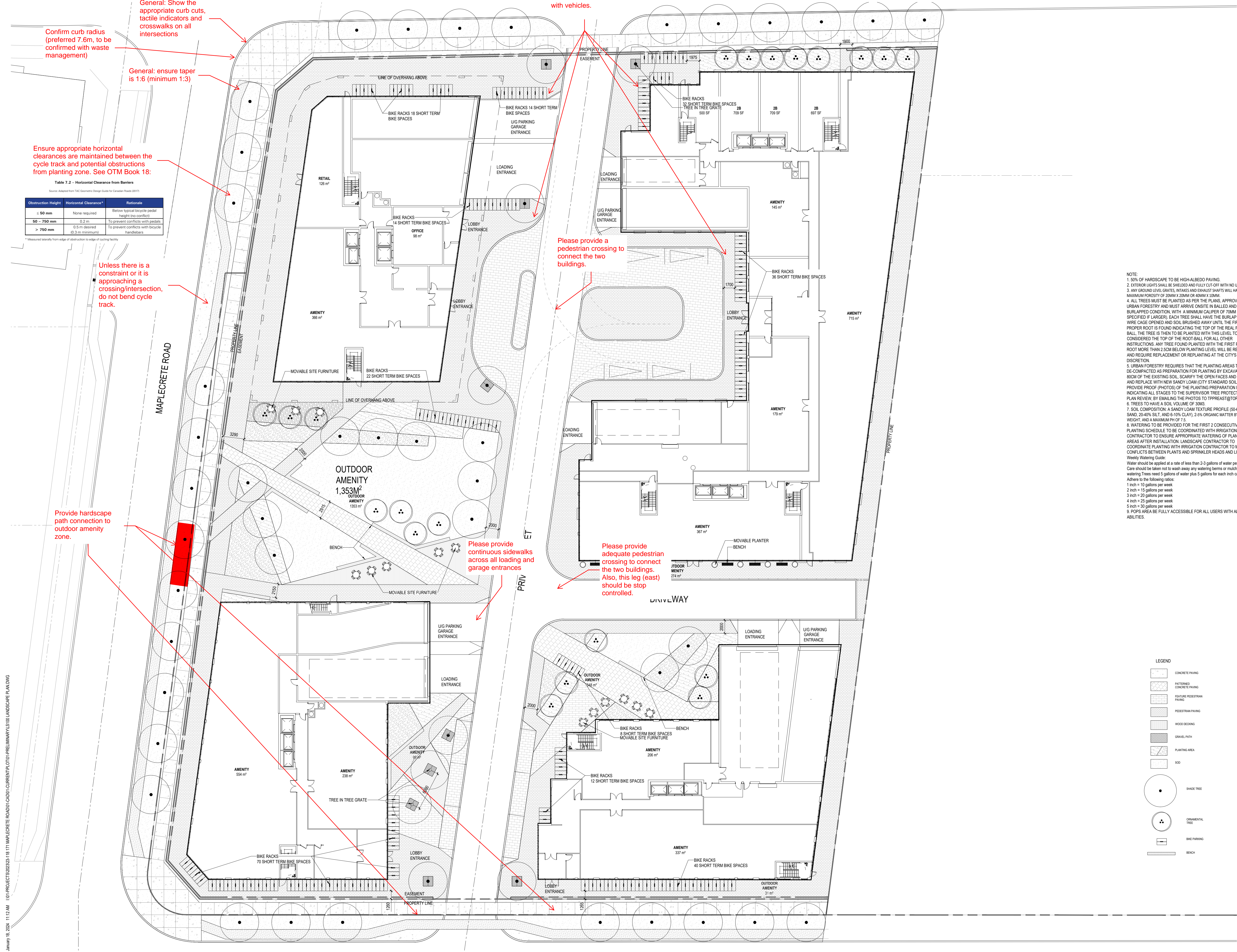
Drawing Name

LANDSCAPE PLAN

Sheet Number

NOT FOR CONSTRUCTION

LS100



Confirm curb radius (preferred 7.6m, to be confirmed with waste management)

General: Show the appropriate curb cuts, tactile indicators and crosswalks on all intersections

Ensure appropriate horizontal clearances are maintained between the cycle track and potential obstructions from planting zone. See OTM Book 18:

Table 7.2 - Horizontal Clearance from Barriers

Obstruction Height	Horizontal Clearance*	Rationale
≤ 50 mm	None required	Below typical bicycle pedal height (no conflict)
50 - 750 mm	0.2 m	To prevent conflicts with pedals
> 750 mm	0.5 m desired (0.3 m minimum)	To prevent conflicts with bicycle handlebars

*Measured laterally from edge of obstruction to edge of cycling facility

Unless there is a constraint or it is approaching a crossing/intersection, do not bend cycle track.

Provide hardscape path connection to outdoor amenity zone.

Please provide a pedestrian crossing to connect the two buildings.

Please provide continuous sidewalks across all loading and garage entrances

Please provide adequate pedestrian crossing to connect the two buildings. Also, this leg (east) should be stop controlled.

NOTE:

- 50% OF HARDSCAPE TO BE HIGH-ALBEDO PAVING.
- EXTERIOR LIGHTS SHALL BE SHIELDED AND FULLY CUT-OFF WITH NO UPLIGHTING.
- ANY GROUND LEVEL GRATES, INTAKES AND EXHAUST SHAFTS WILL HAVE MAXIMUM POROSITY OF 20MM X 20MM OR 40MM X 10MM.
- ALL TREES MUST BE PLANTED AS PER THE PLANS, APPROVED BY URBAN FORESTRY AND MUST ARRIVE ON-SITE IN BALLED AND BURLAPPED CONDITION, WITH A MINIMUM CALIPER OF 70MM (OR AS SPECIFIED IF LARGER). EACH TREE SHALL HAVE THE BURLAP AND WIRE CAGE OPENED AND SOIL BRUSHED AWAY UNTIL THE FIRST PROPER ROOT IS FOUND INDICATING THE TOP OF THE REAL ROOT BALL. THE TREE IS THEN TO BE PLANTED WITH THIS LEVEL TO BE CONSIDERED THE TOP OF THE ROOT BALL FOR ALL OTHER INSTRUCTIONS. ANY TREE FOUND PLANTED WITH THE FIRST PROPER ROOT MORE THAN 2.5CM BELOW PLANTING LEVEL WILL BE REJECTED AND REQUIRE REPLACEMENT OR REPLANTING AT THE CITY'S DISCRETION.
- URBAN FORESTRY REQUIRES THAT THE PLANTING AREAS TO BE DE-COMPACTED AS PREPARATION FOR PLANTING BY EXCAVATING 80CM OF THE EXISTING SOIL, SCARIFY THE OPEN FACES AND BOTTOM AND REPLACE WITH NEW SANDY LOAM (CITY STANDARD SOIL). PLEASE PROVIDE PROOF (PHOTOS) OF THE PLANTING PREPARATION PROCESS INDICATING ALL STAGES TO THE SUPERVISOR TREE PROTECTION AND PLAN REVIEW. BY EMAILING THE PHOTOS TO TPPEAST@TORONTO.CA
- TREES TO HAVE A SOIL VOLUME OF 30M3.
- SOIL COMPOSITION: A SANDY LOAM TEXTURE PROFILE (50-60% SAND, 20-40% SILT, AND 0-10% CLAY), 2-5% ORGANIC MATTER BY DRY WEIGHT, AND A MAXIMUM PH OF 7.5.
- WATERING TO BE PROVIDED FOR THE FIRST 2 CONSECUTIVE YEARS. PLANTING SCHEDULE TO BE COORDINATED WITH IRRIGATION CONTRACTOR TO ENSURE APPROPRIATE WATERING OF PLANTING AREAS AFTER INSTALLATION. LANDSCAPE CONTRACTOR TO COORDINATE PLANTING WITH IRRIGATION CONTRACTOR TO MINIMIZE CONFLICTS BETWEEN PLANTS AND SPRINKLER HEADS AND LINES. Weekly Watering Guide:
Water should be applied at a rate of less than 2-3 gallons of water per minute. Care should be taken not to wash away any watering berms or mulch when watering. Trees need 5 gallons of water plus 5 gallons for each inch caliper.
- Adhere to the following ratios:
1 inch = 10 gallons per week
2 inch = 15 gallons per week
3 inch = 20 gallons per week
4 inch = 25 gallons per week
5 inch = 30 gallons per week
- POPS AREA BE FULLY ACCESSIBLE FOR ALL USERS WITH ALL ABILITIES.

(INSERT COMPANY LETTERHEAD/LOGO)

(INSERT DATE)

Development Engineering Department
City of Vaughan
2141 Major Mackenzie Drive
Vaughan, ON L6A 1T1

Re: Reliance Letter for (INSERT FULL SITE ADDRESS OR LEGAL DESCRIPTION, AND PROJECT NAME AND REFERENCE NUMBER)

(INSERT NAME OF YOUR COMPANY) (the “Consultant”) understands that **(INSERT NAME OF THE OWNER/PROPONENT)** (the “Client”) is seeking approval of their development application from the City of Vaughan (the “City”) regarding the above-referenced property (the “Site”). The Client has requested that the City accept the following report(s), which was/were prepared by the Consultant for the exclusive benefit and use of the Client:

- **(INSERT THE TITLES OF ALL APPLICABLE ENVIRONMENTAL REPORTS, REFERENCE NUMBERS, AND DATES)**

The Consultant agrees that the City and its peer reviewer may use and rely on the report(s) as if the report(s) had been prepared for the use and benefit of the City notwithstanding any statement to the contrary contained in the report(s) and excluding any limitations agreed to by the Client. The Consultant further agrees that in the case of any inconsistency between this Reliance Letter and any limitations within any report(s) provided to the City, the provisions in this Reliance Letter shall prevail.

The Consultant agrees that the City will utilize the report(s) identified herein for the purposes of assessing the environmental risk of the Site. The Consultant certifies that the report(s) was/were prepared in accordance with the requirements of Ontario Regulation (O.Reg.) 153/04 (as amended) by or under the supervision of a Qualified Person (QP) as defined under O.Reg. 153/04 (as amended) and was/were prepared in accordance with environmental laws and regulations applicable at the time of the investigation.

The Consultant has appended to this Reliance Letter evidence of insurance, indicating Professional Liability insurance coverage of at least \$2,000,000 per claim.

Yours very truly,

(INSERT SIGNATURE OF QUALIFIED PERSON, AS DEFINED UNDER O. REG. 153/04 (AS AMENDED))

(INSERT YOUR NAME, FOLLOWED BY PROFESSIONAL DESIGNATION) (I.E., P. ENG OR P. GEO)

(INSERT YOUR TITLE)

(INSERT YOUR CONTACT INFORMATION)

I have the authority to bind the corporation of the Consultant.



THIS CERTIFICATE OF INSURANCE IS TO CERTIFY TO:
 THE CORPORATION OF THE CITY OF VAUGHAN
 2141 MAJOR MACKENZIE DR., VAUGHAN, ON, L6T 1A1

That Policy(ies) of Insurance as herein described have been issued to the insured named below and are in force on the indicated dates.

CERTIFICATE TYPE:	Blanket	Covering the Named Insured for all work or activities performed for the City of Vaughan and/or for agreements with the City of Vaughan and/or for operations conducted within the City of Vaughan
	Project / Service Specific Agreement	City File No. and/or Description:
Insured:		Address:

#	TYPE OF INSURANCE	POLICY NO.	EFFECTIVE dd/mm/yyyy	EXPIRY dd/mm/yyyy	LIMIT (if other than CDN \$ indicate)	Deductible
1	COMMERCIAL GENERAL LIABILITY (occurrence form)				\$ per occurrence \$ general aggregate \$ completed operations	\$
	Non-Owned Auto				\$	\$
	Employer's Liability				\$	\$
	Sudden & Accidental Pollution				\$	\$
2	AUTOMOBILE LIABILITY				\$	\$
3	UMBRELLA LIABILITY				\$ per occurrence	\$
					\$ general aggregate	
4	GARAGE LIABILITY				\$ per occurrence	\$
					\$ comprehensive	
					\$ collision	
5	ALL RISK PROPERTY				\$	\$
6	BOILER AND MACHINERY				\$	\$
7	CRIME				\$ employee dishonesty	\$
8	CONTRACTOR'S EQUIPMENT				\$	\$
9	PROFESSIONAL LIABILITY (Errors & Omissions)				\$ per claim	\$
					\$ general aggregate	
10	ENVIRONMENTAL IMPAIRMENT				\$ per claim / occurrence	\$
					\$ aggregate	
11	BUILDER'S RISK / INSTALLATION FLOATER				\$	\$
12	WRAP UP LIABILITY				\$ per occurrence	\$
					\$ general aggregate	
13	DIRECTOR'S & OFFICER'S LIABILITY				\$ per claim	\$
					\$ general aggregate	
14	AVIATION LIABILITY				\$ per claim	\$
					\$ general aggregate	

15	CYBER LIABILITY					
	Network & Information Security (3rd party) Liability				\$ per claim	\$
					\$ general aggregate	\$
	Privacy Liability				\$ per claim	\$
					\$ general aggregate	\$
16	PERSONAL LIABILITY				\$ per claim	\$
					\$ general aggregate	\$
17	EXCESS PERSONAL LIABILITY				\$ per claim	\$
					\$ general aggregate	\$

REQUIRED PROVISIONS:

- Commercial General Liability policy is extended to include Personal Injury Liability, Contractual Liability, Products - Completed Operations, Contingent Employer's Liability, Cross Liability and Severability of Interest.
- It is agreed and understood that the deductible or self insured retention (SIR) arranged between the Named Insured and the Insurers must be declared herein and is subject to approval by The City of Vaughan. It is further understood and agreed that claims arising out of the operations of the above mentioned project, which fall within the deductible or SIR limit, are the sole responsibility of the Named Insured.
- If the insurance provided under the said policy(ies) is cancelled or if coverage is reduced, the Insuring Company will give thirty (30) days written notice of cancellation to the address above.
- The policy(ies) identified above shall apply as primary insurance and not excess to any other insurance available to the Additional Insured as set out below.

Required Additional Insured(s) with respect to General Liability. It is understood and agreed that entity(ies) identified below is/are added as an Additional Insured(s) to the Commercial General Liability and Umbrella Liability Policies with respect to liability arising out of the operations of the Named Insured performed / supplied / conducted for/to the City of Vaughan.

*The City of Vaughan Other _____

The Regional Municipality of York Other _____

Toronto and Region Conservation Authority (TRCA) Other _____

*The City of Vaughan and its respective directors, officers, council members, boards and employees, including; Vaughan Fire and Rescue Services, the Vaughan Public Library Board, Vaughan Hydro Inc., Tourism Vaughan Corporation and Hydro Vaughan Energy Corporation.

DATE ISSUED	NAME & ADDRESS	#
	OF INSURANCE	#
	COMPANY(IES)	#
	Indicate line #s if	#
	multiple insurers	#

NOTICE AND RECEIPT. Enter the information for the respective City department below to ensure receipt of the certificate by the appropriate City contact. Policy changes affecting the insurance requirements outlined in the respective Project/Service Agreement are to be provided in writing in accordance with item 3.

Contact:	Department:
Email:	Phone No:

CERTIFICATION I certify that the insurance is in effect as stated in this certificate and that I have authorization to issue this certificate for and on behalf of the insurer(s). This certificate is valid until the expiration date(s) shown unless notice is given in writing in accordance with item 3.

Broker Name & Address Tel. No.: E-mail Contact Address:	SIGNATURE AND STAMP OF CERTIFYING OFFICIAL
---	---

The City of Vaughan reserves the right to contact your broker or insurer directly to obtain a renewal certificate on your behalf should your insurance coverage expire during the term of your contract with the City of Vaughan

THIS FORM MUST BE COMPLETED BY AND AUTHORIZED INSURANCE BROKER OR INSURANCE PROVIDER.



ENVIRONMENTAL REMEDIAL ACTION PLAN (RAP) REQUIREMENTS

The following checklist ensures proponents submit an environmental Remedial Action Plan (RAP), where required, containing complete and comprehensive information. The checklist shall be completed and submitted along with the RAP to the City for review and acceptance.

The checklist is based on recommended Ministry of the Environment (MOE) guidelines as well as requirements outlined under *O. Reg. 153/04* (as amended). It is not meant to be an exhaustive list and additional requirements may be identified by the City in the course of the environmental documents review.

REMEDIAL ACTION PLAN CHECKLIST	
1. Description of the proposed remediation objective and strategy to reduce/eliminate concentrations of contaminants exceeding the applicable MOE site condition standards for soil, ground water, and/or sediment (e.g., excavation and soil removal; offsite treatment; in-situ treatment, containment/isolation etc.).	<input type="checkbox"/>
2. Site Plan Figure(s) showing locations and concentrations of contaminants exceeding the applicable MOE site condition standards for soil, ground water, and/or sediment.	<input type="checkbox"/>
3. Site Plan Figure(s) showing the estimated vertical and horizontal extent of contamination on the site.	<input type="checkbox"/>
4. The estimated quantity of contaminated soil/ground water/sediment required to be remediated (in m ³ or tonnes and/or litres).	<input type="checkbox"/>
5. The estimated quantity of imported clean fill material required following remediation (if applicable) (in m ³ or tonnes).	<input type="checkbox"/>
6. A management plan outlining how the remediation of contaminated soil / ground water/sediment will be undertaken along with associated onsite activities (e.g., provide details on segregation of soils, stockpiling, offsite removal, onsite screening of contaminants, long-term monitoring requirements, onsite supervision schedule, management of impacted groundwater, dust, noise and traffic issues, etc.).	<input type="checkbox"/>
7. The confirmatory sampling strategy detailing the media, the specific analyses to be undertaken on the media, and the number and location of confirmatory samples to be submitted for chemical analysis following implementation of the remediation strategy. If excavation is proposed, confirmatory sampling should follow the minimum floor and wall sampling requirements for excavations as presented in <i>O. Reg. 153/04, Schedule E- Part V, Table 3</i> (as amended). If stockpiles are present on site, confirmatory sampling follow the minimum stockpile sampling frequency as presented in <i>O. Reg. 153/04, Schedule E- Part V, Table 2</i> (as amended).	<input type="checkbox"/>
8. If import of clean fill material is required for site restoration, the proposed confirmatory sampling strategy for imported clean fill material to confirm it meets the applicable MOE site condition standards.	<input type="checkbox"/>
9. The Contingency Remediation Plan in the event confirmatory sampling results indicate concentrations exceeding the applicable MOE site condition standards.	<input type="checkbox"/>
10. The Quality Assurance/ Quality Control Plan and Health and Safety Plan to be implemented during remedial activities.	<input type="checkbox"/>
11. The Communications and Reporting Plan to the City and other regulatory agencies.	<input type="checkbox"/>
12. The approximate timelines and/or stages of the implemented remediation strategy.	<input type="checkbox"/>
13. Description of regulatory agency approval requirements and/or other responsible authorities , if applicable (e.g., MOE Environmental Compliance Approval (ECA) for discharges such as air/noise, water, waste treatment, TRCA approval, etc.).	<input type="checkbox"/>
14. Proponent's Qualified Person (QP) Sign-off and Owner's Certification of Implementation of the RAP (include title, designations and stamps, signatures, and printed names).	<input type="checkbox"/>

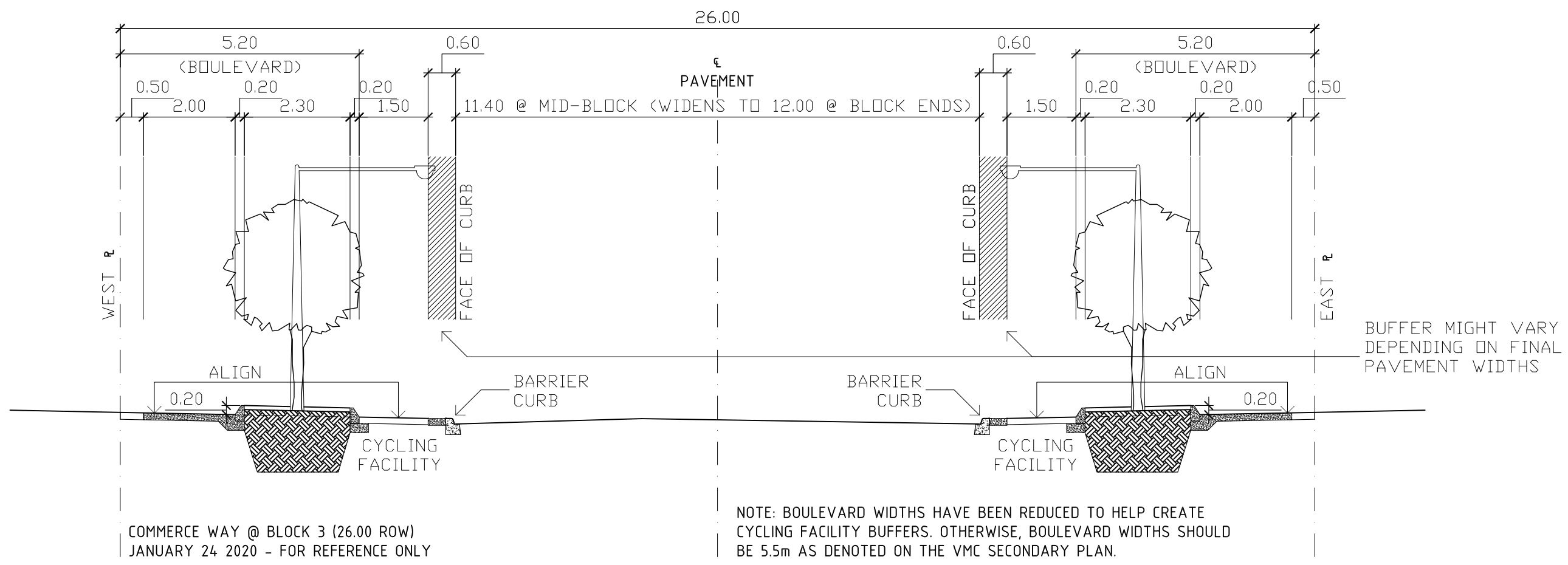
Checklist Completed by (print name): _____

Signature: _____

Company: _____

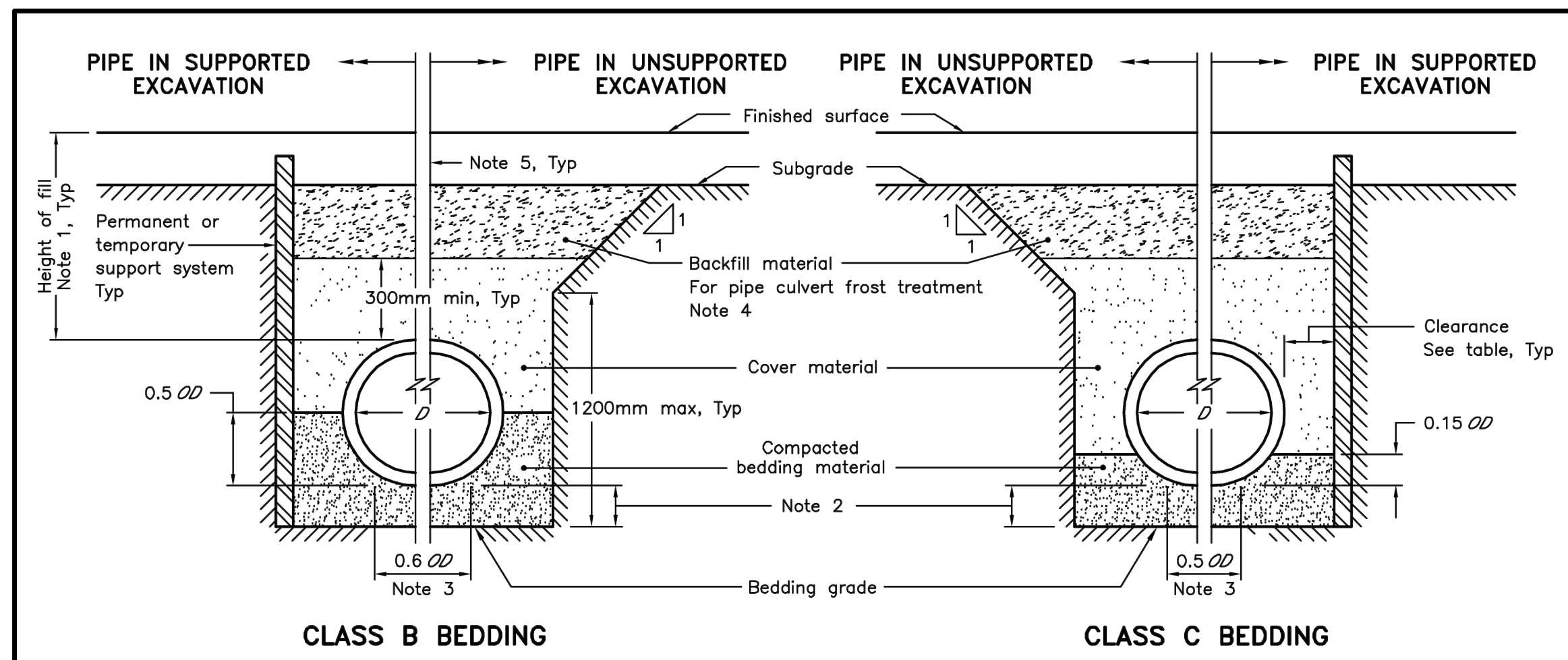
Date: _____

INFORMATION ONLY



COMMERCE WAY @ BLOCK 3 (26.00 ROW)
 JANUARY 24 2020 - FOR REFERENCE ONLY

NOTE: BOULEVARD WIDTHS HAVE BEEN REDUCED TO HELP CREATE CYCLING FACILITY BUFFERS. OTHERWISE, BOULEVARD WIDTHS SHOULD BE 5.5m AS DENOTED ON THE VMC SECONDARY PLAN.



NOTES:
 1 Height of fill is measured from the finished surface to top of pipe.
 2 The minimum bedding depth below the pipe shall be 0.15D. In no case shall this dimension be less than 150mm or greater than 300mm.
 3 The pipe bed shall be compacted and shaped to receive the bottom of the pipe.
 4 Pipe culvert frost treatment shall be according to OPSD 803.030 and 803.031.
 5 Condition of excavation is symmetrical about centreline of pipe.
 A Soil types as defined in the Occupational Health and Safety Act and Regulations for Construction Projects.
 B All dimensions are in metres unless otherwise shown.

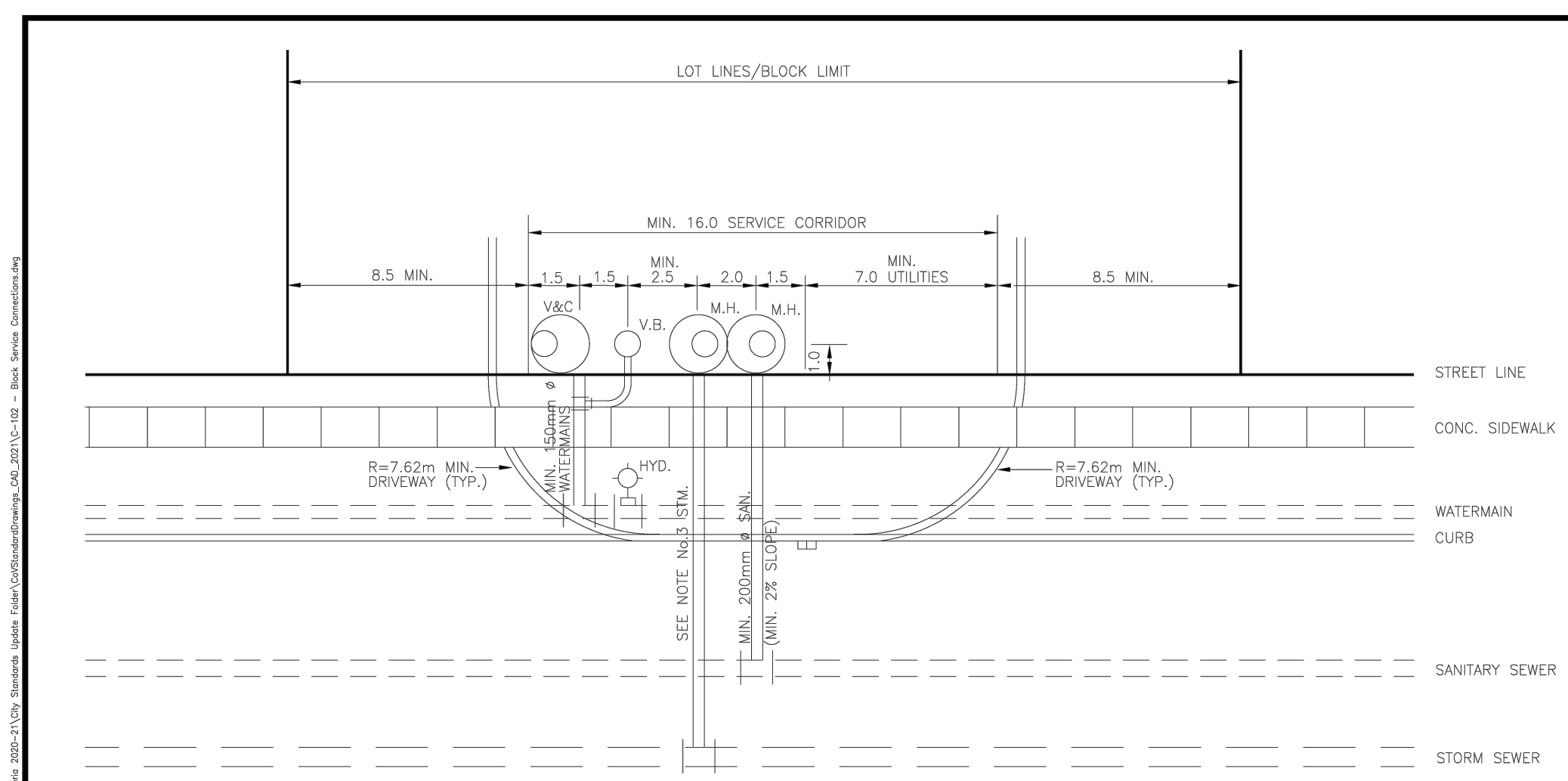
LEGEND:
 D – Inside diameter
 OD – Outside diameter

Pipe Inside Diameter mm	Clearance mm
900 or less	300
Over 900	500

ONTARIO PROVINCIAL STANDARD DRAWING Nov 2015 Rev 3

RIGID PIPE BEDDING, COVER, AND BACKFILL
 TYPE 1 OR 2 SOIL – EARTH EXCAVATION

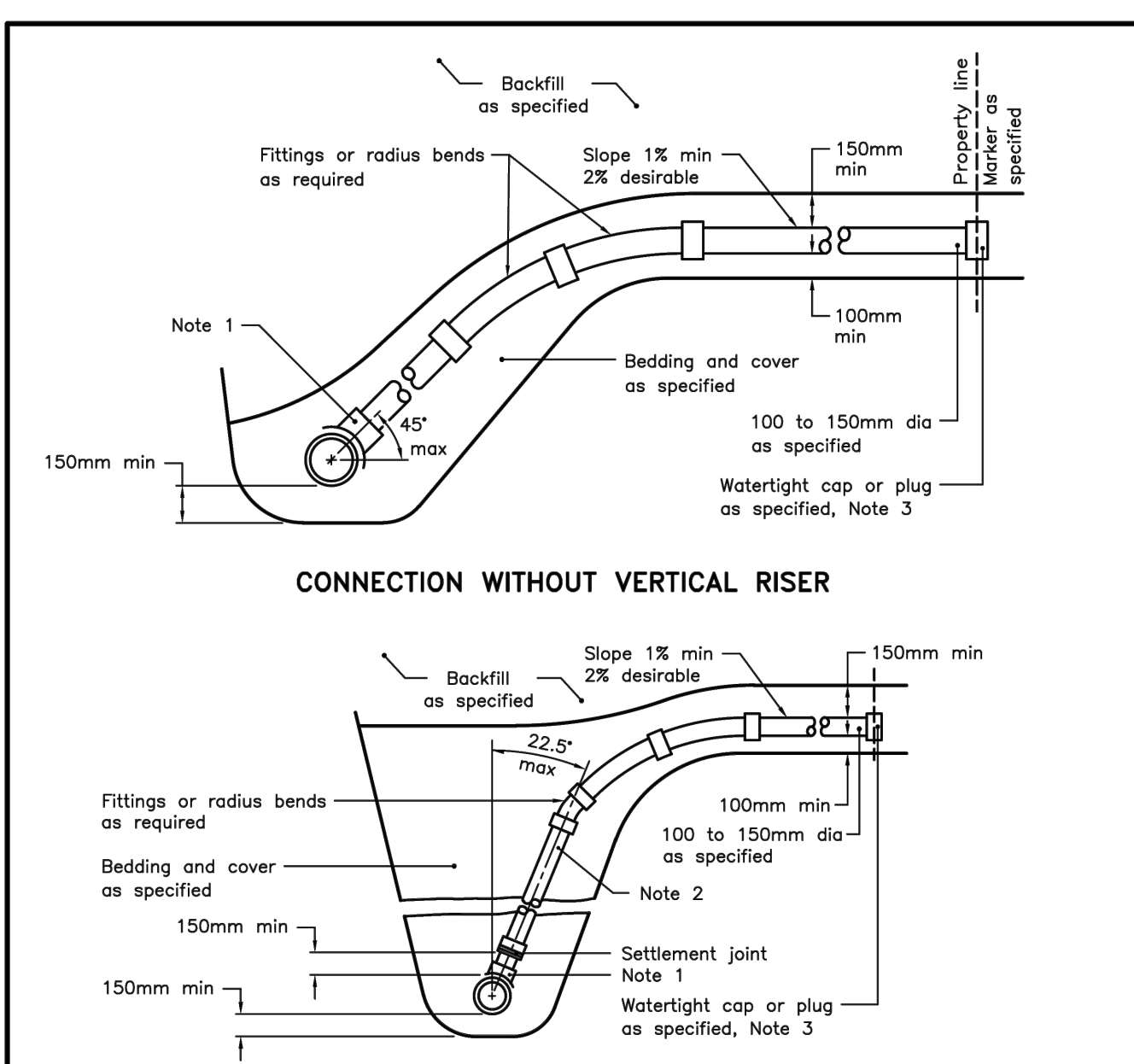
OPSD 802.030



VAUGHAN
 CITY OF VAUGHAN ENGINEERING STANDARD
BLOCK SERVICE CONNECTIONS

NOT TO SCALE DESIGNED: DATE: REC. 2020 STD. DWG. C-102

NOTES:
 1. ABOVE GROUND UTILITIES TO BE LOCATED MINIMUM 1.0m FROM CURBS, SIDEWALK AND DRIVEWAYS.
 2. MAINTENANCE HOLES AND VALVE CHAMBERS FOR SERVICE CONNECTIONS TO BE LOCATED AT 1.0m OFF STREET LINE ON PRIVATE PROPERTY.
 3. STORM SEWER CONNECTIONS SHALL BE SIZED BASED UPON STORM WATER MANAGEMENT REQUIREMENTS. SEE STD. C-104.
 4. WATER SERVICE CONNECTIONS SHALL BE AS DETAILED ON STANDARD DRAWING C-103.
 5. CB TO BE LOCATED WITHIN SERVICE CORRIDOR.

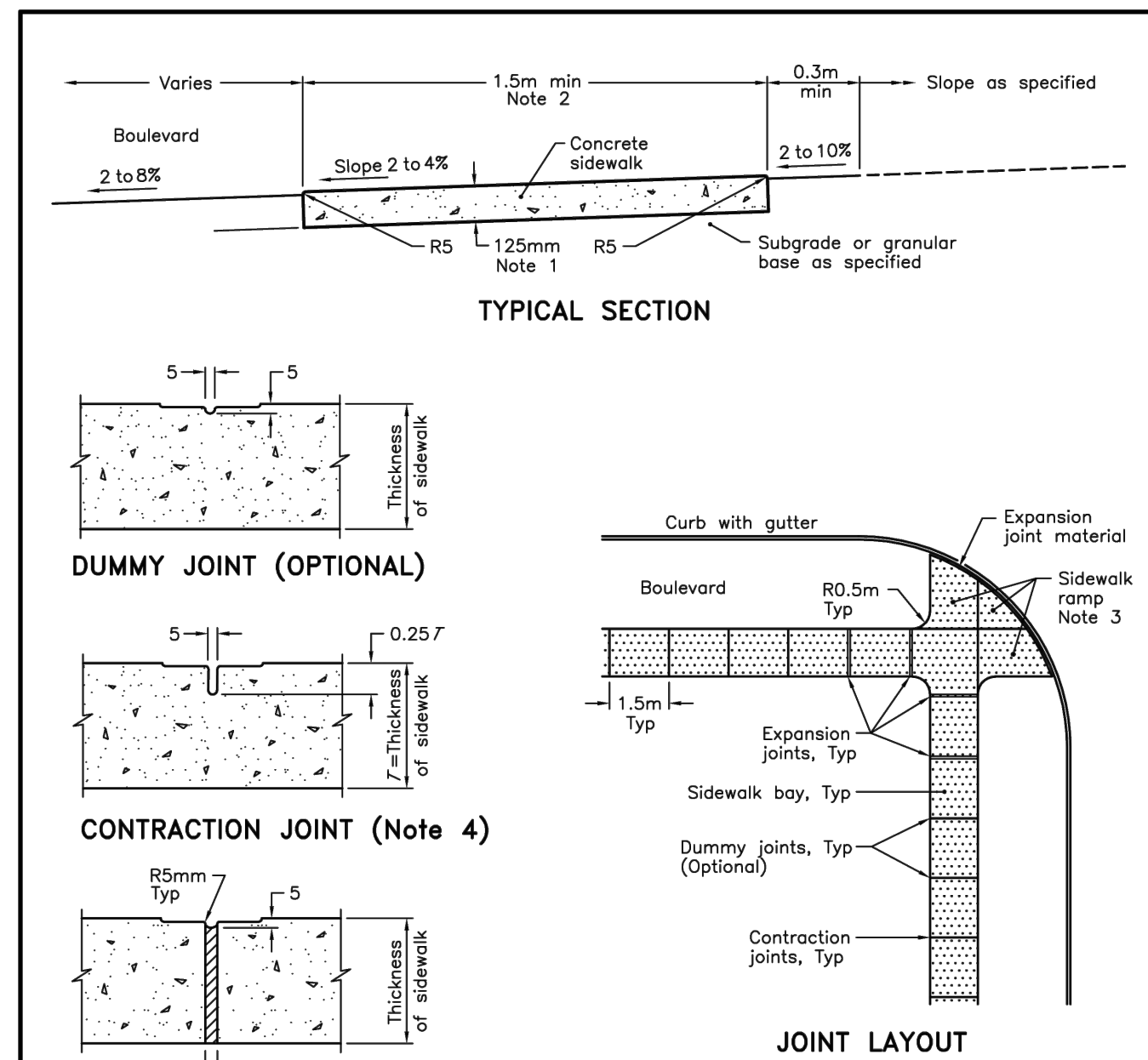


NOTES:
 1 Sewer service connections to the main pipe sewer shall be made using factory made tees, strap-on-saddles, or other approved saddles.
 2 Vertical risers shall be as specified.
 3 Cap or plug at property line shall be adequately braced.
 A Maintenance holes shall be used at the main sewer to connect service connections greater than 200mm.
 B For new construction, saddles shall be installed on the main pipe before that pipe is laid.
 C Approved cut-in tool shall be used for field made connections.
 D All dimensions are in millimetres unless otherwise shown.

ONTARIO PROVINCIAL STANDARD DRAWING Nov 2021 Rev 4

SEWER SERVICE CONNECTIONS FOR MAIN PIPE SEWER

OPSD 1006.010

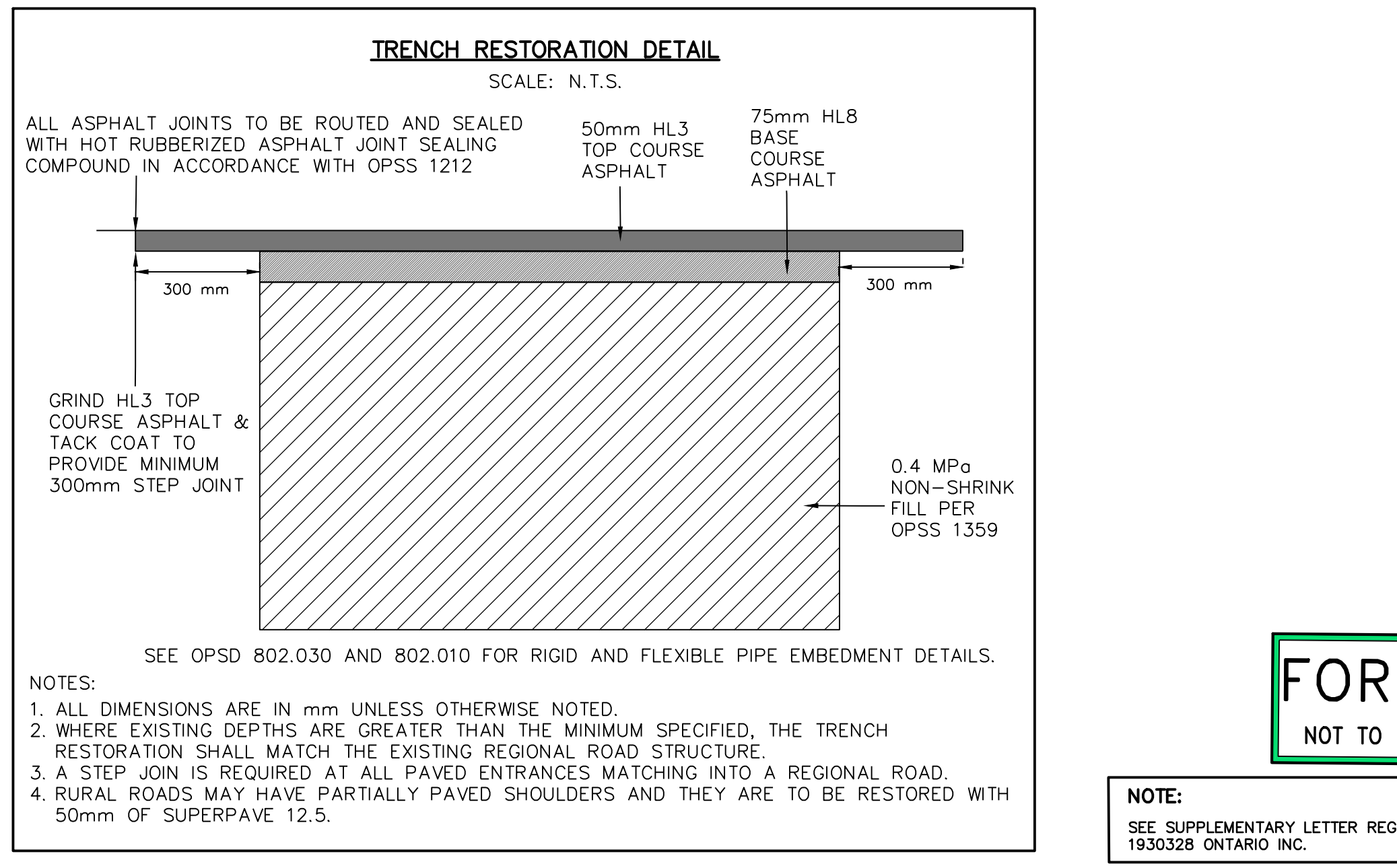
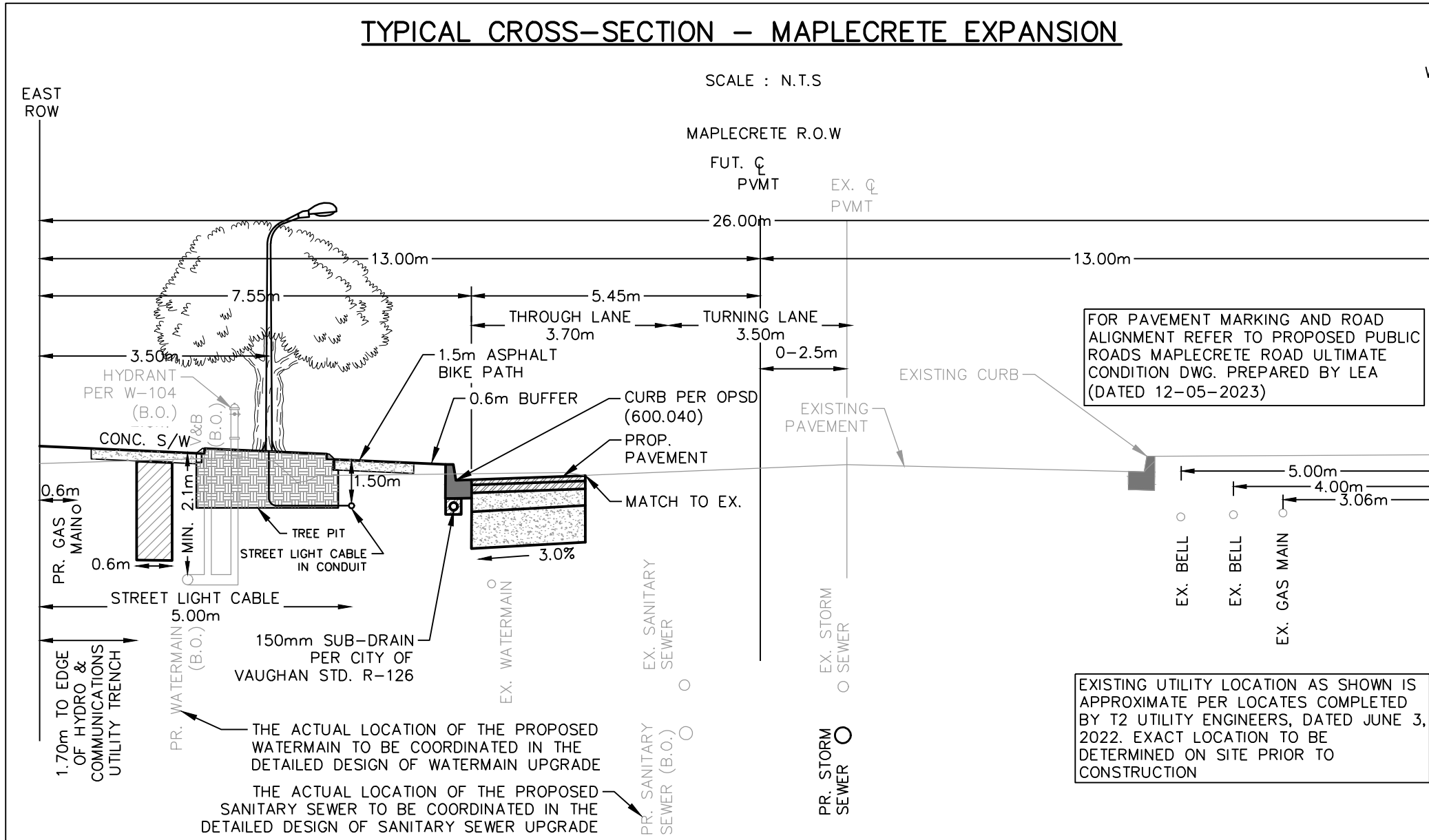


NOTES:
 1 Sidewalk thickness at residential driveways and adjacent to curb shall be 150mm. At commercial and industrial driveways, the thickness shall be 200mm.
 2 Sidewalk width shall be wider when specified.
 3 This OPSD shall be read in conjunction with OPSD 310.030, 310.031, 310.033, and 310.039.
 4 Contraction Joint may be tooled or sawcut.
 A All dimensions are in millimetres unless otherwise shown.

ONTARIO PROVINCIAL STANDARD DRAWING Nov 2019 Rev 3

CONCRETE SIDEWALK

OPSD 310.010



NOTES:
 1. ALL DIMENSIONS ARE IN mm UNLESS OTHERWISE NOTED.
 2. WHERE EXISTING DEPTHS ARE GREATER THAN THE MINIMUM SPECIFIED, THE TRENCH RESTORATION SHALL MATCH THE EXISTING REGIONAL ROAD STRUCTURE.
 3. A STEP JOIN IS REQUIRED AT ALL PAVED ENTRANCES MATCHING INTO A REGIONAL ROAD.
 4. RURAL ROADS MAY HAVE PARTIALLY PAVED SHOULDERS AND THEY ARE TO BE RESTORED WITH 50mm OF SUPERPAVE 12.5.

INFORMATION ONLY

APPROVED AS TO FORM IN RELIANCE, UPON THE PROFESSIONAL SKILL AND ABILITY OF THE CONSULTING ENGINEERS AS TO DESIGN AND SPECIFICATIONS

DIRECTOR OF DEVELOPMENT ENGINEERING AND INFRASTRUCTURE PLANNING

DATE: REGION FILE NO.: 19T-18V005 (SUBP.18.V0019) SITE PLAN DEVELOPMENT CITY FILE NO.: DA.18.037

4	PERFECT SUBMISSION	2023/JUN/20
3	ISSUED FOR 4TH SUBMISSION	2023/MAY/17
2	ISSUED FOR 3RD SUBMISSION TO CITY	2023/FEB/24
1	ISSUED FOR 2ND SUBMISSION TO CITY	2022/OCT/11
0	ISSUED FOR 1ST SUBMISSION TO CITY	2022/JUN/17

TEMPORARY BENCHMARKS
 BEARINGS ARE UTM GRID, DERIVED FROM OBSERVED REFERENCE POINTS 1 AND 2. BY REAL TIME NETWORK (RTN) OBSERVATIONS, UTM ZONE 17, NAD83 (ORIGINAL). OBSERVED REFERENCE POINTS (ORP): UTM ZONE 17, NAD83 (ORIGINAL). COORDINATES TO URBAN ACCURACY PER SEC. 14(2) OF O REG 216/10
 ORP 1 – NORTHING: 4850162.198 EASTING: 619083.869
 ORP 2 – NORTHING: 4850000.680 EASTING: 619114.676

SCHAEFFER DZALDOV BENNET LTD.
 SUE SURVEY ALONG MAPLECRETE RD. AND DOUGHTON RD. BY T2 UTILITY ENGINEERS ATEO 06/06/22 PROJECT NO. 61002331

1. THIS DRAWING IS THE EXCLUSIVE PROPERTY OF C.F. CROZIER & ASSOCIATES INC. AND THE REPRODUCTION OF ANY PART WITHOUT PRIOR WRITTEN CONSENT OF THIS OFFICE IS STRICTLY PROHIBITED.
 2. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS, LEVELS, AND DATUMS ON SITE AND REPORT ANY DISCREPANCIES OR OMISSIONS TO THIS OFFICE PRIOR TO CONSTRUCTION.
 3. THIS DRAWING IS TO BE READ AND UNDERSTOOD IN CONJUNCTION WITH ALL OTHER PLANS AND DOCUMENTS APPLICABLE TO THIS PROJECT.
 4. DO NOT SCALE THE DRAWINGS.
 5. ALL EXISTING UNDERGROUND UTILITIES TO BE VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO CONSTRUCTION.

VAUGHAN

Project: 1930328 ONTARIO INC. 2901 HIGHWAY 7 CITY OF VAUGHAN

Drawing: DETAILS

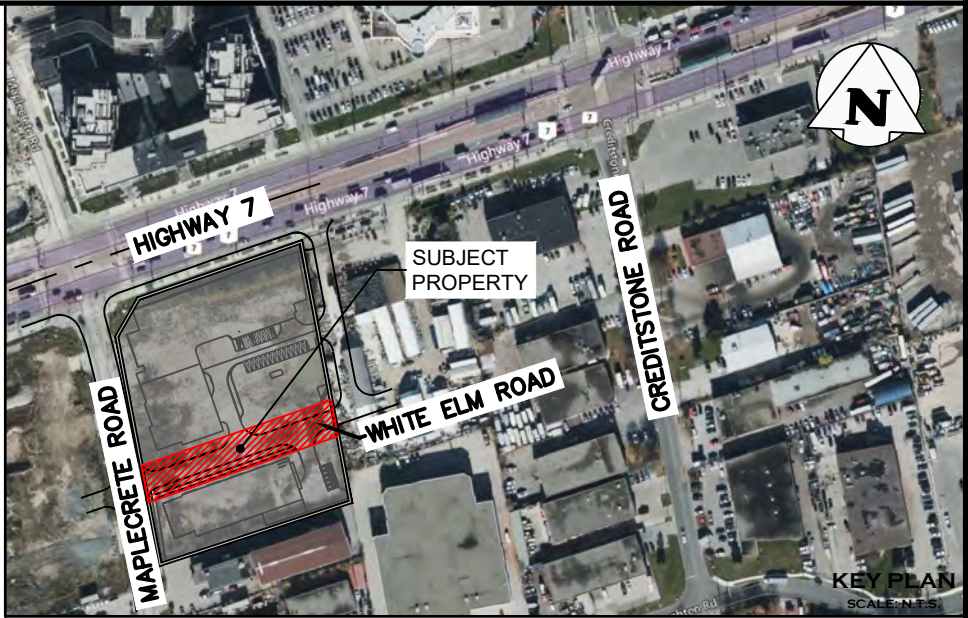
FOR APPROVAL
 NOT TO BE USED FOR CONSTRUCTION

CROZIER CONSULTING ENGINEERS

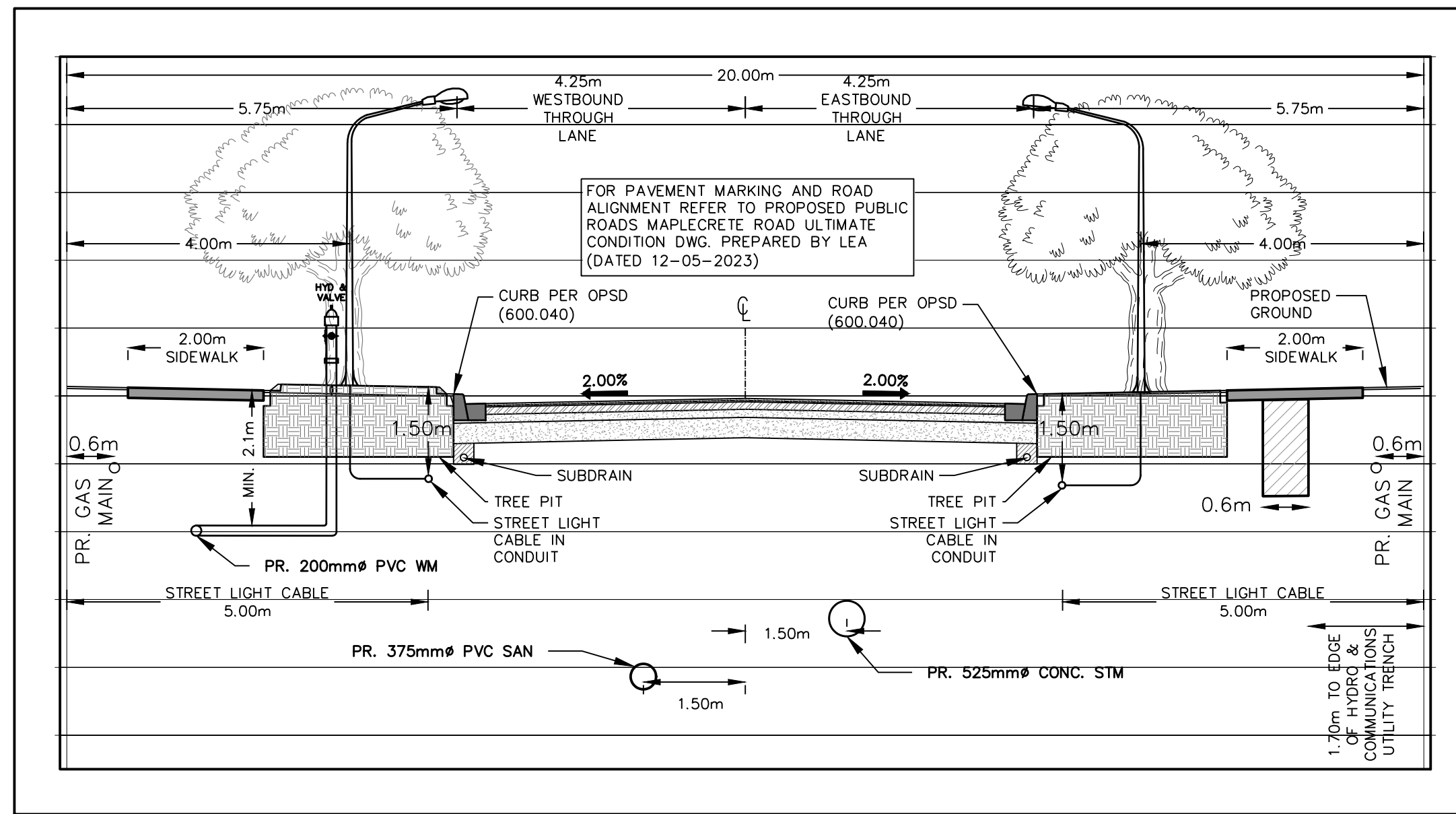
51 JOHN STREET WEST
 P.O. Box 1011
 BRADFORD, ON L3Z 2A9
 905-952-3111 T
 WWW.CFCROZIER.CA

Drawn: S.V.N. Design: C.C. Project No: 1525-6298
 Check: D.L. Check: J.M.S. Scale: N/A Dwg: C203F

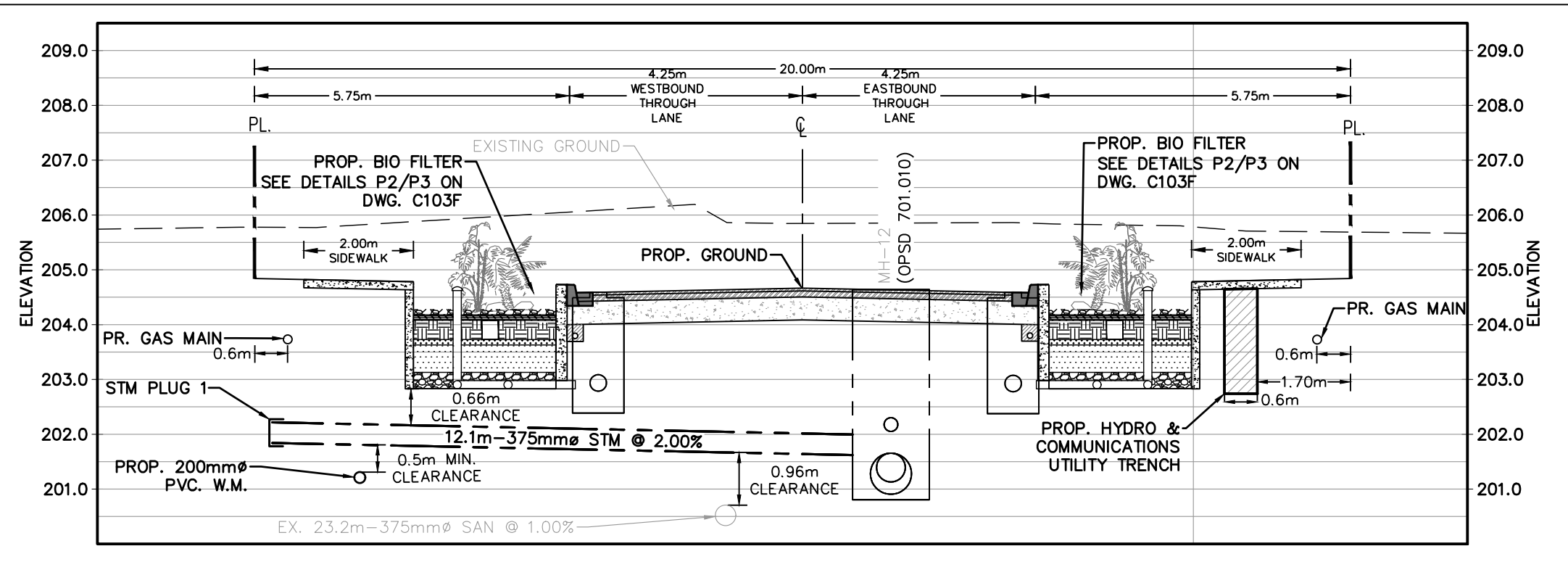
NOTE:
 SEE SUPPLEMENTARY LETTER REGARDING SCOPE OF WORK PREPARED BY 1930328 ONTARIO INC.



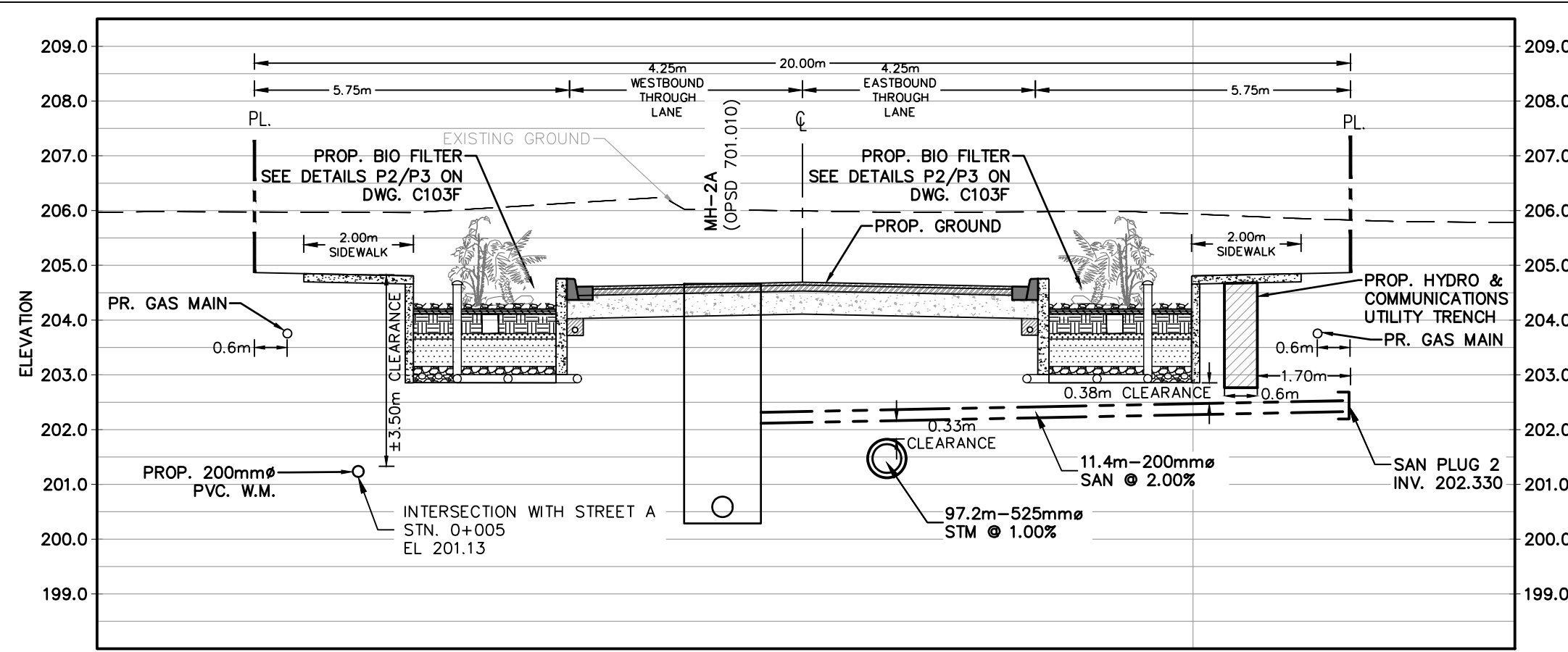
TYPICAL CROSS SECTION - WHITE ELM ROAD
SCALE: NTS



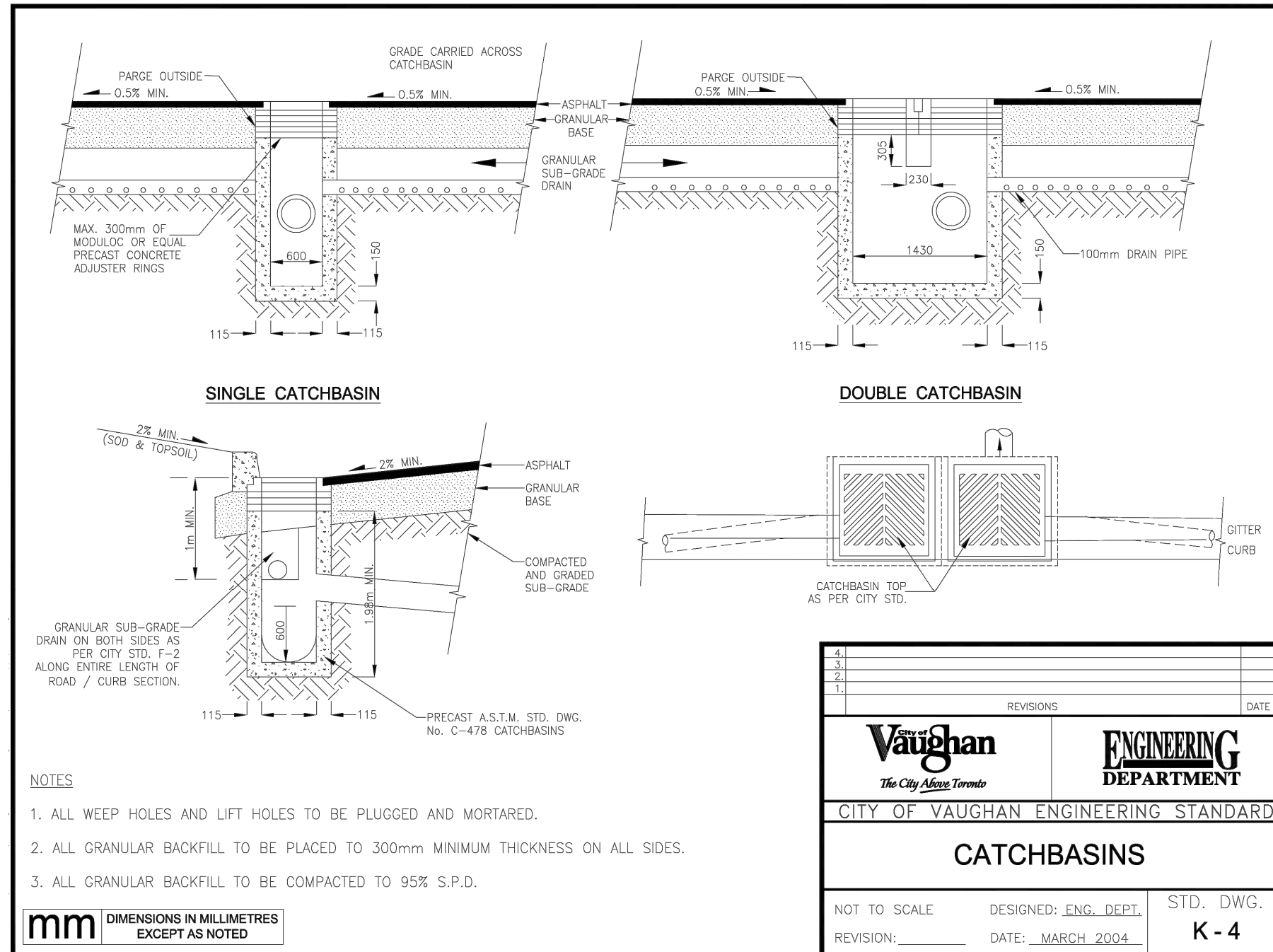
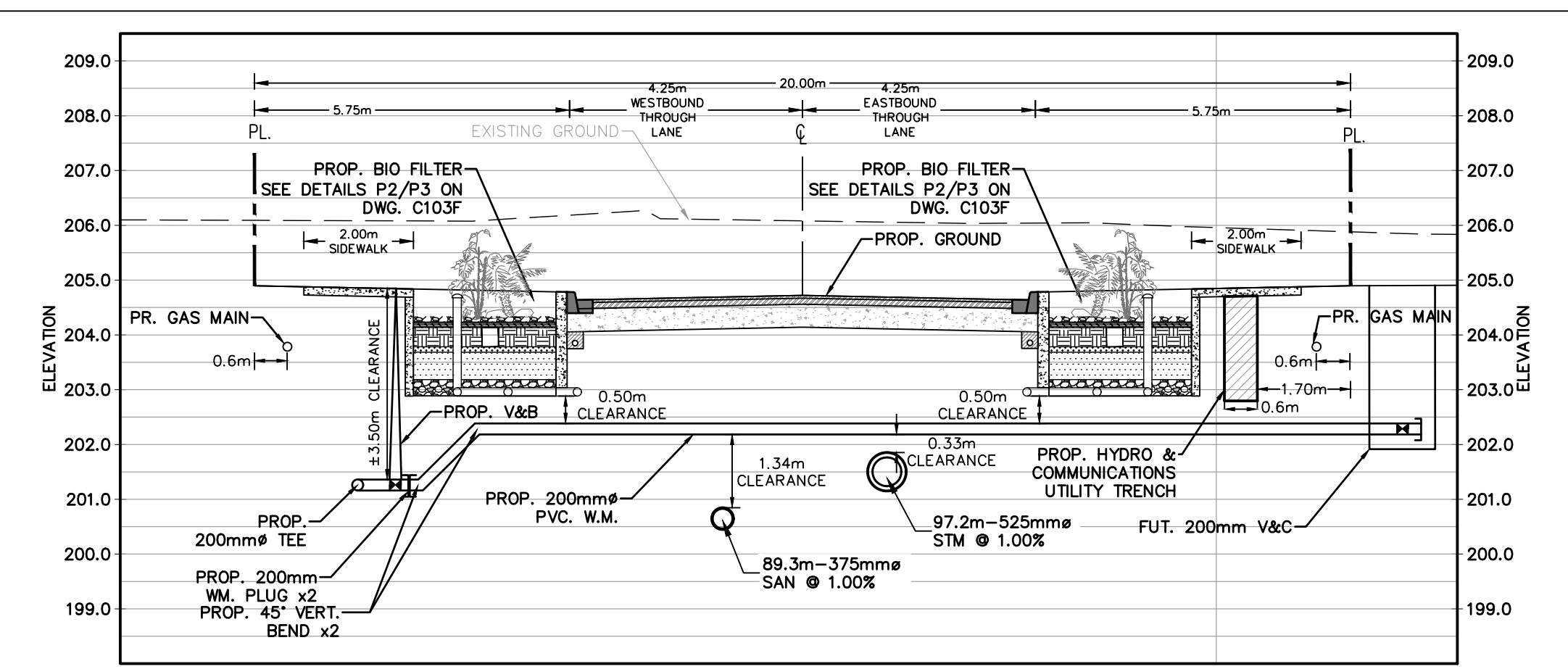
SITE SERVICING SECTION X-X
SCALE: 1:100



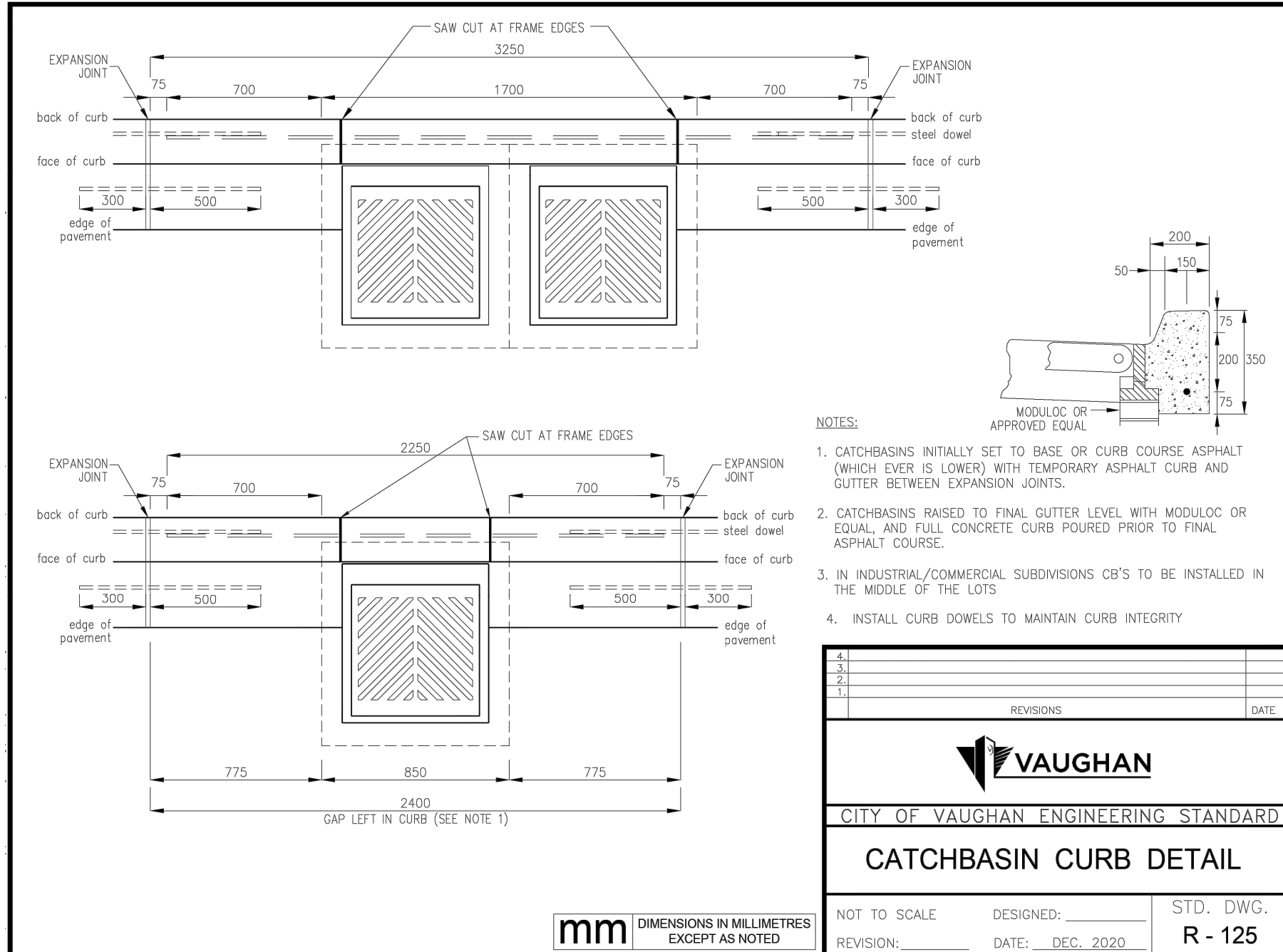
SITE SERVICING SECTION Y-Y
SCALE: 1:100



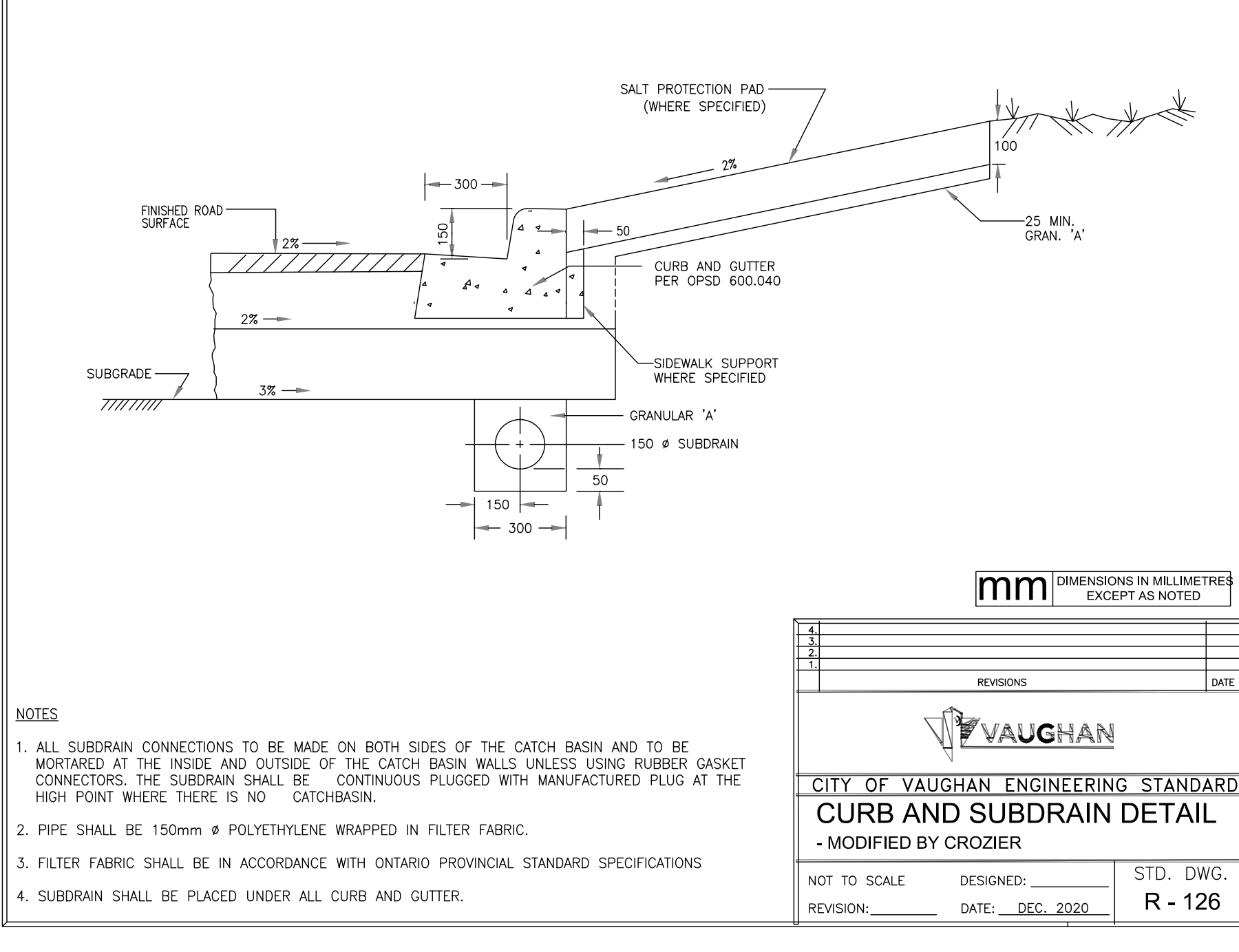
SITE SERVICING SECTION Z-Z
SCALE: 1:100



VAUGHAN ENGINEERING
CITY OF VAUGHAN ENGINEERING STANDARD
CATCHBASINS
NOT TO SCALE DESIGNED: ENG. DEPT. STD. DWG.
REVISION: DATE: MARCH, 2004 K-4



VAUGHAN ENGINEERING
CITY OF VAUGHAN ENGINEERING STANDARD
CATCHBASIN CURB DETAIL
NOT TO SCALE DESIGNED: STD. DWG.
REVISION: DATE: DEC, 2020 R-125



VAUGHAN ENGINEERING
CITY OF VAUGHAN ENGINEERING STANDARD
CURB AND SUBDRAIN DETAIL - MODIFIED BY CROZIER
NOT TO SCALE DESIGNED: STD. DWG.
REVISION: DATE: DEC, 2020 R-126

APPROVED AS TO FORM IN RELIANCE, UPON THE PROFESSIONAL SKILL AND ABILITY OF THE CONSULTING ENGINEERS AS TO DESIGN AND SPECIFICATIONS



DIRECTOR OF DEVELOPMENT ENGINEERING AND INFRASTRUCTURE PLANNING



DATE: REGION FILE NO.: 19T-18V005 (SUBP.18.V0019) SITE PLAN DEVELOPMENT CITY FILE NO.: DA.18.037

4	PERFECT SUBMISSION	2023/JUN/20
3	ISSUED FOR COORDINATION	2023/JUN/08
2	ISSUED FOR 3RD SUBMISSION TO CITY	2023/FEB/22
1	ISSUED FOR 2ND SUBMISSION TO CITY	2022/OCT/11
0	ISSUED FOR 1ST SUBMISSION TO CITY	2022/JUN/17
No.	ISSUE / REVISION	YYYY/MM/DD

TEMPORARY BENCHMARKS
BEARINGS ARE UTM GRID, DERIVED FROM OBSERVED REFERENCE POINTS 1 AND 2. BY REAL TIME NETWORK (RTN) OBSERVATIONS, UTM ZONE 17, NAD83 (ORIGINAL). OBSERVED REFERENCE POINTS (ORP): UTM ZONE 17 NAD83 (ORIGINAL). COORDINATES TO URBAN ACCURACY PER SEC. 14(2) OF OREG 216/10
ORP 1- NORTHING: 4850162.198 EASTING: 619083.869
ORP 2- NORTHING: 4850000.680 EASTING: 619114.676

SCHAEFFER DZALDOV BENNET LTD.
SUE SURVEY ALONG MAPLECRETE RD. AND DOUGHTON RD. BY T2 UTILITY ENGINEERS ATED 06/06/22 PROJECT NO. 61002331
1. THIS DRAWING IS THE EXCLUSIVE PROPERTY OF C.F. CROZIER & ASSOCIATES INC. AND THE REPRODUCTION OF ANY PART WITHOUT PRIOR WRITTEN CONSENT OF THIS OFFICE IS STRICTLY PROHIBITED.
2. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS, LEVELS, AND DATUMS ON SITE AND REPORT ANY DISCREPANCIES OR OMISSIONS TO THIS OFFICE PRIOR TO CONSTRUCTION.
3. THIS DRAWING IS TO BE READ AND UNDERSTOOD IN CONJUNCTION WITH ALL OTHER PLANS AND DOCUMENTS APPLICABLE TO THIS PROJECT.
4. DO NOT SCALE THE DRAWINGS.
5. ALL EXISTING UNDERGROUND UTILITIES TO BE VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO CONSTRUCTION.

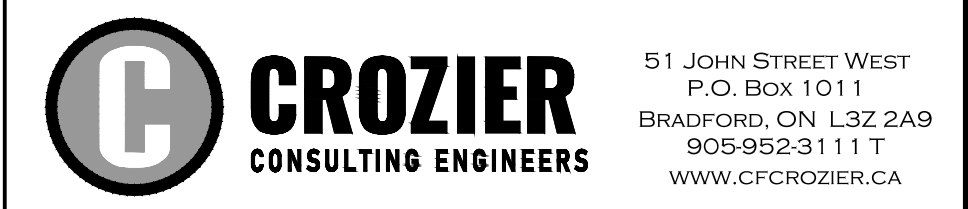


Project: 1930328 ONTARIO INC. 2901 HIGHWAY 7 CITY OF VAUGHAN

Drawing: DETAILS & TYPICAL CROSS-SECTION

FOR APPROVAL
NOT TO BE USED FOR CONSTRUCTION

NOTE: SEE SUPPLEMENTARY LETTER REGARDING SCOPE OF WORK PREPARED BY 1930328 ONTARIO INC.



Drawn: C.C. Design: C.C. Project No: 1525-6298
Check: D.L. Date: J.M.S. Scale: N/A Dwg: C103B

Date: March 11, 2024

To: Matthew Peverini, Senior Planner, VMC

From: Andrea Shotlander, Project Manager, Planning and Urban Design, VMC
Anna Rosen, Project Manager, Parks Development, VMC

Cc: Cory Gray, Manager, Parks & Strategic Initiatives, VMC
Gaston Soucy, Senior Manager, Planning and Urban Design, VMC

Re: CIHA Application
Urban Design and Parks Comments (1st Circulation)

File No: CIHA.23.003

Applicant: 171 Maplecrete LP and 1930328 Ontario Inc.

Location: 171 Maplecrete Rd, 140-160 Doughton Rd, and Block 2 on Plan 65M-4973

Urban Design and Parks Planning Staff have reviewed the 1st Circulation of the CIHA Application for the above-mentioned site, and provide the following comments:

URBAN DESIGN

General Comments

1. Applicant to provide Geo-referenced CAD drawings for CIHA submission. Georeferenced plans must be submitted in the initial submission and last submission.

Site Plan Organization

2. Please revise the architectural site plan to provide a minimum tower step-back of 3m as noted in section 10.2.2. Zone category "V1" of the City By-Law.
3. In the event that the future North-South Private road on the westerly neighbouring lands is not delivered, a building setback of 12.5m will be required from the property line to building face for Building NE-E and SE to ensure a total tower separation distance of 25m is achieved per VMC Secondary Plan policy 8.7.18.
4. In anticipation of a future north-south local road along the east frontage of the proposed development, and as per VMC Secondary Plan Policy 8.0 and 8.7.2, ensure the built form of development frames streets and open spaces and supports an inviting, comfortable and active public realm.
5. Staff appreciate the inclusion of retail space within the ground floor of the northwest building which is intended to have office and a hotel as part of its mixed-uses. The VMC Secondary Plan Update envisions Doughton Road as a primary retail street as such City staff encourage the applicant to also include retail uses along Doughton Road.
6. Clearly differentiate line type between the property line and easement boundary across the site plan and landscape plan.

Public Realm

7. Demonstrate that the proper road Right-of-Ways (ROW) are being provided along White Elm Road (20m ROW), Maplecrete Road (26m ROW), Doughton Road (26m ROW). Ensure there are no conflicts proposed between proposed utilities, plantings and LID Features in any of these roads ROW. Clearly label and dimension distances from all road centrelines to their respective property lines in the architectural and landscape site plans. Include additional dimensions for all the pedestrian boulevards from the face of each road barrier curb to their corresponding property lines.
8. The City-Wide Streetscape Implementation Manual, the VMC Streetscape and Open Space (SOS) Plan, the VMC Urban Design Guidelines and the VMC Streetscape Identity Framework (Draft), identify levels of service for all road streetscapes in the VMC.

Doughton Road is attributed an enhanced level of service streetscape. To satisfy this requirement, and following the previously-mentioned document's recommendations, introduce unit pavers and street furniture, including bike stands, in the amenity zone between planters along Doughton Road and study if pedestrian lighting will be required as part of the public ROW lighting strategy to emphasize a safer, more pedestrian friendly environment, which would contribute to the activation of the planned retail frontage. Ensure the proposed Doughton Road pedestrian boulevard designs adhere to the following breakdown: 0.60 buffer (from face of road concrete barrier curb to cycling facility asphalt paving), 1.50m cycling facility asphalt paving, 0.20m (raised curb at planter), 2.30m amenity zone (open tree planter), 0.20m (raised curb at planter), 2.00m pedestrian clearway zone (concrete sidewalk), 0.50m frontage zone (from concrete sidewalk to property line.)

The design for Maplecrete Road must conform to the Standard Urban Level of Service as per the Vaughan City-Wide Streetscape Implementation Manual and the VMC Streetscape and Open Space Plan. Boulevard breakdown is as follows: 0.55 buffer (from face of road concrete barrier curb to cycling facility asphalt paving), 1.50m cycling facility asphalt paving, 0.20m (raised curb at planter), 2.30m amenity zone (open tree planter), 0.20m (raised curb at planter), 2.00m pedestrian clearway zone (concrete sidewalk), 0.50m frontage zone (from concrete sidewalk to property line.)

The design for White Elm Road and boulevard was previously approved in the Liberty Cement Development with a standard urban level of service. Please refer to the approved plans for the design and breakdown.

9. Remove the proposed layby parking along Maplecrete and normalize the cycling facility to provide a continuous northbound path of travel between the south and the north mixing zones.

Pedestrian Level Wind Study

10. The applicant is required to submit a Pedestrian Level Wind Study, as per the City's Terms of Reference for Wind Study. Please note, a detailed wind tunnel analysis will also be required for the future Site Plan submission.

Sun Shadow Study

11. The provided Sun Shadow Study should be revised to include the most current development files within the surrounding vicinity. Please ensure the Liberty Maplecrete Development is included in this exercise.

The following preliminary comments are provided for information and reference purposes ahead of the future Site Plan application process. Staff reserves the right to comment further once a complete Site Plan application is submitted.

Urban Design

1. Applicant is required to submit 1:50 to 1:100 Scaled Detailed Colour Building Elevations. Clearly identify all exterior design features, materials, and window types. A digital architectural materials board will also be required providing high quality photos of the exterior finish materials with specifications.

Landscape

2. Ensure the following are provided in the complete Landscape Package:
 - a. detailed landscape cross sections of the pedestrian boulevards, at minimum 1:20 scale, taken through all proposed future public roads. Landscape cross section must clearly illustrate and annotate dimensions, locations of existing and proposed services, cycling facilities, property lines, raised planter curbs, materials, and other features, as applicable and must demonstrate that appropriate planter soil depths are provided to ensure healthy tree growth. Boulevard cross section must include longitudinal and cross-sectional sections through the open planters, furniture zone and any alternative proposed boulevard conditions.
 - b. clearly annotated Landscape Layout and Surface and Materials Plan indicating all critical dimensions, offsets and material callouts identifying the corresponding detail within the detail sheets.
 - c. soil calculations demonstrating that all trees within the public realm have access to soil volumes that provide, or exceed, 30 cubic meters of suitable growth medium per tree in a single standalone planter and 20 cubic meters of suitable growth medium per tree in a shared planter with a minimum net soil depth of 1.2 meters.
 - d. planting plans indicating all proposed public and private trees, shrubs and understory planting. Tree species composition strategies must adhere to clusters of not more than 4 trees of a same genus grouped together in public areas. Additionally, the total amount of the same species per application should not exceed 10% to guarantee diversity.
 - e. exterior lighting and photometric plans (in lux) demonstrating safe and appropriate lighting levels are being achieved within the public ROW as per the intended uses. Lighting and photometric plans must clearly denote and label the exact location, information, and specification of all pedestrian and street luminaires.
 - f. landscape details and furniture cut sheets parting to all landscape elements within the public realm.
 - g. public utility plan shadowed over the landscape plan

Landscape Cost Estimate

3. As a requirement of the future Site Plan Submission, please provide a detailed landscape cost estimate that separates works within the site property lines from works within the public-right-of-way. The private works should also be separated by specific areas such as the peripheral frontage zones, private roads, private amenity areas, rooftop amenity terraces, green roofs, etc. The public-right-of-way works breakdown should be separated by individual streets, namely Doughton Road and Maplecrete Road in both interim and ultimate conditions.

Cycling Facility

4. Indicate intended material of cycling facility and buffer strip on landscape drawings. The cycling facility along Doughton Road and Maplecrete Road must be composed of a vehicular load asphalt paving. Provide a detail to that effect.
5. Staff recommend the use of coloured stamped concrete in a 100x300 running bond pattern, similar to the promenade paver pattern. Stamped concrete should be coloured per Interstar Colour Code BN-1703R “Copper” pigment. See attachments for colour chart.

Bird-Friendly Design

6. Given the high amount of large pane glazing below the 16m height limit, bird-friendly glazing (bird frit) is strongly recommended as per Policy Planning and Environmental Sustainability in accordance with CSA A460:19 Bird Friendly Building Design standards. Bird-friendly design information can also be found on the following link:
https://www.vaughan.ca/services/business/urban_design/General%20Documents/202301%20Vaughan%20Bird%20Safe%20Standards%20Info%20Package.pdf

Elevation drawings note bird-friendly glazing should be applied but further information shall be provided with a detail and a descriptive note how these standards will be implemented in the architectural drawing package. Additionally, bird friendly glazing must be provided around all rooftop amenity spaces where landscape and planting are being proposed.

Signage

7. With the understanding that this is a mixed-use development, please submit a signage design and lighting package prepared by a professional consultant and consisting of the following:
 - a. a site plan indicating the location of the proposed signs
 - b. existing building photos or elevations showing the proposed signs in relation with the structure(s)
 - c. detailed, descriptive and dimensioned plans (in metric), cross-sections and elevations showing the materials used, electrical and mechanical components
 - d. illumination details
 - e. colour elevations with Pantone colour index reference numbers

The signage design strategy should meet the City’s Sign By-law, the Vaughan Metropolitan Centre Streetscape and Open Space Plan 6.1 and the Urban Design Guidelines 6.14.

Digital 3D Models and 2D CAD

8. Prior to site plan approval, final and updated 3D digital models will be required. The models must include all aspects of the proposed development and meet the following conditions:
 - a. models shall be high precision and unified spatial reference and data type to geo-reference to the UTM projection (UTM zone 17 NAD83.)
 - b. two types of models will be required throughout the review process as required: (1) basic massing models without details and (2) highly detailed models
 - c. copies of the original file formats are to be provided in a software supported by the City, i.e. Sketch-Up.

Urban Design and Sustainability Brief

9. The applicant is required to submit a Urban Design and Sustainability Brief that provides a detailed framework of the development proposal. The provide shall provide design direction to demonstrate how design goals and objectives of the official plan will be achieved. Refer to the City-Wide Urban Design Guidelines for technical requirements.

Pedestrian and Bicycle Circulation Plan

10. Applicant is to provide a pedestrian and bicycle circulation plan that clearly defines safe pedestrian and cycling connections within the development site and between the development and the neighbouring community. The Plan should indicate a clear hierarchy of circulation routs and identify building entrances, mid-block connections, transit access and routes to public destinations. This may be included as part of the Urban Design and Sustainability Brief.

Tree Protection and Removal Plan, Arborist Report and Tree Protection Agreement (TPA)

11. The Owner shall provide an Arborist report and Tree Inventory and Preservation Plan for all trees within the subject property or within 6m of the property boundary. Efforts should be made to preserve as many trees as possible on site and compensate for the tree removals through replacement planting or monetary contributions.
12. Staff would like to note that as part of the future Site Plan submission, the applicant will be required to enter into a tree protection agreement (TPA) with the City in accordance with the City of Vaughan council-enacted tree by-law 052-2018 and the Tree Protection Protocol. The agreement must be entered prior to execution of Site Plan Agreement and issuance of building permit.

PARKS PLANNING

13. For high-density residential development, the Owner shall, prior to the issuance of a Building Permit, pay to Vaughan by way of certified cheque, payment-in-lieu of the dedication of parkland at the rate of 1 ha per 1000 net residential units, or at a fixed unit rate, at Vaughan's discretion, in accordance with the Planning Act and the City of Vaughan Parkland Dedication By-law.

Notwithstanding the above, parkland contribution in the form of payment-in-lieu as determined by the City shall be subject to a cap of (i) 10% of the Lands or value of the Lands if the Lands are 5 ha or less; or (ii) 15% of the Lands or value of the Lands if the Lands are greater than 5 ha.

Should the Owner choose to proceed with the alternative requirement in the Planning Act, the City will require a current appraisal to determine land value at the time of building permit.

Submission of land value appraisal should allow sufficient review time and be cognizant of expiry associated with appraisals.

Andrea Shotlander

Project Manager of Urban Design
T. 905-832-8585 ext.8035
E. Andrea.Shotlander@vaughan.ca

Anna Rosen

Project Manager of Parks Development
T. 905-832-8585 ext.3637
E. Anna.Rosen@vaughan.ca

END OF MEMORANDUM

DATE : February 23, 2024
TO : Matthew Peverini, Development Planning Department
FILE No. : CIHA 23.003
APPLICANT : Liberty Development
PROPERTY : 171 Maplecrete Road, 160, 140 Doughton Road Block 2 65M-4793
LOCATION

Documents Reviewed:

DWG #	Drawing Title	Rev. #	Date
A001	Context Plan and Stats		Oct 2021
A100	Site Plan		
A101-103	Parking Levels		
A104	Ground Floor Plan		Oct 2021
A105	2 nd Floor		
A106	Podium Floor Plan		Oct 2021
A107	Hotel and Amenity Floor Plan		Oct 2021
A108	Typical Tower Floor Plan		Oct 2021
A201-206	Elevations		Oct 2021
A301-304	Sections		Oct 2021

Compliance with Transition Provisions under Section 1.6 of Zoning By-law 001-2021, as amended:

1. This application has been determined to not be transitioned in accordance with Section 1.6 and therefore is subject to Zoning By-law 1-88, as amended, and Zoning By-law 001-2021, as amended.

Comments associated with Zoning By-law 001-2021, as amended:

Zoning Designation:

The subject lands are currently zoned EM1 and review as V1, Vaughan Metropolitan Centre Station Zone subject to *Exception 14.XXXX* under By-law 001-2021.

Comments:

Please note the following comments are based on the information provided within the documents listed above.

1. The subject property is a corner lot as defined under 001-2021 and therefor White Elm Rd is the front lot line frontage.
2. Are the proposed 182 rental units not part of the contemplated 1565 total units? Are they included in that calculation? (they should form part of that calculation)

3. The DZBL states that frontage will be White Elm, Maplecrete Rd and Doughton Rd. (this isn't required).
4. The draft zoning by-law (DZBL) proposes a minimum front yard setback of 3.0 metres. Please have applicant verify the 3.0 m dimension at the closest point between the property and the North/West (NW) Tower. The dimension seems to taper towards the sight-triangle and may be less than 3.0 m.
5. The DZBL proposes an exterior side yard of 3.0 m. Please have the applicant verify the dimension towards the sight-triangle area as it appears to be 2.95 m.
6. DZBL requests that the Build-to-zone section not apply to the proposal.
7. DZBL should remove the Street Wall Height requirement of Table 10-3.
8. DZBL establishes a maximum podium height of 27 m or 6 storeys.
9. DZBL establishes the Residential Tower setback from any Rear Lot line and Interior Side Lot line to be 12.5 m. The DZBL requires to provide relief to permit a rear yard setback of 5 m for the SW and SE towers, 5 m for the interior side yard setback for the SE and E towers, and 5 m for the NE tower also.
10. DZBL proposes a minimum 2.0 m landscape strip. They have a list of permitted encroachments which includes "Hardscape" the term should be Hard Landscaping as defined to avoid confusion.
11. DZBL establishes a requirement of 4 sqm /unit for indoor/outdoor amenity.
12. DZBL wording for part 9 of the draft should be to permit a minimum setback of 0.0 m from all lot lines for any part of a building below grade.
13. Should the DZBL make allowances for Section 5.15.3: Air ventilation Shafts and staircases?
14. DZBL should make provisions for parking requirements. The subject lands are located outside the VMC boundary, and therefore parking requirements would be under those for "other zones". (although it will be given a V1 zoning category). The DZBL should permit a parking ratio of 0.4 spaces per unit. The DZBL should make provision to permit a minimum of 269 shared parking spaces for visitor and non-residential uses (being office and retail).
15. Please provide details for compliance of Section 6.3.3 Obstructed Parking Spaces if it applies, a typical dimensioned space and the required additional area should be shown in addition to the typical spaces provided.
16. Based on the proposed parking, a minimum of 21 Barrier Free Parking is required. That would be 10 Type A and 11 Type B spaces. A minimum of 10 Type A and 10 Type B are proposed. 1 Additional Type B space is required. Please note, that 9 Type B spaces were actually shown on all 3 levels of parking, please verify the total number of Type B spaces proposed.
17. The DZBL shall make provision to permit 3 Type D, 2 Type B and a shared Type B and D for the site.
18. Temporary parking within the circler Aisle of the NE tower entrance is not permitted.
19. Temporary parking along the access aisle between the SW and SE towers provide a dimension width of 2.5 m (which is deficient) and no lengths are provide. They should be revised to comply or removed.

Please ensure that all Zoning issues are adequately addressed prior to re-submission of Site Plan drawings, and that all subsequent submissions are accompanied by a covering letter indicating the proposed changes to the Site Plan. Additional comments may be forthcoming. If you have any questions or concerns, I may be contacted at the extension below.

Please note the drawings submitted for Building Permit shall match identically with the final approved Development Approval drawings including page numbers, revision numbers and revision dates. Any discrepancies may require further approval through the Development Planning Department.

Regards,

Gregory Seganfredo
Senior Plans Examiner (Zoning)
Building Standards Department
Ext. 8245

DATE : February 27th, 2024
TO : Matthew Peverini, Development Planning Department
FILE No. : CIHA 23.003
APPLICANT : Liberty Development
PROPERTY : 171 Maplecrete Road, 160, 140 Doughton Road Block 2 65M-4793
LOCATION

Documents Reviewed:

DWG #	Drawing Title	Rev. #	Date
A001	Context Plan and Stats		Oct 2021
A100	Site Plan		
A101-103	Parking Levels		
A104	Ground Floor Plan		Oct 2021
A105	2 nd Floor		
A106	Podium Floor Plan		Oct 2021
A107	Hotel and Amenity Floor Plan		Oct 2021
A108	Typical Tower Floor Plan		Oct 2021
A201-206	Elevations		Oct 2021
A301-304	Sections		Oct 2021

Compliance with Transition Provisions under Section 1.6 of Zoning By-law 001-2021, as amended:

1. This application has been determined to not be transitioned in accordance with Section 1.6 and therefore is subject to Zoning By-law 1-88, as amended, and Zoning By-law 001-2021, as amended.

Comments associated with Zoning By-law 1-88, as amended: (for reference)

Zoning Designation:

The subject lands are currently zoned EM1 and EM2 and review as C9, Corporate Centre Zone subject to *Exception 9(.XXXX)* under By-law 1-88a.a.

Comments:

Please note the following comments are based on the information provided within the documents listed above.

1. The subject property is a corner lot as defined under By-law 1-88a.a and therefor White Elm Rd is the front lot line frontage.
2. Are the proposed 182 rental units not part of the contemplated 1565 total units? Are they included in that calculation? (they should form part of that calculation).

3. The DZBL states that frontage will be White Elm, Maplecrete Rd and Doughton Rd. (this isn't required).
4. The draft zoning by-law (DZBL) proposes a minimum front yard setback of 3.0 metres. Please have applicant verify the 3.0 m dimension at the closest point between the property and the North/West (NW) Tower. The dimension seems to taper towards the sight-triangle and may be less than 3.0 m.
5. The DZBL proposes an exterior side yard of 3.0 m. Please have the applicant verify the dimension towards the sight-triangle area as it appears to be 2.95 m.
6. DZBL requests that the Build-to-zone section not apply to the proposal.
7. DZBL establishes a maximum gross floor area of 112,500 sqm. (there is no requirement for maximum g.f.a under By-law 1-88).
8. DZBL establishes a maximum podium height of 27 m or 6 storeys.
9. DZBL establishes the maximum building heights of 130 metres or 40 Storeys(although By-law 1-88 only refers to height in metres).
10. DZBL proposes a minimum 2.0 m landscape strip. They have a list of permitted encroachments which includes "Hardscape" the term should be Hard Landscaping as defined to avoid confusion.
11. DZBL establishes a requirement of 4 sqm /unit for indoor/outdoor amenity.
12. DZBL wording for part 9 of the draft should be to permit a minimum setback of 0.0 m from all lot lines for any part of a building below grade.
13. Should the DZBL make allowances for Section 5.15.3: Air ventilation Shafts and staircases?
14. DZBL should make provisions for parking requirements. The subject lands are located outside the VMC boundary, and therefore parking requirements would be under those for "other zones". (although it will be given a V1 zoning category). The DZBL should permit a parking ratio of 0.4 spaces per unit. The DZBL should make provision to permit a minimum of 269 shared parking spaces for visitor and non-residential uses (being office and retail and Hotel uses).
15. Based on the proposed parking, a minimum of 21 Barrier Free Parking is required. That would be 10 Type A and 11 Type B spaces. A minimum of 10 Type A and 10 Type B are proposed. 1 Additional Type B space is required. Please note, that 9 Type B spaces were actually shown on all 3 levels of parking, please verify the total number of Type B spaces proposed.
16. The DZBL shall make provision to permit 3 Type D, 2 Type B and a shared Type B and D for the site.
17. Temporary parking within the circler Aisle of the NE tower entrance is not permitted.
18. Temporary parking along the access aisle between the SW and SE towers provide a dimension width of 2.5 m (which is deficient) and no lengths are provide. They should be revised to comply or removed.
19. DZBL does not require to make provision for Tower step-back of 2 m, nor tower separation between all buildings of 25 m. A rear setback of 12.5 is also not required nor the Rear lot line or interior side lot line setback of 5 m (those are 001-2021 requirements).
20. The DZBL has a provision to establish a minimum of 12.5 % of g.f.a to be non-residential uses, there is no requirement for this.

Please ensure that all Zoning issues are adequately addressed prior to re-submission of Site Plan drawings, and that all subsequent submissions are accompanied by a covering letter indicating the proposed changes to the Site Plan. Additional comments may be forthcoming. If you have any questions or concerns, I may be contacted at the extension below.

Please note the drawings submitted for Building Permit shall match identically with the final approved Development Approval drawings including page numbers, revision numbers and revision dates. Any discrepancies may require further approval through the Development Planning Department.

Regards,

Gregory Seganfreddo
Senior Plans Examiner (Zoning)
Building Standards Department
Ext. 8245



Hi-Density & Commercial Residential Combined
(Commercial less than 20% of the Gross Floor Area)
(Residential greater than 5 storeys & 10 residential units)

DATE: 2/29/2024
TO: DEVELOPMENT PLANNING
ATTENTION: MATTHEW PEVERINI
SUBJECT: REQUEST FOR COMMENTS
FILE #: CIHA.23.003
171 MAPLECRETE ROAD, 140-160 DOUGHTON ROAD AND
BLOCK 2 ON REGISTERED PLAN 65M-4793
APPLICANT: LATIF FAZEL
C/O 171 MAPLECRETE LP & 1930328 ONTARIO INC.

Comment:
(see below)

No Comment:

“For high-density residential development, the Owner shall, prior to the issuance of a Building Permit, convey land at the rate of 1 ha per 600 net residential units and/or pay to Vaughan by way of certified cheque, cash-in-lieu of the dedication of parkland at the rate of 1 ha per 1000 net residential units, or at a fixed unit rate, at Vaughan’s discretion, in accordance with the *Planning Act* and the City of Vaughan Parkland Dedication By-law. Notwithstanding the above, such parkland contribution—whether in the form of parkland conveyance or cash-in-lieu as determined by the City—shall be subject to a cap of (i) 10% of the Lands or value of the Lands if the Lands are 5 ha or less; or (ii) 15% of the Lands or value of the Lands if the Lands are greater than 5 ha.”

“Prior to the issuance of a Building Permit, the Owner shall pay to the City of Vaughan by way of certified cheque a community benefits charge equivalent to 4% of the value of the subject lands in accordance with Section 37 of the *Planning Act* and the City’s Community Benefits Charge By-law. The Owner shall submit an appraisal of the subject lands, pursuant to City’s Community Benefits Charge By-law, prepared by an accredited appraiser for approval by the Vaughan Real Estate Department, and the approved appraisal shall form the basis of the calculation of the community benefits charge payment.”

Paul Salerno

Paul Salerno
Director of Real Estate
(Ext. 8473)

Matthew Peverini

From: Abanoub Abadeer
Sent: Tuesday, January 30, 2024 11:20 AM
To: Matthew Peverini
Subject: RE: Request for Comments - CIHA.23.003 - 171 Maplecrete Road, 140-160 Doughton Road and Block 2 on Registered Plan 65M-4793

Hello Mathew,

We have looked over the information provided for the File. The package does not show or state any fire related information therefore I have added below some of the general comments that are normally associated with this type of development. Please only take as General Comments i.e. no comments.

The following comments raised to provide a minimum level of fire safety and protection to the environment and the building construction stage which will support firefighting operations when required. All applicable building code requirements shall be addressed.

1. Water supply for firefighting, including hydrants, municipal or private shall be identified in the plans and code requirements to be installed and operational prior to construction of building.
2. Hydrants/Fire department connections shall be unobstructed and ready for use at all times.
3. Access roadways shall be maintained and suitable for large heavy vehicles.
4. Roads shall be complete to a minimum base coat of asphalt capable of carrying emergency vehicle loading prior to construction of buildings.
5. Temporary Municipal address to be posted and visible for responding emergency vehicles satisfactory to the City.
6. Based on site drawing the private roadway is a fire route and parking on the street is prohibited. This very well could be an issue based on the single car garage and parking for one vehicle in front of the garage.
7. Fire Route to be posted prior to construction. (ensure the signs are approved with the by-law 1-96 is on the sign).
8. Fire route to be in accordance with the 2012 Ontario Building Code
9. Width of the roadway
10. Is parking permitted
 - o Is the fire route posted as per by-laws
 - o General concerns of vehicle parking on the roads which would reduce the width below the minimum code requirements for fire route.

Thank You

Abanoub (Abby) Abadeer
Fire Protection Technologist
416-522-7319 | Abanoub.Abadeer@vaughan.ca

City of Vaughan | Vaughan Fire & Rescue Service
2141 Major Mackenzie Dr., Vaughan, ON L6A 1T1

Date: **2/7/2024**

To: Matthew Peverini, Senior Planner - VMC, Policy Planning & Special Programs

From: Ryan Cochrane, Forestry Inspector, Parks, Forestry and Horticulture Operations

Cc: Forestry Circulations;

Re: Forestry's Comments - Circulation #:1

File Reference Number:

Related Files: CIHA.23.003 and 65M-4793

Ward: 4

Agent: Thanh Nguyen c/o Liberty Development Corporation

Owner: Latif Fazel c/o 171 Maplecrete LP & 1930328 Ontario Inc.

Location: 171 Maplecrete Road, 140-160 Doughton Road and Block 2 on Registered Plan 65M-4793

Forestry Staff have reviewed the Arborist Report and Tree Inventory, Preservation & Removals Plan of the proposed development concept for the above-mentioned site, circulated on 1/29/2024 and provide the following comments:

General Comments

1. Provide an arborist report and tree inventory protection plan for all trees 20 cm and greater located on the subject property, municipally owned trees of any size, and all trees within 6m of the property boundary.

Ryan Cochrane
Forestry Inspector
Parks, Forestry and Horticulture Operations
Ryan.Cochrane@vaughan.ca

Matthew Peverini

From: Sharon Walker
Sent: Wednesday, February 28, 2024 1:23 PM
To: Matthew Peverini
Subject: RE: Request for Comments - CIHA.23.003 - 171 Maplecrete Road, 140-160 Doughton Road and Block 2 on Registered Plan 65M-4793

Matthew

My apologies for the late response. If it has not already been requested by another department,

- a land use compatibility study using the D1 through 6 guidelines is required given the nature of industrial operations in the area.

Regards

Sharon Walker, BPA, ABCP, CMM III EMP
Manager, Emergency Planning
905-832-8585, ext. 6322 | sharon.walker@vaughan.ca
Cell: 416-524-7923

Vaughan Fire and Rescue Service | Emergency Planning
2800 Rutherford Road, Vaughan, ON L4K 2N9
www.vaughan.ca/PrepE



Matthew Peverini

From: Lindsay Davidson
Sent: Wednesday, January 31, 2024 5:43 PM
To: Matthew Peverini
Cc: Alyssa Pangilinan
Subject: RE: Request for Comments - CIHA.23.003 - 171 Maplecrete Road, 140-160 Doughton Road and Block 2 on Registered Plan 65M-4793

Hi Matthew,

Thanks for sharing. From Economic Development:

- The retail component is very limited given the three-building footprint
- Economic Development supports the consideration to expand the retail footprint Given there is very little else in terms of retail currently in the immediate vicinity for the residential component and visitors

Thank you,
Lindsay

Matthew Peverini

From: Alexandre Thibault <Alexandre.Thibault@cn.ca> on behalf of Proximity <proximity@cn.ca>
Sent: Thursday, February 8, 2024 3:36 PM
To: Matthew Peverini
Subject: [External] 2024-02-08_CN Comments -Vaughan_ CIHA.23.003 - 171 Maplecrete Road, 140-160 Doughton Road and Block 2 on Registered Plan 65M-4793
Attachments: Circulation Internal External Letter_CIHA.23.003.pdf

CAUTION! This is an external email. Verify the sender's email address and carefully examine any links or attachments before clicking. If you believe this may be a phishing email, please use the Phish Alert Button.

Hello Matthew,

Thank you for consulting CN on the application mentioned in subject. It is noted that the subject site is within 1000 meters of CN railway operations including the CN Yard's. CN has concerns of developing/densifying residential uses in proximity to railway operations. Development of sensitive uses in proximity to railway operations cultivates an environment in which land use incompatibility issues are exacerbated. CN's guidelines reinforce the safety and well-being of any existing and future occupants of the area. Please refer to CN's guidelines for the development of sensitive uses in proximity to railways. These policies have been developed by the Railway Association of Canada and the Federation of Canadian Municipalities. CN encourages the municipality to pursue the implementation of the following criteria as conditions of an eventual project approval:

- The following clause should be inserted in all development agreements, offers to purchase, and agreements of Purchase and Sale or Lease of each dwelling unit within 1000m of the railway right-of-way:

"Warning: Canadian National Railway Company or its assigns or successors in interest has or have a right-of-way within 1000 metres from the land the subject hereof. There may be alterations to or expansions of the railway facilities on such rights-of-way in the future including the possibility that the railway or its assigns or successors as aforesaid may expand its operations, which expansion may affect the living environment of the residents in the vicinity, notwithstanding the inclusion of any noise and vibration attenuating measures in the design of the development and individual dwelling(s). CNR will not be responsible for any complaints or claims arising from use of such facilities and/or operations on, over or under the aforesaid rights-of-way."

- The Owner shall through restrictive covenants to be registered on title and all agreements of purchase and sale or lease provide notice to the public that the noise and vibration isolation measures implemented are not to be tampered with or altered and further that the Owner shall have sole responsibility for and shall maintain these measures to the satisfaction of CN.
- The Owner shall enter into an Agreement with CN stipulating how CN's concerns will be resolved and will pay CN's reasonable costs in preparing and negotiating the agreement.
- The Owner shall be required to grant CN an environmental easement for operational noise and vibration emissions, registered against the subject property in favour of CN.

CN is currently reviewing the noise impact study provided and will provide further comments once the review is complete .

Thank you and do not hesitate to contact me with any questions.

Best regards,

Alexandre Thibault, B.Sc. Urb

Urbaniste stagiaire / Urban Planner Intern (CN Proximity)
Planning, Landscape Architecture and Urban Design
Urbanisme, architecture de paysage et design urbain



E : proximity@cn.ca
1600, René-Lévesque Ouest, 11e étage
Montréal (Québec)
H3H 1P9 CANADA
wsp.com

Matthew Peverini

From: Lingard, Norman <norman.lingard@bell.ca>
Sent: Thursday, February 8, 2024 11:21 AM
To: Matthew Peverini
Subject: [External] CIHA.23.003 - 171 Maplecrete Road, 140-160 Doughton Road and Block 2 on Registered Plan 65M-4793

CAUTION! This is an external email. Verify the sender's email address and carefully examine any links or attachments before clicking. If you believe this may be a phishing email, please use the Phish Alert Button.

Good morning Matthew,

Bell Canada thanks you for the opportunity to participate in the City of Vaughan's CIHA request for the above noted. Bell Canada is currently evaluating the most effective means to collaborate with municipalities towards the goals of the '*More Homes for Everyone*' and '*More Homes Built Faster*' Acts. This includes assessing how we can consistently and appropriately process and respond to pre-consultation circulations for *Planning Act* applications from municipalities across Ontario.

As an interim solution, we are providing the following standard response to all pre-consultation circulations from municipalities. If there are development proposal-specific concerns that are not addressed by our standard response, we are requesting that questions and concerns be sent directly to planninganddevelopment@bell.ca.

About Bell Canada

Bell Canada is Ontario's principal telecommunications infrastructure provider, developing and maintaining an essential public service. The Bell Canada Act, a federal statute, requires that Bell supply, manage and operate most of the trunk telecommunications system in Ontario. Bell is therefore also responsible for the infrastructure that supports most 911 emergency services in the Province. The critical nature of Bell's services is declared in the Bell Canada Act to be "for the general advantage of Canada" and the Telecommunications Act affirms that the services of telecommunications providers are "essential in the maintenance of Canada's identity and sovereignty."

Provincial policy further indicates the economic and social functions of telecommunications systems and emphasizes the importance of delivering cost-effective and efficient services:

- The 2020 Provincial Policy Statement (PPS) requires the development of coordinated, efficient and cost-effective infrastructure, including telecommunications systems (Section 1.6.1).
- Section 1.7.1 I) of the 2020 PPS recognizes that "efficient and coordinated telecommunications infrastructure" is a component of supporting long-term economic prosperity.
- We note that the definition of infrastructure in the 2020 PPS is inclusive of communications / telecommunications, which is indicative of the importance in providing efficient telecommunications services to support current needs and future growth (Section 1.6.1).
- Furthermore, the 2020 PPS states that infrastructure should be "strategically located to support the effective and efficient delivery of emergency management services" (Section 1.6.4), which is relevant to telecommunications since it is an integral component of the 911 emergency service.

To support the intent of the Bell Canada Act and Telecommunications Act and ensure consistency with Provincial policy, Bell Canada has become increasingly involved in municipal policy and infrastructure initiatives. We strive to establish partnerships which allow for a solid understanding of the parameters of Bell's infrastructure and provisioning needs and the goals and objectives of the municipality related to utilities.

Comments on the CIHA

We would like to thank you for the opportunity to be included in this process and the City's recognition of the importance of telecommunications and broadband infrastructure as critical components to development and economic viability. As such, to facilitate the provisioning of this infrastructure, we appreciate the City's continued support in ensuring that sufficient notice and time to comment on planning applications are provided, particularly for Draft Plan of Condominium, Draft Plan of Subdivision and Site Plan Control/Approval. This ensures an understanding by applicants of Bell's conditions and provisioning requirements, and that the following paragraphs are to be included as a condition of approval for any Draft of Plan of Subdivision, Draft Plan of Condominium and/or Site Plan application(s):

"The Owner acknowledges and agrees to convey any easement(s) as deemed necessary by Bell Canada to service this new development. The Owner further agrees and acknowledges to convey such easements at no cost to Bell Canada.

The Owner agrees that should any conflict arise with existing Bell Canada facilities where a current and valid easement exists within the subject area, the Owner shall be responsible for the relocation of any such facilities or easements at their own cost.

It shall be noted that it is the responsibility of the Owner to provide entrance/service duct(s) from Bell Canada's existing network infrastructure to service this development. In the event that no such network infrastructure exists, in accordance with the Bell Canada Act, the Owner may be required to pay for the extension of such network infrastructure.

If the Owner elects not to pay for the above noted connection, Bell Canada may decide not to provide service to this development".

The above-noted conditions will remain in effect until said conditions are officially cleared by Bell Canada as part of the formal approval process.

Bell would also emphasize that receiving engineering and servicing/composite utility plans/drawings, as soon as possible in the process, assists in the development and expedition of our provisioning plan. As a result, we would strongly recommend that this consideration be highlighted in any pre-circulation/consultation meetings with prospective applicants. This will assist Bell in providing comments and clearance letters in an efficient manner, assisting the Municipality in meeting approval times. Such drawings should be submitted to: planninganddevelopment@bell.ca by the applicant/their agents.

Future Involvement

We would like to thank you again for the opportunity to comment, and would request that Bell continue to be circulated on any future materials and/or decisions released by the City of Vaughan in relation to this initiative. Please forward all future documents to circulations@wsp.com and should you have any specific questions, please contact the undersigned.

Have a great remainder of your week.

Yours truly,

Norm Lingard
Senior Consultant – Municipal Liaison

Network Provisioning

norman.lingard@bell.ca | ☎ 365.440.7617



Please note that WSP operates Bell Canada's development, infrastructure and policy tracking systems, which includes the intake and processing of municipal circulations. However, all responses to circulations and requests for information will come directly from Bell Canada, and not from WSP. WSP is not responsible for the provision of comments or other responses.

This email message, and any attachments, may contain information or material that is confidential, privileged and/or subject to copyright or other rights. Any unauthorized viewing, disclosure, retransmission, dissemination or other use of or reliance on this message, or anything contained therein, is strictly prohibited and may be unlawful. If you believe you may have received this message in error, kindly inform the sender by return email and delete this message from your system

ATTACHMENT 10

YORK REGION COMMENTS

March 12, 2024

Matthew Peverini, RPP, MCIP
Senior Planner, VMC
City of Vaughan
Development Planning Department
2141 Major Mackenzie Drive
Vaughan, ON L6A 1T1

Dear Matthew Peverini:

**Re: Community Infrastructure and Housing Accelerator (CIHA)
C/O 171 MapleCrete LP & 1930328 Ontario Inc.
171 Maplecrete Road, 140-160 Doughton Road, and
Block 2 on Registered Plan 65M-4793
City File Nos.: CIHA.23.003
York Region File Nos.: PROV.24.V.0001**

This is in response to your first circulation and request for comments for the Community Infrastructure and Housing Accelerator (CIHA) application.

The 1.41 ha subject site is located on the northeast corner of Maplecrete Road and Doughton Road. The proposed development consists of 1,565 dwelling units (128 being rental), 5,840m² of Office GFA, 8,243m² of hotel GFA, 126 m² of retail GFA, approximately 2,643m² of outdoor amenity area and 3,617m² of indoor amenity area. The development will be serviced by 919 parking spaces in 3 levels of underground parking. The overall built density is 8.0 FSI.

Previous Development Applications

In March 2023, the applicant submitted Official Plan Amendment File OP.23.002 and Zoning By-law Amendment File Z.23.004 to the City of Vaughan. The OPA proposed to redesignate the subject lands from "General Employment" to "Station Precinct", having the effect of adding the subject lands to the VMC Secondary Plan area, and to permit maximum building height of 50 storeys, maximum density of 11.4 FSI, and increase the 750 m² residential tower floorplate size to 850 m².

Our planning comments concluded that, "The City is currently undertaking a process to update the Vaughan Metropolitan Centre Secondary Plan, which will expand the boundary to include the subject site and will identify preferred land uses, the appropriate level of development, and the

required hard and soft services needed to accommodate new growth. Therefore, the approval of this site-specific OPA should await further direction from the City on an updated Secondary Plan, as it will provide guidance on the appropriate height, density, urban design, transportation and water and wastewater requirements in a comprehensive and coordinated manner.”

The applicant has now withdrawn the OPA and rezoning applications and has expanded the scope of the development area (to include other lands owned by the applicant) and transitioned the file to a Community Infrastructure and Housing Accelerator application.

Purpose and Intent of the Community Infrastructure and Housing Accelerator (CIHA) Order

According to the applicant’s Planning Justification Report, the use of a CIHA Order is justifiable, as the Order would be quicker than progressing through the traditional OPA and rezoning municipal process and their proposed development meets Provincial and Vaughan’s criteria for CIHA Orders.

Vaughan Official Plan

According to the applicant’s Planning Justification report, prepared by MacNaughton Hermsen Britton Clarkson Planning Limited, dated January 2024, the subject lands are partially within the Vaughan Metropolitan Centre (VMC) Secondary Plan area, with the majority of the southern portion of the subject lands currently designated “General Employment” by the 2010 Vaughan Official Plan.

The City of Vaughan is currently conducting a review of the VMC Secondary Plan, with the intention of expanding the VMC boundary to include the subject lands and other lands on the northside of Doughton Road.

Conformity with the 2022 York Region Official Plan

The 2022 York Region Official Plan (ROP) contains policies that guide economic, environmental and community building decisions to manage growth. These policies strengthen the connections between the natural and built environment, job opportunities, human services, transportation, public health and fiscal capacity. The Regional Official Plan policies also coordinate and set the stage for more detailed planning by local municipalities.

The Regional Official Plan prescribes an urban structure based on an intensification matrix whereby Regional Centres and Corridors are intended to accommodate the highest concentration of intensification, followed by GO transit train stations, bus terminals and subway stations and further down the matrix, Local Centres and Corridors. As such, it is a Regional interest to ensure appropriate levels of intensification occurs within all intensification areas.

Through the last Municipal Comprehensive Review, the subject lands progressed through an employment land conversion and was subsequently not included in an employment area.

According to the 2022 York Region Official Plan (ROP), the subject lands are designated Community Area. The Community Area permits a wide range of urban uses, including residential, population-related employment and community services.

An important planning policy direction for intensification areas is the creation of pedestrian oriented, 15-minute complete communities. The provision of retail and personal service establishments within close walkable proximity helps reduce the dependence of automobile trips. ROP policy 4.4.9 states, *“To direct a significant amount of mixed-uses, including street-related commercial, to strategic growth areas.”* The proposed development’s 126 m² of ground floor commercial space is only 0.1% of the total 112,451 m² of gross floor area. The applicant is encouraged to provide a greater amount of population serving retail floor space.

The ROP also encourages larger family sized units for intensification projects (Policy 4.4.8). The proposed development has 1,304 one-bedroom units. This represents 83% of the total residential units. The applicant is encouraged to provide a greater proportion of family sized units and additional purpose-built rental housing units.

Regional Planning Comments

While Regional Development Services do not oppose the use of a CIHA Order to add the subject lands to the VMC Secondary Plan area, the appropriate level of intensification, relative to the provision of a complete community is best determined by the City of Vaughan. The proposed CIHA continues a development pattern that exceeds planned growth, which may have a cumulative impact on the Region’s planned transportation and water and wastewater networks.

The City of Vaughan is encouraged to complete the update to the VMC Secondary Plan, as comprehensive planning is needed to determine appropriate height, density, urban design, transportation and water and wastewater requirements in a coordinated manner.

Departmental Comments

Below is a summary of comments received from Regional Departments.

Transportation

Transportation Planning staff has reviewed the development application along with the updated Transportation Impact Study dated January 2024 prepared by LEA Consulting and provide the following comment, which shall be addressed to the satisfaction of the Region.

- 1) Maplecrete Road between Highway 7 and Doughton Road shall be built to its ultimate configuration to accommodate the subject development. Additionally, the south approach to the Highway 7 and Maplecrete Road intersection shall be implemented with the required storage lengths for 95th percentile queues.

Transportation Planning staff require the following conditions be included in the CIHA Order.

Prior to final approval:

1. Implement the planned internal road/connections network as shown in the approved VMC Secondary Plan.
2. Provide preliminary design and cost estimate to construct south approach to the Highway 7 and Maplecrete Road intersection as per Regional standards.
3. Provide a drawing to show the layout of active transportation facilities and connections internal to the site and to the boundary roads.
4. Provide a revised Transportation Study to address all comments related to the Transportation Impact Study dated January 2024 prepared by LEA Consulting.

The Owner shall agree to:

1. Provide direct shared pedestrian/cycling facilities and connections from the proposed development to boundary roadways to support active transportation and public transit, where appropriate.
2. Implement/construct Maplecrete Road between Highway 7 and Doughton Road to its ultimate configuration to accommodate the subject development. Additionally, the south approach to the Highway 7 and Maplecrete Road intersection shall be implemented with the required storage lengths for 95th percentile queues.
3. Implement all recommendations, including TDM measures, as recommended in the revised Traffic Impact Study, to the satisfaction of the Region

Sustainable Mobility Comments

Sustainable Mobility staff has reviewed the Transportation Impact Study (TIS), prepared by LEA, dated January 2024, and provides the following comments:

1. The applicant is advised to coordinate with City of Vaughan staff to understand any requirements to provide an in-boulevard cycle track along the subject site's frontage of Maplecrete Road and Doughton Road.
2. The TDM Checklist (Table 11-1) shall be revised to include a line item identifying a PRESTO card transit incentive, pre-loaded with the value of a one-month YRT pass, to be provided to each residential unit. The cost of the transit incentive is to be borne solely by the applicant and will not be the responsibility of the Region. The Region has identified this to be an appropriate TDM initiative given the context identified in the TIS. The development has significant access and connectivity to transit (Section 2.2) and active transportation infrastructure (Section 2.3), furthermore the identified transit incentive will be a key driver for travel behavior change in order to achieve the target modal split identified (Table 4-2). The applicant is advised that the average forecasted cost for transit passes is \$155/pass/month.

Water and Wastewater Servicing

Infrastructure Asset Management (IAM) staff have reviewed the application in conjunction with the Functional Servicing and Stormwater Management Report (FSR) prepared by Valdor Engineering, dated January 2024, and provide the follow comments:

Servicing Allocation

- 1) Lands located in and in close proximity to the Vaughan Metropolitan Centre (VMC) Secondary Plan area, including the subject lands, are contemplating higher density growth than planned in the recently adopted Regional Official Plan. Please note that the Region's recently completed 2022 Water and Wastewater Master Plan has not considered the level of intensification proposed. As such, the potential impact of proposed growth in the VMC (and upstream and downstream areas) on the Regional infrastructure system must be monitored and addressed through various capacity management tools which include: the capacity assignment program (managed in collaboration with local municipalities), future master plan studies, phasing of growth in alignment with available capacity, etc. At this time, the Region's servicing capacity commitment remains within the limits of currently assigned capacity to the City of Vaughan. It is in the City's jurisdiction to allocate the Region assigned capacity to individual developments based on their growth priorities.
- 2) The residential development proposed within the subject development area will require water and wastewater servicing allocation from the City of Vaughan. If the City of Vaughan does not grant this development the required allocation from the Region's existing capacity assignments to date, then the development may require additional infrastructure based on conditions of future capacity assignment.
- 3) While Industrial/Commercial/Institutional developments typically do not require servicing capacity allocation, hotel suites containing full in-suite kitchen amenities require servicing capacity allocation due to the resulting higher water/wastewater servicing needs. More detailed information or floor plans for the proposed hotel suites is needed to determine the amount of servicing capacity allocation required to facilitate the proposed development. IAM requests further information on this item.

Water Servicing

- 4) The FSR and water servicing plans indicate that water servicing will be provided by connecting to watermain on Maplecrete Road, which is planned to be upsized from a 150mm diameter pipe to 400mm by the City by 2024. An additional connection is also proposed to the existing 300mm watermain on Doughton Road and future 200mm watermain on the future White Elm Road, north of the site. Hydrant flow tests on Maplecrete and Doughton were completed on November 10, 2022, which indicated that adequate flows and pressures to support the development are anticipated, including required fireflows.

Wastewater Servicing

- 5) The FSR and wastewater servicing plans indicate that wastewater servicing will be provided by connecting to the local municipal sanitary sewer on Maplecrete Road, which is planned to be upsized from a 200mm diameter pipe to 450mm by the City by 2024. The flows from this development are tributary to the Region's Black Creek Sewage Pumping Station.
- 6) The FSR notes that the water and wastewater servicing strategy are aligned with the FSSR for the VMC completed by the City. Should changes occur to the servicing strategy, an updated FSR shall be provided to the Region for review and record.

Dewatering

- 7) The Owner is advised temporary discharge to the municipal sanitary sewer system will only be considered when no other alternatives are feasible. The Region does not support permanent discharge of groundwater to the sanitary system. Please incorporate in the design accordingly.

Other

- 8) It is noted that water and wastewater servicing plan drawings (FSP-1, FSG-1) are required to be signed and sealed by a licensed professional engineer, prior to final approval of the development.

Water Resources

As this site is not within any Source Protection vulnerable areas, Water Resources has no comment with the site application as it relates to Source Protection policy.

Summary

The proposed development consists of 1,565 dwelling units (128 being rental), 5,840m² of Office GFA, 8,243m² of hotel GFA, 126 m² of retail GFA, approximately 2,643m² of outdoor amenity area and 3,617m² of indoor amenity area. The development will be serviced by 919 parking spaces in 3 levels of underground parking. The overall built density is 8.0 FSI.

The applicant has requested the City of Vaughan to endorse the use of a Provincial Community Infrastructure and Housing Accelerator (CIHA) Order. York Region Development Planning staff do not oppose the use of a CIHA Order to add the subject lands to the VMC Secondary Plan area. However, the appropriate level of intensification, relative to the provision of a complete community is best determined by the City of Vaughan.

Should you have any questions or require further information regarding our comments, please contact Augustine Ko, Senior Planner, at 1-877-464-9675, ext. 71524, or by email at Augustine.Ko@york.ca.

Sincerely,



Karen Whitney, MCIP, RPP
Director, Development Services

AK/

May 16, 2024

Matthew Peverini, RPP, MCIP
Senior Planner, VMC
City of Vaughan
Development Planning Department
2141 Major Mackenzie Drive
Vaughan, ON L6A 1T1

Dear Matthew Peverini:

**Re: Community Infrastructure and Housing Accelerator (CIHA)
C/O 171 MapleCrete LP & 1930328 Ontario Inc.
171 Maplecrete Road, 140-160 Doughton Road, and
Block 2 on Registered Plan 65M-4793
City File Nos.: CIHA.23.003
York Region File Nos.: PROV.24.V.0001**

Further to our March 12, 2024 comment letter, York Region staff met with Liberty Developments about our Transportation conditions to be included in the CIHA Order. In order to provide more clarity and accuracy, please replace the March conditions with the Revised Conditions, as set out below.

March conditions to be replaced

Transportation Planning staff require the following conditions be included in the CIHA Order.

Prior to final approval:

1. Implement the planned internal road/connections network as shown in the approved VMC Secondary Plan.
2. Provide preliminary design and cost estimate to construct south approach to the Highway 7 and Maplecrete Road intersection as per Regional standards.
3. Provide a drawing to show the layout of active transportation facilities and connections internal to the site and to the boundary roads.
4. Provide a revised Transportation Study to address all comments related to the Transportation Impact Study dated January 2024 prepared by LEA Consulting.

The Owner shall agree to:

1. Provide direct shared pedestrian/cycling facilities and connections from the proposed development to boundary roadways to support active transportation and public transit, where appropriate.
2. Implement/construct Maplecrete Road between Highway 7 and Doughton Road to its ultimate configuration to accommodate the subject development. Additionally, the south approach to the Highway 7 and Maplecrete Road intersection shall be implemented with the required storage lengths for 95th percentile queues.
3. Implement all recommendations, including TDM measures, as recommended in the revised Traffic Impact Study, to the satisfaction of the Region.

Revised Conditions

Transportation Planning staff require the following conditions be included in the CIHA Order:

Prior to final Site Plan approval:

1. Implement the planned internal road/connections network as shown in the approved VMC Secondary Plan.
2. If not already arranged or completed by others, provide preliminary design and cost estimate to construct south approach to the Highway7 and Maplecrete Road intersection as per Regional standards.
3. Provide a drawing to show the layout of active transportation facilities and connections internal to the site and to the boundary roads.
4. Provide a revised Transportation Study to address all comments related to the Transportation Impact Study dated January 2024 prepared by LEA Consulting.

The Owner shall agree to:

1. Provide direct shared pedestrian/cycling facilities and connections from the proposed development to boundary roadways to support active transportation and public transit, where appropriate.
2. If not already arranged or completed by others, implement/construct Maplecrete Road between Highway 7 and White Elm Road to its ultimate configuration to accommodate the subject development.
3. Implement all recommendations, including TDM measures, as recommended in the revised Traffic Impact Study, to the satisfaction of the Region.

Should you have any questions or require further information regarding our comments, please contact Augustine Ko, Senior Planner, at 1-877-464-9675, ext. 71524, or by email at Augustine.Ko@york.ca.

Sincerely,

A handwritten signature in green ink, appearing to read 'Karen Whitney', is written over a light blue horizontal line.

Karen Whitney, MCIP, RPP
Director, Development Services

AK/

YORK-#16158471-v2-CIHA_23_003_YR_Revised_Conditions

ATTACHMENT 11

MARCH 5, 2024 PUBLIC MEETING
WRITTEN COMMUNICATION

COMMITTEE OF THE WHOLE (PUBLIC MEETING) – MARCH 5, 2024

COMMUNICATIONS

Distributed March 1, 2024

Item No.

- | | | |
|-----|---|---|
| C1. | Billy Tung, KLM Planning Partners Inc., Jardin Drive, Concord, dated March 1, 2024. | 1 |
|-----|---|---|

Disclaimer Respecting External Communications

Communications are posted on the City's website pursuant to Procedure By-law Number 7-2011. The City of Vaughan is not responsible for the validity or accuracy of any facts and/or opinions contained in external Communications listed on printed agendas and/or agendas posted on the City's website.

Please note there may be further Communications.



64 JARDIN DRIVE, UNIT 1B
CONCORD, ONTARIO L4K 3P3

T 905.669.4055

KLMPLANNING.COM

KLM File: P-3365

March 1, 2024

Todd Coles, City Clerk
Office of the City Clerk
Vaughan City Hall
2141 Major Mackenzie Drive
Vaughan, Ontario, L6A 1T1

C1.
Communication
CW(PM) – March 5, 2024
Item No. 1

Attention: Todd Coles, City Clerk and Mayor and Members of Vaughan Council

Re: Committee of the Whole (Public Meeting) – Written Submission
Public Meeting Date: March 5, 2024 at 7:00 PM
Agenda Item #3.1 and Name: 171 Maplecrete LP and 1930328 Ontario Inc.
City File #: CIHA.23.003

General Location: Northeast Corner of Maplecrete Road and Doughton Road, VMC
Property Description: 171 Maplecrete Road, 140 and 160 Doughton Road, and
Block 2 on Registered Plan 65M-4793

Dear City Clerk, and Mayor and Members of Council:

KLM Planning Partners Inc. (“**KLM**”) is the land use planning firm representing our client, GB (Maplecrete) Limited Partnership (“**GB**” or “**client**”), who is the owner of the lands located directly to the east of the subject lands of this Public Meeting Item.

Background – 130 Doughton Road (GB (Maplecrete) Limited Partnership)

On June 2, 2023, our client submitted Official Plan and Zoning By-law Amendment applications to the City of Vaughan to amend the current in-effect Vaughan Metropolitan Centre (VMC) Secondary Plan and Comprehensive Zoning By-law (CZBL) 001-2021 on their lands municipally known as 130 Doughton Road, to propose high density residential development. City Planning staff deemed the OPA and ZBA applications (Files OP.23.007 and Z.23.012) to be “complete” by way of a notice issued under the Planning Act on September 7, 2023.

In light of the policy changes envisioned under the forthcoming VMC Secondary Plan, our client has had several meetings with the Deputy City Manager of Planning, Director of Policy Planning and Special Projects, and the Senior Manager of VMC Program regarding accommodating possible changes to the site and building design. Without prejudice discussions have revolved around height, density, number of towers, and other matters, including the location and alignment of a potential north-south road that is envisioned in the new Secondary Plan but is not a requirement under the current in-effect Plan (discussions included consideration of a 50-50 road split with the westerly neighbour).

There have also been discussions with VMC Program staff regarding the means of obtaining development approvals in advance of the Secondary Plan Review outcome. That is, through the normal planning process through Vaughan Council with the client having rights of appeal under the Planning

Act, or alternatively, through the Community Infrastructure and Housing Accelerator (CIHA) process with the Minister of Municipal Affairs and Housing being the approval authority with no right of appeal for the client as identified in the Planning Act. All of the discussion matters held to date are under consideration by our client and nothing has been settled or confirmed with VMC Program staff; there is no revised development concept agreed to or formally submitted other than the original June 2, 2023 formal submission plan.

Our client's OPA and ZBA applications have yet to have a statutory Public Meeting to amend the in-effect VMC Secondary Plan and CZBL 001-2021. Also, more than 120 days have elapsed since the City issued its Notice of Complete Application's letter on September 7, 2023. Our client has the right to appeal their OPA and ZBA applications to the Ontario Land Tribunal (OLT) for Council's failure to render a decision on their applications within the timeframes stipulated under the Planning Act. However, our client has been working co-operatively and in good faith with VMC Program staff since last September with the intention of achieving a positive and mutual development outcome. This should include the City also working with our client's westerly neighbour in good faith to ensure fair development treatment amongst landowners, the resolution of planning issues, and a mutually beneficial planning outcome in the public interest.

Comments on Westerly Neighbour's CIHA Application (171 Maplecrete LP and 1930328 Ontario Inc.) ("the Liberty CIHA Application")

Although our client does not generally have any objections to the land use, heights and density being proposed, they do object to the following issues that impact the viability to develop and maximize the amount of development on their own site given the westerly neighbours site and building design, as follows:

1. The current in-effect VMC Secondary Plan does not contemplate a north-south road. VMC staff are seeking a north-south road through the VMC Secondary Plan Review and have made their intentions known to have a road sited somewhere between our client's lands and the westerly neighbour. The westerly neighbour's development, as proposed (see attached Composite Plan showing both the Liberty and client's proposals), would require the entire 20 metre road allowance to be placed on our client's lands, given the placement of their buildings extremely close to their easterly lot line and the possible easterly extension of an internal driveway that could intersect, whereas it is Vaughan policy that the provision of a new road where it is possible to do so be split 50/50 amongst adjacent landowners. The westerly neighbour (Liberty) and the City through the review process should be taking into consideration sharing the responsibility of providing the land and costs to construct this potential north-south road.
2. **Without any agreement with the City, the road should be split 50/50 with the westerly neighbour or determined by the City to be not required at all. These matters need to be addressed before the City advances Liberty's CIHA application.**
3. The westerly neighbour's building placements along their east property line proposes a 3-metre podium setback and a 5-metre tower setback, which is insufficient when there are three towers with heights of 24, 29 and 29-storeys and a 6-storey podium straddling the entire mutual property line. We are concerned their proposed development does not currently take into consideration our client's filed applications nor does it provide for the possibility to accommodate 50% of the potential north-south road being sought by Staff through the VMC Secondary Plan review.
4. In consideration of our client's applications, we question the likelihood that the westerly neighbour can meet the required 25-metre tower separation distance from our client's future towers given the closeness of their buildings along the mutual property line. Each developer must place their towers a minimum of 12.5 metres from their respective property lines to ensure

that there will be a minimum 25 metres total distance separation of towers between adjacent properties (as provided for by our client's applications). Liberty's proposed towers are setback 5 metres from their east property line which should be setback an additional 7.5 metres to achieve a setback of 12.5 metres. This minimum setback would also provide for the opportunity to accommodate the potential north-south road between the two proposed developments.

5. Our client's site plan shows a 2.94-metre road widening on the south side of their property to facilitate half (13 metres to centreline) of the total 26 metre Doughton Road right-of-way. It appears that the Liberty proposed development is only providing a 2.44 metre road widening, which is insufficient to create the full Doughton Road right-of-way. The VMC transportation staff will need to review this matter further to resolve the discrepancy.
6. Our client is extremely concerned that should the City disregard the comments contained in this letter and endorse the westerly neighbour's development proposal without any changes that our client will not have any right to appeal their neighbour's development under the Planning Act once the Minister of Municipal Affairs and Housing approves the CIHA, which will have extreme and unfair consequences on the development opportunities and viability for developing our client's lands as discussed in this letter as it relates to the location and provision of the north-south road, and the provision of appropriate setbacks and tower separation distances.

In light of the above concerns by our client, we ask that Vaughan Council direct VMC staff to take our client's comments into consideration and have Liberty re-work their development proposal in concert with our client. The current proposal by Liberty should not proceed as-is without revisions being made that will require them to dialogue and co-operate with our client in the interest of fair and quality development in the VMC. **The City has the obligation to consider the location and provision of the north-south road, and the provision of appropriate setbacks and tower separation distances prior to any further advancement and Council endorsement of this CIHA application so as to avoid prejudicial impacts on our client's active development applications and development opportunities.**

We ask that the City Clerk and VMC Program staff forward notices of all future statutory and non-statutory meetings related to file CIHA.23.003 to both KLM Planning Partners Inc. and GB (Maplecrete) Limited Partnership, c/o Melrose Investments Inc, in order that we can monitor and respond, accordingly.

If anyone has any questions, please feel free to reach out to me or my client, and we would be pleased to have a discussion to address the issues raised above.

Yours truly,

KLM PLANNING PARTNERS INC.



Billy Tung BES, MCIP, RPP
Partner

GU/

Attached: Composite Plan Showing Client and Liberty Proposals

Copy: Alireza Khosrowshahi, Melrose Investments Inc.
Kirill Blotskii, Melrose Investments Inc.
Olenka Karetnik, Melrose Investments Inc.
Grant Uyeyama, KLM Planning Partners Inc.

All Drawings, Specifications, and Related Documents are the Copyright of the Architect. The Architect retains all rights to control all uses of these documents for the intended issuance/use as identified below. Reproduction of these Documents, without permission from the Architect, is strictly prohibited. The Authorities Having Jurisdiction are permitted to use, distribute, and reproduce these drawings for the intended issuance as noted and dated below, however the extended permission to the Authorities Having Jurisdiction in no way devalues or limits the Copyright of the Architect, or control of use of these documents by the Architect.

Do not scale the drawings. This Drawing is Not To Be Used For Construction Until Signed By The Architect.

Date:

36 SHORT TERM BIKE PARKING SPACES, REFER TO LANDSCAPE DWG.
CONVEX MIRROR, REFER TO TRAFFIC CONSULTANTS' DWG.

KIRKOR

ARCHITECTS AND PLANNERS

20 De Boers Drive Suite 400
Toronto, ON M3J 0H1

Revisions:		Date:
No.:	Revision:	Date:

1	OPA, ZBA and Subdivision	May 19, 2023
No.:	Issued For:	Date:

Client:
GB (Doughton) Limited Partnership

130 Doughton Road
Proposed Residential Development

Composite Site Plan

Scale:
1 : 300

Drawn by:
MZ, TT

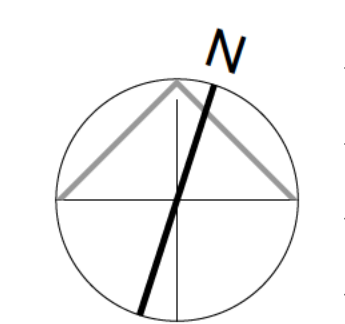
Checked by:
DS.

Project No.:

22-107

Date:
Feb. 26, 2024

Drawing No.:



dA 0



171 MAPLECRETE ROAD

130 DOUGHTON ROAD

DOUGHTON ROAD

WHITE ELM ROAD

MAPLECRETE ROAD

A204

A206

A206

A205

A205

A25.01

A25.01

A25.02

A25.03

A25.04

A25.05

A25.06

ATTACHMENT 12

Community Infrastructure and Housing Accelerator (CIHA) Order Conditions and Rationale City of Vaughan, File CIHA.23.003

171 Maplecrete Road, 140-160 Doughton Road, and Block 2 on Registered Plan 65M-4793

Item No. in Draft Order	Proposed Condition	Rationale
City of Vaughan: VMC Development Engineering Conditions		
1.	For City Council to adopt a resolution allocating sewage and water supply capacity in accordance with the City's approved Servicing Capacity Distribution Policy assigning capacity to the Subject Lands	There is limited available city-wide servicing capacity.
2.	The Owner shall provide an updated Functional Servicing Report to the satisfaction of the VMC Development Engineering.	To determine required improvements to the municipal servicing infrastructure and any mitigation measures to minimize impacts.
3.	The Owner shall provide an updated Transportation Impact Study where such report requires additional information and addresses all Transportation Engineering comments prior to the final approval of the application, to the satisfaction of the City.	It is anticipated that the Development may introduce an acceptable transportation impact on the surrounding road network, however, revisions and clarifications to the submitted study are required.
4.	The Owner shall enter into a Development Agreement with the City to design and construct all applicable external municipal infrastructure (i.e. road widening, boulevard improvements and servicing upgrades) required that are necessary to benefit the subject lands to the satisfaction of the City. The Development Agreement shall be registered against the subject lands to which it applies, and upon execution, the Owner shall satisfy conditions of the City, financial or otherwise, all to the satisfaction of the City	Mitigation measures identified through forthcoming studies and civil drawings may require further external infrastructure improvements to permit the contemplated Development.

<p>5.</p>	<p>The Owner shall provide an Updated Air Quality/ Land Use Compatibility Study which includes the entire subject lands to assess for any potential adverse effects on the proposed sensitive land use from the adjacent industrial facilities and transportation right of ways to the satisfaction of the Development Engineering Department</p>	<p>The Owner submitted a Noise Study to investigate the potential environmental noise impact on the Subject Lands from road traffic and surrounding land uses. Further clarification and details are to be finalized to inform on the specific noise mitigation measures that will be required for the Development.</p> <p>Submitted Letter from MTE Consultants Inc. acknowledges that they have been retained to update the previous Air Quality Assessment to include an adjacent property to the north, which was not part of the Subject Lands at the time of their original assessment. While this work is still in progress, MTE noted that it does not "anticipate identifying additional local air quality concerns" or that existing conditions "would have changed since 2023 impacting local air quality".</p>
<p>6.</p>	<p>The Owner's ESA reports have identified areas of contamination in excess of applicable Ministry of the Environment, Conservation and Parks (MECP) at the subject lands. As a result, the Owner is requested to submit a Remedial Action Plan (RAP) that addresses these exceedances, in accordance with the City's RAP checklist. The Owner is also requested to provide the City with a reliance letter for all environmental reports (Phase One ESAs, Phase Two ESAs and the RAP), in accordance with the City's reliance letter template.</p>	<p>ESA Reports submitted recommended remedial excavations to address pH levels and petroleum hydrocarbon (PHC) exceedance. A remedial action plan (RAP) that addresses the exceedances in accordance with the City's RAP checklist.</p> <p>The Owner is required to provide the City with a Reliance Letter for all ESA Reports</p>

7.	As a result of the required remediation of the site, as well as the proposed change to a more sensitive land use, the Owner shall submit a copy of a Ministry of the Environment, Conservation, and Parks (MECP) Record of Site Condition filed on the environmental site registry covering the subject lands or implementation and confirmation of completion of the RAP to the satisfaction of the City.	A copy of the MECP Record of Site Condition will confirm implementation and completion of the Remedial Action Plan to the City prior to issuance of an above-grade building permit.
City of Vaughan: VMC Urban Design Division Conditions		
8.	Prior to removing this CIHA Order condition, final Site Plan, Colour Rendered Elevations and Perspective drawings, Context Map, Landscape Master Plan, Urban Design and Sustainability Brief, Tree Inventory and Arborist Report, Tree Preservation Plan, Pedestrian Level Wind Study and Sun/Shadow Study will be approved to the satisfaction of the VMC Program	Documentation that would typically be required in order to establish the principal of development, is to be deferred to the Site Plan application stage in order to expedite the proposal through the CIHA tool.
The Regional Municipality of York ('York Region') Conditions		
9.	Prior to final Site Plan approval, implement the planned internal road/connections network as shown in the approved VMC Secondary Plan	This condition is required to implement improvements to the road network to service development in the area.
10.	Prior to final Site Plan approval, if not already arranged or completed by others, provide preliminary design and cost estimate to construct south approach to the Highway 7 and Maplecrete Road intersection as per Regional standards	This condition is required to implement improvements to the road network to service development in the area.
11.	Prior to final Site Plan approval, provide a drawing to show the layout of active transportation facilities and connections internal to the site and to the boundary roads	Coordinate to realize any requirements to provide an in-boulevard cycle track along Maplecrete Road and Doughton Road frontages

12.	The Owner shall agree to provide direct shared pedestrian/cycling facilities and connections from the proposed development to boundary roadways to support active transportation and public transit, where appropriate	Coordinate to realize any requirements to provide an in-boulevard cycle track along Maplecrete Road and Doughton Road frontages.
13.	Prior to final Site Plan approval, provide a revised Transportation Study to address all comments related to the Transportation Impact Study dated January 2024 prepared by LEA Consulting	Revisions and clarifications to the submitted Transportation Impact Study are required to ensure appropriate provisions and mitigation measures are in place to accommodate for the Development.
14.	The Owner shall agree to, if not already arranged or completed by others, implement/construct Maplecrete Road between Highway 7 and White Elm Road to its ultimate configuration to accommodate the subject development	This condition is required to implement improvements to the road network to service development in the area.
15.	The Owner shall agree to implement all recommendations, including TDM measures, as recommended in the revised Traffic Impact Study, to the satisfaction of the Region	This condition aids in implementing the findings of the Traffic Impact Study submitted to support the proposed Development.
Canadian National Railway (CN Rail) Conditions		
16.	CN Rail will review the submitted Noise Impact Study. Subject to the review of the Report, the CN Rail may consider other mitigation measures recommended by the Noise Consultant.	The Subject Lands are within proximity to the CN Rail yard. The Noise Study will provide for any mitigation measures to be included in the Development.

ATTACHMENT 13: DRAFT CIHA ORDER

ORDER

With respect to the City of Vaughan, Regional Municipality of York, CIHA Order XXX-XXX

Subsection 34.1(9) of the *Planning Act*, R.S.O. 1990, c. P.13, as amended

1. I hereby order pursuant to clause 34.1(9)(a) of the *Planning Act*, further to a request from the Council of the City of Vaughan set out in a resolution dated [DATE], that:
 - a. The zoning requirements attached as Appendices A and B apply to the lands in the City of Vaughan, Regional Municipality of York, set out in Appendix C.
 - b. Pursuant to subsection 34.1(12) of the *Planning Act*, the following do not apply to any Site Plan Application, Draft Plan of Subdivision Application, and Draft Plan of Condominium Application:
 - i. A policy statement issued under subsection 3(1) of the *Planning Act*,
 - ii. A Provincial Plan; and
 - iii. An Official Plan; and
 - c. Appendices A, B, C and D, attached to this Order, form part of this Order.
2. This Order comes into force on the day this order is made.

Made by:

PAUL CALANDRA
Minister of Municipal Affairs and Housing

Date made:

APPENDIX A

With respect to City of Vaughan, Regional Municipality of York Subsection 34.1(9) of the *Planning Act* and Zoning By-law 001-2021

Definitions

1. In this Order,

“Architectural Features” means features used to enhance the visual appearance of a building, which may include pilasters, brackets, cornices, columns, balustrades, and similar building features that are attached to the main walls of a building.

“CIHA Order Conditions” means the requirements listed in Appendix D to this Order, pursuant to subsection 34.1(13) of the *Planning Act* that shall be satisfied prior to the approval of any future Site Plan Application.

“Height” means in reference to a building or structure, the vertical distance measured from established grade to:

- i. In the case of a flat roof, including any roof where more than half of the roof area has a slope of 15 degrees or less above the horizontal, the highest point of the roof surface or parapet, whichever is the greater;
- ii. in the case of a sloped roof, the mean height between the eaves and the ridge; or,
- iii. In the case of any structure with no roof, the highest point of the structure

“Lands” or “Subject Lands” means the entirety of the parcel outlined in red on Appendix C.

“Order” means the order made by the Minister of Municipal Affairs and Housing pursuant to clause 34.1(9) (a) of the *Planning Act* with respect to the City of Vaughan, Regional Municipality of York.

“Parking Space” means a rectangular area measuring at least 2.7m by 5.7m, exclusive of any aisles or ingress or egress lanes, used for the temporary parking of motor vehicles, and shall include a private garage or carport and private driveway leading thereto.

“Phase 1” means a portion of the development consisting of:

- A minimum of 8,200 m² of Hotel uses, a minimum of 5,800 m² of Office uses, a minimum of 182 Purpose-Built Rental Dwelling Units, and up to an additional 800 Residential Dwelling Units.

“Phase 2” means the balance of the proposed development subsequent to Phase 1.

“Purpose-Built Rental” means housing built specifically for long-term rental accommodation.

“Zoning By-law” means Zoning By-Law No. 001-2021 of the City of Vaughan.

Application

2. This Order applies to lands in the City of Vaughan in the Regional Municipality of York, in the Province of Ontario, being the Lands outlined in red on a map attached to the order as Appendix C.

Zoning By-law

3. That Zoning By-law 001-2021 of the City of Vaughan, as amended from time to time and in its entirety, applies to the lands shown on the map set out in Appendix C of this order, and subject to the exceptions noted below.

Vaughan Metropolitan Centre Station Zone (V1)

4. Every use of land and every erection, location or use of any building or structure is prohibited on the Lands identified as “*Vaughan Metropolitan Centre Station Zone*” (V1) on the map referred to in Appendix C of this order except as noted below:

Permitted Uses:

- (a) Uses permitted under Table 10-2, Vaughan Metropolitan Centre Station Zone (V1) inclusive of all listed additional requirements of the Zoning By-law, save and except for Note 4.

Lot and Building Requirements:

- (b) For the purpose of determining zoning compliance for the lands, the front yard shall be White Elm Road.
- (c) The minimum front yard setback shall be 3.0 m.
- (d) The minimum rear yard setback shall be 3.0 m.
- (e) The minimum interior yard setback shall be 3.0 m.
- (f) The minimum exterior yard setback shall be 3.0 m.
- (g) The Build-to-zone requirements shall not apply.
- (h) The setback to all sight triangles shall be 1.5 m.
- (i) The minimum setback from a lot line to the nearest part of a building below finished grade shall be 0.0 m.
- (j) Architectural Features and elements may encroach 1.0 m into a required front, rear or exterior side yard.
- (k) The maximum residential tower floor plate for each building shall be 850 m².
- (l) The minimum combined amenity space requirement for indoor and outdoor amenity areas shall be 4.0 m² per unit.
- (m) Provisions 4.3.2 and 4.3.3 shall not apply
- (n) A minimum of 0.4 parking spaces per dwelling unit are required.
- (o) A minimum of 269 shared parking spaces for visitor and non-residential uses are required.
- (p) A minimum of 3 Type D, 2 Type B and 1 shared Type B and D loading spaces are required.
- (q) The maximum height of any building on the lands shall be the greater of:
 - i. 40-storeys; or

- ii. The height permitted by the in-effect Vaughan Metropolitan Centre Secondary Plan

Podium and Tower Requirements

- (r) The maximum podium height shall be 27.0 m or 6-storeys.
- (s) The minimum tower separation between all buildings shall be 25.0 m.
- (t) The minimum tower step-back shall be 2.0 m.
- (u) Minimum setback to the tower portion of any building on the Subject Lands to the east property line shall be 12.5m;
 - i. Notwithstanding the above, the minimum east side yard setback to the tower portion of any building may be reduced to 5 m, only if development applications are approved on the lands to the east and includes a 20 m road abutting the full length of the east property line of the Subject Lands, and that the application approvals shall be in full force and effect; or, the Owner enters into an Agreement with the adjacent landowner to the east agreeing to the reduced setback to the satisfaction of the City.

Landscape Requirements

- (v) The minimum landscape strip abutting any street line shall be 2.0 m, however, a minimum landscape strip of 1.5 m shall be provided to the sight triangle at the southwest corner of Maplecrete Road and Doughton Road.
- (w) Permitted encroachments into the landscape strip:
 - i. Bicycle Parking
 - ii. Hard Landscaping
 - iii. Entry/Access areas to buildings
 - iv. Architectural Features/elements

Special Provisions

- 5. In addition to the uses and requirement listed above, the following provisions shall apply:
 - (a) The maximum total Gross Floor Area (GFA) for all uses on the lands shall be the greater of:
 - i. 112,451 m²; or
 - ii. The density permitted by the in-effect Vaughan Metropolitan Centre Secondary Plan.
 - (b) Servicing allocation for the Subject Lands shall be capped at 1,565 dwelling units. Further allocation in excess of 1,565 dwelling units shall be subject to an updated Functional Servicing Report and Transportation Impact Study Addendum and/or Letter to demonstrate sufficient capacity exists for the additional dwelling units.
 - (c) The total Gross Floor Area of the development shall consist of a minimum of 11.5% non-residential uses.
 - (d) The minimum number of purpose-built rental dwelling units shall be 11.5% of the total dwelling units.
 - (e) The minimum Phase 1 development shall consist of the following:
 - i. 8,200 m² of Hotel uses;
 - ii. 5,800 m² of Office uses; and

- iii. 182 purpose-built rental dwelling units.
- (f) A long-term bicycle parking space shall be located wholly within the building where the principal use is located and for which the bicycle parking space is required.
- (g) A long-term bicycle parking space required for a dwelling unit shall be required to be located within the following areas of a building:
 - Within the ground floor area; or
 - On the 2nd story or mezzanine within the ground floor area; or
 - On the first or second level located below grade; or
 - Bicycle parking may be provided in one level increments below grade commencing with the third level below grade and moving down, provided that a combined minimum of 50% of the parking area for the first and second parking levels below grade are dedicated to long-term bicycle parking spaces
- (h) The Subject Lands shall be deemed to be one lot, regardless of the number of buildings constructed thereon, the creation of separate units and/or lots by way of condominium, consent, conveyance of private or public roads, strata title agreements, or other permissions, and any easements or registrations that are granted.
- (i) The Owner agrees the Site Development Agreement for **Phase 2** will not be executed by the City of Vaughan until all the required building permits for **Phase 1** have been issued by the City.

Terms of Use

- 6.** (1) Every use of land and every erection, location or use of any building or structure shall be in accordance with this Order.
- (2) Nothing in this Order prevents the use of any land, building or structure for any use prohibited by this Order if the land, building, or structure is lawfully so use on the day this Order comes into force.
- (3) Nothing in this Order prevents the reconstruction of any building or structure that is damaged or destroyed by causes beyond the control of the owner if the dimensions of the original building or structure are not increased and its original use is not altered.
- (4) Nothing in this Order prevents the strengthening or restoration to a safe condition of any building or structure.

APPENDIX B

With respect to City of Vaughan, Regional Municipality of York Subsection 34.1(9) of the *Planning Act* and Zoning By-law 1-88

Definitions

1. In this Order,

“Architectural Features” means features used to enhance the visual appearance of a building, which may include pilasters, brackets, cornices, columns, balustrades, and similar building features that are attached to the main walls of a building.

“CIHA Order Conditions” means the requirements listed in Appendix D to this Order, pursuant to subsection 34.1(13) of the *Planning Act* that shall be satisfied prior to the approval of any future Site Plan Application.

“Gross Floor Area” means the aggregate of the floor areas of all storeys of a building measured from the outside of the exterior walls, but excluding any basement, attic, mechanical room, electrical room, elevator shaft, refuse chute, escalators, vehicle and bicycle parking areas, loading areas located above or below grade.

“Height” means in reference to a building or structure, the vertical distance measured from established grade to:

- iv. In the case of a flat roof, including any roof where more than half of the roof area has a slope of 15 degrees or less above the horizontal, the highest point of the roof surface or parapet, whichever is the greater;
- v. in the case of a sloped roof, the mean height between the eaves and the ridge; or,
- vi. In the case of any structure with no roof, the highest point of the structure

“Lands” or “Subject Lands” means the entirety of the parcel outlined in red on Appendix C.

“Order” means the order made by the Minister of Municipal Affairs and Housing pursuant to clause 34.1(9) (a) of the *Planning Act* with respect to the City of Vaughan, Regional Municipality of York.

“Parking Space” means a rectangular area measuring at least 2.7m by 5.7m, exclusive of any aisles or ingress or egress lanes, used for the temporary parking of motor vehicles, and shall include a private garage or carport and private driveway leading thereto.

“Phase 1” means a portion of the development consisting of:

- A minimum of 8,200 m² of Hotel uses, a minimum of 5,800 m² of Office uses, a minimum of 182 Purpose-Built Rental Dwelling Units, and up to an additional 800 Residential Dwelling Units.

“Phase 2” means the balance of the proposed development subsequent to Phase 1.

“Purpose-Built Rental” means housing built specifically for long-term rental accommodation.

“Zoning By-law” means Zoning By-Law No. 001-2021 of the City of Vaughan.

Application

2. This Order applies to lands in the City of Vaughan in the Regional Municipality of York, in the Province of Ontario, being the Lands outlined in red on a map attached to the order as Appendix C.

Zoning By-law

3. That Zoning By-law 1-88 of the City of Vaughan, as amended from time to time and in its entirety, applies to the lands shown on the map set out in Appendix C of this order, and subject to the exceptions noted below.

Corporate Centre Zone (C9)

4. Every use of land and every erection, location or use of any building or structure is prohibited on the Lands identified as “Corporate Centre Zone” (C9) on the map referred to in Appendix C of this order except as noted below:

Permitted Uses:

- (a) All the permitted uses under Section 5.10 for the C9 Corporate Centre Zone.

Lot and Building Requirements:

- (b) For the purpose of determining zoning compliance for the lands, the front yard shall be White Elm Road.
- (c) The minimum front yard setback shall be 3.0 m.
- (d) The minimum rear yard setback shall be 3.0 m.
- (e) The minimum interior yard setback shall be 3.0 m.
- (f) The minimum exterior yard setback shall be 3.0 m.
- (g) The Build-to-zone requirements shall not apply.
- (h) The minimum setback to all sight triangles shall be 1.5 m.
- (i) The minimum setback from a lot line to the nearest part of a building below finished grade shall be 0.0 m.
- (j) Architectural features and elements may encroach 1.0 m into a required front, rear or exterior side yard.
- (k) The maximum residential tower floor plate for each building shall be 850 m².
- (l) The minimum combined amenity space requirement for indoor and outdoor amenity areas shall be 4.0 m² per unit.
- (m) The maximum height of any building on the lands shall be the greater of:
 - i. 40-storeys; or
 - ii. The height permitted by the in-effect Vaughan Metropolitan Centre Secondary Plan

Podium and Tower Requirements

- (n) The maximum podium height shall be 27.0 m or 6-storeys.

- (o) The minimum tower separation between all buildings shall be 25.0 m.
- (p) Minimum setback to the tower portion of any building on the Subject Lands to the east property line shall be 12.5m;
 - i. Notwithstanding the above, the minimum east side yard setback to the tower portion of any building may be reduced to 5 m, only if development applications are approved on the lands to the east and includes a 20 m road abutting the full length of the east property line of the Subject Lands, and that the application approvals shall be in full force and effect; or, the Owner enters into an Agreement with the adjacent landowner to the east agreeing to the reduced setback to the satisfaction of the City.

Parking Requirements

- (q) A minimum of 0.4 parking spaces per dwelling unit are required.
- (r) A minimum of 0.15 visitor parking spaces per dwelling unit are required.
- (s) A minimum of 269 shared parking spaces for visitor and non-residential uses are required.
- (t) A minimum of 3 Type D, 2 Type B and shared Type B and D loading spaces loading spaces are required.
- (u) A minimum of 0.5 spaces per 100 m² GFA of Office uses is required.
- (v) A minimum of 0.25 spaces per guest room is required.
- (w) A minimum of 0.7 spaces per 100 m² GFA of Retail uses is required.

Landscape Requirements

- (x) Notwithstanding Sections 3.13 and 5.1.1, only a minimum landscape strip of 3.0 m shall be provided along a lot line which abuts a street line and shall be used for no other purpose than landscaping. 1.5 m shall be permitted abutting the site triangle at the southwest corner of Maplecrete Road and Doughton Road.
- (y) Permitted encroachments into the landscape strip:
 - i. Bicycle Parking
 - ii. Hard Landscaping
 - iii. Entry/Access areas to buildings
 - iv. Architectural Features/elements

Special Provisions

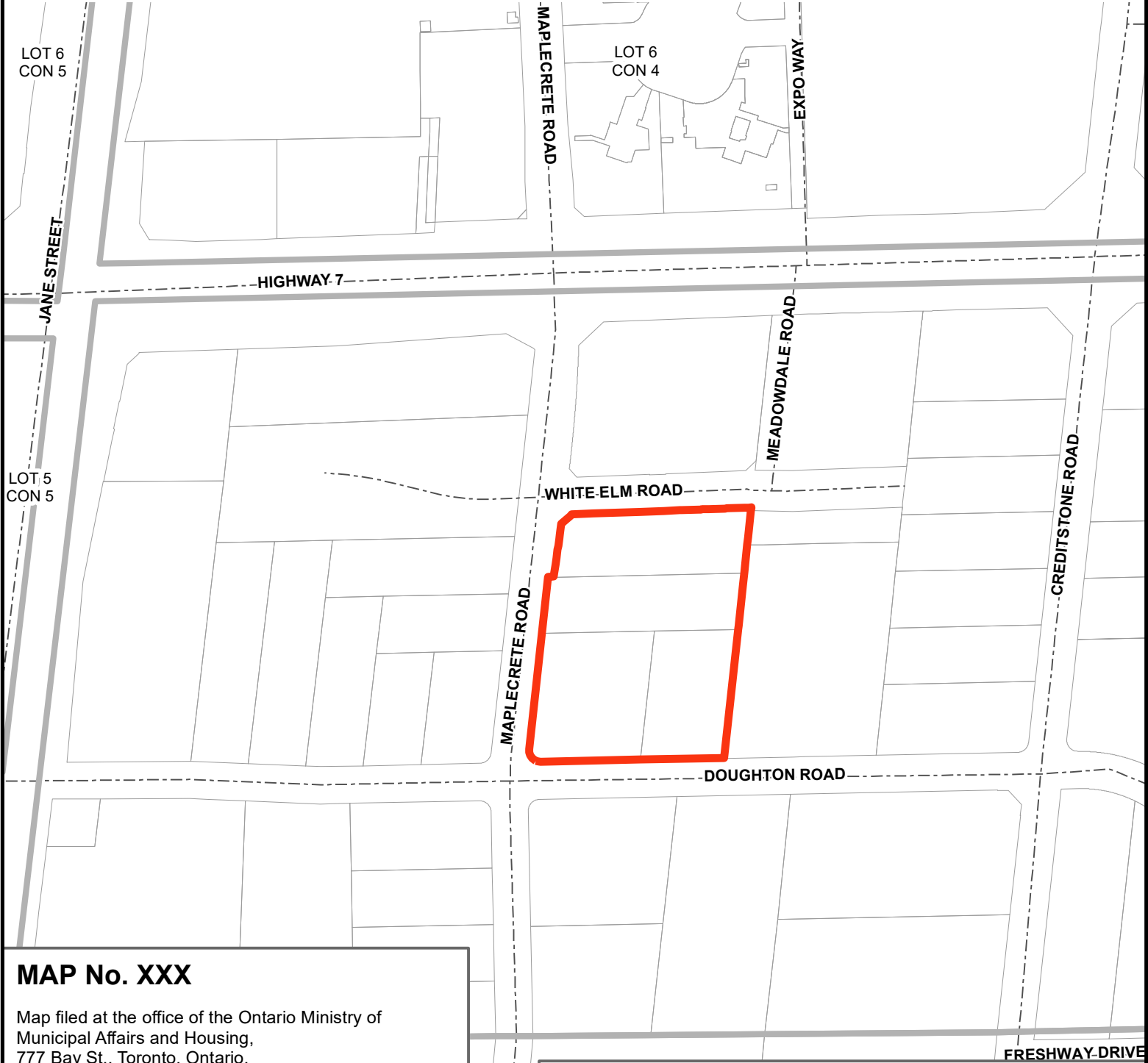
- 5. In addition to the uses and requirement listed above, the following provisions shall apply:
 - (j) The maximum total Gross Floor Area (GFA) for all uses on the lands shall be the greater of:
 - iii. 112,451 m²; or
 - iv. The density permitted by the in-effect Vaughan Metropolitan Centre Secondary Plan.
 - (k) Servicing allocation for the Subject Lands shall be capped at 1,565 dwelling units. Further allocation in excess of 1,565 dwelling units shall be subject to an updated Functional Servicing Report and Transportation Impact Study Addendum and/or Letter to demonstrate sufficient capacity exists for the additional dwelling units.

- (l) The total Gross Floor Area of the development shall consist of a minimum of 11.5% non-residential uses.
- (m) The minimum number of purpose-built rental dwelling units shall be 11.5% of the total dwelling units.
- (n) The minimum Phase 1 development shall consist of the following:
 - i. 8,200 m² of Hotel uses;
 - ii. 5,800 m² of Office uses; and
 - iii. 182 purpose-built rental dwelling units.
- (o) A long-term bicycle parking space shall be located wholly within the building where the principal use is located and for which the bicycle parking space is required.
- (p) A long-term bicycle parking space required for a dwelling unit shall be required to be located within the following areas of a building:
 - Within the ground floor area; or
 - On the 2nd story or mezzanine within the ground floor area; or
 - On the first or second level located below grade; or
 - Bicycle parking may be provided in one level increments below grade commencing with the third level below grade and moving down, provided that a combined minimum of 50% of the parking area for the first and second parking levels below grade are dedicated to long-term bicycle parking spaces
- (q) The Subject Lands shall be deemed to be one lot, regardless of the number of buildings constructed thereon, the creation of separate units and/or lots by way of condominium, consent, conveyance of private or public roads, strata title agreements, or other permissions, and any easements or registrations that are granted.
- (r) The Owner agrees the Site Development Agreement for **Phase 2** will not be executed by the City of Vaughan until all the required building permits for **Phase 1** have been issued by the City.
- (s) Notwithstanding the provisions of this Order, as it applies to By-law 1-88, in the event of a conflict, the requirements of By-law 001-2021 and CIHA Order XXX-XXX shall prevail.

Terms of Use

- 6. (1) Every use of land and every erection, location or use of any building or structure shall be in accordance with this Order.
- (2) Nothing in this Order prevents the use of any land, building or structure for any use prohibited by this Order if the land, building, or structure is lawfully so use on the day this Order comes into force.
- (3) Nothing in this Order prevents the reconstruction of any building or structure that is damaged or destroyed by causes beyond the control of the owner if the dimensions of the original building or structure are not increased and its original use is not altered.
- (4) Nothing in this Order prevents the strengthening or restoration to a safe condition of any building or structure.

Lots 4, 5, and 6, Plan 7797 and
Block 2, 65M-4793
Part of Lot 5, Concession 4,
City of Vaughan, Regional Municipality of York



MAP No. XXX

Map filed at the office of the Ontario Ministry of
Municipal Affairs and Housing,
777 Bay St., Toronto, Ontario,





Planning Act

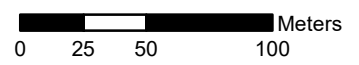
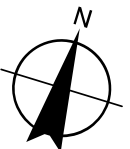
Ontario Regulation: XXX/24

Date:

Original Signed By: **Minister of Municipal Affairs
and Housing**

Legend

-  Lands Subject to CIHA Order
-  Roads
-  Parcel Boundary
-  Lot & Concession



APPENDIX D

CIHA Order Conditions

The following materials, items, and tasks shall be submitted/completed to the satisfaction of the Minister of Municipal Affairs and Housing and/or the City of Vaughan ('the City'):

City of Vaughan: VMC Development Engineering Conditions

1. For City Council to adopt a resolution allocating sewage and water supply capacity in accordance with the City's approved Servicing Capacity Distribution Policy assigning capacity to the subject lands.
2. The Owner shall provide an updated Functional Servicing Report to the satisfaction of the VMC Development Engineering.
3. The Owner shall provide an updated Transportation Impact Study where such report requires additional information and addresses all Transportation Engineering comments prior to the final approval of the application, to the satisfaction of the City.
4. The Owner shall enter into a Development Agreement with the City to design and construct all applicable external municipal infrastructure (such as road widening, boulevard improvements and servicing upgrade) required that are necessary to benefit the subject lands to the satisfaction of the City. The Development Agreement shall be registered against the subject lands to which it applies, and upon execution, the Owner shall satisfy conditions of the City, financial or otherwise, all to the satisfaction of the City.
5. The Owner shall provide an Updated Air Quality/ Land Use Compatibility Study which includes the entire subject lands to assess for any potential adverse effects on the proposed sensitive land use from the adjacent industrial facilities and transportation right of ways to the satisfaction of the Development Engineering Department.
6. The Owner's ESA reports have identified areas of contamination in excess of applicable Ministry of the Environment, Conservation and Parks (MECP) at the subject lands. As a result, the Owner is requested to submit a Remedial Action Plan (RAP) that addresses these exceedances, in accordance with the City's RAP checklist. The Owner is also requested to provide the City with a reliance letter for all environmental reports (Phase One ESAs, Phase Two ESAs and the RAP), in accordance with the City's reliance letter template.
7. As a result of the required remediation of the site, as well as the proposed change to a more sensitive land use, the Owner shall submit a copy of a Ministry of the Environment, Conservation, and Parks (MECP) Record of Site Condition filed on the environmental site registry covering the subject lands or implementation and confirmation of completion of the RAP to the satisfaction of the City.

City of Vaughan: VMC Urban Design Conditions

8. Prior to removing this CIHA Order condition, final Site Plan, Colour Rendered Elevations and Perspective drawings, Context Map, Landscape Master Plan, Urban Design and Sustainability Brief, Tree Inventory and Arborist Report, Tree Preservation Plan, Pedestrian Level Wind Study and Sun/Shadow Study will be approved to the satisfaction of the VMC Program

The following materials, items, and tasks shall be submitted/completed to the satisfaction of the Minister of Municipal Affairs and Housing and/or the Regional Municipality of York ('York Region').

York Region Conditions

9. Prior to final Site Plan approval, implement the planned internal road/connections network as shown in the approved VMC Secondary Plan
10. Prior to final Site Plan approval, if not already arranged or completed by others, provide preliminary design and cost estimate to construct south approach to the Highway 7 and Maplecrete Road intersection as per Regional standards
11. Prior to final Site Plan approval, provide a drawing to show the layout of active transportation facilities and connections internal to the site and to the boundary roads
12. The Owner shall agree to provide direct shared pedestrian/cycling facilities and connections from the proposed development to boundary roadways to support active transportation and public transit, where appropriate
13. Prior to final Site Plan approval, provide a revised Transportation Study to address all comments related to the Transportation Impact Study dated January 2024 prepared by LEA Consulting
14. The Owner shall agree to, if not already arranged or completed by others, implement/construct Maplecrete Road between Highway 7 and White Elm Road to its ultimate configuration to accommodate the subject development
15. The Owner shall agree to implement all recommendations, including TDM measures, as recommended in the revised Traffic Impact Study, to the satisfaction of the Region

Canadian National Railway (CN Rail) Conditions

16. CN Rail will review the submitted Noise Impact Study. Subject to the review of the Report, the CN Rail may consider other mitigation measures recommended by the Noise Consultant

ATTACHMENT 14

INDIGENOUS ENGAGEMENT
SUMMARY

**RESPONSE
LETTERS**

From: [Mark LaForme](#)
To: [Francis Kwashie](#)
Cc: [Abby LaForme](#)
Subject: RE: 171 Maplecrete Rd - Indigenous Engagement
Date: January-08-24 2:38:56 PM
Attachments: [image004.png](#)
[image005.png](#)

Good afternoon and Happy New Year to you, Francis!

My apologies for the delay in responding to your previous emails. While we have no immediate concerns/comments on the proposed development, we do require full engagement for all archaeological work associated with this proposed development.

Full engagement means that the proponent will enter into an agreement/contract with MCFN that provides for MCFN community members to be on site for all archaeological work. These community members are employed by the Department of Consultation and Accommodation as Field Liaison Representatives (FLR's) to represent the MCFN during field work.

If you have any questions going forward, please reach out to Abby LaForme who is cc'd on this email.

Thank you.

Regards,

Mark LaForme (he/him)
Director
MCFN-DOCA
4065 Hwy. 6
Hagersville, ON N0A 1H0
Phone: 905-768-4260

<http://mncfn.ca/doca>

Google Maps: <https://www.google.ca/maps/place/MNCFN-DOCA/@42.9718566,-80.0429177,15z/data=!4m5!3m4!1s0x0:0xd52b4642633e9aa2!8m2!3d42.9718566!4d-80.0429177>

This email and any files transmitted with it are confidential and intended solely for the use of the individual or entity to whom they are addressed. If you are not the intended recipient you are notified that disclosing, copying, distributing or taking any action in reliance on the contents of this information is strictly prohibited.

From: Francis Kwashie <fkwashie@mhbcplan.com>
Sent: Tuesday, January 2, 2024 10:50 AM
To: Mark LaForme <Mark.LaForme@mncfn.ca>
Subject: RE: 171 Maplecrete Rd - Indigenous Engagement

Good morning and Happy New Year, Mr. LaForme.

I am following up on my previous correspondence and I would like to confirm that you have no concerns/comments on the proposed development.

Thanks,

FRANCIS K. KWASHIE, B.A (Hons), MSc, MES, MCIP, RPP | Associate

MHBC Planning, Urban Design & Landscape Architecture

7050 Weston Road, Suite 230 | Woodbridge | ON | L4L 8G7 | T 905 761 5588 x 249 | C 416 819 5021 | F 905 761 5589 | fkwashie@mhbcplan.com

Follow us: [Webpage](#) | [Linkedin](#) | [Facebook](#) | [Twitter](#) | [Vimeo](#)



This communication is intended solely for the named addressee(s) and may contain information that is privileged, confidential, protected or otherwise exempt from disclosure. No waiver of confidence, privilege, protection or otherwise is made. If you are not the intended recipient of this communication, please advise us immediately and delete this email without reading, copying or forwarding it to anyone.

From: Francis Kwashie

Sent: Tuesday, November 21, 2023 3:45 PM

To: mark.laforme@mncfn.ca

Subject: 171 Maplecrete Rd - Indigenous Engagement

Good afternoon Mr. LaForme:

I hope this email finds you well.

On behalf of our client, 171 Maplecrete LP, we are reaching out to you regarding a proposed Community Infrastructure and Housing Accelerator ("CIHA") application for the property at 171 Maplecrete Road, 140 & 160 Doughton Road and Block 2 (65M-4793) in the City of Vaughan. The proposal would redevelop the site with 5 mixed-use towers with heights between 24 and 30 storeys. Additional details regarding the proposed development are attached to this email.

As part of the application, the Ministry of Municipal Affairs and Housing ("MMAH") requires engagement with Indigenous communities. To this end, we would welcome any comments you may have by December 20, 2023.

Feel free to reach out to me should you have any questions.

Sincerely,

FRANCIS K. KWASHIE, B.A (Hons), MSc, MES, MCIP, RPP | Associate

MHBC Planning, Urban Design & Landscape Architecture

7050 Weston Road, Suite 230 | Woodbridge | ON | L4L 8G7 | T 905 761 5588 x 249 | C 416 819 5021 | F

905 761 5589 | fkwashie@mhbcplan.com

Follow us: [Webpage](#) | [Linkedin](#) | [Facebook](#) | [Twitter](#) | [Vimeo](#)



This communication is intended solely for the named addressee(s) and may contain information that is privileged, confidential, protected or otherwise exempt from disclosure. No waiver of confidence, privilege, protection or otherwise is made. If you are not the intended recipient of this communication, please advise us immediately and delete this email without reading, copying or forwarding it to anyone.

From: [Dominic Ste-Marie](#)
To: [Francis Kwashie](#)
Cc: [Lori-Jeanne Bolduc](#); [Mario Gros Louis](#)
Subject: RE: 171 Maplecrete Rd - Indigenous Engagement
Date: December-22-23 11:19:33 AM
Attachments: [image001.png](#)
[image004.png](#)
[image005.png](#)

Kwe Francis,

Thank you for this precision, please make sure that we are also notified of any archeological resources found because this area of southern Ontario is filled with Wendat archeological sites and even if the site seems to not be Wendat it remains useful for us to know.

Tiawenhk chia' önenh

Dominic Ste-Marie

NATION HURONNE-WENDAT

Bureau du Nionwentsïo

Dominic Ste-Marie

Conseiller en Gestion du Territoire

255, place Chef Michel Laveau
Wendake (QC) G0A 4V0
T : 418 843-3767
@ : dominic.ste-marie@wendake.ca
WENDAKE.CA

De : Francis Kwashie <fkwashie@mhbcpplan.com>

Envoyé : 8 décembre 2023 14:57

À : Dominic Ste-Marie <Dominic.Sainte-Marie@wendake.ca>

Cc : Lori-Jeanne Bolduc <Lori-Jeanne.Bolduc@wendake.ca>; Mario Gros Louis <Mario.GrosLouis@wendake.ca>

Objet : RE: 171 Maplecrete Rd - Indigenous Engagement

Some people who received this message don't often get email from fkwashie@mhbcpplan.com. [Learn why this is important](#)

Good afternoon Mr. Ste-Marie:

As a follow-up, the City or Region did not request an archaeological study.

The City typically includes the conditions below as part of the Site Plan Approval process which the applicant will abide by.

1. Should archaeological resources be found on the property during construction activities, all work must cease, and the Ontario Ministry of Tourism, Culture and Sport and the City of Vaughan's Development Planning Department, Urban Design and Cultural Heritage Division shall be notified immediately.
2. In the event that human remains are encountered during construction activities, the

Owner must immediately cease all construction activities. The Owner shall contact the York Regional Police Department, the Regional Coroner and the Registrar of the Cemeteries Regulation Unit of the Ministry of Government and Consumer Services.

3. That, where burial sites are encountered during any excavation or other action, the provisions of the Funeral, Burial and Cremation Services Act, and its regulations shall apply:
 1. Where First Nations burials are discovered, consultation shall occur in accordance with the "York Region First Nations Consultation Protocol", with the nearest First Nation and the First Nation with the closest cultural affiliation, if that can be determined; and
 2. Shall ensure adequate archaeological assessments and consultation with appropriate agencies, including the Ministry of Citizenship and Multiculturalism, when an identified and marked or unmarked cemetery is affected by land use development. The provisions under the Ontario Heritage Act and Funeral, Burial and Cremation Services Act shall also apply.

I trust the above is satisfactory.

Thanks,

FRANCIS K. KWASHIE, B.A (Hons), MSc, MES, MCIP, RPP | Associate

MHBC Planning, Urban Design & Landscape Architecture

7050 Weston Road, Suite 230 | Woodbridge | ON | L4L 8G7 | T 905 761 5588 x 249 | C 416 819 5021 | F 905 761 5589 | fkwashie@mhbcplan.com

Follow us: [Webpage](#) | [Linkedin](#) | [Facebook](#) | [Twitter](#) | [Vimeo](#)



This communication is intended solely for the named addressee(s) and may contain information that is privileged, confidential, protected or otherwise exempt from disclosure. No waiver of confidence, privilege, protection or otherwise is made. If you are not the intended recipient of this communication, please advise us immediately and delete this email without reading, copying or forwarding it to anyone.

From: Dominic Ste-Marie <Dominic.Sainte-Marie@wendake.ca>

Sent: Tuesday, November 28, 2023 11:03 AM

To: Francis Kwashie <fkwashie@mhbcplan.com>

Cc: Lori-Jeanne Bolduc <Lori-Jeanne.Bolduc@wendake.ca>; Mario Gros Louis <Mario.GrosLouis@wendake.ca>

Subject: RE: 171 Maplecrete Rd - Indigenous Engagement

Kwe Francis,

Thank you for your email. Could you please let us know if any archaeological studies or fieldwork will

be necessary as part of this project?

Please note that we have updated our way of processing consultations. Any new consultation or project notice must be sent to the following email address: consultations@wendake.ca. We also kindly ask that you remove any other email addresses that you have on file from all your mailing lists.

Tiawenhk chia' önenh

Dominic Ste-Marie

NATION HURONNE-WENDAT

Bureau du Nionwentsiö

Dominic Ste-Marie

Conseiller en Gestion du Territoire

255, place Chef Michel Laveau

Wendake (QC) G0A 4V0

T : 418 843-3767

@ : dominic.ste-marie@wendake.ca

WENDAKE.CA

De : Francis Kwashie <fkwashie@mhbcpplan.com>

Envoyé : 21 novembre 2023 15:53

À : Dominic Ste-Marie <Dominic.Sainte-Marie@wendake.ca>

Objet : 171 Maplecrete Rd - Indigenous Engagement

Vous ne recevez pas souvent de courriers de la part de fkwashie@mhbcpplan.com. [Découvrez pourquoi cela est important](#)

Good afternoon Mr. Ste-Marie:

I hope this email finds you well.

On behalf of our client, 171 Maplecrete LP, we are reaching out to you regarding a proposed Community Infrastructure and Housing Accelerator ("CIHA") application for the property at 171 Maplecrete Road, 140 & 160 Doughton Road and Block 2 (65M-4793) in the City of Vaughan. The proposal would redevelop the site with 5 mixed-use towers with heights between 24 and 30 storeys. Additional details regarding the proposed development are attached to this email.

As part of the application, the Ministry of Municipal Affairs and Housing ("MMAH") requires engagement with Indigenous communities. To this end, we would welcome any comments you may have by December 20, 2023.

Feel free to reach out to me should you have any questions.

Sincerely,

FRANCIS K. KWASHIE, B.A (Hons), MSc, MES, MCIP, RPP | Associate

MHBC Planning, Urban Design & Landscape Architecture

7050 Weston Road, Suite 230 | Woodbridge | ON | L4L 8G7 | T 905 761 5588 x 249 | C 416 819 5021 | F 905 761 5589 | fkwashie@mhbcpplan.com

Follow us: [Webpage](#) | [Linkedin](#) | [Facebook](#) | [Twitter](#) | [Vimeo](#)



This communication is intended solely for the named addressee(s) and may contain information that is privileged, confidential, protected or otherwise exempt from disclosure. No waiver of confidence, privilege, protection or otherwise is made. If you are not the intended recipient of this communication, please advise us immediately and delete this email without reading, copying or forwarding it to anyone.

**INITIAL ENGAGEMENT
CORRESPONDENCE**



PLANNING
URBAN DESIGN
& LANDSCAPE
ARCHITECTURE

November 21, 2023

Chief Donna Big Canoe
Chippewas of Georgina Island

Sent by E-mail: donna.bigcanoe@georginaisland.com

Dear Chief Big Canoe:

**RE: CONSULTATION REQUEST
COMMUNITY INFRASTRUCTURE & HOUSING ACCELERATOR
171 MAPLECRETE ROAD, 140 & 160 DOUGHTON ROAD, AND BLOCK 2 (65M-4793)
VAUGHAN, ONTARIO
OUR FILE: 1512'W'**

On behalf of our client, 171 Maplecrete LP, we are contacting you regarding a proposed Community Infrastructure and Housing Accelerator ("CIHA") application for the property municipally known as 171 Maplecrete Road, 140 & 160 Doughton Road and Block 2 (65M-4793) in the City of Vaughan (the "Subject Lands").

On March 21, 2023, City Council adopted Guiding Principles to assist in identifying appropriate development proposals for CIHA requests. In light of this, we intend to submit a CIHA application as follows:

- Redevelopment is proposed on the Subject Lands which contain a site area of 14,065 square metres (151,401 square feet). The Subject Lands are generally located at the northeast corner of Maplecrete Road and Doughton Road in the City of Vaughan.
- The Application proposes 5 mixed-use towers comprising heights between 24 and 30 storeys.
- The Application proposes approximately 1,567 residential units, of which 182 are purpose-built rental housing units that are consistent with a Priority Housing Type outlined in the Guiding Principles. The remaining 1,385 units will be residential condominiums.
- The proposal also contains a mixed-use building containing approximately 5,711 square metres (61,476 square feet) of Office Development and 120 Hotel Suites.

As part of the application, the Ministry of Municipal Affairs and Housing ("MMAH") requires engagement with Indigenous communities. As such, please provide, within a reasonable amount of time, any concerns related to Aboriginal and treaty rights that may be impacted by this proposal. As the Subject Lands have previously been developed for employment purposes, our client will contact

the appropriate authorities and Indigenous communities should any archaeological findings be discovered during the construction of the development.

Please let us know if you have any questions, require any additional information or if you would like to meet to discuss the proposed CIHA application.

Yours truly,

MHBC



David A. McKay, MSc, MLAI, MCIP, RPP
Vice President & Partner

cc. Client, City



PLANNING
URBAN DESIGN
& LANDSCAPE
ARCHITECTURE

November 21, 2023

Chief Keith Knott
Curve Lake First Nation

Sent by E-mail: keithK@CurveLake.ca

Dear Chief Knott:

**RE: CONSULTATION REQUEST
COMMUNITY INFRASTRUCTURE & HOUSING ACCELERATOR
171 MAPLECRETE ROAD, 140 & 160 DOUGHTON ROAD, AND BLOCK 2 (65M-4793)
VAUGHAN, ONTARIO
OUR FILE: 1512'W'**

On behalf of our client, 171 Maplecrete LP, we are contacting you regarding a proposed Community Infrastructure and Housing Accelerator ("CIHA") application for the property municipally known as 171 Maplecrete Road, 140 & 160 Doughton Road and Block 2 (65M-4793) in the City of Vaughan (the "Subject Lands").

On March 21, 2023, City Council adopted Guiding Principles to assist in identifying appropriate development proposals for CIHA requests. In light of this, we intend to submit a CIHA application as follows:

- Redevelopment is proposed on the Subject Lands which contain a site area of 14,065 square metres (151,401 square feet). The Subject Lands are generally located at the northeast corner of Maplecrete Road and Doughton Road in the City of Vaughan.
- The Application proposes 5 mixed-use towers comprising heights between 24 and 30 storeys.
- The Application proposes approximately 1,567 residential units, of which 182 are purpose-built rental housing units that are consistent with a Priority Housing Type outlined in the Guiding Principles. The remaining 1,385 units will be residential condominiums.
- The proposal also contains a mixed-use building containing approximately 5,711 square metres (61,476 square feet) of Office Development and 120 Hotel Suites.

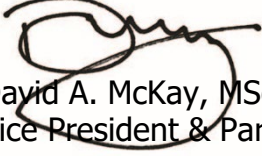
As part of the application, the Ministry of Municipal Affairs and Housing ("MMAH") requires engagement with Indigenous communities. As such, please provide, within a reasonable amount of time, any concerns related to Aboriginal and treaty rights that may be impacted by this proposal. As the Subject Lands have previously been developed for employment purposes, our client will contact

the appropriate authorities and Indigenous communities should any archaeological findings be discovered during the construction of the development.

Please let us know if you have any questions, require any additional information or if you would like to meet to discuss the proposed CIHA application.

Yours truly,

MHBC

A handwritten signature in black ink, appearing to read 'David McKay', written over a circular stamp or seal.

David A. McKay, MSc, MLAI, MCIP, RPP
Vice President & Partner

cc. Client, City



PLANNING
URBAN DESIGN
& LANDSCAPE
ARCHITECTURE

November 21, 2023

Mr. Dominic Ste-Marie
Huron-Wendat Nation

Sent by E-mail: dominic.ste-marie@wendake.ca

Dear Mr. Ste-Marie:

**RE: CONSULTATION REQUEST
COMMUNITY INFRASTRUCTURE & HOUSING ACCELERATOR
171 MAPLECRETE ROAD, 140 & 160 DOUGHTON ROAD, AND BLOCK 2 (65M-4793)
VAUGHAN, ONTARIO
OUR FILE: 1512'W'**

On behalf of our client, 171 Maplecrete LP, we are contacting you regarding a proposed Community Infrastructure and Housing Accelerator ("CIHA") application for the property municipally known as 171 Maplecrete Road, 140 & 160 Doughton Road and Block 2 (65M-4793) in the City of Vaughan (the "Subject Lands").

On March 21, 2023, City Council adopted Guiding Principles to assist in identifying appropriate development proposals for CIHA requests. In light of this, we intend to submit a CIHA application as follows:

- Redevelopment is proposed on the Subject Lands which contain a site area of 14,065 square metres (151,401 square feet). The Subject Lands are generally located at the northeast corner of Maplecrete Road and Doughton Road in the City of Vaughan.
- The Application proposes 5 mixed-use towers comprising heights between 24 and 30 storeys.
- The Application proposes approximately 1,567 residential units, of which 182 are purpose-built rental housing units that are consistent with a Priority Housing Type outlined in the Guiding Principles. The remaining 1,385 units will be residential condominiums.
- The proposal also contains a mixed-use building containing approximately 5,711 square metres (61,476 square feet) of Office Development and 120 Hotel Suites.

As part of the application, the Ministry of Municipal Affairs and Housing ("MMAH") requires engagement with Indigenous communities. As such, please provide, within a reasonable amount of time, any concerns related to Aboriginal and treaty rights that may be impacted by this proposal. As

the Subject Lands have previously been developed for employment purposes, our client will contact the appropriate authorities and Indigenous communities should any archaeological findings be discovered during the construction of the development.

Please let us know if you have any questions, require any additional information or if you would like to meet to discuss the proposed CIHA application.

Yours truly,

MHBC

David A. McKay, MSc, MLAI, MCIP, RPP
Vice President & Partner

cc. Client, City



PLANNING
URBAN DESIGN
& LANDSCAPE
ARCHITECTURE

November 21, 2023

Mr. Don Richardson
IBA Braiding Consulting on behalf of the Mississaugas of Scugog Island

Sent by E-mail: consultation@scugogfirstnation.com

Dear Mr. Richardson:

**RE: CONSULTATION REQUEST
COMMUNITY INFRASTRUCTURE & HOUSING ACCELERATOR
171 MAPLECRETE ROAD, 140 & 160 DOUGHTON ROAD, AND BLOCK 2 (65M-4793)
VAUGHAN, ONTARIO
OUR FILE: 1512'W'**

On behalf of our client, 171 Maplecrete LP, we are contacting you regarding a proposed Community Infrastructure and Housing Accelerator ("CIHA") application for the property municipally known as 171 Maplecrete Road, 140 & 160 Doughton Road and Block 2 (65M-4793) in the City of Vaughan (the "Subject Lands").

On March 21, 2023, City Council adopted Guiding Principles to assist in identifying appropriate development proposals for CIHA requests. In light of this, we intend to submit a CIHA application as follows:

- Redevelopment is proposed on the Subject Lands which contain a site area of 14,065 square metres (151,401 square feet). The Subject Lands are generally located at the northeast corner of Maplecrete Road and Doughton Road in the City of Vaughan.
- The Application proposes 5 mixed-use towers comprising heights between 24 and 30 storeys.
- The Application proposes approximately 1,567 residential units, of which 182 are purpose-built rental housing units that are consistent with a Priority Housing Type outlined in the Guiding Principles. The remaining 1,385 units will be residential condominiums.
- The proposal also contains a mixed-use building containing approximately 5,711 square metres (61,476 square feet) of Office Development and 120 Hotel Suites.

As part of the application, the Ministry of Municipal Affairs and Housing ("MMAH") requires engagement with Indigenous communities. As such, please provide, within a reasonable amount of time, any concerns related to Aboriginal and treaty rights that may be impacted by this proposal. As the Subject Lands have previously been developed for employment purposes, our client will contact

the appropriate authorities and Indigenous communities should any archaeological findings be discovered during the construction of the development.

Please let us know if you have any questions, require any additional information or if you would like to meet to discuss the proposed CIHA application.

Yours truly,

MHBC

A handwritten signature in black ink, appearing to read 'David A. McKay', is written over a circular stamp or seal.

David A. McKay, MSc, MLAI, MCIP, RPP
Vice President & Partner

cc. Client, City



PLANNING
URBAN DESIGN
& LANDSCAPE
ARCHITECTURE

November 21, 2023

Mr. Mark LaForme
Mississaugas of the Credit First Nation

Sent by E-mail: mark.laforme@mncfn.ca

Dear Mr. LaForme:

**RE: CONSULTATION REQUEST
COMMUNITY INFRASTRUCTURE & HOUSING ACCELERATOR
171 MAPLECRETE ROAD, 140 & 160 DOUGHTON ROAD, AND BLOCK 2 (65M-4793)
VAUGHAN, ONTARIO
OUR FILE: 1512'W'**

On behalf of our client, 171 Maplecrete LP, we are contacting you regarding a proposed Community Infrastructure and Housing Accelerator ("CIHA") application for the property municipally known as 171 Maplecrete Road, 140 & 160 Doughton Road and Block 2 (65M-4793) in the City of Vaughan (the "Subject Lands").

On March 21, 2023, City Council adopted Guiding Principles to assist in identifying appropriate development proposals for CIHA requests. In light of this, we intend to submit a CIHA application as follows:

- Redevelopment is proposed on the Subject Lands which contain a site area of 14,065 square metres (151,401 square feet). The Subject Lands are generally located at the northeast corner of Maplecrete Road and Doughton Road in the City of Vaughan.
- The Application proposes 5 mixed-use towers comprising heights between 24 and 30 storeys.
- The Application proposes approximately 1,567 residential units, of which 182 are purpose-built rental housing units that are consistent with a Priority Housing Type outlined in the Guiding Principles. The remaining 1,385 units will be residential condominiums.
- The proposal also contains a mixed-use building containing approximately 5,711 square metres (61,476 square feet) of Office Development and 120 Hotel Suites.

As part of the application, the Ministry of Municipal Affairs and Housing ("MMAH") requires engagement with Indigenous communities. As such, please provide, within a reasonable amount of time, any concerns related to Aboriginal and treaty rights that may be impacted by this proposal. As the Subject Lands have previously been developed for employment purposes, our client will contact

the appropriate authorities and Indigenous communities should any archaeological findings be discovered during the construction of the development.

Please let us know if you have any questions, require any additional information or if you would like to meet to discuss the proposed CIHA application.

Yours truly,

MHBC

A handwritten signature in black ink, appearing to read "David A. McKay", is written over a circular stamp or seal.

David A. McKay, MSc, MLAI, MCIP, RPP
Vice President & Partner

cc. Client, City



PLANNING
URBAN DESIGN
& LANDSCAPE
ARCHITECTURE

November 21, 2023

Chief Joanne Sandy
Beausoleil First Nation

Sent by E-mail: council@chimnissing.ca

Dear Chief Sandy:

**RE: CONSULTATION REQUEST
COMMUNITY INFRASTRUCTURE & HOUSING ACCELERATOR
171 MAPLECRETE ROAD, 140 & 160 DOUGHTON ROAD, AND BLOCK 2 (65M-4793)
VAUGHAN, ONTARIO
OUR FILE: 1512'W'**

On behalf of our client, 171 Maplecrete LP, we are contacting you regarding a proposed Community Infrastructure and Housing Accelerator ("CIHA") application for the property municipally known as 171 Maplecrete Road, 140 & 160 Doughton Road and Block 2 (65M-4793) in the City of Vaughan (the "Subject Lands").

On March 21, 2023, City Council adopted Guiding Principles to assist in identifying appropriate development proposals for CIHA requests. In light of this, we intend to submit a CIHA application as follows:

- Redevelopment is proposed on the Subject Lands which contain a site area of 14,065 square metres (151,401 square feet). The Subject Lands are generally located at the northeast corner of Maplecrete Road and Doughton Road in the City of Vaughan.
- The Application proposes 5 mixed-use towers comprising heights between 24 and 30 storeys.
- The Application proposes approximately 1,567 residential units, of which 182 are purpose-built rental housing units that are consistent with a Priority Housing Type outlined in the Guiding Principles. The remaining 1,385 units will be residential condominium.
- The proposal also contains a mixed-use building containing approximately 5,711 square metres (61,476 square feet) of Office Development and 120 Hotel Suites.

As part of the application, the Ministry of Municipal Affairs and Housing ("MMAH") requires engagement with Indigenous communities. As such, please provide, within a reasonable amount of time, any concerns related to Aboriginal and treaty rights that may be impacted by this proposal. As the Subject Lands have previously been developed for employment purposes, our client will contact

the appropriate authorities and Indigenous communities should any archaeological findings be discovered during the construction of the development.

Please let us know if you have any questions, require any additional information or if you would like to meet to discuss the proposed CIHA application.

Yours truly,

MHBC

A handwritten signature in black ink, appearing to read 'David A. McKay', is written over a circular stamp or seal.

David A. McKay, MSc, MLAI, MCIP, RPP
Vice President & Partner

cc. Client, City



PLANNING
URBAN DESIGN
& LANDSCAPE
ARCHITECTURE

November 21, 2023

Chief Laurie Carr Hiawatha First Nation

[Sent by E-mail: chiefcarr@hiawathafn.ca](mailto:chiefcarr@hiawathafn.ca)

Dear Chief Carr:

**RE: CONSULTATION REQUEST
COMMUNITY INFRASTRUCTURE & HOUSING ACCELERATOR
171 MAPLECRETE ROAD, 140 & 160 DOUGHTON ROAD, AND BLOCK 2 (65M-4793)
VAUGHAN, ONTARIO
OUR FILE: 1512'W'**

On behalf of our client, 171 Maplecrete LP, we are contacting you regarding a proposed Community Infrastructure and Housing Accelerator ("CIHA") application for the property municipally known as 171 Maplecrete Road, 140 & 160 Doughton Road and Block 2 (65M-4793) in the City of Vaughan (the "Subject Lands").

On March 21, 2023, City Council adopted Guiding Principles to assist in identifying appropriate development proposals for CIHA requests. In light of this, we intend to submit a CIHA application as follows:

- Redevelopment is proposed on the Subject Lands which contain a site area of 14,065 square metres (151,401 square feet). The Subject Lands are generally located at the northeast corner of Maplecrete Road and Doughton Road in the City of Vaughan.
- The Application proposes 5 mixed-use towers comprising heights between 24 and 30 storeys.
- The Application proposes approximately 1,567 residential units, of which 182 are purpose-built rental housing units that are consistent with a Priority Housing Type outlined in the Guiding Principles. The remaining 1,385 units will be residential condominium.
- The proposal also contains a mixed-use building containing approximately 5,711 square metres (61,476 square feet) of Office Development and 120 Hotel Suites.

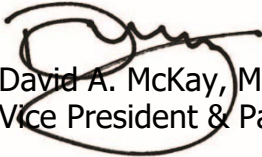
As part of the application, the Ministry of Municipal Affairs and Housing ("MMAH") requires engagement with Indigenous communities. As such, please provide, within a reasonable amount of time, any concerns related to Aboriginal and treaty rights that may be impacted by this proposal. As the Subject Lands have previously been developed for employment purposes, our client will contact

the appropriate authorities and Indigenous communities should any archaeological findings be discovered during the construction of the development.

Please let us know if you have any questions, require any additional information or if you would like to meet to discuss the proposed CIHA application.

Yours truly,

MHBC

A handwritten signature in black ink, appearing to read 'David A. McKay', is written over a circular stamp or seal.

David A. McKay, MSc, MLAI, MCIP, RPP
Vice President & Partner

cc. Client, City

**FOLLOW-UP
ENGAGEMENT
CORRESPONDENCE**

From: [Francis Kwashie](#)
To: Abby.LaForme@mncfn.ca
Cc: [Thanh Nguyen](#)
Subject: 171 Maplecrete Rd - Indigenous Engagement
Date: January-03-24 10:27:00 AM
Attachments: [171 Maplecrete Rd - Indigenous Engagement .msg](#)
[image001.png](#)
[RE 171 Maplecrete Rd - Indigenous Engagement .msg](#)

Good morning and Happy New Year, Abby.

I am reaching out regarding the CIHA proposal at the above-noted site. I previously reached out to Mark, but I saw your involvement with a similar project at 8083 Jane Street.

Please see attached all the previous correspondence and I appreciate your response at your earliest convenience.

Feel free to reach out to me should you have any questions.

Cheers,

FRANCIS K. KWASHIE, B.A (Hons), MSc, MES, MCIP, RPP | Associate

MHBC Planning, Urban Design & Landscape Architecture

7050 Weston Road, Suite 230 | Woodbridge | ON | L4L 8G7 | T 905 761 5588 x 249 | C 416 819 5021 | F 905 761 5589 | fkwashie@mhbcplan.com

Follow us: [Webpage](#) | [Linkedin](#) | [Facebook](#) | [Twitter](#) | [Vimeo](#)

50 Logo Email Signature



This communication is intended solely for the named addressee(s) and may contain information that is privileged, confidential, protected or otherwise exempt from disclosure. No waiver of confidence, privilege, protection or otherwise is made. If you are not the intended recipient of this communication, please advise us immediately and delete this email without reading, copying or forwarding it to anyone.

From: [Francis Kwashie](#)
To: mark.laforme@mncfn.ca
Subject: RE: 171 Maplecrete Rd - Indigenous Engagement
Date: January-02-24 10:49:00 AM
Attachments: [image001.png](#)

Good morning and Happy New Year, Mr. LaForme.

I am following up on my previous correspondence and I would like to confirm that you have no concerns/comments on the proposed development.

Thanks,

FRANCIS K. KWASHIE, B.A (Hons), MSc, MES, MCIP, RPP | Associate

MHBC Planning, Urban Design & Landscape Architecture

7050 Weston Road, Suite 230 | Woodbridge | ON | L4L 8G7 | T 905 761 5588 x 249 | C 416 819 5021 | F 905 761 5589 | fkwashie@mhbcplan.com

Follow us: [Webpage](#) | [Linkedin](#) | [Facebook](#) | [Twitter](#) | [Vimeo](#)

50 Logo Email Signature



This communication is intended solely for the named addressee(s) and may contain information that is privileged, confidential, protected or otherwise exempt from disclosure. No waiver of confidence, privilege, protection or otherwise is made. If you are not the intended recipient of this communication, please advise us immediately and delete this email without reading, copying or forwarding it to anyone.

From: Francis Kwashie
Sent: Tuesday, November 21, 2023 3:45 PM
To: mark.laforme@mncfn.ca
Subject: 171 Maplecrete Rd - Indigenous Engagement

Good afternoon Mr. LaForme:

I hope this email finds you well.

On behalf of our client, 171 Maplecrete LP, we are reaching out to you regarding a proposed Community Infrastructure and Housing Accelerator ("CIHA") application for the property at 171 Maplecrete Road, 140 & 160 Doughton Road and Block 2 (65M-4793) in the City of Vaughan. The proposal would redevelop the site with 5 mixed-use towers with heights between 24 and 30 storeys. Additional details regarding the proposed development are attached to this email.

As part of the application, the Ministry of Municipal Affairs and Housing ("MMAH") requires engagement with Indigenous communities. To this end, we would welcome any comments you may have by December 20, 2023.

Feel free to reach out to me should you have any questions.

Sincerely,

FRANCIS K. KWASHIE, B.A (Hons), MSc, MES, MCIP, RPP | Associate

MHBC Planning, Urban Design & Landscape Architecture

7050 Weston Road, Suite 230 | Woodbridge | ON | L4L 8G7 | T 905 761 5588 x 249 | C 416 819 5021 | F 905 761 5589 | fkwashie@mhbcplan.com

Follow us: [Webpage](#) | [Linkedin](#) | [Facebook](#) | [Twitter](#) | [Vimeo](#)

50 Logo Email Signature



This communication is intended solely for the named addressee(s) and may contain information that is privileged, confidential, protected or otherwise exempt from disclosure. No waiver of confidence, privilege, protection or otherwise is made. If you are not the intended recipient of this communication, please advise us immediately and delete this email without reading, copying or forwarding it to anyone.

From: [Francis Kwashie](#)
To: consultation@scugogfirstnation.com
Subject: RE: 171 Maplecrete Rd - Indigenous Engagement
Date: January-02-24 10:51:00 AM
Attachments: [image001.png](#)

Good morning and Happy New Year, Mr. Richardson.

I am following up on my previous correspondence and I would like to confirm that you have no concerns/comments on the proposed development.

Thanks,

FRANCIS K. KWASHIE, B.A (Hons), MSc, MES, MCIP, RPP | Associate

MHBC Planning, Urban Design & Landscape Architecture

7050 Weston Road, Suite 230 | Woodbridge | ON | L4L 8G7 | T 905 761 5588 x 249 | C 416 819 5021 | F 905 761 5589 | fkwashie@mhbcplan.com

Follow us: [Webpage](#) | [Linkedin](#) | [Facebook](#) | [Twitter](#) | [Vimeo](#)

50 Logo Email Signature



This communication is intended solely for the named addressee(s) and may contain information that is privileged, confidential, protected or otherwise exempt from disclosure. No waiver of confidence, privilege, protection or otherwise is made. If you are not the intended recipient of this communication, please advise us immediately and delete this email without reading, copying or forwarding it to anyone.

From: Francis Kwashie
Sent: Tuesday, November 21, 2023 3:49 PM
To: consultation@scugogfirstnation.com
Subject: 171 Maplecrete Rd - Indigenous Engagement

Good afternoon Mr. Richardson:

I hope this email finds you well.

On behalf of our client, 171 Maplecrete LP, we are reaching out to you regarding a proposed Community Infrastructure and Housing Accelerator ("CIHA") application for the property at 171 Maplecrete Road, 140 & 160 Doughton Road and Block 2 (65M-4793) in the City of Vaughan. The proposal would redevelop the site with 5 mixed-use towers with heights between 24 and 30 storeys. Additional details regarding the proposed development are attached to this email.

As part of the application, the Ministry of Municipal Affairs and Housing ("MMAH") requires engagement with Indigenous communities. To this end, we would welcome any comments you may have by December 20, 2023.

Feel free to reach out to me should you have any questions.

Sincerely,

FRANCIS K. KWASHIE, B.A (Hons), MSc, MES, MCIP, RPP | Associate

MHBC Planning, Urban Design & Landscape Architecture

7050 Weston Road, Suite 230 | Woodbridge | ON | L4L 8G7 | T 905 761 5588 x 249 | C 416 819 5021 | F 905 761 5589 | fkwashie@mhbcplan.com

Follow us: [Webpage](#) | [Linkedin](#) | [Facebook](#) | [Twitter](#) | [Vimeo](#)

50 Logo Email Signature



This communication is intended solely for the named addressee(s) and may contain information that is privileged, confidential, protected or otherwise exempt from disclosure. No waiver of confidence, privilege, protection or otherwise is made. If you are not the intended recipient of this communication, please advise us immediately and delete this email without reading, copying or forwarding it to anyone.

From: [Francis Kwashie](#)
To: chiefcarr@hiawathafn.ca
Subject: RE: 171 Maplecrete Rd - Indigenous Engagement
Date: January-02-24 10:48:00 AM
Attachments: [image001.png](#)

Good morning and Happy New Year, Chief Carr.

I am following up on my previous correspondence and I would like to confirm that you have no concerns/comments on the proposed development.

Thanks,

FRANCIS K. KWASHIE, B.A (Hons), MSc, MES, MCIP, RPP | Associate

MHBC Planning, Urban Design & Landscape Architecture

7050 Weston Road, Suite 230 | Woodbridge | ON | L4L 8G7 | T 905 761 5588 x 249 | C 416 819 5021 | F 905 761 5589 | fkwashie@mhbcplan.com

Follow us: [Webpage](#) | [Linkedin](#) | [Facebook](#) | [Twitter](#) | [Vimeo](#)

50 Logo Email Signature



This communication is intended solely for the named addressee(s) and may contain information that is privileged, confidential, protected or otherwise exempt from disclosure. No waiver of confidence, privilege, protection or otherwise is made. If you are not the intended recipient of this communication, please advise us immediately and delete this email without reading, copying or forwarding it to anyone.

From: Francis Kwashie
Sent: Tuesday, November 21, 2023 3:39 PM
To: chiefcarr@hiawathafn.ca
Subject: 171 Maplecrete Rd - Indigenous Engagement

Good afternoon Chief Carr:

I hope this email finds you well.

On behalf of our client, 171 Maplecrete LP, we are reaching out to you regarding a proposed Community Infrastructure and Housing Accelerator ("CIHA") application for the property at 171 Maplecrete Road, 140 & 160 Doughton Road and Block 2 (65M-4793) in the City of Vaughan. The proposal would redevelop the site with 5 mixed-use towers with heights between 24 and 30 storeys. Additional details regarding the proposed development are attached to this email.

As part of the application, the Ministry of Municipal Affairs and Housing ("MMAH") requires engagement with Indigenous communities. To this end, we would welcome any comments you may have by December 20, 2023.

Feel free to reach out to me should you have any questions.

Sincerely,

FRANCIS K. KWASHIE, B.A (Hons), MSc, MES, MCIP, RPP | Associate

MHBC Planning, Urban Design & Landscape Architecture

7050 Weston Road, Suite 230 | Woodbridge | ON | L4L 8G7 | T 905 761 5588 x 249 | C 416 819 5021 | F 905 761 5589 | fkwashie@mhbcplan.com

Follow us: [Webpage](#) | [Linkedin](#) | [Facebook](#) | [Twitter](#) | [Vimeo](#)

50 Logo Email Signature



This communication is intended solely for the named addressee(s) and may contain information that is privileged, confidential, protected or otherwise exempt from disclosure. No waiver of confidence, privilege, protection or otherwise is made. If you are not the intended recipient of this communication, please advise us immediately and delete this email without reading, copying or forwarding it to anyone.

From: [Francis Kwashie](#)
To: keithK@CurveLake.ca
Subject: RE: 171 Maplecrete Rd - Indigenous Engagement
Date: January-02-24 10:53:00 AM
Attachments: [image001.png](#)

Good morning and Happy New Year, Chief Knott.

I am following up on my previous correspondence and I would like to confirm that you have no concerns/comments on the proposed development.

Thanks,

FRANCIS K. KWASHIE, B.A (Hons), MSc, MES, MCIP, RPP | Associate

MHBC Planning, Urban Design & Landscape Architecture

7050 Weston Road, Suite 230 | Woodbridge | ON | L4L 8G7 | T 905 761 5588 x 249 | C 416 819 5021 | F 905 761 5589 | fkwashie@mhbcplan.com

Follow us: [Webpage](#) | [Linkedin](#) | [Facebook](#) | [Twitter](#) | [Vimeo](#)

50 Logo Email Signature



This communication is intended solely for the named addressee(s) and may contain information that is privileged, confidential, protected or otherwise exempt from disclosure. No waiver of confidence, privilege, protection or otherwise is made. If you are not the intended recipient of this communication, please advise us immediately and delete this email without reading, copying or forwarding it to anyone.

From: Francis Kwashie
Sent: Tuesday, November 21, 2023 3:59 PM
To: keithK@CurveLake.ca
Subject: 171 Maplecrete Rd - Indigenous Engagement

Good afternoon Chief Knott:

I hope this email finds you well.

On behalf of our client, 171 Maplecrete LP, we are reaching out to you regarding a proposed Community Infrastructure and Housing Accelerator ("CIHA") application for the property at 171 Maplecrete Road, 140 & 160 Doughton Road and Block 2 (65M-4793) in the City of Vaughan. The proposal would redevelop the site with 5 mixed-use towers with heights between 24 and 30 storeys. Additional details regarding the proposed development are attached to this email.

As part of the application, the Ministry of Municipal Affairs and Housing ("MMAH") requires engagement with Indigenous communities. To this end, we would welcome any comments you may have by December 20, 2023.

Feel free to reach out to me should you have any questions.

Sincerely,

FRANCIS K. KWASHIE, B.A (Hons), MSc, MES, MCIP, RPP | Associate

MHBC Planning, Urban Design & Landscape Architecture

7050 Weston Road, Suite 230 | Woodbridge | ON | L4L 8G7 | T 905 761 5588 x 249 | C 416 819 5021 | F 905 761 5589 | fkwashie@mhbcplan.com

Follow us: [Webpage](#) | [Linkedin](#) | [Facebook](#) | [Twitter](#) | [Vimeo](#)

50 Logo Email Signature



This communication is intended solely for the named addressee(s) and may contain information that is privileged, confidential, protected or otherwise exempt from disclosure. No waiver of confidence, privilege, protection or otherwise is made. If you are not the intended recipient of this communication, please advise us immediately and delete this email without reading, copying or forwarding it to anyone.