

ATTACHMENT 9

CITY DEPARTMENTS AND EXTERNAL AGENCIES COMMENTS

Date: March 6, 2024

To: Thanh Nguyen, Liberty Development Corporation

From: Matthew Peverini, Senior Planner - VMC

Cc: Gaston Soucy – Senior Manager, Planning and Urban Design – VMC
Christina Bruce – Director, Policy Planning and Special Programs

Re: **Planning Comments (1st Circulation) – File No. CIHA.23.003**

Applicant: 171 Maplecrete LP and 1930328 Ontario Inc.

Location: 171 Maplecrete Rd., 140-160 Doughton Road, and Block 2 on 65M-4793

VMC Planning Staff have reviewed the 1st Circulation of the Community Infrastructure and Housing Accelerator ('CIHA') Application on the above-mentioned site, circulated on January 22, 2024, and provide the following comments:

1. The Planning Justification Report ('PJR') acknowledges that a priority issue pertaining to the VMCSPP update is that the intensity of development trends are exceeding growth targets. The lands are forecasted to have a maximum density of 5.0 FSI to 7.5 FSI (dual) through the VMC Update process. The application contemplates 8.0 FSI which continues to exacerbate the priority issue of managing overdevelopment within the VMC.
2. Please coordinate with the neighbour immediately west respecting the forecasted n-s Local ROW along the east property line and tower separation distances. See VMCSPP policy 8.7.18, and VOP2010 policy 9.2.3.6 (d) which speaks to separation distances.
3. Staff support ~12.5% non-residential component of the project, as it achieves the minimum 11.5% "Mixed-Use (Non-Residential Requirement)" forecasted on the lands through the ongoing VMCSPP Update. Per comments from Economic Development, consider inclusion of additional ground-floor retail space. Note the VMCSPP update envisions Doughton Rd. to have a "required retail, service commercial, community facility or public use frontage".
 - A "Potential Minor Civic Facility" is forecasted in the vicinity of the Subject Lands through the VMCSPP update. Given this is a CIHA application, is there an opportunity to include "community infrastructure" component (or something along the same lines) on the ground floor along the Doughton Road frontage?
4. Staff encourage a more considerate effort to include a greater portion of family sized units. The proposed 2 and 3-BR currently represent less than 15% of the total unit yield. Additional family sized units within the Development were a point of discussion from Committee at the Public Meeting. Neighbouring approvals are composed of upwards of 30% of 2-BR and 3BR composition per project.

- 2951-2957 Hwy 7: approximately 34%
 - 216-220 Doughton: approximately 35%
 - 185 Doughton: approximately 33%
 - 2901 Hwy 7: over 50%
 - 2851 Hwy 7: approximately 40%
5. How are the 182 purpose-built rental units intended to be owned/operated? Any contemplation for “Affordable” purpose-built rental units (see definition of affordable in YROP 2022? Explore [Financial Incentives for Complete Communities | York Region](#) that may be applicable to the proposal.
 - For office, YR provides an 18 month deferral and purpose built rental has minimum 36 months DC deferral up to 20 years depending on scale of the project and if the units are affordable
 6. In the PJR, there are references to “in keeping with the intent of affordable housing policies by the region”. *Affordable* is defined by the YROP 2022, what component of this application is “in keeping with the intent” of the definition, and how is *affordable* being implemented?
 7. Consider VMCS Policy 8.7.17 regarding podiums. Generally podiums should be 4-stores. Consider a reduction with respect to the shared podium for the NE and E building.
 8. Please indicate all the tower floorplate sizes on the site plan and statistics. The Draft Order includes a blanket 850 m² permission for all towers, whereas 8.7.18 of the VMCS (for reference) permits 750 m². Variation in floor plate size is encouraged. Further, how does a larger floorplate contribute to “reducing negative impacts associated with shadows” as stated on page 21 of the PJR?
 9. Respecting Section 3.2 Transit-Oriented Development in the PJR, please clarify on how the forecasted population and jobs generated for the development were established. Assuming PPU is for population, but no basis for the office portion.
 10. Staff acknowledge Indigenous engagement sought as part of the initial submission. With the subsequent submission, please include a follow up (and response) from those communities that did not respond from the first round of consultation.
 11. Please clarify whether there is intent to connect to the lands east through the driveway, or whether the driveway is intended to terminate at the lot line. If there is ultimately a road along the east property line as forecasted through the VMCS update, consideration should be given to relocating the access point from Doughton Rd. onto the future road.
 12. Confirm dimensions of loading doors for waste collection. 7.5m minimum is required to facilitate Municipal Waste Collection.

13. Please clarify the strategy for the Condominium tenure for the build out of the subject lands. How will a phased tenure be addressed (matters like Zoning compliance, access etc.).

Draft Orders Comments

14. Review/ incorporate comments from Zoning to ensure appropriate relief to the Zoning By-law is recognized to ensure smooth implementation.
15. The Orders require significant improvement beyond what has been formally submitted. Several components of relief required from the ZBLs were not captured in the Draft Orders. Prior to resubmission, review and incorporate comments from Zoning and consult with staff as required. A Draft Order in the following link for Reference: [DRAFT CIHA Order](#)
16. Staff recommend that a phasing clause be included in the Order to ensure that the NW building is built as part of Phase 1 and before Phase 2. For example – “The approval of the Site Development Application for Phase 2 of the Subject Lands shall not proceed until a Building Permit has been issued for the minimum 8,200 m² of Hotel Uses and minimum 5,800 m² of Office Uses within the NW Building in Phase 1”.
17. Under **Definitions**, add “CIHA Order Conditions” means the requirements listed in Schedule ___ that shall be satisfied prior to the Minister of Municipal Affairs and Housing issuing the Order.
18. Under **Application**, remove Item 4 a). as lands are not brought into a secondary plan area through a Zoning Order. That is an Official Plan exercise.
19. Under **Permitted Uses** for V1 zone – need to add units on the ground floor as a permitted use for all towers (except NW). See Table 10-2 in By-law 001-2021.
20. Under **Zoning Requirements**, ensure definitions (i.e. height, lot, GFA, etc) and parking requirements (as a rate are included. Further, there is only one property line, unless you are proposing to carve up the subject lands. Please clarify.
21. There should be a Schedule (for reference) to the Order which labels the Towers and delineates the Phases.

Matthew Peverini, MCIP, RPP

Senior Planner, VMC

T. 905-832-8585 ext.3636

E. Matthew.Peverini@vaughan.ca

END OF MEMORANDUM

DATE: March 08, 2024

[sent via email only]

TO: Matthew Peverini, Senior Planner, VMC

RE: **Latif Fazel c/o 171 Maplecrete LP & 1930328 Ontario Inc.**
171 Maplecrete Road, 140-160 Doughton Road and Block 2 on 65M-4793
File No. CIHA.23.003 (Related File: PAC.22.079)
Engineering Review -1st Submission

The Policy Planning & Special Programs (VMC Development Engineering) staff have reviewed the submission in support of the above noted applications and provide the following comments:

The following materials were reviewed and formed the basis of our comments:

- Preliminary Functional Servicing Plan, Project No. 22151, Dwg. FSP-1, dated Jan. 19, 2024 prepared by Valdor Engineering Inc.
- Preliminary Functional Grading Plan, Project No. 11161, Dwg. FSG-1, dated Jan. 19, 2024 prepared by Valdor Engineering Inc.
- Functional Servicing and Stormwater Management Report – 171 Maplecrete Road, Project No. 22151, dated January 2024 prepared by Valdor Engineering Inc.
- Revised Report on Preliminary Geotechnical Investigation Proposed Highrise Buildings 171 Maplecrete Road, 140-160 Doughton Road & Block 2, Registered Plan 65M-4793, Vaughan, Ontario, Project No. 21-405-100R1, dated January 16, 2024 prepared by DS Consultants Ltd.
- Preliminary Hydrogeological Investigation Proposed Residential Development 171 Maplecrete Road, 140-160 Doughton Road & Block 2, Registered Plan 65M-4793, Vaughan, Ontario, Project No. 21-405-100, dated January 16, 2024 prepared by DC Consultants Ltd
- Architectural Drawings – 171 Maplecrete Rd, Project No. 140526, Dwg. A100, dated Jan. 19, 2024 prepared by Arcadis
- Landscape Plan – 171 Maplecrete Road, Project No. 23-118, Dwg. LS100, Rev. 01 dated Jan. 19, 2024 prepared by Studio tla
- Noise Impact Study – 171 Maplecrete Road, Vaughan, ON, Project No. GTR-22028965-A0, dated Jan. 19, 2024 prepared by EXP
- Transportation Impact Study - 171 Maplecrete Rd, Project No. 23198, dated January 2024 prepared by LEA

Site Description

The subject lands is located northeast corner of Doughton Road and Maplecrte Road. The development proposal is comprised of total 1.406 hectares of land:

- NE Tower (29 Storeys Residential)
- E Tower (29 Storeys Residential)
- NW Tower (30 Storeys Mixed-Use)
- SW Tower (27 Storeys Residential)
- SE Tower (24 Storeys Residential)
- Three (3) levels of underground parking

Preliminary Engineering Comments:

1. The Owner has submitted a report. “*Functional Servicing and Stormwater Management Report – 171 Maplecrete Road, Project No. 22151, dated January 2024 prepared by Valdor Engineering Inc.*”, the comments are as follows:

(Section 2.2) To maintain the water quality under the interim condition, please note that flushing program will be required for the dead-end watermain on White Elm Road.

(Section 2.3 & Fig. 2B) The report shall include the calculations pertaining to the size of the proposed water service connections. Complete the table below with supporting calculations to confirm the residual pressures at the building face is above the City’s requirements (min. 40 psi for Peak Hour Demand and min. 20 psi for Max. Day Demand + Fire Demand).

Service connections off White Elm Road, Maplecrete Road & Doughton Road	Flow (L/s)	Head Loss (psi)	Head Loss (kPa)	Residual pressure at main (psi/kPa)	Residual pressure at Building (psi/kPa)
100mm Domestic (Peak Hour)					
150mm Fire (Max. Day + Fire)					

(Fig. 2A) Please note that the existing Doughton watermain (300mm dia.) between Maplecrete Road and Jane Street will be upgraded to 400mm dia. by the City.

(Section 3.2 & Fig. 3A) Please note that the existing sanitary sewer that drains westerly on Doughton road will be upgraded to 600mm diameter prior to connecting to existing 900mm diameter trunk sanitary sewer on Jane Street.

(Fig. 3A) Please be advised that the design and construction of sanitary sewer upgrade on Maplecrete Road and Doughton Road to be delivered by the City. Please remove the reference to Croziers drawings and replace with “Accardi Scheaffers Prj. No. RFP22-093”

(Section 4.3) The report shall discuss the long-term dewatering requirement for Phase 1 & 2, see Hydrogeological Report – Section 4.5.2. Please note that the long-term groundwater discharge should be controlled as part of the allowable release rate for the site prior to discharging to the municipal storm sewer system. Please note, should there be any site related changes (relative to the hydrogeological predictions) that may result in higher groundwater discharge rates will also have to be managed to the allowable release rate. Confirmation on groundwater quality will also be required. Quality consideration need to be accounted for including any treatment and its associated backwash flows. It is encouraged to explore the options to re-use the groundwater on-site (such as irrigation, toilet flushing) to eliminate the requirement of permanent dewatering for this development.

Subject to the final recommendations in the Hydrogeological Report, a design brief for the long-term dewatering system shall be submitted to identify the layout of the pump setup, pre-treatment requirement and facilities, flow meter, sampling port, discharging point, emergency storage and space allocated for future proof the pre-treatment installation for the proposed development. Coordination between civil, architectural & geotechnical consultants will be required in order to complete the design of the permanent groundwater discharge system for the proposed development.

Please note the following requirements for the long-term dewatering discharge to municipal storm sewers:

- A long-term discharge approval is required for each connection to the City's storm sewer. Each groundwater discharge line will require a flow meter and sampling port. Sampling ports to be shown on Drawings.
- Upon transfer of ownership from developer to condominium corporation (s), if multiple condominiums exist, transfer of discharge approval ownership and responsibilities will need to be accounted for at transfer of ownership by developer/condo corporations.
- All discharge will need to be in compliance with the Sewer Use bylaw including water quality requirements.
- To future proof the discharge for compliance with the Storm Sewer bylaw limits:
 - i. Space is to be provided to accommodate treatment, to be supported with Mechanical Drawings showing space allotted for potential future treatment.
 - ii. Note should treatment be deemed necessary in the future, additional requirements will apply depending on the treatment specifics.
- For discharge approval requirements, please refer to: [Dewatering Discharge Approval \(vaughan.ca\)](#)

*Additional stormwater management comments prepared by Dana Khademi, CoV Storm Drainage Engineer:

Item	Subject	City Comments
1	Quantity Control	It is noted that the runoff from the subject site will be controlled to the target release rate (2 year post development with maximum 80% imperviousness) via a 250mm orifice and underground storage tank within P1 underground parking level. Please provide details of the SWM tank, orifice, outlet pipe and connection to the storm sewer within Mapcrete.
2	Storm Sewer Capacity	Redevelopment of the subject site may proceed prior the Mapcrete Road storm sewer upgrade. Therefore, please demonstrate with supporting calculations/storm sewer design sheet that the downstream storm sewer has capacity to convey the controlled flow from the subject site. Please consider the existing storm sewer network and the proposed storm sewer network as per the Liberty Cement submission.
3	HGL and On-Site SWM Measures	Please confirm if mitigation measures are required for the on-site SWM measure(s) based on the HGL, which was referenced to be established for

		the Liberty Cement/2901 Hwy. 7 development.
4	15mm Retention Volume	It is noted that the retention volume required for the 15mm on-site retention criterion will be achieved via rainwater harvesting and reuse for irrigation. At the site plan/detailed design stage, please provide supporting calculations demonstrating the retention volume will be fully utilized for irrigation. Another use for the harvested rainwater may be required to ensure the full retention volume is utilized in a timely manner.
5	Site Drainage Area	The existing and proposed drainage conditions figures (4 and 5, respectively) illustrate the pervious, impervious and roof areas within 171 Maplecrete, 160 & 140 Doughton Road. These land-uses are not shown for Block 2 of 2901 Hwy. 7. Please update these figures to include 2901 Hwy. 7 south of the proposed White Elm Road.
6	Maplecrete and Doughton Road Widening	Please provide design details for the widening of Maplecrete Road and Doughton Road adjacent to the subject site. Improvements/widening of RoWs/roads within the VMC southeast quadrant are required to provide quality control until a new SWM strategy for the southeast quadrant is established. With the subdivision/RoW submission, please ensure quality control is provided for the road runoff.

2. The Owner has submitted a noise report “*Noise Impact Study – 171 Maplecrete Road, Vaughan, ON, Project No. GTR-22028965-A0, dated Jan. 19, 2024 prepared by EXP*”, the comments are as follows:

(Section 3.0) What about air traffic? Is the subject land under any flight path?

(Section 3.0) The study shall include the assessment of ground borne vibration from Toronto-York Spadina Subway line which terminates at the VMC.

(Section 3.1.3, Table 7) The noise assessment shall include the Jane Street, Hwy 400, Hwy 407, White Elm Road. Future local road just east of the subject lands.

Apply the following **ultimate AADT** for the noise analysis and update the assessment accordingly:

Road	Ultimate AADT
Jane Street	45,000
Hwy 400	TBD – Consult with MTO
Hwy 407	TBD – Consult with 407ETR
White Elm Road (20m ROW)	*min. 10,000
Future Local Road (20m ROW) – at adjacent property (130 Doughton Road) between White Elm Road and Doughton Road	*min. 10,000

*Consult with traffic consultant, LEA, for the expected traffic volume and the ultimate AADT based on the Transportation Impact Study (Prj. # 23198) dated January 2024 prepared for the site. Supportive road traffic data to be included in the Appendix C.

(Section 3.1) The noise assessment shall clearly define the recommendations under the interim (Phase 1) and ultimate (Phase 2) conditions of the re-development.

(Table 9 - Column 1) - Clarify the Receptor ID with respective the Building (i.e. similar to Table 10)

(Section 3.1.4.1 & Fig. 3) For the outdoor amenity area, control measures (barriers) shall be designed to reduce the sound level as close to 55dBA. Otherwise, the proponent has to demonstrate and satisfy the City that it is not feasible to meet the Leq 55dBA sound level criterion. Provide a table to summarize the required sound barrier height to achieve 55dBA for OLA (in the format of 1dBA increments). Detail design of the barrier shall be shown on the site grading plan.

(Section 3.2.1) The noise assessment shall include the nearby Class III industrial facility (MacMillan Rail Yard, Toromont Industries Limited (Toromont) located at 3131 Highway 7)

(Section 3.2.3) The report shall include the assessment of the stationary noise from the nearby residential, commercial and industrial facilities/buildings within 300m of the site and the roof top equipment. Include detailed mapping showing the area subject to the proposed development and all industrial facilities, including zoning of vacant lands in the vicinity, and any other sources of adverse effects. The mapping should include radius's showing the minimum separation distances and influence areas of applicable Classes and the identification of facilities with the potential to cause adverse effects and their industrial classification.

(Section 3.2.7) Justify the potential impacts of emergency generators and mechanical roof top as part of the final building design. The noise assessment shall include the loading dock and garbage pick-up for the development.

3. The Owner has submitted a geotechnical report "*Revised Report on Preliminary Geotechnical Investigation Proposed Highrise Buildings 171 Maplecrete Road, 140-160 Doughton Road & Block 2, Registered Plan 65M-4793, Vaughan, Ontario, Project No. 21-405-100R1, dated January 16, 2024 prepared by DS Consultants Ltd.*", the comments are as follows:

(Section 4.8) The underground garage roof slab below the private road and driveway shall be designed to support the garbage truck & emergency vehicles such as fire truck. Table 4.8 shall recommend the pavement structure and the required fill materials between the pavement and P1 slab.

The Owner shall ensure that recommendations contained in the final report are being incorporated in the design and adequate field inspection is provided to validate the requirements during the construction.

4. The Owner has submitted a hydrogeological report “*Preliminary Hydrogeological Investigation Proposed Residential Development 171 Maplecrete Road, 140-160 Doughton Road & Block 2, Registered Plan 65M-4793, Vaughan, Ontario, Project No. 21-405-100, dated January 16, 2024 prepared by DC Consultants Ltd*”, the comments are as follows:

(Section 4.2) The final report shall discuss the feasibility of providing fully waterproofed system as an option to eliminate the long-term dewatering. If permanent dewatering is required, consultant team will be required to provide the clear scheme with respect to any groundwater pumping and discharging. Detail design shall include the layout of the pump setup, sampling port, discharging point and emergency storage and the treatment facilities based on the City’s Sewer Use By-law 087-2016, as amended.

5. At the **SITE PLAN** submission stage, the Owner shall also submit the following plans/reports for review and approval:

Servicing and Grading Plans

Please find the enclosed redlined plans (FSP1 & FSG1) for the preliminary comments.

External Lighting Plan for the Site

The plan to show lighting levels in units of lux. Please note, section 3.15 of By-law 1-88 states the following: “Any outdoor floodlighting associated with the use of any lot in any zone shall be directed inward and downward or in such a manner as to not illuminate any part of any adjacent lot.” The drawings shall bear the seal, date and signature of the Licensed Professional Engineer.

External Works – Road Widening on Maplecrete Road and Doughton Road (26m ROW)

a) Detailed design of road widening shall be part of the engineering submission. The drawing set shall include cover sheet, general notes sheet, grading plan, composite utility plan, plan and profile drawings, storm tributary area, standard detail plans, landscape and streetscape plan, streetlighting plan, erosion and sediment control plan, pavement marking and signage plan. Add the Director of Development Engineering approval block to the subdivision engineering submission drawings.

- White Elm Road (20M ROW) – Follow the cross-section previously approved for application 2901 Hwy 7. Details of the boulevard breakdown shall refer to the approved landscape drawings.
- Maplecrete Road (26M ROW) - Follow the cross-section previously approved for application 2901 Hwy 7. Details of the boulevard breakdown shall refer to the approved landscape drawings.
- Doughton Road (26m ROW) – Follow the Commerce St. cross-section (attached)

b) Street Lighting Design

The streetlighting plan including photometric analysis shall be submitted to the satisfaction of the VMC Program Department. Design drawings shall bear the seal, date and signature of the Licensed Professional Engineer.

(Design Parameters for Minor Collector – 26m ROW)

Luminaire type: LUMEC Capella LED (fitted with a 7-pin NEMA photocell receptacle conforming to ANSI C136.41 and prewired for 0-10V control)

Pole: straight, round in shape, base mounted

Material: Cast Aluminum

Colour & Finishes: Galvanized and then powder coated to texture (i.e. US2 fine iron glimmer)

Pole spacing: 25-35m

Overall pole height: max 7.62m (25 ft.)

Ave. illuminance: 10.0 Lux

Uniformity average/min: 3.0

Uniformity max/min: 6.0

Parallel/opposite pattern

(Design Parameters for Local Road – 20m ROW)

Luminaire type: LUMEC Capella LED (fitted with a 7-pin NEMA photocell receptacle conforming to ANSI C136.41 and prewired for 0-10V control)

Pole: straight, round in shape, base mounted

Material: Cast Aluminum

Colour & Finishes: Galvanized and then powder coated to texture (i.e. US2 fine iron glimmer)

Pole spacing: 25-35m

Overall pole height: max 7.62m (25 ft.)

Ave. illuminance: 7.0 Lux

Uniformity average/min: 6.0

Parallel/opposite pattern

NOTE:

- Design shall be based on medium pedestrian traffic
- Sidewalk Illuminance (Avg. 5 lux, Avg/Min. 4.0)
- Intersection Illuminance (refer to RP8-14 lighting guidelines for details)
- For White Elm Road, please refer to the approved street lighting plan for details (2901 Hwy 7, Photometric Analysis, Project No. 05628-00, Dwg. Photo-1, R7 dated Aug. 10, 2023 prepared by RTG Systems Inc.)

Development Agreement

The Owner shall enter into Development Agreement for the external works, to satisfy all conditions, financial or otherwise, of the City with regard to such matters the municipality may consider necessary including payment of the development levies, the provision of roads and municipal services landscaping and fencing.

The Owner shall convey sufficient property for the road widening at no cost and free of charge and encumbrance to the City. Details as follows:

- Convey sufficient property for the Maplecrete Road widening (26.0m R.O.W.). New property line shall be measured 13m from the centerline of the road.

- Convey sufficient property for the Doughton Road widening (26.0m R.O.W.). New property line shall be measured 13m from the centerline of the road.
- Maintain the 5.0m x 5.0m daylight triangle with 0.3m reserve at the south-east corner of White Elm Road and Maplecrete Road.
- Maintain the 8.0m x 8.0m daylight triangle and 3.0m return with 0.3m reserve at the north-east corner of Doughton Road and Maplecrete Road.
- Provide the 5.0m x 5.0m daylight triangle easement at the south-east corner of the subject land to protect the sight-line for the future north-south road on the adjacent property to the east that intersects with Doughton Road.
- Provide the 3.0m x 3.0m daylight triangle easement at the north-east corner of the subject land to protect the sight-line for the future north-south road on the adjacent property to the east that intersects with White Elm Road.

Dewatering Plan for Construction

The Owner shall coordinate with the civil and geotechnical consultants to provide a detail dewatering plan for the construction.

The submission shall include:

- The route of dewatering and the discharge point with erosion measures.
- The capacity of receiving system and any impacts on downstream flooding.
- The contingency plan during the course of construction and during heavy rainfall events.
- The detail of dewatering process for building foundation and underground parking structure.
- The bedding requirements for the installation of underground services.
- Supporting reports/plans/drawings detail but not limited to such as, discharge rate, duration of discharge, length of discharge operation, monitoring plan, contingency plan, sampling/testing method, location of site, discharge location and type of sewer system, type of flow meter and installation location, flow data logging, etc.

Shoring Excavation Plan

The Owner shall submit the shoring and tie-back design drawings for review and approval.

- No part of the shoring system shall be constructed on or within the City's lands or the public road allowance with the exception of the tiebacks.
- Tiebacks shall be constructed at a depth of no less than three (3) metres below the ground surface on City lands to provide adequate clearance for the maintenance of underground services/utilities.
- The Owner shall design the future structure so the tiebacks will become redundant once the permanent underground building structure is fully completed. The Owner shall provide the City with an Engineer's certificate to this effect to the satisfaction of the City.
- Refer to the Geotechnical Report for the site specific recommendations.
- Consent from adjacent landowner will be required for works on the adjacent property.

- The Owner shall enter into Encroachment Agreement as part of the Road Occupancy Permit, to satisfy all conditions, financial or otherwise, of the City with regard to construction/tieback encroachment.

Encroachment Agreement

Prior to commencement of construction, the Owner shall apply the Road Occupancy Permit for the construction and enter into Encroachment Agreement to satisfy all conditions, financial or otherwise, of the City (including shoring system, hoarding, construction access, crane swing and construction dewatering).

The following information shall be provided to prepare the Encroachment Agreement:

- OWNER: (The corporate name shall be consistent in the site plan application.)
- REPRESENTATIVE: (The Owner's representative who is charged with the administration of the terms of the agreement.)
- OWNER'S LANDS: (The description shall describe the relevant Lot, Concession, and designed Parts on a Reference Plans.)
- CITY LANDS: (The description shall describe the relevant Lot, Concession, and designed Parts on a Reference Plans.)
- COMMENCEMENT DATE/EXPIRY DATE:
- SIGNING OFFICIER: (Signing officers shall have the authority to bind the corporation.)
- EMERGENCY CONTACT INFORMATION OF OWNER: (name/phone no./email)
- CONSTRUCTION LOGISTICS PLAN: (Identify the following information on plan: property line, nearby driveways, covered/designated walkway, gates, limit of shoring, construction fence/hoarding, tower crane, crane reach, crane pickup area, lay down area, construction hoist, hoist platform, staging area, site offices, road traffic direction, construction traffic path)
- CRANE SWING PLAN: (The plan shall be prepared and certified by P.Eng.)

Construction Logistics Plan to clarify the construction traffic direction, staging area and signage installation.

Tentative Construction Schedule and Phasing Plan (including roads, underground parking, site servicing and all the above ground structure.)

1. OTHER**a) TRANSPORTATION REVIEW**

- Please find the attached memorandum from Jillian Britto, Transportation Project Manager, dated March 8, 2024 for transportation comments.
- Refer to the attached redline drawing (LS100) for the active transportation comments

b) ENVIRONMENTAL SITE ASSESSMENT

The following materials were reviewed and formed the basis of the environmental comments:

- Site Screening Questionnaire, dated January 19, 2024

- Noise Impact Study, 171 Maplecrete Road, Vaughan, ON, prepared by EXP Services Inc. (EXP), dated January 19, 2024
- Phase One ESA, 140 & 160 Doughton Road, 171 Maplecrete Road, Vaughan, Ontario, prepared by DS Consultants Ltd. (DS), dated November 24, 2021
- Phase Two ESA, 140 & 160 Doughton Road, 171 Maplecrete Road, Vaughan, Ontario, prepared by DS, dated June 8, 2022
- Phase One ESA, 2901 Highway 7, Vaughan, Ontario, prepared by DS Consultants Ltd., dated April 8, 2022; and
- Phase Two ESA, Western and Southern Portions of 2901 Highway 7, Vaughan, Ontario, prepared by DS, dated April 26, 2022

Environmental comments are as follows:

1. An Air Quality/Land Use Compatibility study has not been provided with this submission. Given the commercial/industrial uses surrounding the lands that comprise this CIHA (collectively, the Subject Property), we would request that one be completed.
2. In the Noise Impact Study, EXP recommends the use of warning clauses due to a neighbouring stationary source and that further analysis "be performed when information of the proposed mechanical system is available." No comments are made relating to the current report; however, the City requests an updated noise report at the time of the site plan application that addresses EXP's recommendation for further analysis and to review the warning clauses with respect to a more complete design.
3. DS has provided several ESA reports, which as a whole, cover the Subject Property. In these reports, elevated pH levels were identified in surficial soils on the north portion of the Subject Property, a petroleum hydrocarbon (PHC) exceedance was identified in surficial soils on the southeast portion of the Subject Property, and various salt-related parameters in soil and groundwater were identified to exceed the applicable standards across the Subject Property. The salt-related exceedances were attributed to road salting for safety purposes and were discounted under the exemption within the regulation; however, DS recommended remedial excavations to address the pH levels and PHC exceedance. The Owner/DS is requested to submit a remedial action plan (RAP) that addresses these exceedances in accordance with the City's RAP checklist (attached).
4. DS is requested to provide the City with a Reliance Letter for their four reports listed above, in accordance with our template (attached). It is understood that further reports (remediation reports, as well as any Update report(s) which may be required for filing of a Record of Site Condition) will be issued in the future, a separate Reliance Letter will be requested for the additional reports once completed.
5. The Owner shall submit a copy of a Ministry of the Environment, Conservation, and Parks (MECP) Record of Site Condition filed on the environmental site registry covering the subject lands or implementation and confirmation of completion of the Remedial Action Plan to the satisfaction of the City, prior to applying for an above grade building permit.

c) WATER/SANITARY SERVICING ALLOCATION

For City Council to adopt a resolution allocating sewage and water supply capacity in accordance with the City's approved Servicing Capacity Distribution Policy assigning capacity to the Subject Lands.

d) The Owner/Owner is to obtain all necessary approvals from the Region of York, TRCA, and MTO, if applicable.

e) **SITE PLAN FEE (2024 rate)**

The Owner shall pay Development Engineering Site Plan Complex fee pursuant to the Fees and Charges By-law as amended. The fee amount is **\$670,095.22** ($\$657/\text{unit} \times 300 \text{ units} + \$300/\text{unit} \times 1,265 \text{ units} + \$6.58/\text{sqm} \times 14,209\text{sqm}$ – Site Statistics as per Drawing A001, 171 Maplecrete Rd dated Feb. 19, 2024 prepared by Aracadis) and shall be included with the next submission or prior to Development Engineering clearance. If the fee is not paid in the calendar year in which it is calculated, the fee will be subject to increase in the next calendar year. Payment of the Engineering Site Plan Fee shall be a condition of Final Site Plan Approval.

f) **GRADING INSPECTION FEE (2024 rate)**

The Owner shall pay Development Engineering Site Plan Grading Inspection fee pursuant to the Fees and Charges By-law as amended. The fee amount is **\$708.00** (1.41 ha) and shall be included with the next site plan submission. If the fee is not paid in the calendar year in which it is calculated, the fee will be subject to increase in the next calendar year. Payment of the Engineering Grading Inspection Fee shall be a condition of Final Site Plan Approval.

We will require that the above noted comments be addressed by the appropriate consultants and that all applicable reports be revised accordingly and resubmitted for review and approval. Please provide a Response Matrix with the next submission. Staff reserve the right to provide additional comments on road network/access, municipal services and environmental noise analysis as part of the future submissions.

Should you require further assistance, please contact me at extension 8567.

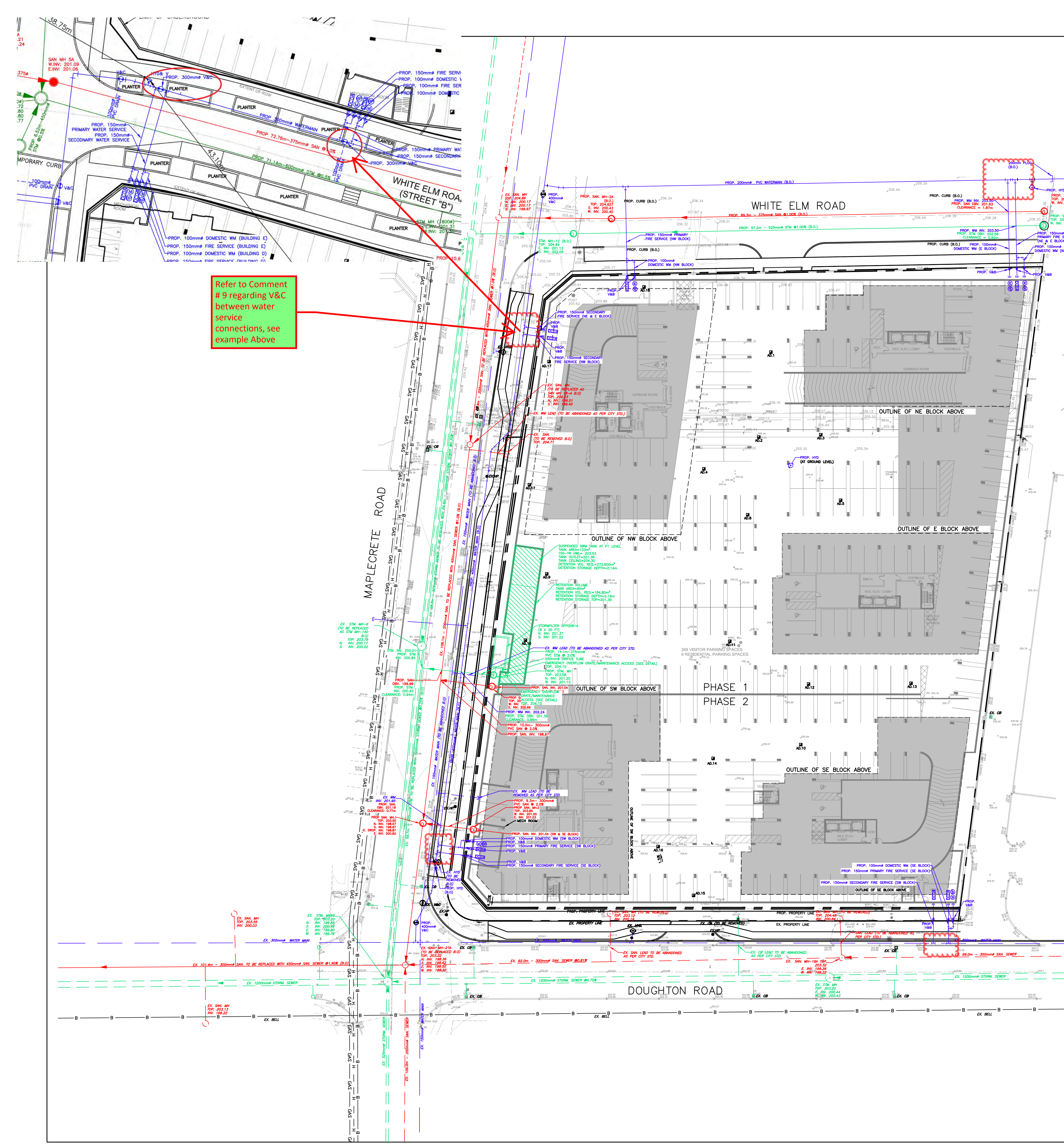
Sincerely,

Danny Woo, P.Eng.
Development Engineering Lead, VMC
Policy Planning & Special Program

Enclosures:

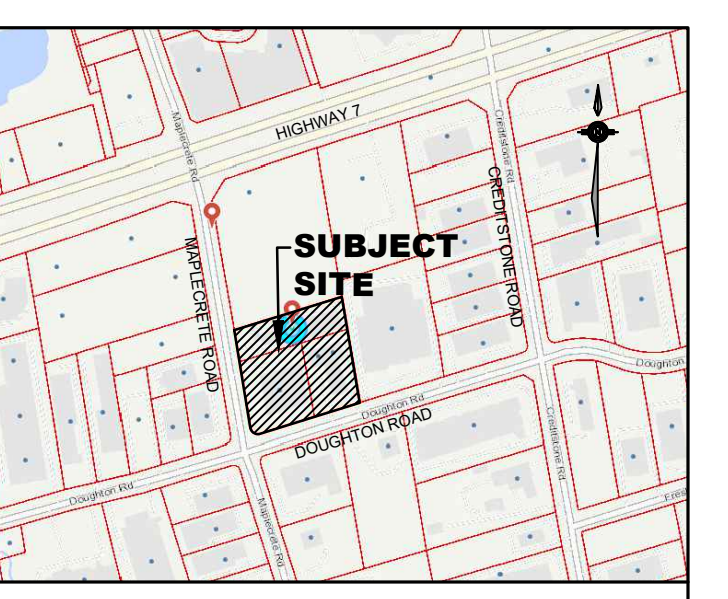
1. Redline Plans (Redlined_SUB1_Jan-19-2024 – 171 Maplecrete Rd_City comments.pdf)
2. Transportation Memo dated March 08, 2024 + Redlined Drawing LS100
3. Letter of Reliance – template
4. City's RAP checklist
5. Road cross-sections (White Elm Road, Maplecrete Road, Commerce Street)

Cc: Musa Deo, Manager of Development Engineering – VMC
Nelson Pereira, Manager of Development Finance



Refer to Comment # 9 regarding V&C between water service connections, see example Above

- Detailed Site Servicing Plan shall include the following:
1. Include the City file no. (DA.XX.XXX)
 2. Identify the dimension(s) of the proposed property line offset (13.0m) from the road centerline to confirm the road widening on Maplecrete Road and Doughton Road. Include the cross sections for road widening and verify any conflicts with existing utilities. The Owner will be responsible for making any adjustments or relocations and inform the affected utility company of the requirement to obtain approvals.
 3. For the road widening along the development frontage on Maplecrete Road and Doughton Road, submit the drawing set which shall include cover sheet, general notes sheet, general grading plan, composite utility plan, standard detail plans, landscape and streetscape plan, streetlighting plan, erosion and sediment control plan, pavement marking and signage plan. Include the Director of Development Engineering approval block to the submission drawings.
 4. Clearly identify the underground parking limit on plan.
 5. Provide cross-section(s) for the proposed service connections on White Elm Road, Maplecrete Road and Doughton Road. Maintain the vertical and horizontal clearance at the pipe crossing per MOE guidelines, where applicable. Identify inverts at the building for the service connections. The proposed lot line control MHs and sanitary/storm laterals shall conform with the City design standard C-102. The control manhole shall be independent from the parking structure (not attached) and place it close to property line. Ensure lot line control MHs are accessible and free of obstruction at all time.
 6. Sanitary service connection shall maintain with min. 2% slope and 2.6m frost cover.
 7. Provide the cross-section of the storm service connection including details such as inverts, frost depth (min. 1.2m) design head, 100YR HWL for the on-site control and the storage tank. To avoid flooding inside the parking garage, a positive outlet from the storage tank shall be designed for emergency overflow and drain directly to the road. Identify the 100 YR HGL for the road sewer and compare the water levels inside the downstream manholes to confirm the free flow condition provided in the design.
 8. Provide min. 2.1m frost cover below the road centerline for the water service connections.
 9. Provide V&C as isolation valve between the water services. (see example)
 10. With the multiple water service connections for the subject land, verify the proposed setup is fulfilling the building code requirement to effectively provide fire protection for the entire development (Ref: O.B.C. 3.2.9.7.)
 11. Identify the Siamese connection. Hydrant must be placed within 45m of any proposed Siamese connection.
 12. The private fire hydrant shall be protected by the internal fire BFP at the fire system riser in the P1 mechanical room. Under code and our Backflow By-law 177-2020, fire hydrants are to be protected by an approved BFP. Anti-tampering device shall be installed on private hydrant.
 13. Please note that one bulk meter per municipal address. The applicant shall clarify the condo. corp. arrangement for the subject development and determine the metering setup. City's Planning Dept. shall be engaged to apply for additional addresses where multiple meters are needed for billing purpose.
 14. Add "Note" to clarify the arrangement of the remote meter reading access on outside face of building that the water meter for each building can be read at the ground level without entering the mechanical room. Please make reference to Std. W-109 for the setup of meter remote wire.
 15. Include a table to summarize the clearance at the pipe crossing based on the MOE guidelines, if any.
 16. Remove existing service connections that will not be utilized by the new development. Identify the details of services decommissioning based on the City's requirements. Refer to City Design Criteria 1.4.8.1 for the details of decommissioning sewer service connections. Refer to City Design Criteria 1.5.7.1 for the details of decommissioning water service connections.
 17. Please note that White Elm Road will be extended on the west side of Maplecrete Road as part of the Liberty Maplecrete development (2951 Hwy 7). Refer to Valdor File: 11161
 18. The design of road widening, intersection alignment and servicing upgrade on Maplecrete Road shall be coordinated with Crozier Consulting Engineers (Project no. 1525-6298) and Accardi Schaeffers (Project no. RFP22-093) and Valdor Engineering (Project no. 11161)
 19. The road and servicing design at the eastern edge shall be coordinated with the adjacent properties shall be coordinated with adjacent development (130 Doughton Road - Civil consultant: Schaeffers Consulting Engineers, Project no. 2022-5268).
 20. If permanent dewatering is required for the development as per the Geotechnical/Hydrogeological Report, the consultant team shall complete the design of the permanent dewatering including the layout of the pump setup, treatment facilities, flow meter, sampling port, discharging point and emergency storage specifically designed for the proposed development. Supporting calculations for the design shall be included in the stormwater management report.
 Long-term discharge of groundwater to the municipal sewer system shall be designed based on the following requirements:
 - Explore the option to re-use the groundwater on-site instead of discharging to the municipal sewer system.
 - Discharge flows are to be discharged to the municipal storm system only. Discharge is not allowed to the sanitary system.
 - Average and maximum daily flow rates are to be provided as well as maximum daily volume.
 - Site drainage and collection/discharge flows are designed/operated in separate systems.
 - The combination of site and collection/discharge flows shall not exceed the allowable site discharge rate.
 - Discharge water shall meet the quality requirements limits as per the City's Sewer Use By-law 087-2016.
 - The permanent collection/discharge system shall include the layout of the pump setup, treatment facilities, flow meter, sampling port, discharging point and emergency storage and space allocated for future proof the pre-treatment installation specifically designed for the proposed development.
 - Identify the layout of permanent under floor drainage facilities on the Site Servicing Plan with the supporting calculations in the stormwater management report.
 - The applicant will be responsible for the ongoing water quality and quantity monitoring and reporting to the City on a yearly basis.
 - Permanent discharge permit will be required as part of the site plan approval.
 21. Provide the SWM summary (see attached template) to justify the breakdown of the actual site release rate in terms of controlled/uncontrolled flow and permanent dewatering to confirm the overall site release. If applicable, the SWM summary shall specify the designed max. long-term dewatering rate that the on-site SWM system can handle. Provide the detailed plan/storage in case you need to cease the dewatering discharge under emergency. It is encouraged to explore the options of having water-tight underground structure (i.e. bathtub design) or re-use the groundwater on-site (such as irrigation, toilet flushing) to eliminate the requirement for permanent dewatering for the development.
 22. To address the long-term dewatering requirements, identify the layout of Sampling Port (at ground surface) and Flow Meter (at P1 level) on the Site Servicing Plan. Please note that the long-term dewatering system shall include



KEY PLAN
N.T.S.

	PROPOSED CATCH BASIN
	STORM MANHOLE
	SANITARY MANHOLE
	VALVE AND BOX
	HYDRANT
	EXTERIOR DOOR LOCATION
	FIRE DEPARTMENT CONNECTION
	STAND ALONE FIRE DEPARTMENT CONNECTION
	EXISTING AREA DRAIN
	EXISTING CATCH BASIN
	WATER METER
	BACKFLOW PREVENTER
	DOUBLE CHECK DETECTOR ASSEMBLY
	EXISTING GAS MAIN
	VALVE AND CHAMBER

VERSION NOTE
NOTES AND ELEVATIONS SHOWN ON THIS PLAN ARE IN METRES CAN BE CONVERTED TO FEET BY DIVIDING BY 0.3048.

RING NOTE (PLAN OF SURVEY)
RINGS ARE GRID DERIVED FROM THE 6' UTM CO-ORDINATE SYSTEM ZONE 17, 83 (ORIGINAL) BASED ON REAL TIME NETWORK OBSERVATIONS.

VIATION NOTE (PLAN OF TOPOGRAPHY)
VIATIONS ARE GEODETIC AND ARE REFERRED TO THE CITY OF VAUGHAN MARK NO. 23-6, HAVING AN ELEVATION OF 202.852 METRES.

DATE	REVISIONS	BY
JAN 19/24	ISSUED FOR COORDINATION	D.G

PRELIMINARY

VALDOR ENGINEERING INC.
Consulting Engineers - Project Managers
571 CHERRYBROOK UNIT 4, 2ND FLOOR, WOODBINE, ONTARIO, L4B 3K7
TEL: (905) 886-0004 FAX: (905) 886-0007
E-MAIL: info@valdor-eng.com www.valdor-engineering.com

PROPOSED RESIDENTIAL DEVELOPMENT
171 MAPLECRETE ROAD
CITY OF VAUGHAN

FUNCTIONAL SERVICING PLAN

SCALE	DATE OF DWG.	PROJECT NO.
1:300	MAR. 30/23	22151
DESIGNED BY	DRAWING NO.	
S.K	FSP-1	
CHECKED BY		
D.G.		

STORM WATER MANAGEMENT ANALYSIS SUMMARY

ALLOWABLE SITE RELEASE RATE (L/SEC): _____

SITE COMPOSITION

SITE CATCHMENT AREA (HA): _____
OFFICE COMPONENT ROOF AREA (HA): _____
WAREHOUSE ROOF AREA (HA): _____
CONTROLLED PAVEMENT AREA (HA): _____
UNCONTROLLED PAVEMENT AREA (HA): _____
CONTROLLED LANDSCAPE AREA (HA): _____
UNCONTROLLED LANDSCAPE AREA (HA): _____

ROOF STORM WATER MANAGEMENT SYSTEM

TOTAL NO. OF HOPPERS: _____
TOTAL NO. OF WEIRS: _____
WEIR RATING (L/SEC/CM): _____
100-YR STM PEAK RELEASE RATE (L/SEC): _____
MAX. DETENTION STORAGE REQUIREMENT (CU.M) _____
DESIGN ROOF SLOPE: _____
MAX. ROOF PONDING DEPTH AT HOPPER (CM) _____
AVE. ROOF PONDING DEPTH (MM): _____
AVE. DRAWDOWN TIME (HRS): _____

SITE ORIFICE CONTROLS

LOCATION: _____
ORIFICE DIA.: _____
INVERT ELEVATION: _____
CENTER-LINE ELEV: _____
DOWNSTREAM HGL: _____
DETENTION STORAGE TWL: _____
DESIGN HEAD: _____
DESIGN PEAK: _____
Q-REL (L/SEC): _____

STORM WATER MANAGEMENT ANALYSIS

PART A - DETENTION VOLUME REQUIREMENT

100-YR REQUIRED DETENTION STORAGE VOLUME: _____
ORIFICE RELEASE RATE AT (L/SEC) _____

MAXIMUM SITE DETENTION STORAGE AVAILABLE:

CATCHMENT AREAS: _____
DESIGN T.W.L: _____
SURFACE PONDING: _____
PIPE STORAGE: _____
STRUCTURE STORAGE: _____

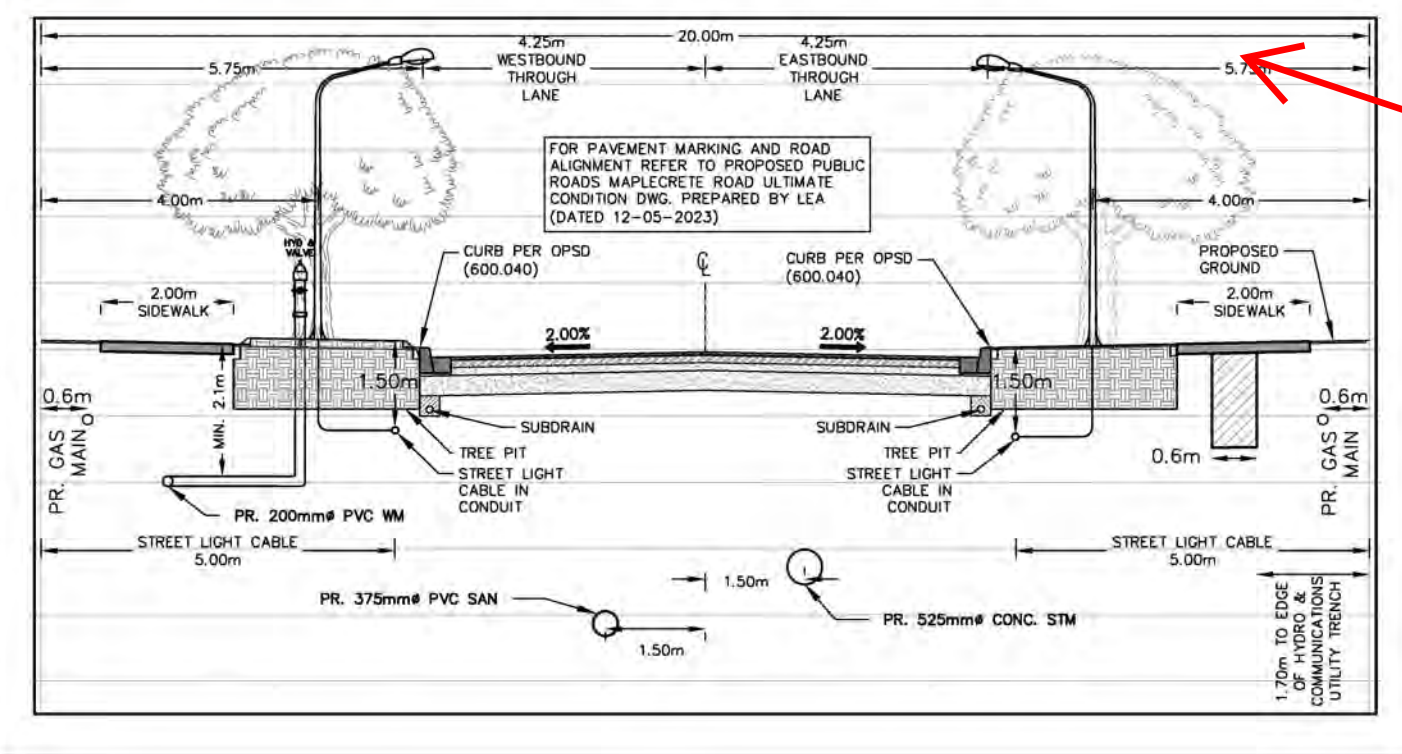
TOTAL _____

PART B-SITE CATCHMENT AREA RELEASE RATE

ACTUAL SITE RELEASE RATE: ORIFICE (L/SEC): _____
UNCONTROLLED (L/SEC) _____
TOTAL (L/SEC): _____

ALLOWABLE SITE RELEASE RATE (L/SEC): _____

INFORMATION ONLY TYPICAL CROSS SECTION - WHITE ELM ROAD
SCALE: N.T.S.



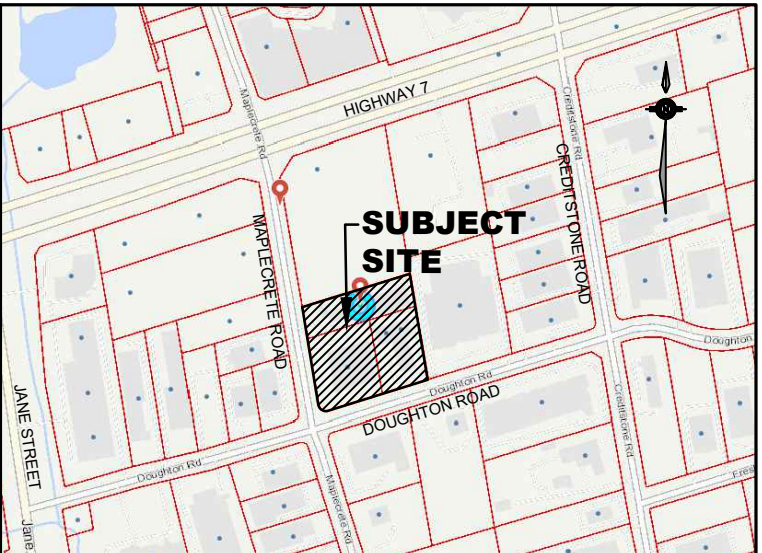
Details of the boulevard breakdown shall refer to the approved landscape drawings for 2901 Hwy 7

Maintain the 5.0m x 5.0m daylight triangle with 0.3m reserve at the south-east corner of White Elm Road and Maplecrete Road.

Maintain Provide the 3.0m x 3.0m daylight triangle easement at the north-east corner of the subject land to protect the sight-line for the future north-south road on the adjacent property to the east that intersects with White Elm Road.

REVIEWED
By Danny Woo at 8:56 am, Mar 08, 2024

NOTE:
THIS PLAN HAS BEEN PREPARED TO DEMONSTRATE FEASIBILITY OF THE PROPOSED DEVELOPMENT WITH RESPECT TO GRADING IN CONJUNCTION WITH THE OFFICIAL PLAN AMENDMENT AND ZONING BY-LAW AMENDMENT APPLICATIONS. DETAILED GRADING DESIGN WILL BE PREPARED AT THE SITE PLAN APPLICATION STAGE.

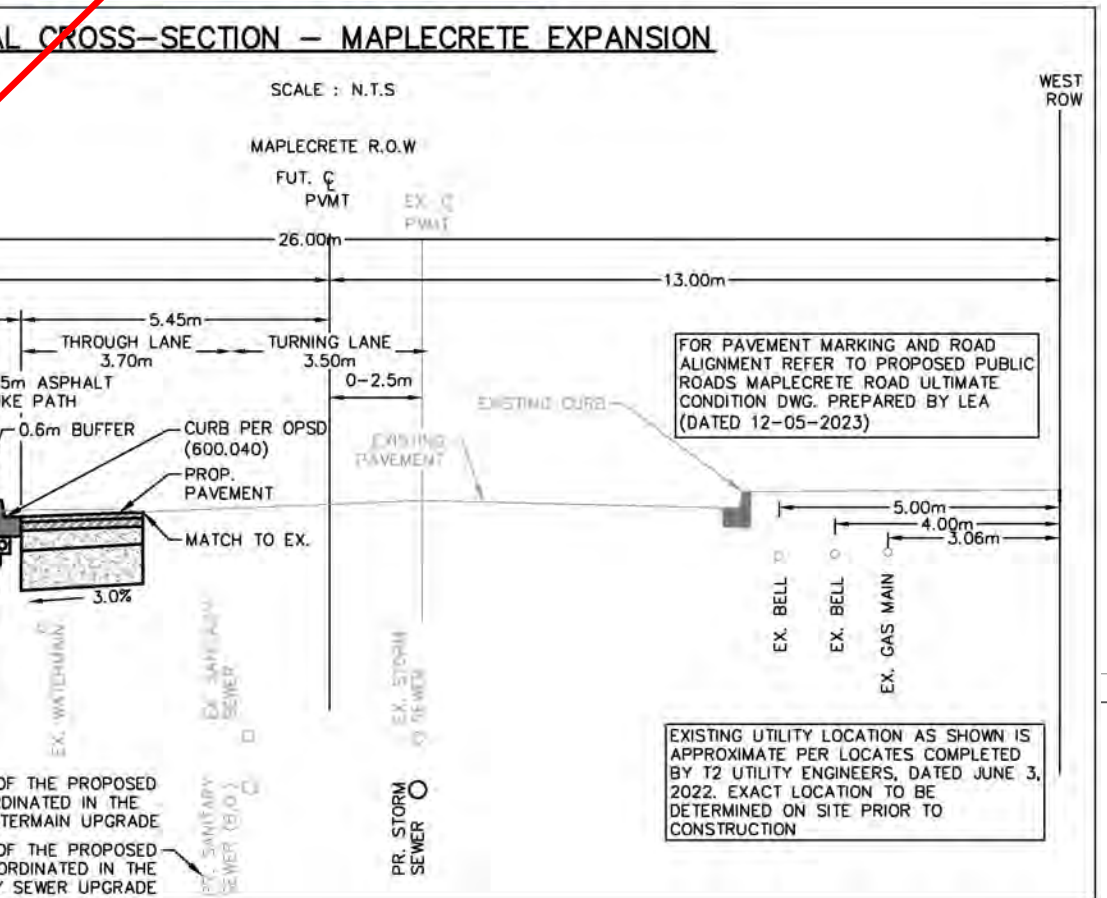


KEY PLAN
N.T.S.

*98.50 EX	EXISTING ELEVATION
2%	DRAINAGE FLOW DIRECTION AND SLOPE
x 98.35	PROPOSED ELEVATION
□	CATCH BASIN
○	STORM MANHOLE
●	SANITARY MANHOLE
⊕	VALVE AND BOX
⊙	VALVED HYDRANT
➔	OVERLAND FLOW ROUTE
● EX HP	EXISTING HYDRO POLE
● GW	GUY WIRE
● SL	STREET LIGHT

Details of the boulevard breakdown shall refer to the approved landscape drawings for 2901 Hwy 7

INFORMATION ONLY



Maintain the 3.0m x 3.0m sight-line clearance to future proof the driveway connection to the North-South road on the adjacent property to the east.

Maintain the 8.0m x 8.0m daylight triangle and 3.0m return with 0.3m reserve at the north-east corner of Doughton Road and Maplecrete Road.

Maintain the 5.0m x 5.0m daylight triangle easement at the south-east corner of the subject land to protect the sight-line for the future north-south road on the adjacent property to the east that intersects with Doughton Road.

- Detailed Site Grading Plan shall include the following:
1. Include the City file no. (DA.XX.XXX)
 2. Identify the following on Grading Plan: Maintain the 5.0m x 5.0m daylight triangle with 0.3m reserve at the south-east corner of White Elm Road and Maplecrete Road.
Maintain the 8.0m x 8.0m daylight triangle and 3.0m return with 0.3m reserve at the north-east corner of Doughton Road and Maplecrete Road.
Maintain the 5.0m x 5.0m daylight triangle easement at the south-east corner of the subject land to protect the sight-line for the future north-south road on the adjacent property to the east that intersects with Doughton Road.
Maintain Provide the 3.0m x 3.0m daylight triangle easement at the north-east corner of the subject land to protect the sight-line for the future north-south road on the adjacent property to the east that intersects with White Elm Road.
Maintain the 3.0m x 3.0m sight-line clearance to future proof the driveway connection to the North-South road on the adjacent property to the east.
 3. The grading at the eastern edge shall be coordinated with the adjacent properties in order to complete the interim and ultimate road/driveway design. Provide grading details at the property limit under interim and ultimate conditions.
 4. Provide details for the interim and ultimate design of the N-S access driveway including the temporary vehicular turn-around at the south limit of Phase 1, if applicable.
 5. Show turning radii, indicate sufficient maneuvering area for access to loading area by semi-trailer truck or smaller shipping trucks, emergency vehicles as appropriate. Verify with CAD based software like Auto Turn or equivalent.
 6. Add note "Consent and agreement from adjacent landowner will be required for works on the private property." Please advise the City regarding the arrangement and coordination details.
 7. 0.3m reserves to be lifted for the legal access, if applicable.
 8. Show entrance driveway widths and radii. Driveway entrance shall have setback of a minimum of 1.0m clear from all aboveground services or other obstructions.
 9. Show cross-section(s) at the development frontages. For coordination purpose, please refer to the attached road cross-section(s) previously developed with other applications in the area. The following information shall be identified in your road cross-section to the satisfaction of the City:
 - Street Lighting
 - Hydrants
 - Gas main
 - Watermain/storm sewers/sanitary sewers (where applicable) with standard clearance
 - Tree trench to provide minimum growth medium volume
 - Filtration trench (LID) for drainage quality control
 - Hydro Transformer (where applicable)
 - Joint Utility Trench (to be reviewed with service providers)
 - Sidewalk/Bicycle Facility
 10. As part of the road widening design, provide the Utility Coordinate Plan which shall be prepared in consultation with all the utility providers.
 11. Municipal sidewalk through a driveway shall be constructed with a 200mm thick sidewalk, as per City Standard Drawing R-128.
 12. Tactile warning plates shall be installed at all depressed curbs leading to pedestrian facility, details per York Region standard.
 13. Identify the fire route on plan, verify the fire truck turning feasibility with CAD based software like Auto Turn or equivalent.
 14. Show snow storage area on plan.
 15. Identify the noise screen/barrier requirements as recommended in the Noise Report.
 16. Provide a georeferenced CAD file for the "Site Grading Plan"
 17. Provide an Erosion & Sediment Control Plan that identifies the erosion and sediment control measures to be in place to the satisfaction of the City and Region of York prior to commencement of any works, which shall be designed and conformed with the TRCA's Erosion & Sediment Control Guideline for Urban Construction dated December 2006 (or most recent version thereof).

CONVERSION NOTE
DISTANCES AND ELEVATIONS SHOWN ON THIS PLAN ARE IN METRES AND CAN BE CONVERTED TO FEET BY DIVIDING BY 0.3048.

BEARING NOTE (PLAN OF SURVEY)
BEARINGS ARE GRID DERIVED FROM THE 6° UTM CO-ORDINATE SYSTEM ZONE 17, NAD 83 (ORIGINAL) BASED ON REAL TIME NETWORK OBSERVATIONS.

ELEVATION NOTE (PLAN OF TOPOGRAPHY)
ELEVATIONS ARE GEODETIC AND ARE REFERRED TO THE CITY OF VAUGHAN BENCH MARK NO. 23-6, HAVING AN ELEVATION OF 202.852 METRES.

NO.	DATE	REVISIONS	BY
01.	JAN 19/24	ISSUED FOR COORDINATION	D.G

PRELIMINARY

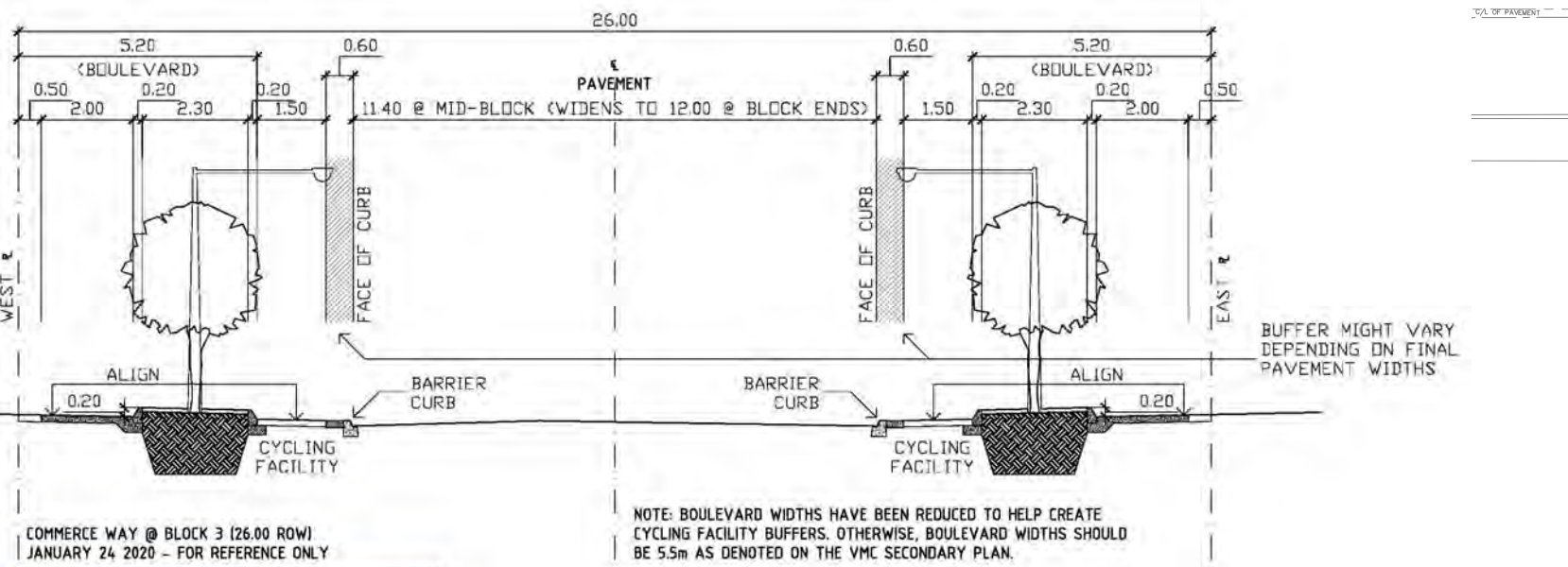
VALDOR ENGINEERING INC.
Consulting Engineers - Project Managers
571 CHERRYL ROAD, UNIT 4, 2ND FLOOR, WOODBINE, ONTARIO, L4B 1B4
TEL: (905) 882-0004 FAX: (905) 882-0007
EMAIL: info@valdor-engineering.com www.valdor-engineering.com

PROPOSED RESIDENTIAL DEVELOPMENT
171 MAPLECRETE ROAD
CITY OF VAUGHAN

FUNCTIONAL GRADING PLAN

SCALE 1:300	DATE OF DWG. MAR. 30/23	PROJECT NO. 22151
DRAWN BY S.K	DRAWING NO. FSG-1	
CHKD BY D.G.		

INFORMATION ONLY



memorandum

Submitted by email only

To: Danny Woo, Development Engineering Lead (VMC)

From: Jillian Britto, Transportation Project Manager, Development Engineering (VMC)
Sandy Yang, Active Transportation Infrastructure Specialist, Infrastructure Planning & Corporate Asset Management
Rosalie Shan, Project Engineer – Transportation & Road Safety, Transportation and Fleet Management Services – Public Works

Date: March 8, 2024

RE: **171 Maplecrete LP and 1930328 Ontario Inc.**
171 Maplecrete Rd., 140-160 Doughton Road, and Block 2 on 65M-4793
File No. CIHA.23.003
Transportation Engineering Review – 1st Submission dated
January 22, 2024

Transportation Engineering staff have reviewed the plans and reports submitted in support of the 1st submission for the above noted application, and provide the following comments:

General Comments

1. Staff discourage private driveway accesses onto minor collector roads; Staff request the applicant reconsider the vehicular connection of the private N/S driveway to Doughton Road for the following reasons:
 - a. Driveway accesses should be located on local roads, where possible, according to the VMC Secondary Plan.
 - b. The access will adversely impact the dedicated cycling facilities on Doughton Road whereas local roads have shared cycling facilities.
 - c. Proximity to the Maplecrete Road at Doughton Road intersection; the Transportation Impact Study (TIS) predicts queues along Doughton Road will extend beyond the access, which will adversely impact traffic flow.
 - d. In the future, this site will be connected to the N/S Local Road to the east, resulting in three driveway accesses, which is not necessary.

Staff request closing the access on Doughton Road and instead locate and protect for a future connection to the N-S local street, similar to the Liberty Cement development application to the north.

2. Please provide a Functional Road Plan illustrating how the proposed development will fit into the broader transportation road network in both the interim and ultimate conditions. This should include Maplecrete Road from White Elm Road to Doughton Road, Doughton Road from Maplecrete Road to the eastern terminus of the site, and the joint vision of how the site will connect to the future N-S local road and the neighbour to the east. The Functional Road Plan should include the following daylight triangle requirements:
 - a. Maplecrete at White Elm – 5 x 5m sight triangle for the northwest corner
 - b. Maplecrete at Doughton – 8 x 8m sight triangle for the southwest corner

- c. Private driveway (N/S) at White Elm – 3 x 3m sight triangles
- d. Private driveway (E/W) – 3 x 3m sightline protection at building corners for future connection to the N/S local road

Transportation Impact Study Comments

1. Please note that the TIS becomes public record upon submission and is available through the City's online PLANit application database, and it is expected that the applicant is relying on the information and recommendations of the report. As such, City Staff request removal of the Disclaimer page in the TIS Report.
2. Trip generation rates from the BA Group TIS Reports for 7800 Jane Street and 175 Millway Avenue were used to determine future trips from the proposed development. However, the context of the two BA Group sites are different to the proposed development in that this application is not adjacent the VMC Mobility Hub. Staff request that the trip generation rates be validated with surveys of sites within the VMC that are at a similar distance from the VMC Mobility Hub.
3. Figures 3-2 and 6-1 illustrate the future Maplecrete Road at White Elm Road intersection as an all-way stop-controlled intersection. Considering Maplecrete Road is a minor collector, it would be expected to be a two-way stop-controlled intersection with northbound and southbound being free flowing. Please confirm the intersection control required for this intersection under future conditions and provide adequate justification to support the recommendation.
4. Please confirm if left-turn lanes are warranted at the Maplecrete Road and White Elm Road intersection, and if so, confirm the impact on the on-street parking supply.
5. Tables 8-2 and 8-3 note the proposed supply of visitor and non-residential shared parking is 296 spaces whereas Table 8-1 notes a proposed supply of 269 spaces. Please confirm the correct number of shared visitor and non-residential parking spaces and revise the tables accordingly.
6. Please provide AutoTURN assessments for all on-street parking and/or passenger pick-up/drop-off (PPUDO) spaces, garage ramp accesses, loading spaces, emergency vehicles, etc.

Active Transportation Comments

Please see Active Transportation comments noted in the attached marked-up "*SUB1_Jan-19-2024 - Landscape Plan - 171 Maplecrete Rd_AT Comments*" drawings.

Site Plan Comments

1. Please remove the proposed lay-by parking on Maplecrete Road; only on-street parking in the curb lane should be illustrated on Maplecrete Road.
2. While the proposed site plan illustrates two jug handle-style PPUDO along the private N/S driveway, the design and the inclusion of temporary parking spaces may be problematic for fire and emergency access. Staff are in support of either a traffic circle with PPUDO spaces and/or parking along the outer lane (similar to the Expo towers) or the lay-by design proposed for the

southerly buildings; we are also open to other ideas that would safely accommodate vehicles, pedestrians crossing between the two buildings, and emergency vehicles.

3. The design of all lay-by parking areas should consider the locations of fire hydrants and any potential conflicts with access/operation for fire services.
4. The applicant should consolidate the number of parking garage entrances.

Conceptual Phasing Plan Comments

1. Please also provide the phasing plan for the proposed parking supply.

Please note that staff reserve the right to provide further comments upon receiving future submission materials. Please do not hesitate to contact the undersigned should you have any questions or require further clarification.

Sincerely,

Jillian Britto, P.Eng., PTOE

Transportation Project Manager, Development Engineering (VMC)

T: 905-832-8585 ext. 8410 | jillian.britto@vaughan.ca

Sandy Yang, RPP, MCIP

Active Transportation Infrastructure Specialist, Infrastructure Planning & Corporate Asset Management

T: 905-832-8585 ext. 8569 | sandy.yang@vaughan.ca

Rosalie Shan, P.Eng., MScE

Project Engineer – Transportation & Road Safety, Transportation and Fleet Management Services

T: 905-832-8577 ext. 6176 | rosalie.shan@vaughan.ca

Cc: Musa Deo, Manager, Development Engineering (VMC)

171 MAPLECRETE ROAD

171 Maplecrete Road
160 Doughton Road
140 Doughton Road
Block 2, Registered Plan 65M-4793
VAUGHAN, ON

Project Number
23-118

Client
171 MAPLECRETE LP

Consultant

Revisions

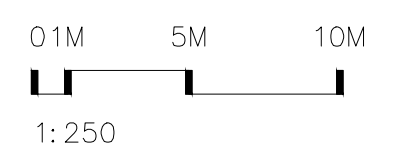
1 ISSUED FOR OHA APPLICATION 2024-01-19 ZW
ISSUED FOR XXX XXXX-MM-DD BY

Stamp

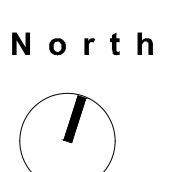


Key map

Scale



Phase
OPA PHASE



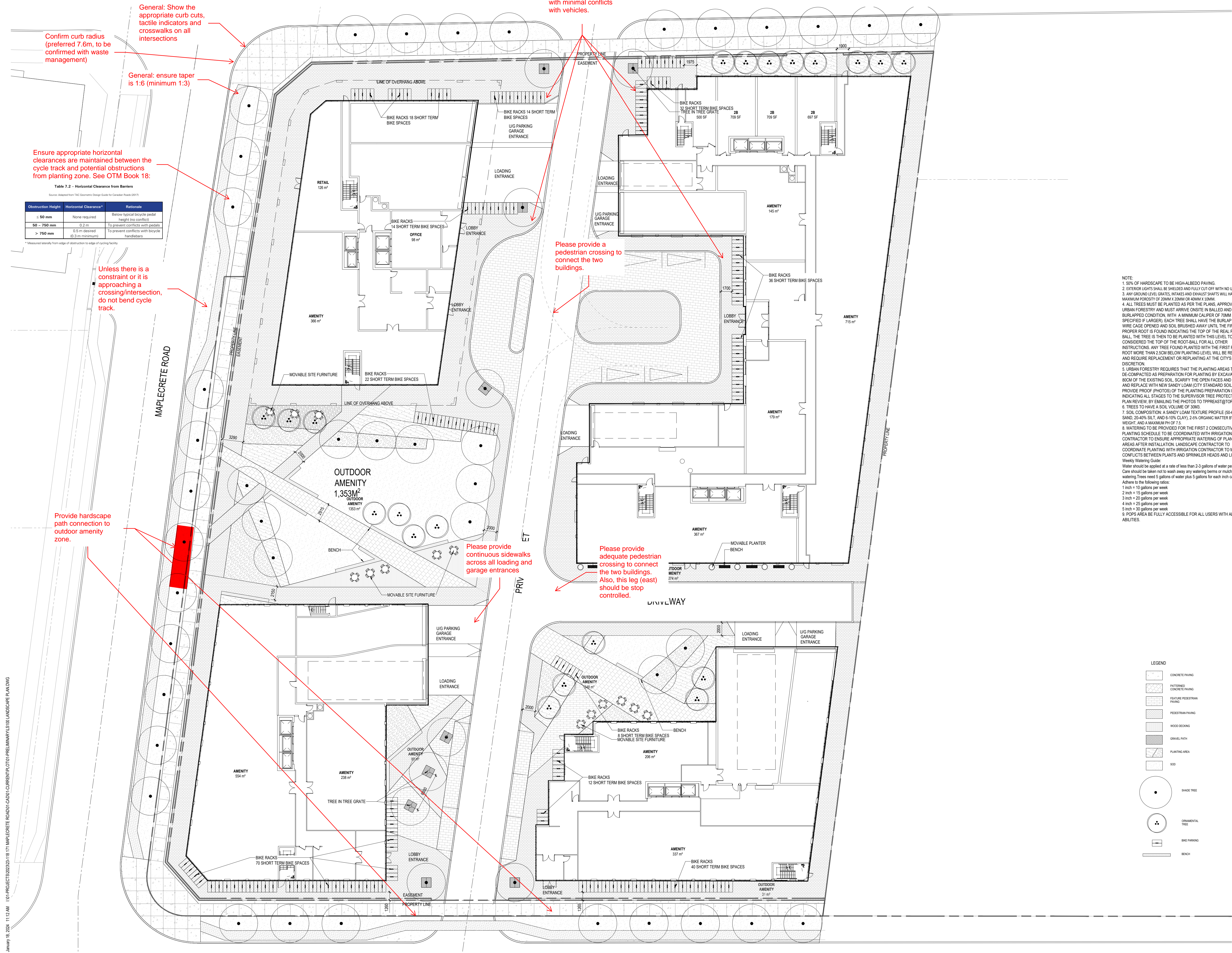
Drawing Name

LANDSCAPE PLAN

Sheet Number

NOT FOR CONSTRUCTION

LS100



Confirm curb radius (preferred 7.6m, to be confirmed with waste management)

General: Show the appropriate curb cuts, tactile indicators and crosswalks on all intersections

General: ensure taper is 1:6 (minimum 1:3)

Ensure appropriate horizontal clearances are maintained between the cycle track and potential obstructions from planting zone. See OTM Book 18:

Table 7.2 - Horizontal Clearance from Barriers

Obstruction Height	Horizontal Clearance*	Rationale
≤ 50 mm	None required	Below typical bicycle pedal height (no conflict)
50 - 750 mm	0.2 m	To prevent conflicts with pedals
> 750 mm	0.5 m desired (0.3 m minimum)	To prevent conflicts with bicycle handlebars

*Measured laterally from edge of obstruction to edge of cycling facility

Unless there is a constraint or it is approaching a crossing/intersection, do not bend cycle track.

Provide hardscape path connection to outdoor amenity zone.

Please provide continuous sidewalks across all loading and garage entrances

Please provide adequate pedestrian crossing to connect the two buildings. Also, this leg (east) should be stop controlled.

Please provide a pedestrian crossing to connect the two buildings.

to the bike spaces with minimal conflicts with vehicles.

NOTE:

- 50% OF HARDSCAPE TO BE HIGH-ALBEDO PAVING.
- EXTERIOR LIGHTS SHALL BE SHIELDED AND FULLY CUT-OFF WITH NO UPLIGHTING.
- ANY GROUND LEVEL GRATES, INTAKES AND EXHAUST SHAFTS WILL HAVE MAXIMUM POROSITY OF 20MM X 20MM OR 40MM X 10MM.
- ALL TREES MUST BE PLANTED AS PER THE PLANS, APPROVED BY URBAN FORESTRY AND MUST ARRIVE ON-SITE IN BALLED AND BURLAPPED CONDITION, WITH A MINIMUM CALIPER OF 70MM (OR AS SPECIFIED IF LARGER). EACH TREE SHALL HAVE THE BURLAP AND WIRE CAGE OPENED AND SOIL BRUSHED AWAY UNTIL THE FIRST PROPER ROOT IS FOUND INDICATING THE TOP OF THE REAL ROOT BALL. THE TREE IS THEN TO BE PLANTED WITH THIS LEVEL TO BE CONSIDERED THE TOP OF THE ROOT BALL FOR ALL OTHER INSTRUCTIONS. ANY TREE FOUND PLANTED WITH THE FIRST PROPER ROOT MORE THAN 2.5CM BELOW PLANTING LEVEL WILL BE REJECTED AND REQUIRE REPLACEMENT OR REPLANTING AT THE CITY'S DISCRETION.
- URBAN FORESTRY REQUIRES THAT THE PLANTING AREAS TO BE DE-COMPACTED AS PREPARATION FOR PLANTING BY EXCAVATING 80CM OF THE EXISTING SOIL, SCARIFY THE OPEN FACES AND BOTTOM AND REPLACE WITH NEW SANDY LOAM (CITY STANDARD SOIL). PLEASE PROVIDE PROOF (PHOTOS) OF THE PLANTING PREPARATION PROCESS INDICATING ALL STAGES TO THE SUPERVISOR TREE PROTECTION AND PLAN REVIEW. BY EMAILING THE PHOTOS TO TPRREST@TORONTO.CA
- TREES TO HAVE A SOIL VOLUME OF 30M3.
- SOIL COMPOSITION: A SANDY LOAM TEXTURE PROFILE (50-60% SAND, 20-40% SILT, AND 0-10% CLAY), 2-5% ORGANIC MATTER BY DRY WEIGHT, AND A MAXIMUM PH OF 7.5.
- WATERING TO BE PROVIDED FOR THE FIRST 2 CONSECUTIVE YEARS. PLANTING SCHEDULE TO BE COORDINATED WITH IRRIGATION CONTRACTOR TO ENSURE APPROPRIATE WATERING OF PLANTING AREAS AFTER INSTALLATION. LANDSCAPE CONTRACTOR TO COORDINATE PLANTING WITH IRRIGATION CONTRACTOR TO MINIMIZE CONFLICTS BETWEEN PLANTS AND SPRINKLER HEADS AND LINES. Weekly Watering Guide: Water should be applied at a rate of less than 2-3 gallons of water per minute. Care should be taken not to wash away any watering berms or mulch when watering. Trees need 5 gallons of water plus 5 gallons for each inch caliper. Adhere to the following ratios:
1 inch = 10 gallons per week
2 inch = 15 gallons per week
3 inch = 20 gallons per week
4 inch = 25 gallons per week
5 inch = 30 gallons per week
- POPS AREA BE FULLY ACCESSIBLE FOR ALL USERS WITH ALL ABILITIES.

(INSERT COMPANY LETTERHEAD/LOGO)

(INSERT DATE)

Development Engineering Department
City of Vaughan
2141 Major Mackenzie Drive
Vaughan, ON L6A 1T1

Re: Reliance Letter for (INSERT FULL SITE ADDRESS OR LEGAL DESCRIPTION, AND PROJECT NAME AND REFERENCE NUMBER)

(INSERT NAME OF YOUR COMPANY) (the “Consultant”) understands that **(INSERT NAME OF THE OWNER/PROPONENT)** (the “Client”) is seeking approval of their development application from the City of Vaughan (the “City”) regarding the above-referenced property (the “Site”). The Client has requested that the City accept the following report(s), which was/were prepared by the Consultant for the exclusive benefit and use of the Client:

- **(INSERT THE TITLES OF ALL APPLICABLE ENVIRONMENTAL REPORTS, REFERENCE NUMBERS, AND DATES)**

The Consultant agrees that the City and its peer reviewer may use and rely on the report(s) as if the report(s) had been prepared for the use and benefit of the City notwithstanding any statement to the contrary contained in the report(s) and excluding any limitations agreed to by the Client. The Consultant further agrees that in the case of any inconsistency between this Reliance Letter and any limitations within any report(s) provided to the City, the provisions in this Reliance Letter shall prevail.

The Consultant agrees that the City will utilize the report(s) identified herein for the purposes of assessing the environmental risk of the Site. The Consultant certifies that the report(s) was/were prepared in accordance with the requirements of Ontario Regulation (O.Reg.) 153/04 (as amended) by or under the supervision of a Qualified Person (QP) as defined under O.Reg. 153/04 (as amended) and was/were prepared in accordance with environmental laws and regulations applicable at the time of the investigation.

The Consultant has appended to this Reliance Letter evidence of insurance, indicating Professional Liability insurance coverage of at least \$2,000,000 per claim.

Yours very truly,

(INSERT SIGNATURE OF QUALIFIED PERSON, AS DEFINED UNDER O. REG. 153/04 (AS AMENDED))

(INSERT YOUR NAME, FOLLOWED BY PROFESSIONAL DESIGNATION) (I.E., P. ENG OR P. GEO)

(INSERT YOUR TITLE)

(INSERT YOUR CONTACT INFORMATION)

I have the authority to bind the corporation of the Consultant.



THIS CERTIFICATE OF INSURANCE IS TO CERTIFY TO:
 THE CORPORATION OF THE CITY OF VAUGHAN
 2141 MAJOR MACKENZIE DR., VAUGHAN, ON, L6T 1A1

That Policy(ies) of Insurance as herein described have been issued to the insured named below and are in force on the indicated dates.

CERTIFICATE TYPE:	Blanket	Covering the Named Insured for all work or activities performed for the City of Vaughan and/or for agreements with the City of Vaughan and/or for operations conducted within the City of Vaughan
	Project / Service Specific Agreement	City File No. and/or Description:
Insured:		Address:

#	TYPE OF INSURANCE	POLICY NO.	EFFECTIVE dd/mm/yyyy	EXPIRY dd/mm/yyyy	LIMIT (if other than CDN \$ indicate)	Deductible
1	COMMERCIAL GENERAL LIABILITY (occurrence form)				\$ per occurrence \$ general aggregate \$ completed operations	\$
	Non-Owned Auto				\$	\$
	Employer's Liability				\$	\$
	Sudden & Accidental Pollution				\$	\$
2	AUTOMOBILE LIABILITY				\$	\$
3	UMBRELLA LIABILITY				\$ per occurrence \$ general aggregate	\$
4	GARAGE LIABILITY				\$ per occurrence \$ comprehensive \$ collision	\$
5	ALL RISK PROPERTY				\$	\$
6	BOILER AND MACHINERY				\$	\$
7	CRIME				\$ employee dishonesty	\$
8	CONTRACTOR'S EQUIPMENT				\$	\$
9	PROFESSIONAL LIABILITY (Errors & Omissions)				\$ per claim \$ general aggregate	\$
10	ENVIRONMENTAL IMPAIRMENT				\$ per claim / occurrence \$ aggregate	\$
11	BUILDER'S RISK / INSTALLATION FLOATER				\$	\$
12	WRAP UP LIABILITY				\$ per occurrence \$ general aggregate	\$
13	DIRECTOR'S & OFFICER'S LIABILITY				\$ per claim \$ general aggregate	\$
14	AVIATION LIABILITY				\$ per claim \$ general aggregate	\$

15	CYBER LIABILITY					
	Network & Information Security (3rd party) Liability				\$ per claim	\$
					\$ general aggregate	\$
	Privacy Liability				\$ per claim	\$
					\$ general aggregate	\$
16	PERSONAL LIABILITY				\$ per claim	\$
					\$ general aggregate	\$
17	EXCESS PERSONAL LIABILITY				\$ per claim	\$
					\$ general aggregate	\$

REQUIRED PROVISIONS:

- Commercial General Liability policy is extended to include Personal Injury Liability, Contractual Liability, Products - Completed Operations, Contingent Employer's Liability, Cross Liability and Severability of Interest.
- It is agreed and understood that the deductible or self insured retention (SIR) arranged between the Named Insured and the Insurers must be declared herein and is subject to approval by The City of Vaughan. It is further understood and agreed that claims arising out of the operations of the above mentioned project, which fall within the deductible or SIR limit, are the sole responsibility of the Named Insured.
- If the insurance provided under the said policy(ies) is cancelled or if coverage is reduced, the Insuring Company will give thirty (30) days written notice of cancellation to the address above.
- The policy(ies) identified above shall apply as primary insurance and not excess to any other insurance available to the Additional Insured as set out below.

Required Additional Insured(s) with respect to General Liability. It is understood and agreed that entity(ies) identified below is/are added as an Additional Insured(s) to the Commercial General Liability and Umbrella Liability Policies with respect to liability arising out of the operations of the Named Insured performed / supplied / conducted for/to the City of Vaughan.

*The City of Vaughan Other _____

The Regional Municipality of York Other _____

Toronto and Region Conservation Authority (TRCA) Other _____

*The City of Vaughan and its respective directors, officers, council members, boards and employees, including; Vaughan Fire and Rescue Services, the Vaughan Public Library Board, Vaughan Hydro Inc., Tourism Vaughan Corporation and Hydro Vaughan Energy Corporation.

DATE ISSUED	NAME & ADDRESS	#
	OF INSURANCE	#
	COMPANY(IES)	#
	Indicate line #s if	#
	multiple insurers	#

NOTICE AND RECEIPT. Enter the information for the respective City department below to ensure receipt of the certificate by the appropriate City contact. Policy changes affecting the insurance requirements outlined in the respective Project/Service Agreement are to be provided in writing in accordance with item 3.

Contact:	Department:
Email:	Phone No:

CERTIFICATION I certify that the insurance is in effect as stated in this certificate and that I have authorization to issue this certificate for and on behalf of the insurer(s). This certificate is valid until the expiration date(s) shown unless notice is given in writing in accordance with item 3.

Broker Name & Address Tel. No.: E-mail Contact Address:	SIGNATURE AND STAMP OF CERTIFYING OFFICIAL
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The City of Vaughan reserves the right to contact your broker or insurer directly to obtain a renewal certificate on your behalf should your insurance coverage expire during the term of your contract with the City of Vaughan

THIS FORM MUST BE COMPLETED BY AND AUTHORIZED INSURANCE BROKER OR INSURANCE PROVIDER.



ENVIRONMENTAL REMEDIAL ACTION PLAN (RAP) REQUIREMENTS

The following checklist ensures proponents submit an environmental Remedial Action Plan (RAP), where required, containing complete and comprehensive information. The checklist shall be completed and submitted along with the RAP to the City for review and acceptance.

The checklist is based on recommended Ministry of the Environment (MOE) guidelines as well as requirements outlined under *O. Reg. 153/04* (as amended). It is not meant to be an exhaustive list and additional requirements may be identified by the City in the course of the environmental documents review.

REMEDIAL ACTION PLAN CHECKLIST	
1. Description of the proposed remediation objective and strategy to reduce/eliminate concentrations of contaminants exceeding the applicable MOE site condition standards for soil, ground water, and/or sediment (e.g., excavation and soil removal; offsite treatment; in-situ treatment, containment/isolation etc.).	<input type="checkbox"/>
2. Site Plan Figure(s) showing locations and concentrations of contaminants exceeding the applicable MOE site condition standards for soil, ground water, and/or sediment.	<input type="checkbox"/>
3. Site Plan Figure(s) showing the estimated vertical and horizontal extent of contamination on the site.	<input type="checkbox"/>
4. The estimated quantity of contaminated soil/ground water/sediment required to be remediated (in m ³ or tonnes and/or litres).	<input type="checkbox"/>
5. The estimated quantity of imported clean fill material required following remediation (if applicable) (in m ³ or tonnes).	<input type="checkbox"/>
6. A management plan outlining how the remediation of contaminated soil / ground water/sediment will be undertaken along with associated onsite activities (e.g., provide details on segregation of soils, stockpiling, offsite removal, onsite screening of contaminants, long-term monitoring requirements, onsite supervision schedule, management of impacted groundwater, dust, noise and traffic issues, etc.).	<input type="checkbox"/>
7. The confirmatory sampling strategy detailing the media, the specific analyses to be undertaken on the media, and the number and location of confirmatory samples to be submitted for chemical analysis following implementation of the remediation strategy. If excavation is proposed, confirmatory sampling should follow the minimum floor and wall sampling requirements for excavations as presented in <i>O. Reg. 153/04, Schedule E- Part V, Table 3</i> (as amended). If stockpiles are present on site, confirmatory sampling follow the minimum stockpile sampling frequency as presented in <i>O. Reg. 153/04, Schedule E- Part V, Table 2</i> (as amended).	<input type="checkbox"/>
8. If import of clean fill material is required for site restoration, the proposed confirmatory sampling strategy for imported clean fill material to confirm it meets the applicable MOE site condition standards.	<input type="checkbox"/>
9. The Contingency Remediation Plan in the event confirmatory sampling results indicate concentrations exceeding the applicable MOE site condition standards.	<input type="checkbox"/>
10. The Quality Assurance/ Quality Control Plan and Health and Safety Plan to be implemented during remedial activities.	<input type="checkbox"/>
11. The Communications and Reporting Plan to the City and other regulatory agencies.	<input type="checkbox"/>
12. The approximate timelines and/or stages of the implemented remediation strategy.	<input type="checkbox"/>
13. Description of regulatory agency approval requirements and/or other responsible authorities , if applicable (e.g., MOE Environmental Compliance Approval (ECA) for discharges such as air/noise, water, waste treatment, TRCA approval, etc.).	<input type="checkbox"/>
14. Proponent's Qualified Person (QP) Sign-off and Owner's Certification of Implementation of the RAP (include title, designations and stamps, signatures, and printed names).	<input type="checkbox"/>

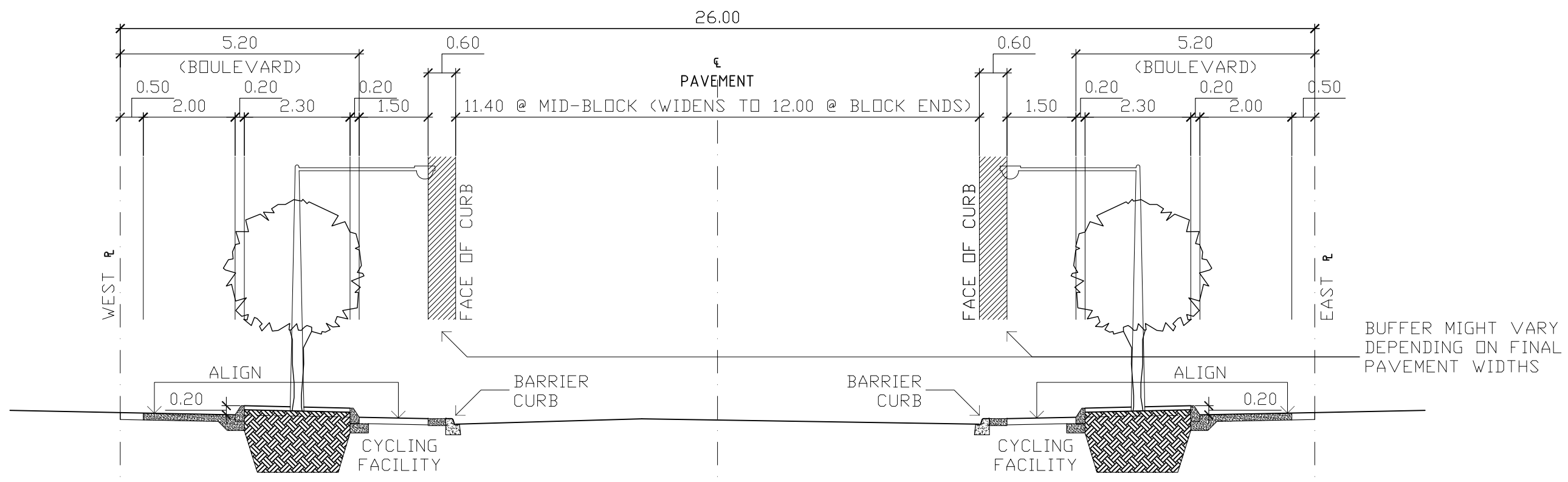
Checklist Completed by (print name): _____

Signature: _____

Company: _____

Date: _____

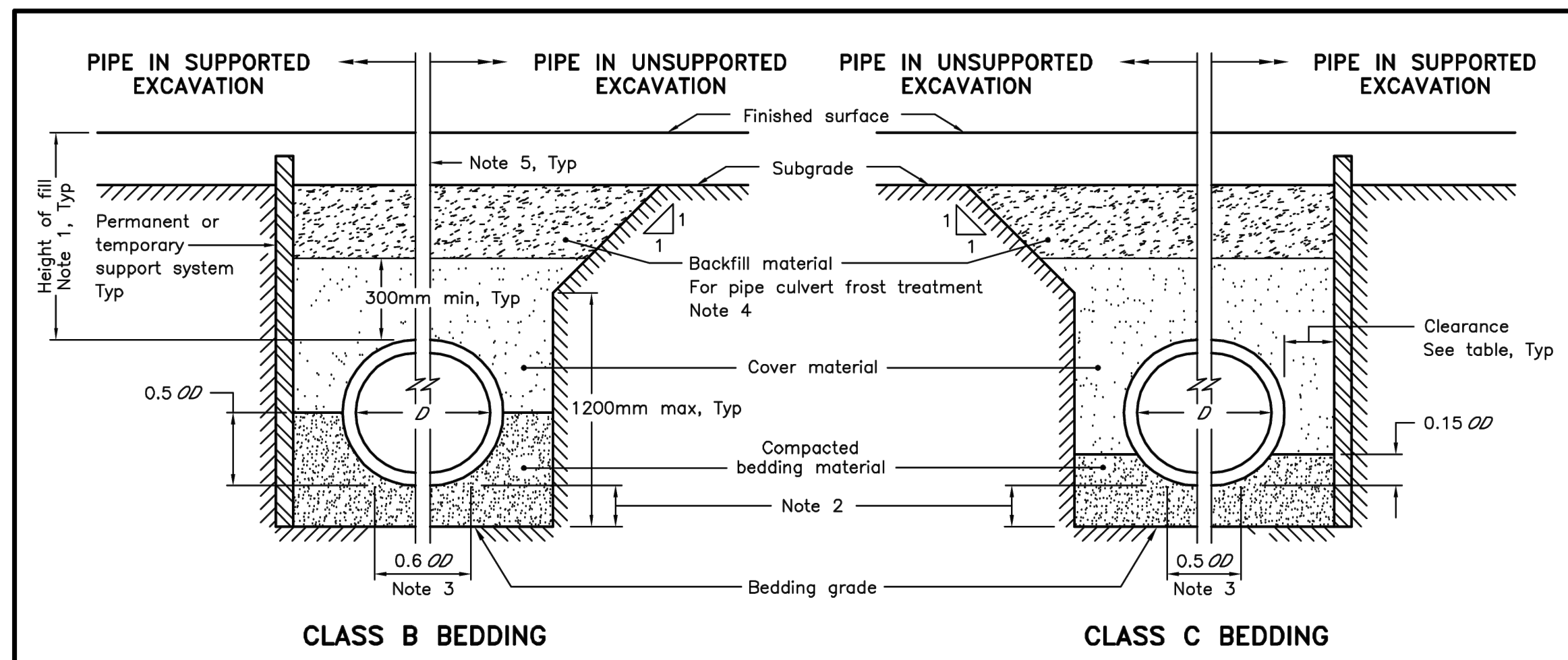
INFORMATION ONLY



COMMERCE WAY @ BLOCK 3 (26.00 ROW)
 JANUARY 24 2020 - FOR REFERENCE ONLY

NOTE: BOULEVARD WIDTHS HAVE BEEN REDUCED TO HELP CREATE CYCLING FACILITY BUFFERS. OTHERWISE, BOULEVARD WIDTHS SHOULD BE 5.5m AS DENOTED ON THE VMC SECONDARY PLAN.

BUFFER MIGHT VARY DEPENDING ON FINAL PAVEMENT WIDTHS



NOTES:
 1 Height of fill is measured from the finished surface to top of pipe.
 2 The minimum bedding depth below the pipe shall be 0.15D. In no case shall this dimension be less than 150mm or greater than 300mm.
 3 The pipe bed shall be compacted and shaped to receive the bottom of the pipe.
 4 Pipe culvert frost treatment shall be according to OPSD 803.030 and 803.031.
 5 Condition of excavation is symmetrical about centreline of pipe.
 A Soil types as defined in the Occupational Health and Safety Act and Regulations for Construction Projects.
 B All dimensions are in metres unless otherwise shown.

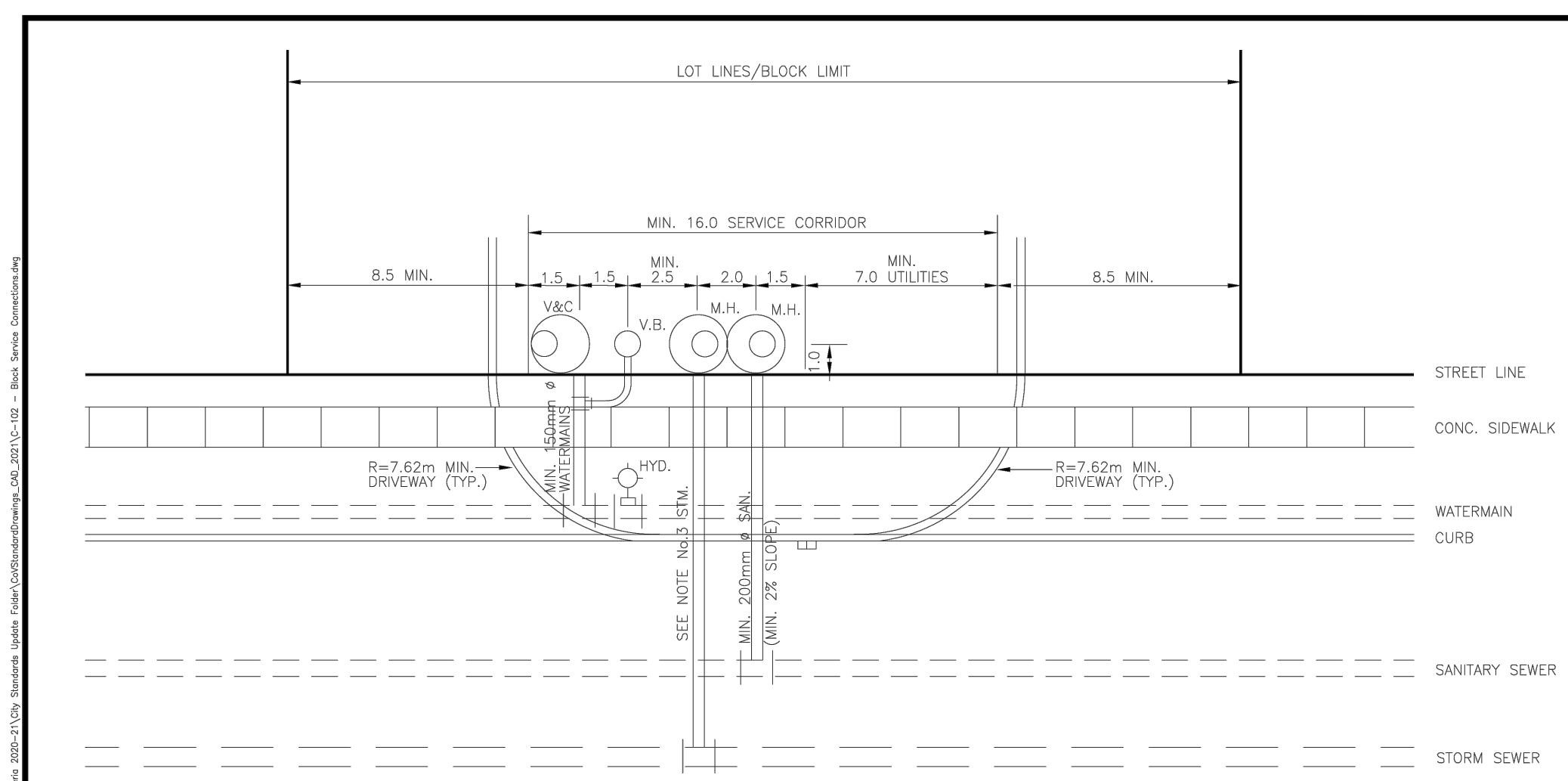
LEGEND:
 D – Inside diameter
 OD – Outside diameter

Pipe Inside Diameter mm	Clearance mm
900 or less	300
Over 900	500

ONTARIO PROVINCIAL STANDARD DRAWING Nov 2015 Rev 3

RIGID PIPE BEDDING, COVER, AND BACKFILL
 TYPE 1 OR 2 SOIL – EARTH EXCAVATION

OPSD 802.030

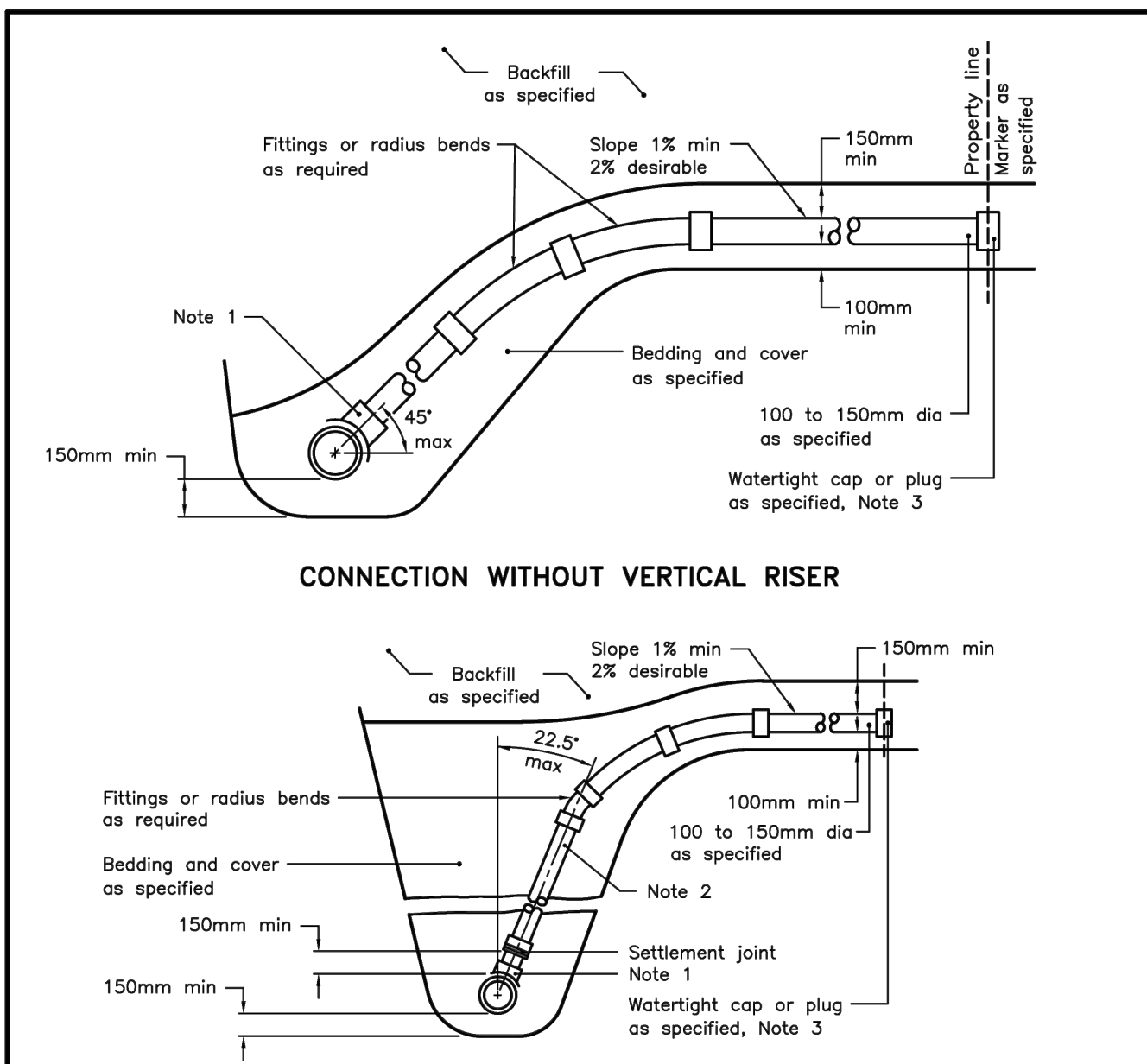


VAUGHAN
 CITY OF VAUGHAN ENGINEERING STANDARD
BLOCK SERVICE CONNECTIONS

NOT TO SCALE DESIGNED: DATE: REC. 2020 STD. DWG. C-102

NOTES:
 1. ABOVE GROUND UTILITIES TO BE LOCATED MINIMUM 1.0m FROM CURBS, SIDEWALK AND DRIVEWAYS.
 2. MAINTENANCE HOLES AND VALVE CHAMBERS FOR SERVICE CONNECTIONS TO BE LOCATED AT 1.0m OFF STREET LINE ON PRIVATE PROPERTY.
 3. STORM SEWER CONNECTIONS SHALL BE SIZED BASED UPON STORM WATER MANAGEMENT REQUIREMENTS. SEE STD. C-104.
 4. WATER SERVICE CONNECTIONS SHALL BE AS DETAILED ON STANDARD DRAWING C-103.
 5. CB TO BE LOCATED WITHIN SERVICE CORRIDOR.

INFORMATION ONLY

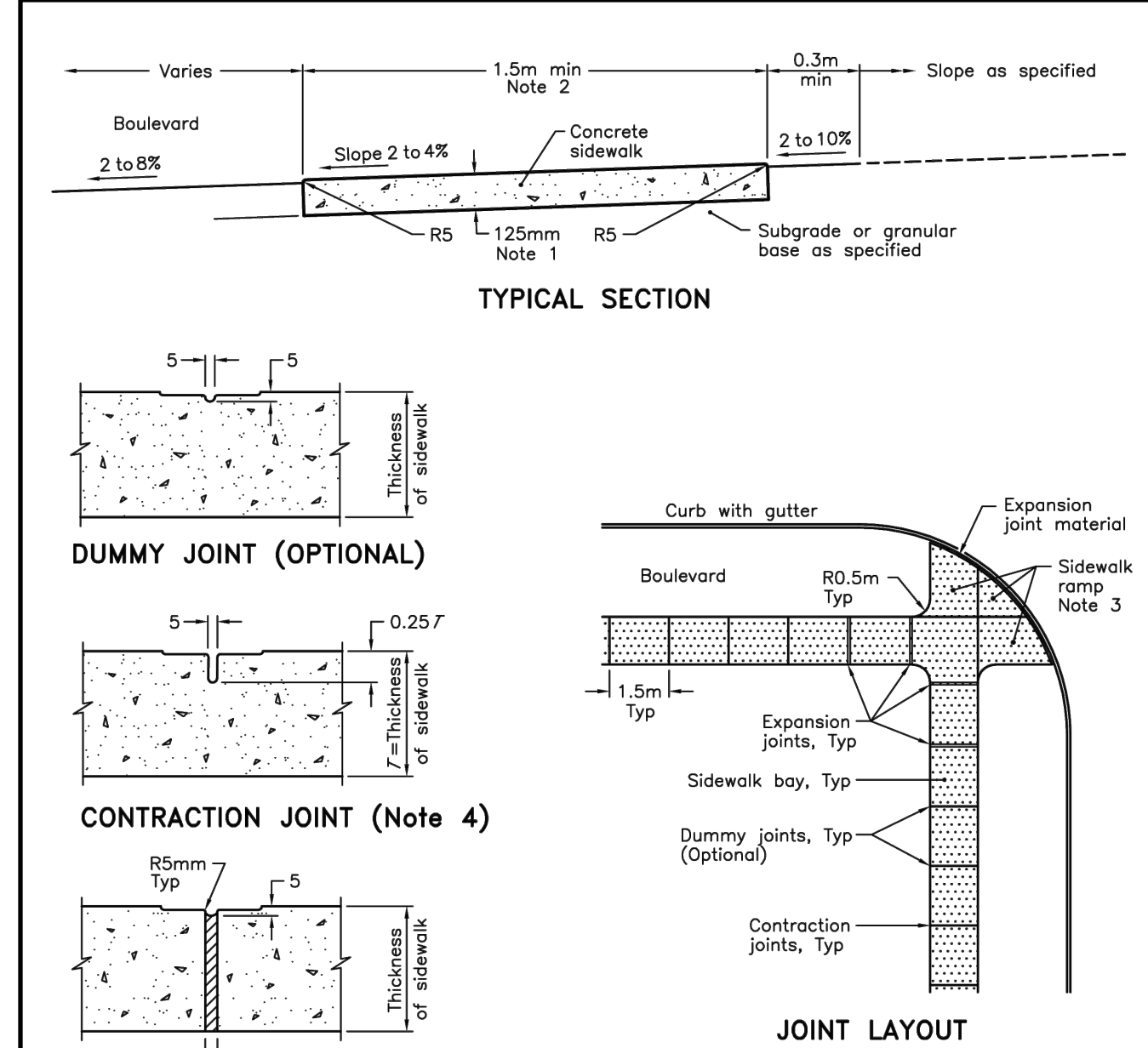


NOTES:
 1 Sewer service connections to the main pipe sewer shall be made using factory made tees, strap-on-saddles, or other approved saddles.
 2 Vertical risers shall be as specified.
 3 Cap or plug at property line shall be adequately braced.
 A Maintenance holes shall be used at the main sewer to connect service connections greater than 200mm.
 B For new construction, saddles shall be installed on the main pipe before that pipe is laid.
 C Approved cut-in tool shall be used for field made connections.
 D All dimensions are in millimetres unless otherwise shown.

ONTARIO PROVINCIAL STANDARD DRAWING Nov 2021 Rev 4

SEWER SERVICE CONNECTIONS FOR MAIN PIPE SEWER

OPSD 1006.010

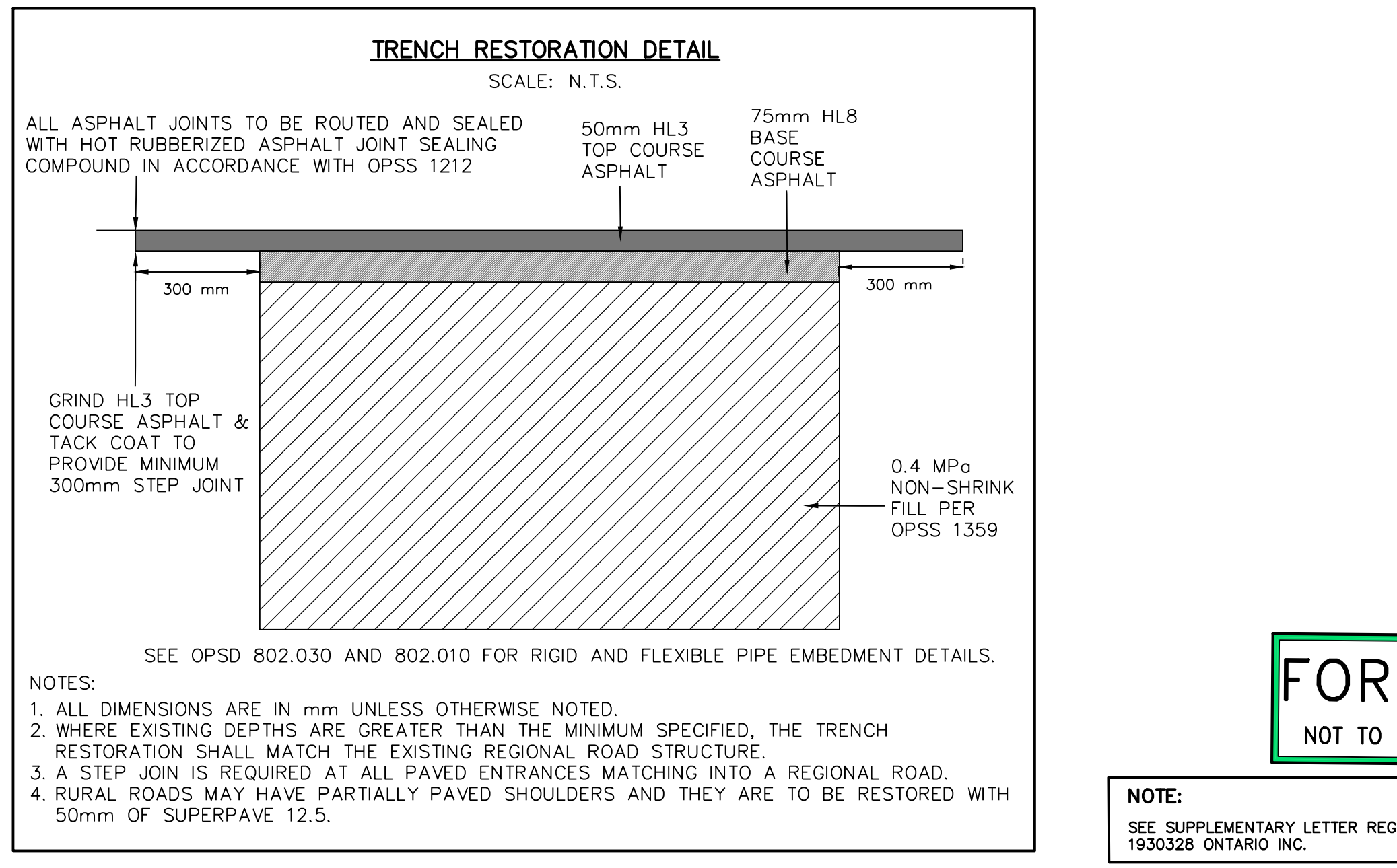
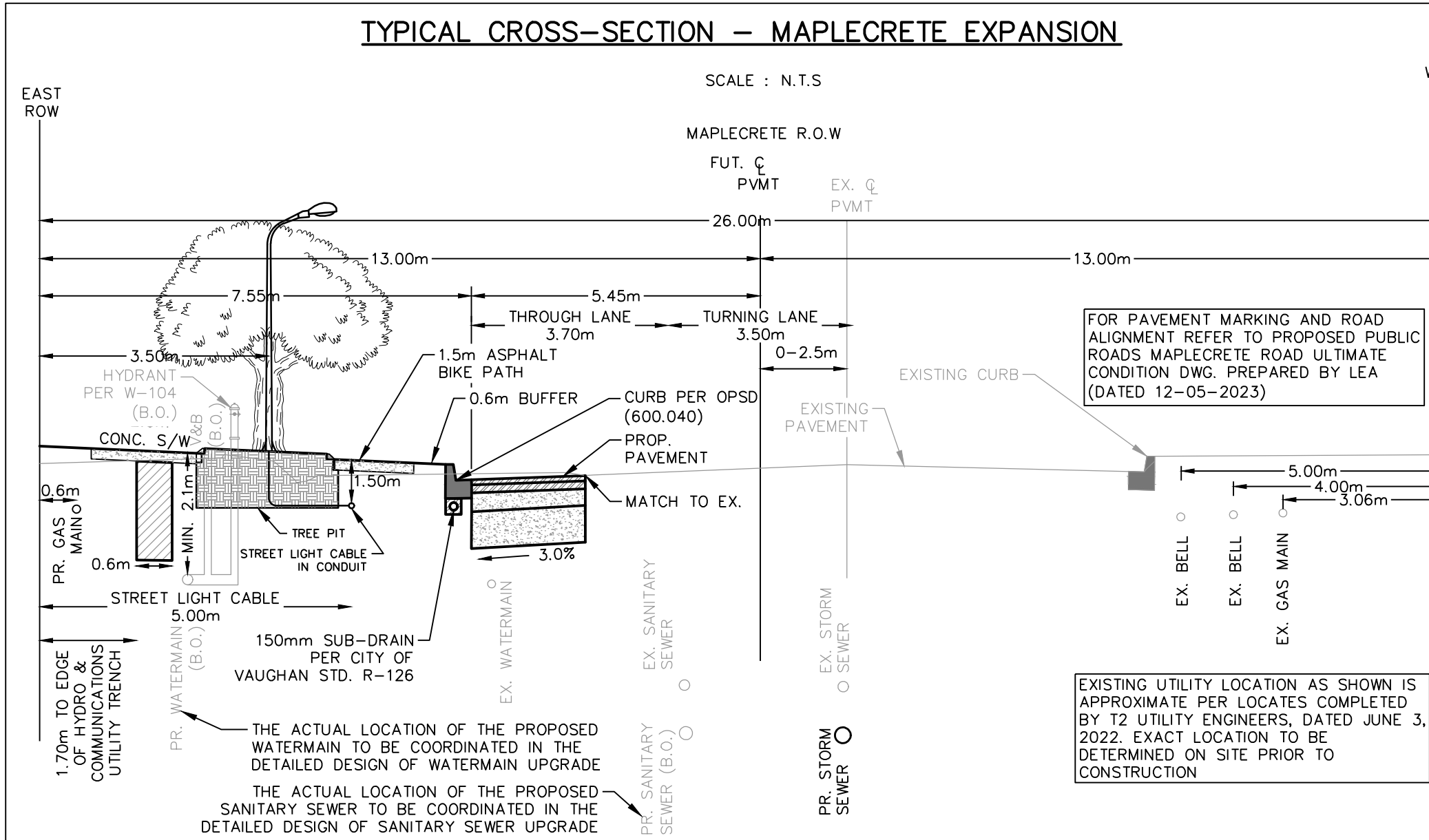


NOTES:
 1 Sidewalk thickness at residential driveways and adjacent to curb shall be 150mm. At commercial and industrial driveways, the thickness shall be 200mm.
 2 Sidewalk width shall be wider when specified.
 3 This OPSD shall be read in conjunction with OPSD 310.030, 310.031, 310.033, and 310.039.
 4 Contraction Joint may be tooled or sawcut.
 A All dimensions are in millimetres unless otherwise shown.

ONTARIO PROVINCIAL STANDARD DRAWING Nov 2019 Rev 3

CONCRETE SIDEWALK

OPSD 310.010



FOR APPROVAL
 NOT TO BE USED FOR CONSTRUCTION

NOTE:
 SEE SUPPLEMENTARY LETTER REGARDING SCOPE OF WORK PREPARED BY 1930328 ONTARIO INC.



APPROVED AS TO FORM IN RELIANCE, UPON THE PROFESSIONAL SKILL AND ABILITY OF THE CONSULTING ENGINEERS AS TO DESIGN AND SPECIFICATIONS

DIRECTOR OF DEVELOPMENT ENGINEERING AND INFRASTRUCTURE PLANNING

DATE: REGION FILE NO.: 19T-18V005 (SUBP.18.V0019) SITE PLAN DEVELOPMENT CITY FILE NO.: DA.18.037

Engineer: J. M. SCOTT 100214864
 Engineer: D. LOZENKO 100213318

No.	ISSUE / REVISION	DATE
4	PERFECT SUBMISSION	2023/JUN/20
3	ISSUED FOR 4TH SUBMISSION	2023/MAY/17
2	ISSUED FOR 3RD SUBMISSION TO CITY	2023/FEB/24
1	ISSUED FOR 2ND SUBMISSION TO CITY	2022/OCT/11
0	ISSUED FOR 1ST SUBMISSION TO CITY	2022/JUN/17

TEMPORARY BENCHMARKS
 BEARINGS ARE UTM GRID, DERIVED FROM OBSERVED REFERENCE POINTS 1 AND 2. BY REAL TIME NETWORK (RTN) OBSERVATIONS, UTM ZONE 17, NAD83 (ORIGINAL). OBSERVED REFERENCE POINTS (ORP): UTM ZONE 17, NAD83 (ORIGINAL). COORDINATES TO URBAN ACCURACY PER SEC. 14(2) OF O REG 216/10
 ORP 1 – NORTHING: 4850162.198 EASTING: 619083.869
 ORP 2 – NORTHING: 4850000.680 EASTING: 619114.676

SCHAEFFER DZALDOV BENNET LTD.
 SUE SURVEY ALONG MAPLECRETE RD. AND DOUGHTON RD. BY T2 UTILITY ENGINEERS ATED 06/06/22 PROJECT NO. 61002331

1. THIS DRAWING IS THE EXCLUSIVE PROPERTY OF C.F. CROZIER & ASSOCIATES INC. AND THE REPRODUCTION OF ANY PART WITHOUT PRIOR WRITTEN CONSENT OF THIS OFFICE IS STRICTLY PROHIBITED.
 2. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS, LEVELS, AND DATUMS ON SITE AND REPORT ANY DISCREPANCIES OR OMISSIONS TO THIS OFFICE PRIOR TO CONSTRUCTION.
 3. THIS DRAWING IS TO BE READ AND UNDERSTOOD IN CONJUNCTION WITH ALL OTHER PLANS AND DOCUMENTS APPLICABLE TO THIS PROJECT.
 4. DO NOT SCALE THE DRAWINGS.
 5. ALL EXISTING UNDERGROUND UTILITIES TO BE VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO CONSTRUCTION.

VAUGHAN

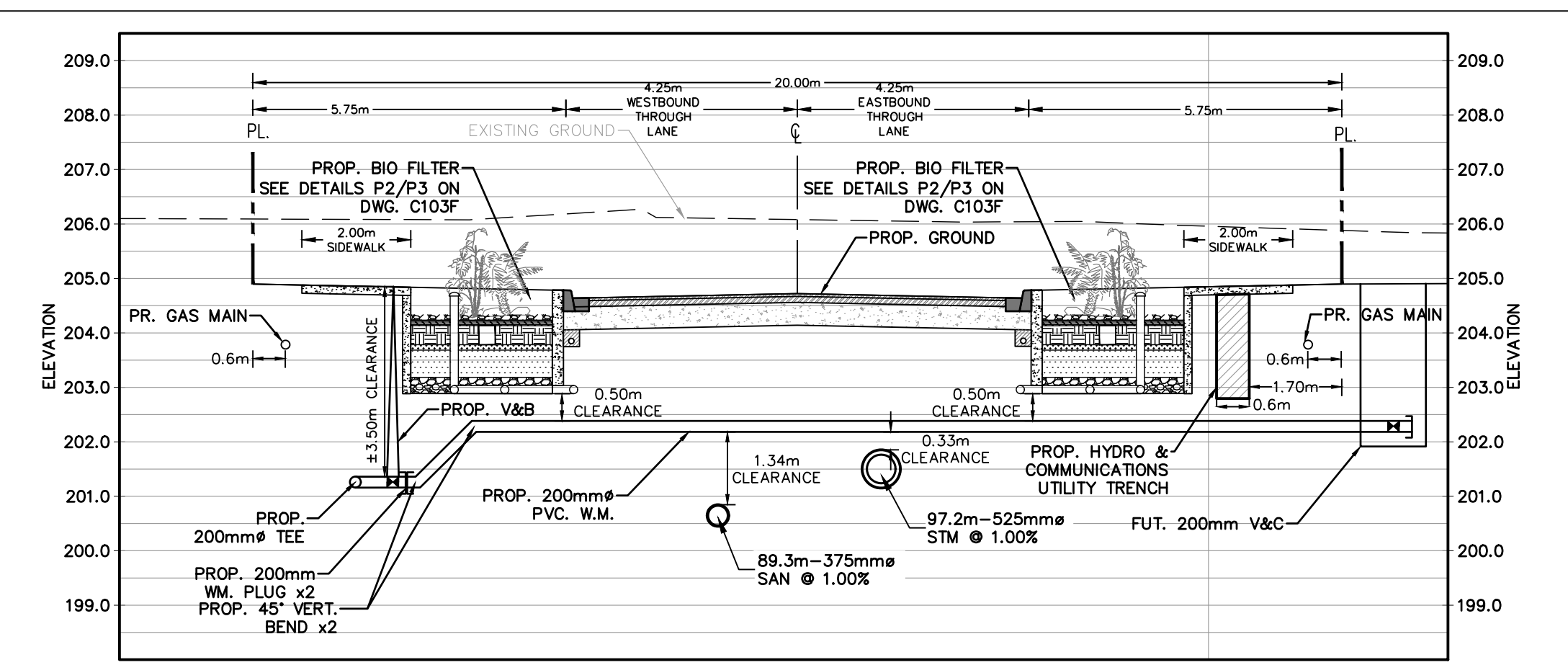
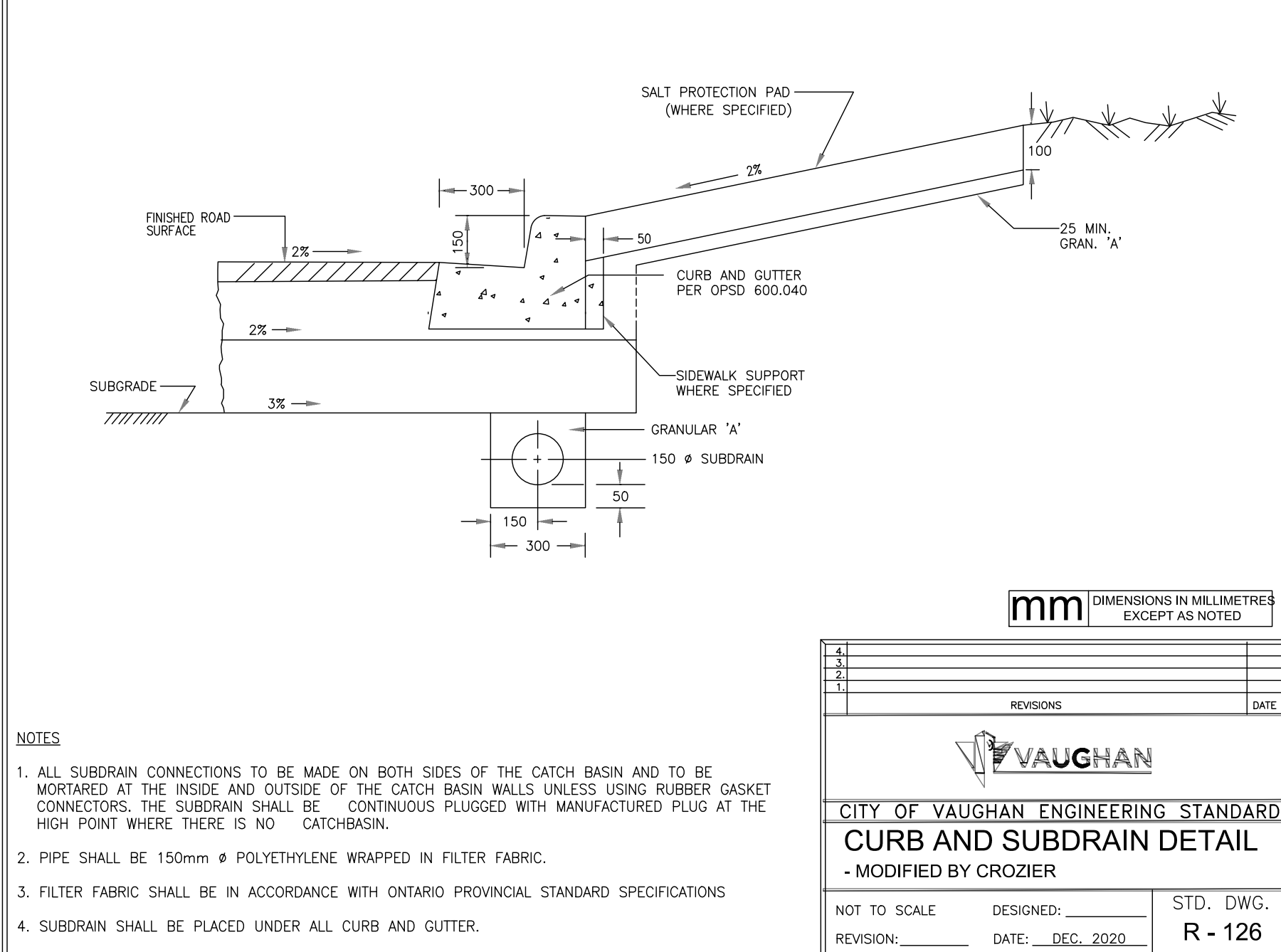
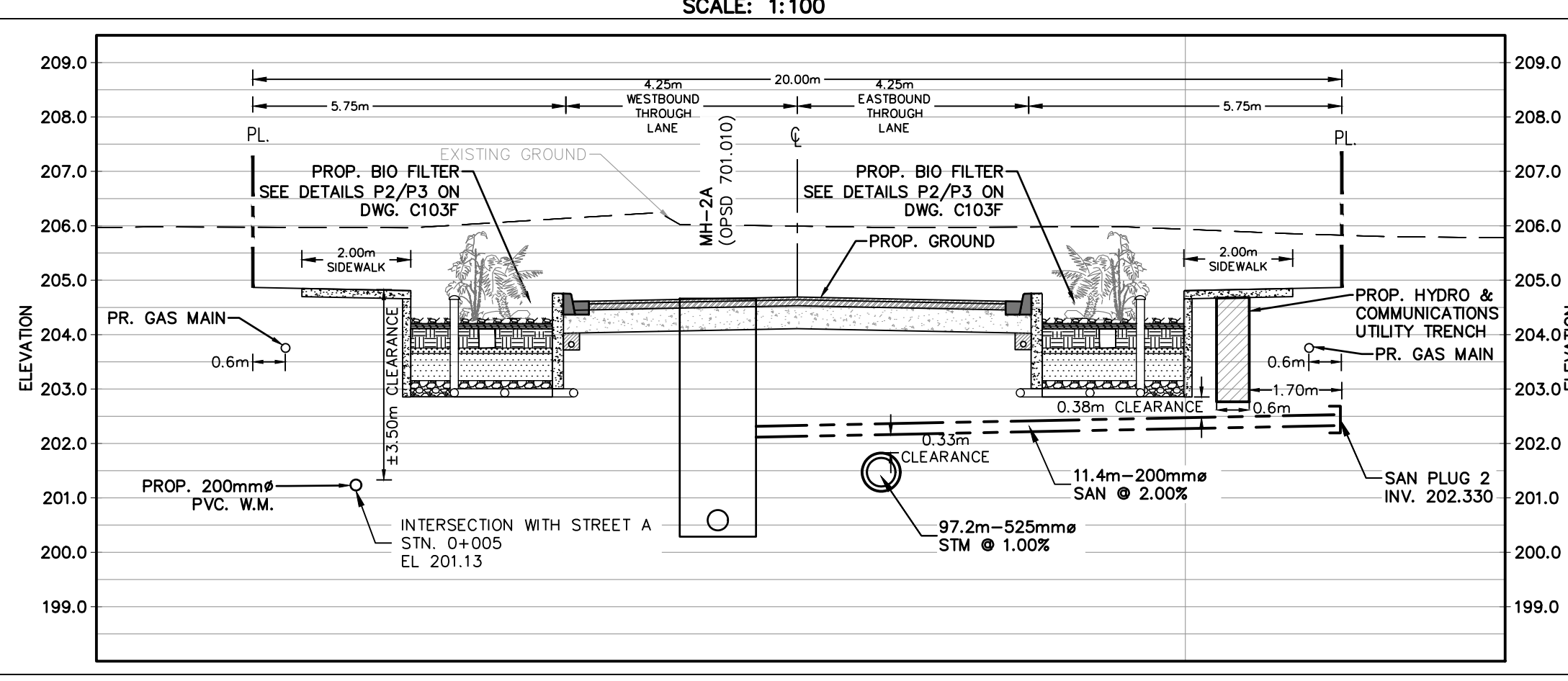
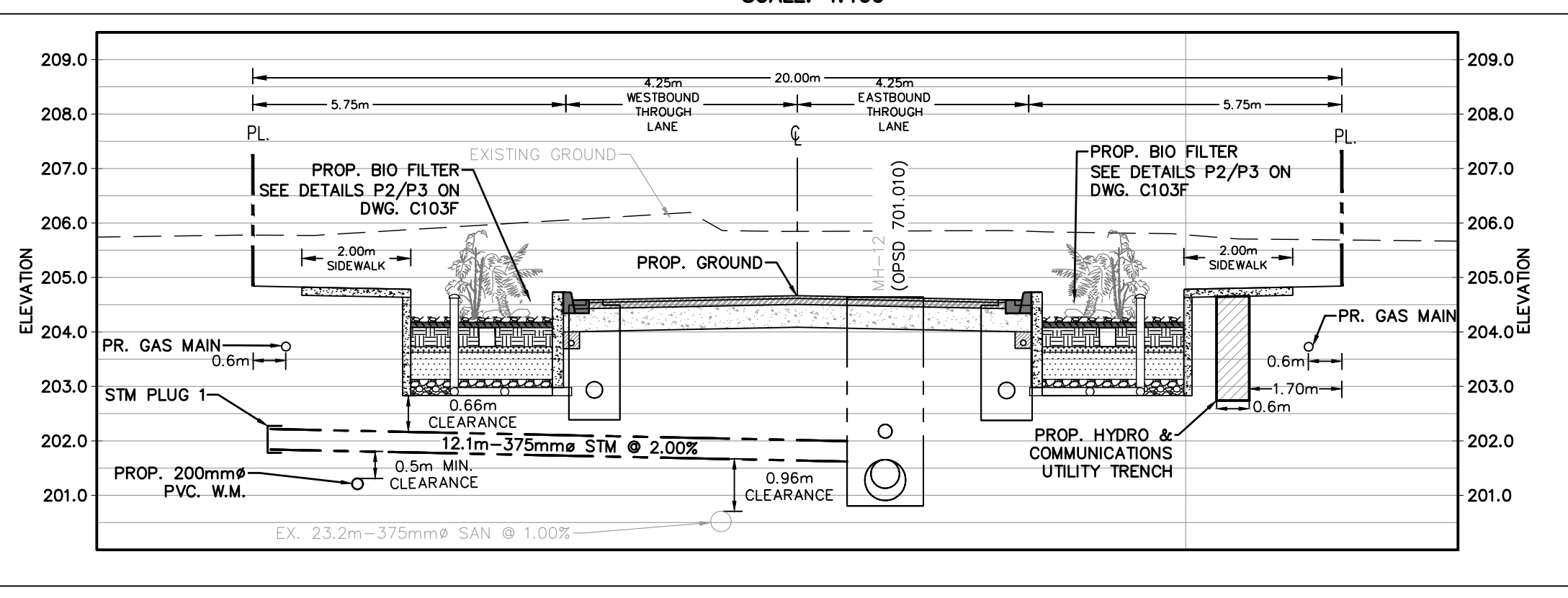
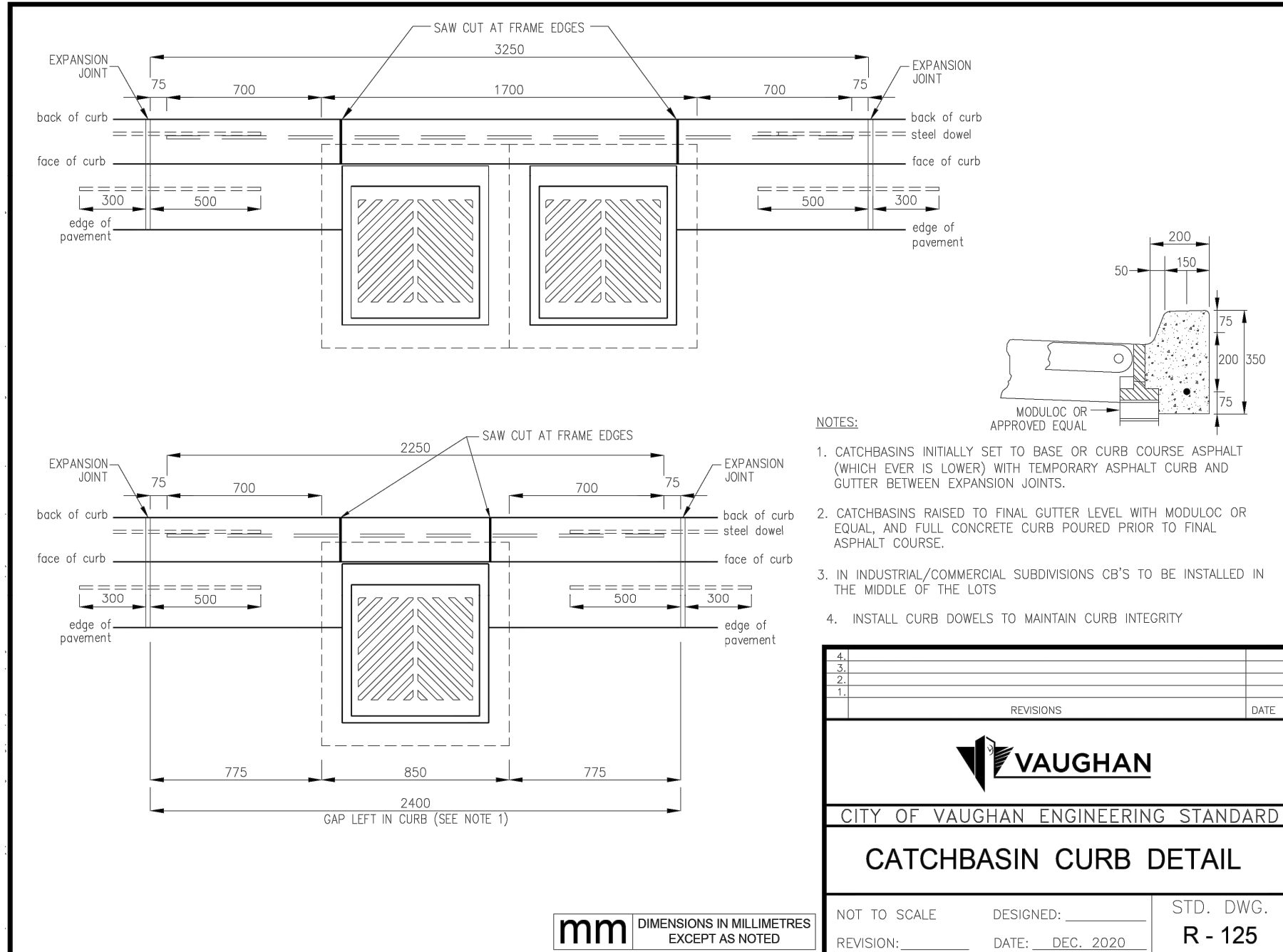
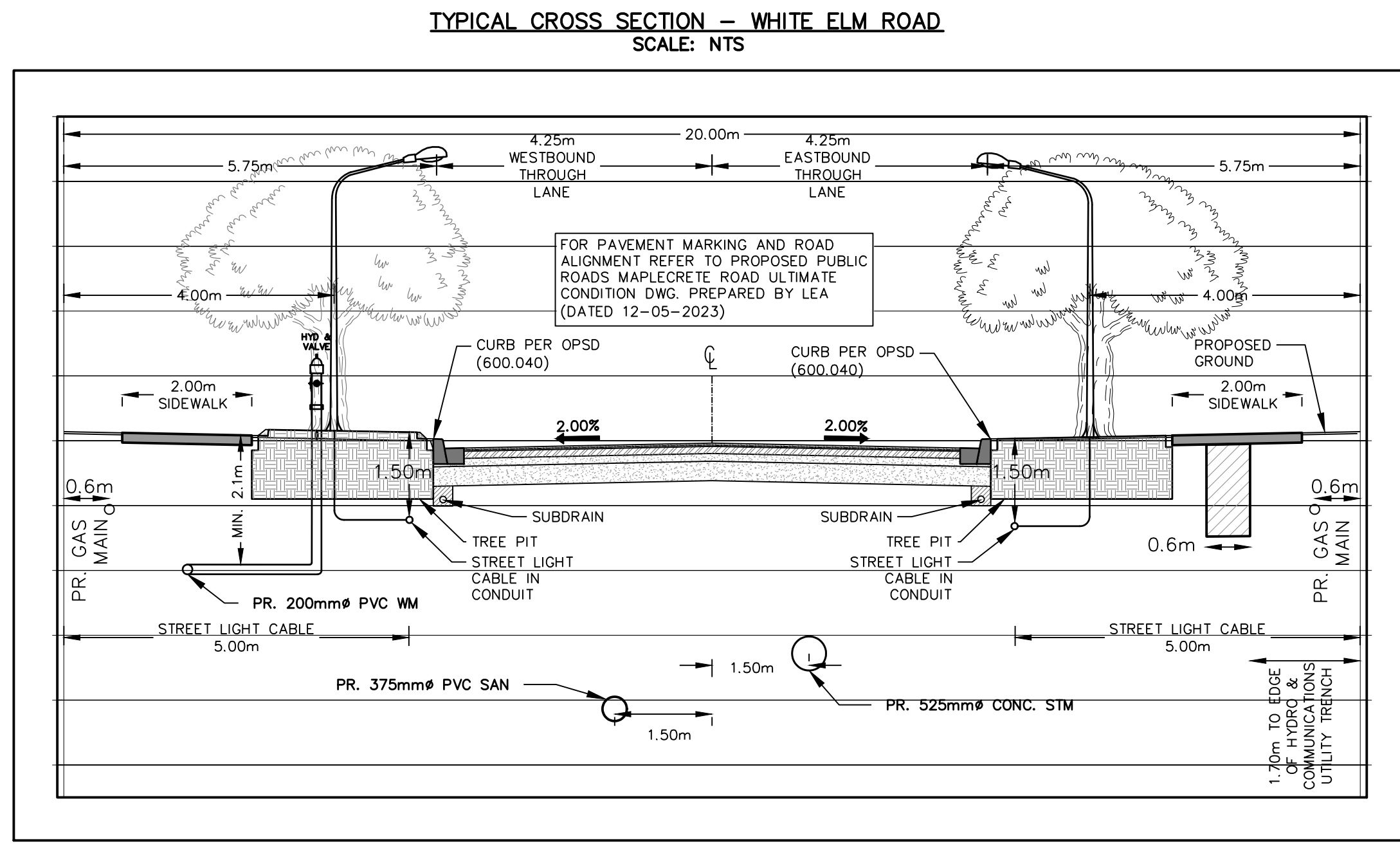
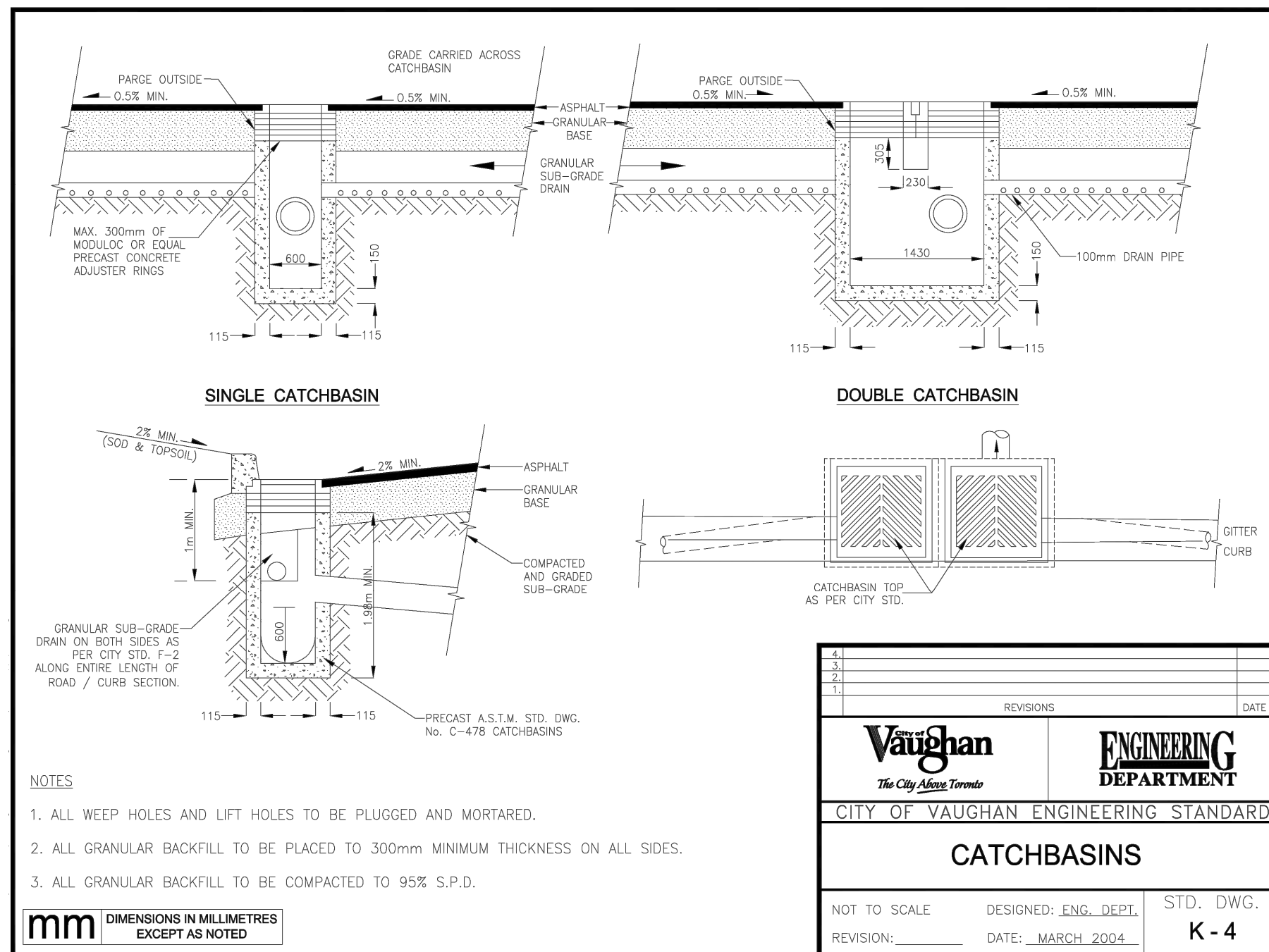
Project: 1930328 ONTARIO INC. 2901 HIGHWAY 7 CITY OF VAUGHAN

Drawing: DETAILS

CROZIER CONSULTING ENGINEERS

51 JOHN STREET WEST
 P.O. Box 1011
 BRADFORD, ON L3Z 2A9
 905-952-3111 T
 WWW.CFCROZIER.CA

Drawn: S.V.N. Design: C.C. Project No: 1525-6298
 Check: D.L. Check: J.M.S. Scale: N/A Dwg: C203F



APPROVED AS TO FORM IN RELIANCE, UPON THE PROFESSIONAL SKILL AND ABILITY OF THE CONSULTING ENGINEERS AS TO DESIGN AND SPECIFICATIONS

Engineer

 J. M. SCOTT
 100214964
 JUNE 20 2023
 PROVINCE OF ONTARIO

DIRECTOR OF DEVELOPMENT ENGINEERING AND INFRASTRUCTURE PLANNING

Engineer

 D. LOZENKO
 100213318
 2022-06-17
 PROVINCE OF ONTARIO

DATE: REGION FILE NO.: 19T-18V005 (SUBP.18.V0019)

SITE PLAN DEVELOPMENT
CITY FILE NO.: DA.18.037

4	PERFECT SUBMISSION	2023/JUN/20
3	ISSUED FOR COORDINATION	2023/JUN/08
2	ISSUED FOR 3RD SUBMISSION TO CITY	2023/FEB/22
1	ISSUED FOR 2ND SUBMISSION TO CITY	2022/OCT/11
0	ISSUED FOR 1ST SUBMISSION TO CITY	2022/JUN/17
No.	ISSUE / REVISION	YYYY/MM/DD

TEMPORARY BENCHMARKS

BEARINGS ARE UTM GRID, DERIVED FROM OBSERVED REFERENCE POINTS 1 AND 2. BY REAL TIME NETWORK (RTN) OBSERVATIONS, UTM ZONE 17, NAD83 (ORIGINAL). OBSERVED REFERENCE POINTS (ORP): UTM ZONE 17 NAD83 (ORIGINAL). COORDINATES TO URBAN ACCURACY PER SEC. 14(2) OF OREG 216/10
 ORP 1- NORTHING: 4850162.198 EASTING: 619083.869
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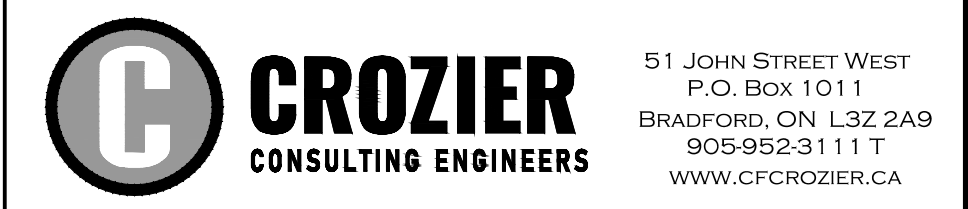


Project: 1930328 ONTARIO INC. 2901 HIGHWAY 7 CITY OF VAUGHAN

Drawing: DETAILS & TYPICAL CROSS-SECTION

FOR APPROVAL
NOT TO BE USED FOR CONSTRUCTION

NOTE: SEE SUPPLEMENTARY LETTER REGARDING SCOPE OF WORK PREPARED BY 1930328 ONTARIO INC.



Drawn	C.C.	Design	C.C.	Project No.	1525-6298
Check	D.L.	Check	J.M.S.	Scale	N/A
				Dwg	C103B

Date: March 11, 2024

To: Matthew Peverini, Senior Planner, VMC

From: Andrea Shotlander, Project Manager, Planning and Urban Design, VMC
Anna Rosen, Project Manager, Parks Development, VMC

Cc: Cory Gray, Manager, Parks & Strategic Initiatives, VMC
Gaston Soucy, Senior Manager, Planning and Urban Design, VMC

Re: CIHA Application
Urban Design and Parks Comments (1st Circulation)

File No: CIHA.23.003

Applicant: 171 Maplecrete LP and 1930328 Ontario Inc.

Location: 171 Maplecrete Rd, 140-160 Doughton Rd, and Block 2 on Plan 65M-4973

Urban Design and Parks Planning Staff have reviewed the 1st Circulation of the CIHA Application for the above-mentioned site, and provide the following comments:

URBAN DESIGN

General Comments

1. Applicant to provide Geo-referenced CAD drawings for CIHA submission. Georeferenced plans must be submitted in the initial submission and last submission.

Site Plan Organization

2. Please revise the architectural site plan to provide a minimum tower step-back of 3m as noted in section 10.2.2. Zone category "V1" of the City By-Law.
3. In the event that the future North-South Private road on the westerly neighbouring lands is not delivered, a building setback of 12.5m will be required from the property line to building face for Building NE-E and SE to ensure a total tower separation distance of 25m is achieved per VMC Secondary Plan policy 8.7.18.
4. In anticipation of a future north-south local road along the east frontage of the proposed development, and as per VMC Secondary Plan Policy 8.0 and 8.7.2, ensure the built form of development frames streets and open spaces and supports an inviting, comfortable and active public realm.
5. Staff appreciate the inclusion of retail space within the ground floor of the northwest building which is intended to have office and a hotel as part of its mixed-uses. The VMC Secondary Plan Update envisions Doughton Road as a primary retail street as such City staff encourage the applicant to also include retail uses along Doughton Road.
6. Clearly differentiate line type between the property line and easement boundary across the site plan and landscape plan.

Public Realm

7. Demonstrate that the proper road Right-of-Ways (ROW) are being provided along White Elm Road (20m ROW), Maplecrete Road (26m ROW), Doughton Road (26m ROW). Ensure there are no conflicts proposed between proposed utilities, plantings and LID Features in any of these roads ROW. Clearly label and dimension distances from all road centrelines to their respective property lines in the architectural and landscape site plans. Include additional dimensions for all the pedestrian boulevards from the face of each road barrier curb to their corresponding property lines.
8. The City-Wide Streetscape Implementation Manual, the VMC Streetscape and Open Space (SOS) Plan, the VMC Urban Design Guidelines and the VMC Streetscape Identity Framework (Draft), identify levels of service for all road streetscapes in the VMC.

Doughton Road is attributed an enhanced level of service streetscape. To satisfy this requirement, and following the previously-mentioned document's recommendations, introduce unit pavers and street furniture, including bike stands, in the amenity zone between planters along Doughton Road and study if pedestrian lighting will be required as part of the public ROW lighting strategy to emphasize a safer, more pedestrian friendly environment, which would contribute to the activation of the planned retail frontage. Ensure the proposed Doughton Road pedestrian boulevard designs adhere to the following breakdown: 0.60 buffer (from face of road concrete barrier curb to cycling facility asphalt paving), 1.50m cycling facility asphalt paving, 0.20m (raised curb at planter), 2.30m amenity zone (open tree planter), 0.20m (raised curb at planter), 2.00m pedestrian clearway zone (concrete sidewalk), 0.50m frontage zone (from concrete sidewalk to property line.)

The design for Maplecrete Road must conform to the Standard Urban Level of Service as per the Vaughan City-Wide Streetscape Implementation Manual and the VMC Streetscape and Open Space Plan. Boulevard breakdown is as follows: 0.55 buffer (from face of road concrete barrier curb to cycling facility asphalt paving), 1.50m cycling facility asphalt paving, 0.20m (raised curb at planter), 2.30m amenity zone (open tree planter), 0.20m (raised curb at planter), 2.00m pedestrian clearway zone (concrete sidewalk), 0.50m frontage zone (from concrete sidewalk to property line.)

The design for White Elm Road and boulevard was previously approved in the Liberty Cement Development with a standard urban level of service. Please refer to the approved plans for the design and breakdown.

9. Remove the proposed layby parking along Maplecrete and normalize the cycling facility to provide a continuous northbound path of travel between the south and the north mixing zones.

Pedestrian Level Wind Study

10. The applicant is required to submit a Pedestrian Level Wind Study, as per the City's Terms of Reference for Wind Study. Please note, a detailed wind tunnel analysis will also be required for the future Site Plan submission.

Sun Shadow Study

11. The provided Sun Shadow Study should be revised to include the most current development files within the surrounding vicinity. Please ensure the Liberty Maplecrete Development is included in this exercise.

The following preliminary comments are provided for information and reference purposes ahead of the future Site Plan application process. Staff reserves the right to comment further once a complete Site Plan application is submitted.

Urban Design

1. Applicant is required to submit 1:50 to 1:100 Scaled Detailed Colour Building Elevations. Clearly identify all exterior design features, materials, and window types. A digital architectural materials board will also be required providing high quality photos of the exterior finish materials with specifications.

Landscape

2. Ensure the following are provided in the complete Landscape Package:
 - a. detailed landscape cross sections of the pedestrian boulevards, at minimum 1:20 scale, taken through all proposed future public roads. Landscape cross section must clearly illustrate and annotate dimensions, locations of existing and proposed services, cycling facilities, property lines, raised planter curbs, materials, and other features, as applicable and must demonstrate that appropriate planter soil depths are provided to ensure healthy tree growth. Boulevard cross section must include longitudinal and cross-sectional sections through the open planters, furniture zone and any alternative proposed boulevard conditions.
 - b. clearly annotated Landscape Layout and Surface and Materials Plan indicating all critical dimensions, offsets and material callouts identifying the corresponding detail within the detail sheets.
 - c. soil calculations demonstrating that all trees within the public realm have access to soil volumes that provide, or exceed, 30 cubic meters of suitable growth medium per tree in a single standalone planter and 20 cubic meters of suitable growth medium per tree in a shared planter with a minimum net soil depth of 1.2 meters.
 - d. planting plans indicating all proposed public and private trees, shrubs and understory planting. Tree species composition strategies must adhere to clusters of not more than 4 trees of a same genus grouped together in public areas. Additionally, the total amount of the same species per application should not exceed 10% to guarantee diversity.
 - e. exterior lighting and photometric plans (in lux) demonstrating safe and appropriate lighting levels are being achieved within the public ROW as per the intended uses. Lighting and photometric plans must clearly denote and label the exact location, information, and specification of all pedestrian and street luminaires.
 - f. landscape details and furniture cut sheets parting to all landscape elements within the public realm.
 - g. public utility plan shadowed over the landscape plan

Landscape Cost Estimate

3. As a requirement of the future Site Plan Submission, please provide a detailed landscape cost estimate that separates works within the site property lines from works within the public-right-of-way. The private works should also be separated by specific areas such as the peripheral frontage zones, private roads, private amenity areas, rooftop amenity terraces, green roofs, etc. The public-right-of-way works breakdown should be separated by individual streets, namely Doughton Road and Maplecrete Road in both interim and ultimate conditions.

Cycling Facility

4. Indicate intended material of cycling facility and buffer strip on landscape drawings. The cycling facility along Doughton Road and Maplecrete Road must be composed of a vehicular load asphalt paving. Provide a detail to that effect.
5. Staff recommend the use of coloured stamped concrete in a 100x300 running bond pattern, similar to the promenade paver pattern. Stamped concrete should be coloured per Interstar Colour Code BN-1703R “Copper” pigment. See attachments for colour chart.

Bird-Friendly Design

6. Given the high amount of large pane glazing below the 16m height limit, bird-friendly glazing (bird frit) is strongly recommended as per Policy Planning and Environmental Sustainability in accordance with CSA A460:19 Bird Friendly Building Design standards. Bird-friendly design information can also be found on the following link:
https://www.vaughan.ca/services/business/urban_design/General%20Documents/202301%20Vaughan%20Bird%20Safe%20Standards%20Info%20Package.pdf

Elevation drawings note bird-friendly glazing should be applied but further information shall be provided with a detail and a descriptive note how these standards will be implemented in the architectural drawing package. Additionally, bird friendly glazing must be provided around all rooftop amenity spaces where landscape and planting are being proposed.

Signage

7. With the understanding that this is a mixed-use development, please submit a signage design and lighting package prepared by a professional consultant and consisting of the following:
 - a. a site plan indicating the location of the proposed signs
 - b. existing building photos or elevations showing the proposed signs in relation with the structure(s)
 - c. detailed, descriptive and dimensioned plans (in metric), cross-sections and elevations showing the materials used, electrical and mechanical components
 - d. illumination details
 - e. colour elevations with Pantone colour index reference numbers

The signage design strategy should meet the City’s Sign By-law, the Vaughan Metropolitan Centre Streetscape and Open Space Plan 6.1 and the Urban Design Guidelines 6.14.

Digital 3D Models and 2D CAD

8. Prior to site plan approval, final and updated 3D digital models will be required. The models must include all aspects of the proposed development and meet the following conditions:
 - a. models shall be high precision and unified spatial reference and data type to geo-reference to the UTM projection (UTM zone 17 NAD83.)
 - b. two types of models will be required throughout the review process as required: (1) basic massing models without details and (2) highly detailed models
 - c. copies of the original file formats are to be provided in a software supported by the City, i.e. Sketch-Up.

Urban Design and Sustainability Brief

9. The applicant is required to submit a Urban Design and Sustainability Brief that provides a detailed framework of the development proposal. The provide shall provide design direction to demonstrate how design goals and objectives of the official plan will be achieved. Refer to the City-Wide Urban Design Guidelines for technical requirements.

Pedestrian and Bicycle Circulation Plan

10. Applicant is to provide a pedestrian and bicycle circulation plan that clearly defines safe pedestrian and cycling connections within the development site and between the development and the neighbouring community. The Plan should indicate a clear hierarchy of circulation routs and identify building entrances, mid-block connections, transit access and routes to public destinations. This may be included as part of the Urban Design and Sustainability Brief.

Tree Protection and Removal Plan, Arborist Report and Tree Protection Agreement (TPA)

11. The Owner shall provide an Arborist report and Tree Inventory and Preservation Plan for all trees within the subject property or within 6m of the property boundary. Efforts should be made to preserve as many trees as possible on site and compensate for the tree removals through replacement planting or monetary contributions.
12. Staff would like to note that as part of the future Site Plan submission, the applicant will be required to enter into a tree protection agreement (TPA) with the City in accordance with the City of Vaughan council-enacted tree by-law 052-2018 and the Tree Protection Protocol. The agreement must be entered prior to execution of Site Plan Agreement and issuance of building permit.

PARKS PLANNING

13. For high-density residential development, the Owner shall, prior to the issuance of a Building Permit, pay to Vaughan by way of certified cheque, payment-in-lieu of the dedication of parkland at the rate of 1 ha per 1000 net residential units, or at a fixed unit rate, at Vaughan's discretion, in accordance with the Planning Act and the City of Vaughan Parkland Dedication By-law.

Notwithstanding the above, parkland contribution in the form of payment-in-lieu as determined by the City shall be subject to a cap of (i) 10% of the Lands or value of the Lands if the Lands are 5 ha or less; or (ii) 15% of the Lands or value of the Lands if the Lands are greater than 5 ha.

Should the Owner choose to proceed with the alternative requirement in the Planning Act, the City will require a current appraisal to determine land value at the time of building permit.

Submission of land value appraisal should allow sufficient review time and be cognizant of expiry associated with appraisals.

Andrea Shotlander

Project Manager of Urban Design
T. 905-832-8585 ext.8035
E. Andrea.Shotlander@vaughan.ca

Anna Rosen

Project Manager of Parks Development
T. 905-832-8585 ext.3637
E. Anna.Rosen@vaughan.ca

END OF MEMORANDUM

DATE : February 23, 2024
TO : Matthew Peverini, Development Planning Department
FILE No. : CIHA 23.003
APPLICANT : Liberty Development
PROPERTY : 171 Maplecrete Road, 160, 140 Doughton Road Block 2 65M-4793
LOCATION

Documents Reviewed:

DWG #	Drawing Title	Rev. #	Date
A001	Context Plan and Stats		Oct 2021
A100	Site Plan		
A101-103	Parking Levels		
A104	Ground Floor Plan		Oct 2021
A105	2 nd Floor		
A106	Podium Floor Plan		Oct 2021
A107	Hotel and Amenity Floor Plan		Oct 2021
A108	Typical Tower Floor Plan		Oct 2021
A201-206	Elevations		Oct 2021
A301-304	Sections		Oct 2021

Compliance with Transition Provisions under Section 1.6 of Zoning By-law 001-2021, as amended:

1. This application has been determined to not be transitioned in accordance with Section 1.6 and therefore is subject to Zoning By-law 1-88, as amended, and Zoning By-law 001-2021, as amended.

Comments associated with Zoning By-law 001-2021, as amended:

Zoning Designation:

The subject lands are currently zoned EM1 and review as V1, Vaughan Metropolitan Centre Station Zone subject to *Exception 14.XXXX* under By-law 001-2021.

Comments:

Please note the following comments are based on the information provided within the documents listed above.

1. The subject property is a corner lot as defined under 001-2021 and therefor White Elm Rd is the front lot line frontage.
2. Are the proposed 182 rental units not part of the contemplated 1565 total units? Are they included in that calculation? (they should form part of that calculation)

3. The DZBL states that frontage will be White Elm, Maplecrete Rd and Doughton Rd. (this isn't required).
4. The draft zoning by-law (DZBL) proposes a minimum front yard setback of 3.0 metres. Please have applicant verify the 3.0 m dimension at the closest point between the property and the North/West (NW) Tower. The dimension seems to taper towards the sight-triangle and may be less than 3.0 m.
5. The DZBL proposes an exterior side yard of 3.0 m. Please have the applicant verify the dimension towards the sight-triangle area as it appears to be 2.95 m.
6. DZBL requests that the Build-to-zone section not apply to the proposal.
7. DZBL should remove the Street Wall Height requirement of Table 10-3.
8. DZBL establishes a maximum podium height of 27 m or 6 storeys.
9. DZBL establishes the Residential Tower setback from any Rear Lot line and Interior Side Lot line to be 12.5 m. The DZBL requires to provide relief to permit a rear yard setback of 5 m for the SW and SE towers, 5 m for the interior side yard setback for the SE and E towers, and 5 m for the NE tower also.
10. DZBL proposes a minimum 2.0 m landscape strip. They have a list of permitted encroachments which includes "Hardscape" the term should be Hard Landscaping as defined to avoid confusion.
11. DZBL establishes a requirement of 4 sqm /unit for indoor/outdoor amenity.
12. DZBL wording for part 9 of the draft should be to permit a minimum setback of 0.0 m from all lot lines for any part of a building below grade.
13. Should the DZBL make allowances for Section 5.15.3: Air ventilation Shafts and staircases?
14. DZBL should make provisions for parking requirements. The subject lands are located outside the VMC boundary, and therefore parking requirements would be under those for "other zones". (although it will be given a V1 zoning category). The DZBL should permit a parking ratio of 0.4 spaces per unit. The DZBL should make provision to permit a minimum of 269 shared parking spaces for visitor and non-residential uses (being office and retail).
15. Please provide details for compliance of Section 6.3.3 Obstructed Parking Spaces if it applies, a typical dimensioned space and the required additional area should be shown in addition to the typical spaces provided.
16. Based on the proposed parking, a minimum of 21 Barrier Free Parking is required. That would be 10 Type A and 11 Type B spaces. A minimum of 10 Type A and 10 Type B are proposed. 1 Additional Type B space is required. Please note, that 9 Type B spaces were actually shown on all 3 levels of parking, please verify the total number of Type B spaces proposed.
17. The DZBL shall make provision to permit 3 Type D, 2 Type B and a shared Type B and D for the site.
18. Temporary parking within the circler Aisle of the NE tower entrance is not permitted.
19. Temporary parking along the access aisle between the SW and SE towers provide a dimension width of 2.5 m (which is deficient) and no lengths are provide. They should be revised to comply or removed.

Please ensure that all Zoning issues are adequately addressed prior to re-submission of Site Plan drawings, and that all subsequent submissions are accompanied by a covering letter indicating the proposed changes to the Site Plan. Additional comments may be forthcoming. If you have any questions or concerns, I may be contacted at the extension below.

Please note the drawings submitted for Building Permit shall match identically with the final approved Development Approval drawings including page numbers, revision numbers and revision dates. Any discrepancies may require further approval through the Development Planning Department.

Regards,

Gregory Seganfredo
Senior Plans Examiner (Zoning)
Building Standards Department
Ext. 8245

DATE : February 27th, 2024
TO : Matthew Peverini, Development Planning Department
FILE No. : CIHA 23.003
APPLICANT : Liberty Development
PROPERTY : 171 Maplecrete Road, 160, 140 Doughton Road Block 2 65M-4793
LOCATION

Documents Reviewed:

DWG #	Drawing Title	Rev. #	Date
A001	Context Plan and Stats		Oct 2021
A100	Site Plan		
A101-103	Parking Levels		
A104	Ground Floor Plan		Oct 2021
A105	2 nd Floor		
A106	Podium Floor Plan		Oct 2021
A107	Hotel and Amenity Floor Plan		Oct 2021
A108	Typical Tower Floor Plan		Oct 2021
A201-206	Elevations		Oct 2021
A301-304	Sections		Oct 2021

Compliance with Transition Provisions under Section 1.6 of Zoning By-law 001-2021, as amended:

1. This application has been determined to not be transitioned in accordance with Section 1.6 and therefore is subject to Zoning By-law 1-88, as amended, and Zoning By-law 001-2021, as amended.

Comments associated with Zoning By-law 1-88, as amended: (for reference)

Zoning Designation:

The subject lands are currently zoned EM1 and EM2 and review as C9, Corporate Centre Zone subject to *Exception 9(.XXXX)* under By-law 1-88a.a.

Comments:

Please note the following comments are based on the information provided within the documents listed above.

1. The subject property is a corner lot as defined under By-law 1-88a.a and therefor White Elm Rd is the front lot line frontage.
2. Are the proposed 182 rental units not part of the contemplated 1565 total units? Are they included in that calculation? (they should form part of that calculation).

3. The DZBL states that frontage will be White Elm, Maplecrete Rd and Doughton Rd. (this isn't required).
4. The draft zoning by-law (DZBL) proposes a minimum front yard setback of 3.0 metres. Please have applicant verify the 3.0 m dimension at the closest point between the property and the North/West (NW) Tower. The dimension seems to taper towards the sight-triangle and may be less than 3.0 m.
5. The DZBL proposes an exterior side yard of 3.0 m. Please have the applicant verify the dimension towards the sight-triangle area as it appears to be 2.95 m.
6. DZBL requests that the Build-to-zone section not apply to the proposal.
7. DZBL establishes a maximum gross floor area of 112,500 sqm. (there is no requirement for maximum g.f.a under By-law 1-88).
8. DZBL establishes a maximum podium height of 27 m or 6 storeys.
9. DZBL establishes the maximum building heights of 130 metres or 40 Storeys(although By-law 1-88 only refers to height in metres).
10. DZBL proposes a minimum 2.0 m landscape strip. They have a list of permitted encroachments which includes "Hardscape" the term should be Hard Landscaping as defined to avoid confusion.
11. DZBL establishes a requirement of 4 sqm /unit for indoor/outdoor amenity.
12. DZBL wording for part 9 of the draft should be to permit a minimum setback of 0.0 m from all lot lines for any part of a building below grade.
13. Should the DZBL make allowances for Section 5.15.3: Air ventilation Shafts and staircases?
14. DZBL should make provisions for parking requirements. The subject lands are located outside the VMC boundary, and therefore parking requirements would be under those for "other zones". (although it will be given a V1 zoning category). The DZBL should permit a parking ratio of 0.4 spaces per unit. The DZBL should make provision to permit a minimum of 269 shared parking spaces for visitor and non-residential uses (being office and retail and Hotel uses).
15. Based on the proposed parking, a minimum of 21 Barrier Free Parking is required. That would be 10 Type A and 11 Type B spaces. A minimum of 10 Type A and 10 Type B are proposed. 1 Additional Type B space is required. Please note, that 9 Type B spaces were actually shown on all 3 levels of parking, please verify the total number of Type B spaces proposed.
16. The DZBL shall make provision to permit 3 Type D, 2 Type B and a shared Type B and D for the site.
17. Temporary parking within the circler Aisle of the NE tower entrance is not permitted.
18. Temporary parking along the access aisle between the SW and SE towers provide a dimension width of 2.5 m (which is deficient) and no lengths are provide. They should be revised to comply or removed.
19. DZBL does not require to make provision for Tower step-back of 2 m, nor tower separation between all buildings of 25 m. A rear setback of 12.5 is also not required nor the Rear lot line or interior side lot line setback of 5 m (those are 001-2021 requirements).
20. The DZBL has a provision to establish a minimum of 12.5 % of g.f.a to be non-residential uses, there is no requirement for this.

Please ensure that all Zoning issues are adequately addressed prior to re-submission of Site Plan drawings, and that all subsequent submissions are accompanied by a covering letter indicating the proposed changes to the Site Plan. Additional comments may be forthcoming. If you have any questions or concerns, I may be contacted at the extension below.

Please note the drawings submitted for Building Permit shall match identically with the final approved Development Approval drawings including page numbers, revision numbers and revision dates. Any discrepancies may require further approval through the Development Planning Department.

Regards,

Gregory Seganfreddo
Senior Plans Examiner (Zoning)
Building Standards Department
Ext. 8245



Hi-Density & Commercial Residential Combined
(Commercial less than 20% of the Gross Floor Area)
(Residential greater than 5 storeys & 10 residential units)

DATE: 2/29/2024
TO: DEVELOPMENT PLANNING
ATTENTION: MATTHEW PEVERINI
SUBJECT: REQUEST FOR COMMENTS
FILE #: CIHA.23.003
171 MAPLECRETE ROAD, 140-160 DOUGHTON ROAD AND
BLOCK 2 ON REGISTERED PLAN 65M-4793
APPLICANT: LATIF FAZEL
C/O 171 MAPLECRETE LP & 1930328 ONTARIO INC.

Comment:
(see below)

No Comment:

“For high-density residential development, the Owner shall, prior to the issuance of a Building Permit, convey land at the rate of 1 ha per 600 net residential units and/or pay to Vaughan by way of certified cheque, cash-in-lieu of the dedication of parkland at the rate of 1 ha per 1000 net residential units, or at a fixed unit rate, at Vaughan’s discretion, in accordance with the *Planning Act* and the City of Vaughan Parkland Dedication By-law. Notwithstanding the above, such parkland contribution—whether in the form of parkland conveyance or cash-in-lieu as determined by the City—shall be subject to a cap of (i) 10% of the Lands or value of the Lands if the Lands are 5 ha or less; or (ii) 15% of the Lands or value of the Lands if the Lands are greater than 5 ha.”

“Prior to the issuance of a Building Permit, the Owner shall pay to the City of Vaughan by way of certified cheque a community benefits charge equivalent to 4% of the value of the subject lands in accordance with Section 37 of the *Planning Act* and the City’s Community Benefits Charge By-law. The Owner shall submit an appraisal of the subject lands, pursuant to City’s Community Benefits Charge By-law, prepared by an accredited appraiser for approval by the Vaughan Real Estate Department, and the approved appraisal shall form the basis of the calculation of the community benefits charge payment.”

Paul Salerno

Paul Salerno
Director of Real Estate
(Ext. 8473)

Matthew Peverini

From: Abanoub Abadeer
Sent: Tuesday, January 30, 2024 11:20 AM
To: Matthew Peverini
Subject: RE: Request for Comments - CIHA.23.003 - 171 Maplecrete Road, 140-160 Doughton Road and Block 2 on Registered Plan 65M-4793

Hello Mathew,

We have looked over the information provided for the File. The package does not show or state any fire related information therefore I have added below some of the general comments that are normally associated with this type of development. Please only take as General Comments i.e. no comments.

The following comments raised to provide a minimum level of fire safety and protection to the environment and the building construction stage which will support firefighting operations when required. All applicable building code requirements shall be addressed.

1. Water supply for firefighting, including hydrants, municipal or private shall be identified in the plans and code requirements to be installed and operational prior to construction of building.
2. Hydrants/Fire department connections shall be unobstructed and ready for use at all times.
3. Access roadways shall be maintained and suitable for large heavy vehicles.
4. Roads shall be complete to a minimum base coat of asphalt capable of carrying emergency vehicle loading prior to construction of buildings.
5. Temporary Municipal address to be posted and visible for responding emergency vehicles satisfactory to the City.
6. Based on site drawing the private roadway is a fire route and parking on the street is prohibited. This very well could be an issue based on the single car garage and parking for one vehicle in front of the garage.
7. Fire Route to be posted prior to construction. (ensure the signs are approved with the by-law 1-96 is on the sign).
8. Fire route to be in accordance with the 2012 Ontario Building Code
9. Width of the roadway
10. Is parking permitted
 - o Is the fire route posted as per by-laws
 - o General concerns of vehicle parking on the roads which would reduce the width below the minimum code requirements for fire route.

Thank You

Abanoub (Abby) Abadeer
Fire Protection Technologist
416-522-7319 | Abanoub.Abadeer@vaughan.ca

City of Vaughan | Vaughan Fire & Rescue Service
2141 Major Mackenzie Dr., Vaughan, ON L6A 1T1

Date: **2/7/2024**

To: Matthew Peverini, Senior Planner - VMC, Policy Planning & Special Programs

From: Ryan Cochrane, Forestry Inspector, Parks, Forestry and Horticulture Operations

Cc: Forestry Circulations;

Re: Forestry's Comments - Circulation #:1

File Reference Number:

Related Files: CIHA.23.003 and 65M-4793

Ward: 4

Agent: Thanh Nguyen c/o Liberty Development Corporation

Owner: Latif Fazel c/o 171 Maplecrete LP & 1930328 Ontario Inc.

Location: 171 Maplecrete Road, 140-160 Doughton Road and Block 2 on Registered Plan 65M-4793

Forestry Staff have reviewed the Arborist Report and Tree Inventory, Preservation & Removals Plan of the proposed development concept for the above-mentioned site, circulated on 1/29/2024 and provide the following comments:

General Comments

1. Provide an arborist report and tree inventory protection plan for all trees 20 cm and greater located on the subject property, municipally owned trees of any size, and all trees within 6m of the property boundary.

Ryan Cochrane
Forestry Inspector
Parks, Forestry and Horticulture Operations
Ryan.Cochrane@vaughan.ca

Matthew Peverini

From: Sharon Walker
Sent: Wednesday, February 28, 2024 1:23 PM
To: Matthew Peverini
Subject: RE: Request for Comments - CIHA.23.003 - 171 Maplecrete Road, 140-160 Doughton Road and Block 2 on Registered Plan 65M-4793

Matthew

My apologies for the late response. If it is has not already been requested by another department,

- a land use compatibility study using the D1 through 6 guidelines is required given the nature of industrial operations in the area.

Regards

Sharon Walker, BPA, ABCP, CMM III EMP
Manager, Emergency Planning
905-832-8585, ext. 6322 | sharon.walker@vaughan.ca
Cell: 416-524-7923

Vaughan Fire and Rescue Service | Emergency Planning
2800 Rutherford Road, Vaughan, ON L4K 2N9
www.vaughan.ca/PrepE



Matthew Peverini

From: Lindsay Davidson
Sent: Wednesday, January 31, 2024 5:43 PM
To: Matthew Peverini
Cc: Alyssa Pangilinan
Subject: RE: Request for Comments - CIHA.23.003 - 171 Maplecrete Road, 140-160 Doughton Road and Block 2 on Registered Plan 65M-4793

Hi Matthew,

Thanks for sharing. From Economic Development:

- The retail component is very limited given the three-building footprint
- Economic Development supports the consideration to expand the retail footprint Given there is very little else in terms of retail currently in the immediate vicinity for the residential component and visitors

Thank you,
Lindsay

Matthew Peverini

From: Alexandre Thibault <Alexandre.Thibault@cn.ca> on behalf of Proximity <proximity@cn.ca>
Sent: Thursday, February 8, 2024 3:36 PM
To: Matthew Peverini
Subject: [External] 2024-02-08_CN Comments -Vaughan_ CIHA.23.003 - 171 Maplecrete Road, 140-160 Doughton Road and Block 2 on Registered Plan 65M-4793
Attachments: Circulation Internal External Letter_CIHA.23.003.pdf

CAUTION! This is an external email. Verify the sender's email address and carefully examine any links or attachments before clicking. If you believe this may be a phishing email, please use the Phish Alert Button.

Hello Matthew,

Thank you for consulting CN on the application mentioned in subject. It is noted that the subject site is within 1000 meters of CN railway operations including the CN Yard's. CN has concerns of developing/densifying residential uses in proximity to railway operations. Development of sensitive uses in proximity to railway operations cultivates an environment in which land use incompatibility issues are exacerbated. CN's guidelines reinforce the safety and well-being of any existing and future occupants of the area. Please refer to CN's guidelines for the development of sensitive uses in proximity to railways. These policies have been developed by the Railway Association of Canada and the Federation of Canadian Municipalities. CN encourages the municipality to pursue the implementation of the following criteria as conditions of an eventual project approval:

- The following clause should be inserted in all development agreements, offers to purchase, and agreements of Purchase and Sale or Lease of each dwelling unit within 1000m of the railway right-of-way:

"Warning: Canadian National Railway Company or its assigns or successors in interest has or have a right-of-way within 1000 metres from the land the subject hereof. There may be alterations to or expansions of the railway facilities on such rights-of-way in the future including the possibility that the railway or its assigns or successors as aforesaid may expand its operations, which expansion may affect the living environment of the residents in the vicinity, notwithstanding the inclusion of any noise and vibration attenuating measures in the design of the development and individual dwelling(s). CNR will not be responsible for any complaints or claims arising from use of such facilities and/or operations on, over or under the aforesaid rights-of-way."

- The Owner shall through restrictive covenants to be registered on title and all agreements of purchase and sale or lease provide notice to the public that the noise and vibration isolation measures implemented are not to be tampered with or altered and further that the Owner shall have sole responsibility for and shall maintain these measures to the satisfaction of CN.
- The Owner shall enter into an Agreement with CN stipulating how CN's concerns will be resolved and will pay CN's reasonable costs in preparing and negotiating the agreement.
- The Owner shall be required to grant CN an environmental easement for operational noise and vibration emissions, registered against the subject property in favour of CN.

CN is currently reviewing the noise impact study provided and will provide further comments once the review is complete .

Thank you and do not hesitate to contact me with any questions.

Best regards,

Alexandre Thibault, B.Sc. Urb

Urbaniste stagiaire / Urban Planner Intern (CN Proximity)
Planning, Landscape Architecture and Urban Design
Urbanisme, architecture de paysage et design urbain



E : proximity@cn.ca
1600, René-Lévesque Ouest, 11e étage
Montréal (Québec)
H3H 1P9 CANADA
wsp.com

Matthew Peverini

From: Lingard, Norman <norman.lingard@bell.ca>
Sent: Thursday, February 8, 2024 11:21 AM
To: Matthew Peverini
Subject: [External] CIHA.23.003 - 171 Maplecrete Road, 140-160 Doughton Road and Block 2 on Registered Plan 65M-4793

CAUTION! This is an external email. Verify the sender's email address and carefully examine any links or attachments before clicking. If you believe this may be a phishing email, please use the Phish Alert Button.

Good morning Matthew,

Bell Canada thanks you for the opportunity to participate in the City of Vaughan's CIHA request for the above noted. Bell Canada is currently evaluating the most effective means to collaborate with municipalities towards the goals of the '*More Homes for Everyone*' and '*More Homes Built Faster*' Acts. This includes assessing how we can consistently and appropriately process and respond to pre-consultation circulations for *Planning Act* applications from municipalities across Ontario.

As an interim solution, we are providing the following standard response to all pre-consultation circulations from municipalities. If there are development proposal-specific concerns that are not addressed by our standard response, we are requesting that questions and concerns be sent directly to planninganddevelopment@bell.ca.

About Bell Canada

Bell Canada is Ontario's principal telecommunications infrastructure provider, developing and maintaining an essential public service. The Bell Canada Act, a federal statute, requires that Bell supply, manage and operate most of the trunk telecommunications system in Ontario. Bell is therefore also responsible for the infrastructure that supports most 911 emergency services in the Province. The critical nature of Bell's services is declared in the Bell Canada Act to be "for the general advantage of Canada" and the Telecommunications Act affirms that the services of telecommunications providers are "essential in the maintenance of Canada's identity and sovereignty."

Provincial policy further indicates the economic and social functions of telecommunications systems and emphasizes the importance of delivering cost-effective and efficient services:

- The 2020 Provincial Policy Statement (PPS) requires the development of coordinated, efficient and cost-effective infrastructure, including telecommunications systems (Section 1.6.1).
- Section 1.7.1 I) of the 2020 PPS recognizes that "efficient and coordinated telecommunications infrastructure" is a component of supporting long-term economic prosperity.
- We note that the definition of infrastructure in the 2020 PPS is inclusive of communications / telecommunications, which is indicative of the importance in providing efficient telecommunications services to support current needs and future growth (Section 1.6.1).
- Furthermore, the 2020 PPS states that infrastructure should be "strategically located to support the effective and efficient delivery of emergency management services" (Section 1.6.4), which is relevant to telecommunications since it is an integral component of the 911 emergency service.

To support the intent of the Bell Canada Act and Telecommunications Act and ensure consistency with Provincial policy, Bell Canada has become increasingly involved in municipal policy and infrastructure initiatives. We strive to establish partnerships which allow for a solid understanding of the parameters of Bell's infrastructure and provisioning needs and the goals and objectives of the municipality related to utilities.

Comments on the CIHA

We would like to thank you for the opportunity to be included in this process and the City's recognition of the importance of telecommunications and broadband infrastructure as critical components to development and economic viability. As such, to facilitate the provisioning of this infrastructure, we appreciate the City's continued support in ensuring that sufficient notice and time to comment on planning applications are provided, particularly for Draft Plan of Condominium, Draft Plan of Subdivision and Site Plan Control/Approval. This ensures an understanding by applicants of Bell's conditions and provisioning requirements, and that the following paragraphs are to be included as a condition of approval for any Draft of Plan of Subdivision, Draft Plan of Condominium and/or Site Plan application(s):

"The Owner acknowledges and agrees to convey any easement(s) as deemed necessary by Bell Canada to service this new development. The Owner further agrees and acknowledges to convey such easements at no cost to Bell Canada.

The Owner agrees that should any conflict arise with existing Bell Canada facilities where a current and valid easement exists within the subject area, the Owner shall be responsible for the relocation of any such facilities or easements at their own cost.

It shall be noted that it is the responsibility of the Owner to provide entrance/service duct(s) from Bell Canada's existing network infrastructure to service this development. In the event that no such network infrastructure exists, in accordance with the Bell Canada Act, the Owner may be required to pay for the extension of such network infrastructure.

If the Owner elects not to pay for the above noted connection, Bell Canada may decide not to provide service to this development".

The above-noted conditions will remain in effect until said conditions are officially cleared by Bell Canada as part of the formal approval process.

Bell would also emphasize that receiving engineering and servicing/composite utility plans/drawings, as soon as possible in the process, assists in the development and expedition of our provisioning plan. As a result, we would strongly recommend that this consideration be highlighted in any pre-circulation/consultation meetings with prospective applicants. This will assist Bell in providing comments and clearance letters in an efficient manner, assisting the Municipality in meeting approval times. Such drawings should be submitted to: planninganddevelopment@bell.ca by the applicant/their agents.

Future Involvement

We would like to thank you again for the opportunity to comment, and would request that Bell continue to be circulated on any future materials and/or decisions released by the City of Vaughan in relation to this initiative. Please forward all future documents to circulations@wsp.com and should you have any specific questions, please contact the undersigned.

Have a great remainder of your week.

Yours truly,

Norm Lingard
Senior Consultant – Municipal Liaison

Network Provisioning

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Please note that WSP operates Bell Canada's development, infrastructure and policy tracking systems, which includes the intake and processing of municipal circulations. However, all responses to circulations and requests for information will come directly from Bell Canada, and not from WSP. WSP is not responsible for the provision of comments or other responses.

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