

<b>ITEM: 6.4</b>	<b>COMMITTEE OF ADJUSTMENT REPORT SUMMARY MINOR VARIANCE APPLICATION FILE NUMBER A136/23 69 KINGLY CREST WAY, WOODBRIDGE</b>
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**THIS REPORT CONTAINS COMMENTS FROM THE FOLLOWING  
DEPARTMENTS & AGENCIES:**

\*Please see **Schedule B** of this report for a copy of Development Planning and Agency correspondence.

Additional comments from departments and agencies may be received after the publication of the Staff Report. These comments will be processed as an addendum and posted on the City's Website.

DEPARTMENTS	Circulated	Comments Received	Conditions	Nature of Comments
Committee of Adjustment	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Building Standards (Zoning Review)	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Building Inspection (Septic)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	No Comments Received to Date
Development Planning	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Recommend Refusal
Development Engineering	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Recommend Approval/No Conditions
Parks, Forestry and Horticulture Operations	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
By-law & Compliance, Licensing & Permits	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	No Comments Received to Date
Development Finance	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Real Estate	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Fire Department	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	No Comments Received to Date
AGENCIES	Circulated	Comments Received	Conditions	Nature of Comments
TRCA	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Ministry of Transportation (MTO)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Region of York	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Alectra	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Bell Canada	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	No Comments Received to Date
YRDSB	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
YCDSB	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
CN Rail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
CP Rail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
TransCanada Pipeline	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	No Comments Received to Date
Metrolinx	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Propane Operator	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	

**PUBLIC & APPLICANT CORRESPONDENCE**

\*Please see **Schedule C** of this report for a copy of the public & applicant correspondence listed below.

The deadline to submit public comments is **noon on the last business day prior to the scheduled hearing date.**

Comments and written public submissions received after the publication of this Staff Report will be processed as an addendum and posted on the City's Website.

All personal information collected because of this public meeting (including both written and oral submissions) is collected under the authority of the Municipal Act, the Municipal Freedom of Information and Protection of Privacy Act (MFIPPA), the Planning Act and all other relevant legislation, and will be used to assist in deciding on this matter. All personal information (as defined by MFIPPA), including (but not limited to) names, addresses, opinions and comments collected will become property of the City of Vaughan, will be made available for public disclosure (including being posted on the internet) and will be used to assist the Committee of Adjustment and staff to process this application.

Correspondence Type	Name	Address	Date Received (mm/dd/yyyy)	Summary
Public	Rose Vella	73 Kingly Crest Way	09/22/2023	No flooding or drainage issues
Public	Frank Paguapas	10 Royal Pine Avenue	09/22/2023	No flooding or drainage issues

**PREVIOUS COA DECISIONS ON THE SUBJECT LAND**

\*Please see **Schedule D** for a copy of the Decisions listed below

File Number	Date of Decision MM/DD/YYYY	Decision Outcome
None		

**ADJOURNMENT HISTORY**

\* Previous hearing dates where this application was adjourned by the Committee and public notice issued.

None	
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**COMMITTEE OF ADJUSTMENT REPORT  
MINOR VARIANCE APPLICATION  
A136/23  
69 KINGLY CREST WAY, WOODBRIDGE**

<b>ITEM NUMBER: 6.4</b>	<b>CITY WARD #: 2</b>
<b>APPLICANT:</b>	Luigi & Consolata Maffeo
<b>AGENT:</b>	None
<b>PROPERTY:</b>	69 Kingly Crest Way, Woodbridge
<b>ZONING DESIGNATION:</b>	See below.
<b>VAUGHAN OFFICIAL PLAN (2010) DESIGNATION:</b>	Vaughan Official Plan 2010 ('VOP 2010'): "Low-Rise Residential"
<b>RELATED DEVELOPMENT APPLICATIONS:</b>	None
<b>PURPOSE OF APPLICATION:</b>	Relief from the Zoning By-law is being requested to permit a covered shed, including roof overhang.

The following variances are being requested from the City's Zoning By-law to accommodate the above proposal:

**The subject lands are zoned R4A(EN) – Fourth Density Residential Zone (Established Neighbourhood) under Zoning By-law 001-2021, as amended.**

#	Zoning By-law 001-2021	Variance requested
1	A residential accessory structure with a height greater than 2.8 m shall not be located closer than 2.4 m to any lot line. Section 4.1.2 1b	To permit a residential accessory structure with a height greater and 2.8m to be setback 0.71m from the rear lot line.
2	A residential accessory structure with a height greater than 2.8 m shall not be located closer than 2.4 m to any lot line. Section 4.1.2 1b	To permit the residential accessory structure with a height greater and 2.8m to be setback 1.22m from the north side interior lot line.
3	A residential accessory structure with a height greater than 2.8 m shall not be located closer than 2.4 m to any lot line. Section 4.1.2 1b	To permit the residential accessory structure with a height greater and 2.8m to be setback 0.71m from the south side interior lot line.
4	In any Residential zone, the maximum lot coverage of all accessory buildings and residential accessory structures shall be 10% or 67m <sup>2</sup> whichever is less. 10% of the lot area is 35.6m <sup>2</sup> . Section 4.1.3 1	To permit a maximum lot coverage of 14% for the residential accessory structure.
5	The maximum permitted encroachment for eaves is 0.50m into the required interior side yard. Section 4.13 Table 4-1	To permit a maximum encroachment of 0.61m into the required interior side yard on the north lot line.
6	Unless otherwise expressly permitted by this By-law, a minimum distance of 0.6 m shall be required from any permitted encroachment to the nearest lot line. Section 4.13 3	To permit a minimum distance of 0.30m to any permitted encroachment to the nearest lot line.

**HEARING INFORMATION**

**DATE OF MEETING:** Thursday, December 7, 2023  
**TIME:** 6:00 p.m.  
**MEETING LOCATION:** Vaughan City Hall, Woodbridge Room (2<sup>nd</sup> Floor), 2141 Major Mackenzie Drive  
**LIVE STREAM LINK:** [Vaughan.ca/LiveCouncil](http://Vaughan.ca/LiveCouncil)

**PUBLIC PARTICIPATION**

If you would like to speak to the Committee of Adjustment at the meeting, either remotely or in person, please complete the [Request to Speak Form](#) and submit to [cofa@vaughan.ca](mailto:cofa@vaughan.ca)

## HEARING INFORMATION

If you would like to submit written comments, please quote file number above and submit by mail or email to:

**Email:** [cofa@vaughan.ca](mailto:cofa@vaughan.ca)

**Mail:** City of Vaughan, Office of the City Clerk, Committee of Adjustment, 2141 Major Mackenzie Drive, Vaughan, ON, L6A 1T1

To speak electronically, pre-registration is required by completing the [Request to Speak Form](#) on-line and submitting it to [cofa@vaughan.ca](mailto:cofa@vaughan.ca) no later than NOON on the last business day before the meeting.

**THE DEADLINE TO REGISTER TO SPEAK ELECTRONICALLY OR SUBMIT WRITTEN COMMENTS ON THE ABOVE NOTED FILE(S) IS NOON ON THE LAST BUSINESS DAY BEFORE THE MEETING.**

## INTRODUCTION

Staff and Agencies act as advisory bodies to the Committee of Adjustment. The comments contained in this report are presented as recommendations to the Committee.

Section 45(1) of the Planning Act sets the criteria for authorizing minor variances to the City of Vaughan's Zoning By-law. Accordingly, review of the application may consider the following:

- That the general intent and purpose of the by-law will be maintained.
- That the general intent and purpose of the official plan will be maintained.
- That the requested variance(s) is/are acceptable for the appropriate development of the subject lands.
- That the requested variance(s) is/are minor in nature.

Public written and oral submissions relating to this application are taken into consideration by the Committee of Adjustment as part of its deliberations and final decision on this matter.

## COMMITTEE OF ADJUSTMENT COMMENTS

<b>Date Public Notice Mailed:</b>	November 23, 2023
<b>Date Applicant Confirmed Posting of Sign:</b>	November 20, 2023
<b>Applicant Justification for Variances:</b> <small>*As provided by Applicant in Application Form</small>	Setback, overhang, and coverage exceptions required.
<b>Adjournment Requests (from staff):</b> <small>*Adjournment requests provided to applicant prior to issuance of public notice</small>	<p>On October 27, 2023, Development Planning provided:</p> <p>After attending the site visit and conducting a detailed review of the proposed application, Development Planning Staff and Urban Design Staff cannot support the proposed structure in its existing state due to massing and articulation concerns. Staff recommends removal of the roof portion of the patio, where only the roof for the covered shed remains, as this will help mitigate the massing of the structure.</p> <p>It was also noted on the site visit that there was no soft landscaping in the backyard – therefore, please ensure the provided site plan is updated to reflect the existing landscaping conditions.</p> <p>If you wish to revise the site plan in response to Staff's recommendations, please provide an updated copy so Zoning Staff can review and update the required variances.</p> <p>On October 27, 2023, Urban Design Provided:</p> <p>The Covered Shed has a big massing that has a negative visual impact on all the surrounding neighbours. Notably, the location of the shed is very close to all three sides of the property lines, and there is no soil area that can accommodate hedge plantings and mitigate the negative visual impact. Note that it is required a minimum of 1.2 m width of the soil strip for the hedge plantings to ensure their health.</p> <p>Staff conducted a site inspection and realized this is an as-built structure but not a proposed structure as indicated on</p>

**COMMITTEE OF ADJUSTMENT COMMENTS**

the site plan. Based on the fact, that the structure is already built and doesn't seem it can be modified easily. Therefore, staff could not provide any contributing suggestions to help mitigate the negative visual impact. In considering the above, staff can not support this application.

If the applicant is willing to make big changes, such as removing the roof of the entire patio portion, and only leaving the covered shed. Staff could support the variances as the overall massing will be much smaller, and the visual impact is not significant.

On November 10, 2023, the Applicant provided additional details regarding existing landscaping at the request of Development Planning.

On November 13, 2023, Zoning confirmed required variances pertaining to the revised site plan submitted November 10.

On November 16, 2023, Development Planning provided:  
Thank you for providing the updated soft landscaping details.

I did want to reiterate the Development Planning staff comments below and the Urban Design comments attached as the structure remains unchanged. Please note that staff are unable to support the structure in its existing state. Please advise if the structure will be altered according to staff comments, and if changes are proposed, please kindly provide updated drawings.

On November 17, 2023, Committee of Adjustment staff confirmed that adjournment fees are applicable after the mailing of public notice.

On November 20, 2023, the applicant advised that wish to proceed to the December 7, 2023 hearing, as is.

On November 28, 2023, Development Planning provided:  
It has been noted that there is a minor discrepancy in the length of the structure in the two drawings provided (site plan vs builders sketch – see both snapshots below). Please confirm which structure dimensions and eaves overhangs provided are accurate.

Please also note that staff have used the rear lot line measurement from the property survey in order to determine the length of the structure (by subtracting the requested setbacks on both sides of the structure from the rear lot line on the survey: 10.40 metres).

On November 29, 2023, the applicant provided revised plans to address Development Planning comments on November 28.

On November 29, 2023, Zoning confirmed no change to variances based on the revised submission.

**Was a Zoning Review Waiver (ZRW) Form submitted by Applicant:**

No

\*ZRW Form may be used by applicant in instances where a revised submission is made, and zoning staff do not have an opportunity to review and confirm variances prior to the issuance of public notice.

\*A revised submission may be required to address staff / agency comments received as part of the application review process.

### COMMITTEE OF ADJUSTMENT COMMENTS

\*Where a zoning review has not been completed on a revised submission, an opportunity is provided to the applicant to adjourn the proposal prior to the issuance of public notice.

**Adjournment Fees:**

In accordance with Procedural By-law 069-2019, an Adjournment Fee is applicable to reschedule an application after the issuance of public notice where a request for adjournment has been provided to the applicant prior to the issuance of public notice.

An Adjournment Fee can only be waived in instances where adjournment of an application is requested by the Committee or staff **after** the issuance of public notice.

**Committee of Adjustment Comments:**

Applicant to submit required Adjournment Fee to accommodate rescheduling of application from the December 7, 2023 Committee of Adjustment hearing, if required.

**Committee of Adjustment Recommended Conditions of Approval:**

None

### BUILDING STANDARDS (ZONING) COMMENTS

\*\*See Schedule B for Building Standards (Zoning) Comments

**Building Standards Recommended Conditions of Approval:**

None

### DEVELOPMENT PLANNING COMMENTS

\*\*See Schedule B for Development Planning Comments.

**Development Planning Recommended Conditions of Approval:**

None

### DEVELOPMENT ENGINEERING COMMENTS

[Link to Grading Permit](#) [Link to Pool Permit](#) [Link to Curb Curt Permit](#) [Link Culvert Installation](#)

The Owner/Applicant's proposed work involves increasing the Lot Coverage of 50.13 m<sup>2</sup> on the subject property. This addition of hardscape could potentially affect the City's Storm Water management system.

Development Engineering highly recommends that the Owner/Applicant incorporate Low-Impact Development (LID) measures, such as bioswales, permeable pavers, rain gardens, rain barrels, etc., to minimize the impact on the stormwater system.

The Owner/Applicant has provided an approved Lot Grading Permit for the proposed covered shed. The Development Engineering Department does not object to the Minor Variance application A136/23.

**Development Engineering Recommended Conditions of Approval:**

None

### PARKS, FORESTRY & HORTICULTURE (PFH) COMMENTS

Forestry has no comment.

**PFH Recommended Conditions of Approval:**

None

### DEVELOPMENT FINANCE COMMENTS

No comment no concerns

**Development Finance Recommended Conditions of Approval:**

None

### BY-LAW AND COMPLIANCE, LICENSING AND PERMIT SERVICES COMMENTS

No comments received to date.

**BCLPS Recommended Conditions of Approval:**

None

### BUILDING INSPECTION (SEPTIC) COMMENTS

No comments received to date.

<b>Building Inspection Recommended Conditions of Approval:</b>	None
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### FIRE DEPARTMENT COMMENTS

No comments received to date.

<b>Fire Department Recommended Conditions of Approval:</b>	None
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### SCHEDULES TO STAFF REPORT

\*See Schedule for list of correspondence

<b>Schedule A</b>	Drawings & Plans Submitted with the Application
<b>Schedule B</b>	Staff & Agency Comments
<b>Schedule C</b> (if required)	Correspondence (Received from Public & Applicant)
<b>Schedule D</b> (if required)	Previous COA Decisions on the Subject Land

Should the Committee find it appropriate to approve this application in accordance with request and the sketch submitted with the application, as required by Ontario Regulation 200/96, the following conditions have been recommended:

### SUMMARY OF RECOMMENDED CONDITIONS OF APPROVAL

All conditions of approval, unless otherwise stated, are considered to be incorporated into the approval “**if required**”. If a condition is no longer required after an approval is final and binding, the condition may be waived by the respective department or agency requesting conditional approval. A condition cannot be waived without written consent from the respective department or agency.

#	DEPARTMENT / AGENCY	CONDITION(S) DESCRIPTION
	None	

### IMPORTANT INFORMATION – PLEASE READ

**CONDITIONS:** It is the responsibility of the owner/applicant and/or authorized agent to obtain and provide a clearance letter from respective department and/or agency (**see condition chart above for contact**). This letter must be provided to the Secretary-Treasurer to be finalized. All conditions must be cleared prior to the issuance of a Building Permit.

**APPROVALS:** Making any changes to your proposal after a decision has been made may impact the validity of the Committee’s decision.

An approval obtained from the Committee of Adjustment, where applicable, is tied to the building envelope shown on the plans and drawings submitted with the application and subject to the variance approval.

A building envelope is defined by the setbacks of the buildings and/or structures shown on the plans and drawings submitted with the application, as required by Ontario Regulation 200/96. Future development outside of an approved building envelope, where a minor variance was obtained, must comply with the provisions of the City’s Zoning By-law.

Elevation drawings are provided to reflect the style of roof (i.e. flat, mansard, gable etc.) to which a building height variance has been applied. Where a height variance is approved, building height is applied to the style of roof (as defined in the City’s Zoning By-law) shown on the elevation plans submitted with the application.

Architectural design features that are not regulated by the City’s Zoning By-law are not to be considered part of an approval unless specified in the Committee’s decision.

**DEVELOPMENT CHARGES:** That the payment of the Regional Development Charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the Regional Development Charges By-law in effect at the time of payment.

That the payment of the City Development Charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the City’s Development Charges By-law in effect at the time of payment.

That the payment of the Education Development Charge if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the Boards of Education By-laws in effect at the time of payment

That the payment of Special Area Development charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and The City’s

### **IMPORTANT INFORMATION – PLEASE READ**

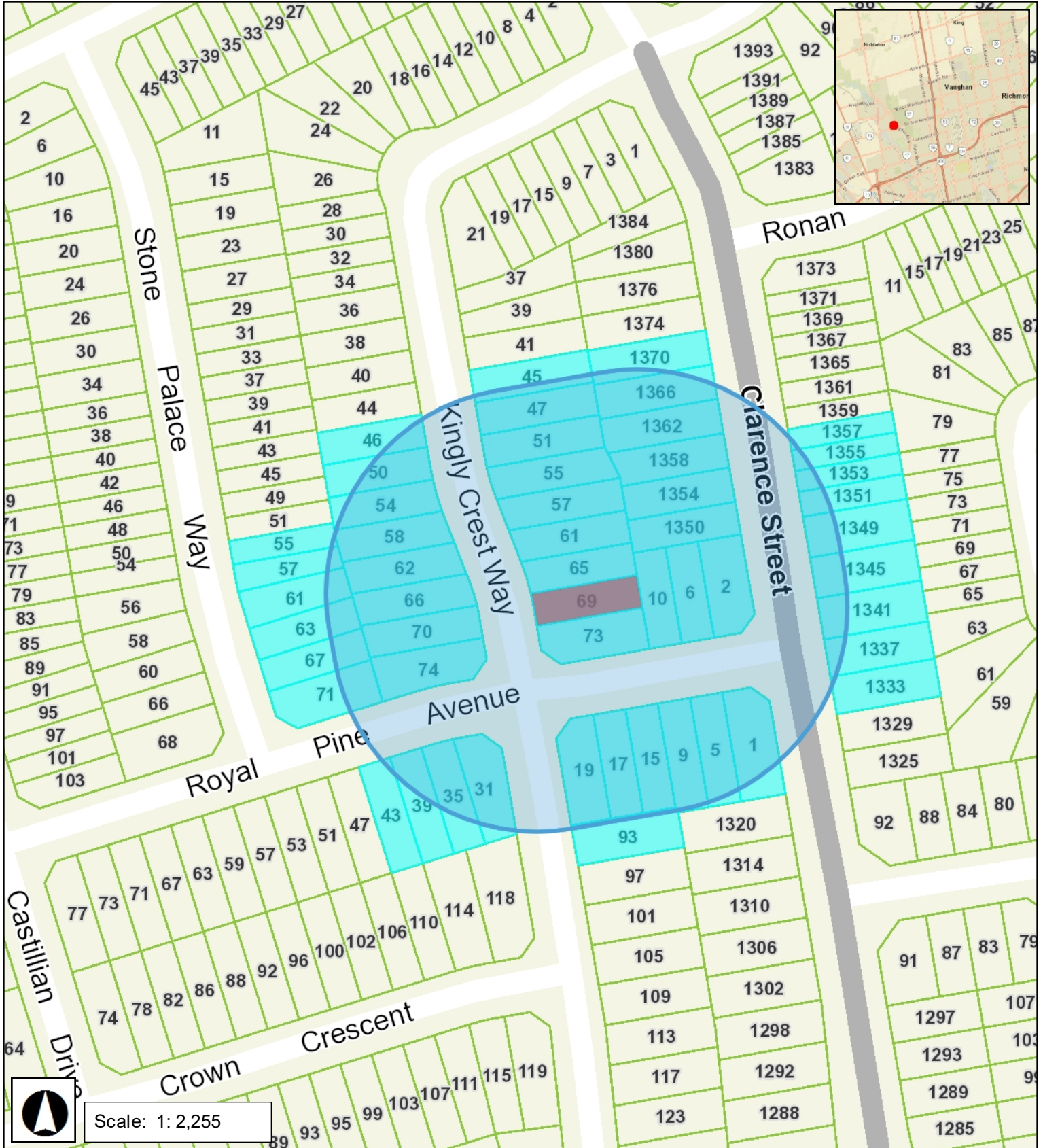
Development Charge By-law in effect at the time of Building permit issuance to the satisfaction of the Reserves/Capital Department.

**NOTICE OF DECISION:** If you wish to be notified of the decision in respect to this application or a related Ontario Land Tribunal (OLT) hearing you must complete a Request for Decision form and submit to the Secretary Treasurer (ask staff for details). In the absence of a written request to be notified of the Committee's decision you will **not** receive notice.



**SCHEDULE A: DRAWINGS & PLANS**

69 Kingly Crest Way, Woodbridge

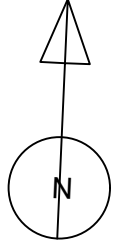


**RECEIVED**  
By attwalap at 9:04 am, Nov 10, 2023

**A136/23**



KEY PLAN



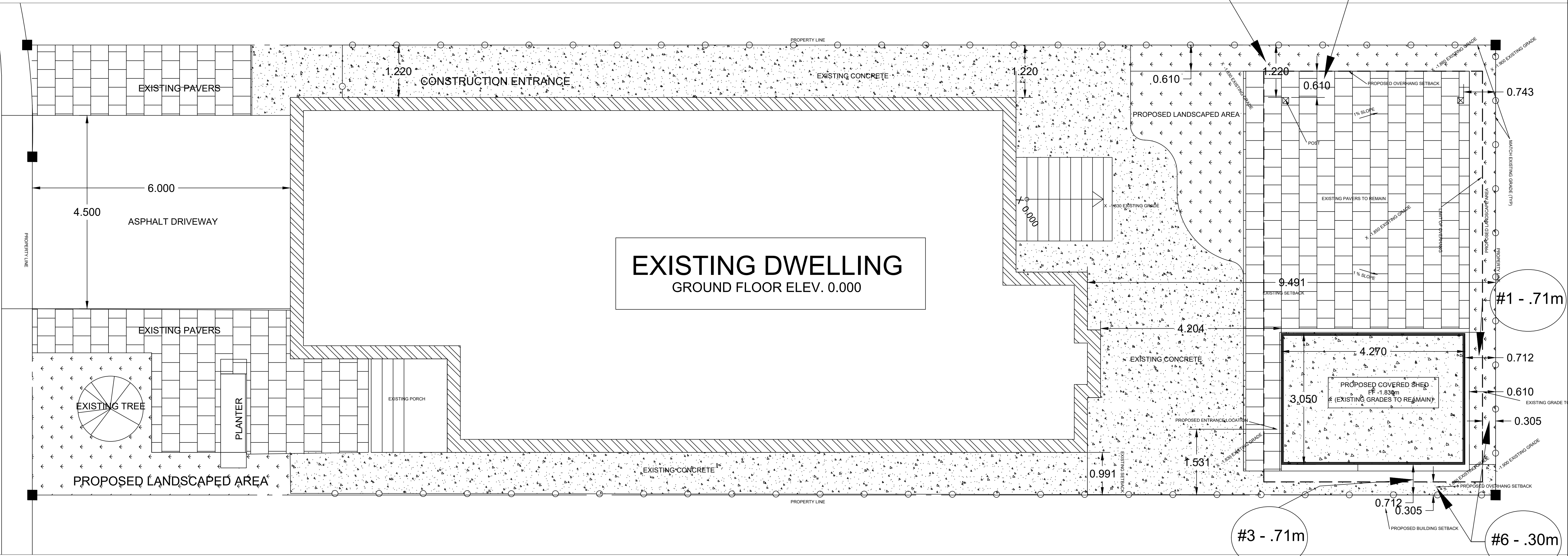
<u>SITE STATISTICS</u>	SQ M	SQ M	
LOT AREA	EXISTING 356.34	PROPOSED	REQUIRED
ZONE	R4A		
<b>BUILDING AREAS</b>			
PROPOSED SHED FOOTPRINT		13.02	
PROPOSED SHED ROOF COVERAGE		50.13	
EXISTING DWELLING FOOTPRINT	138.18		
LOT COVERAGE	38.77%	52.84%	55% MAX

#4 - 50.13sm

#2 - 1.22m

#5 - .61m

KINGLY CREST WAY



**RECEIVED**

By providel at 4:26 pm, Nov 02, 2023

**A136/23**

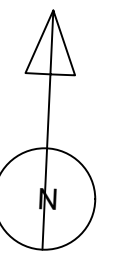
**SITE STATISTICS**

	SQ M	SQ M	
LOT AREA	356.34	PROPOSED	REQUIRED
ZONE	R4A		
<b>BUILDING AREAS</b>			
PROPOSED SHED FOOTPRINT		13.02	
PROPOSED SHED ROOF COVERAGE		50.13	
EXISTING DWELLING FOOTPRINT	138.18		
LOT COVERAGE	38.77%	52.84%	55% MAX

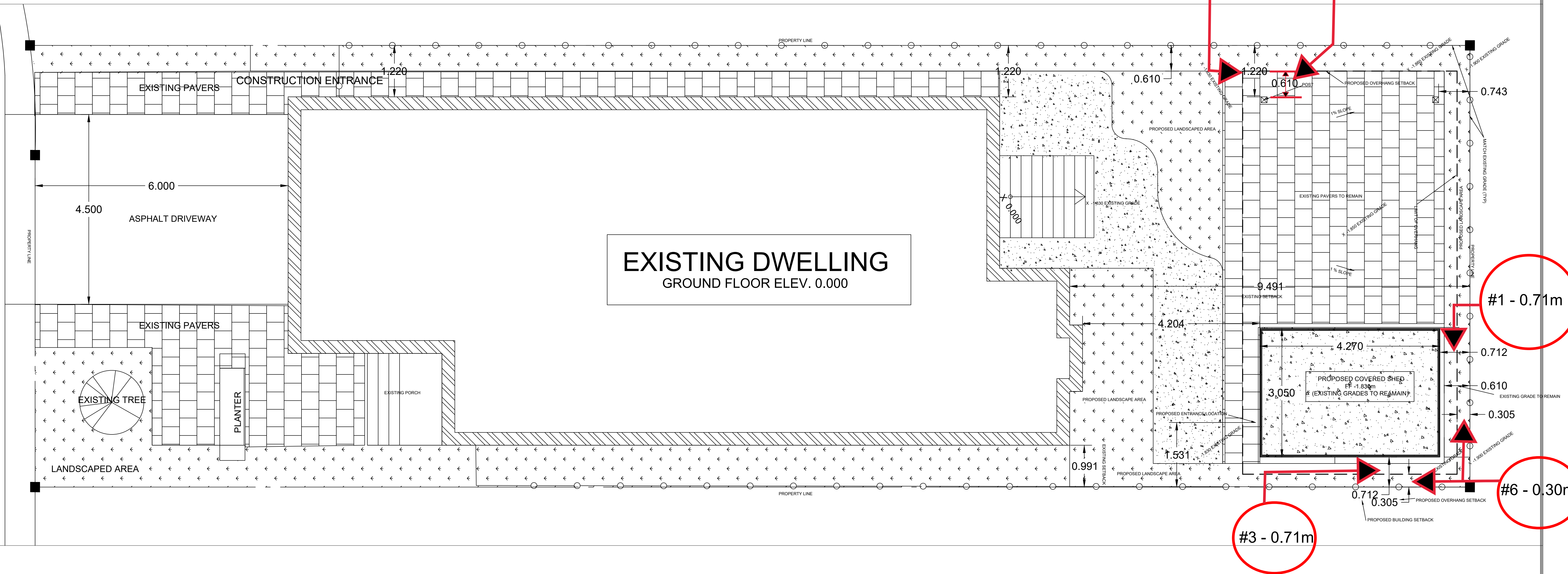
#4 - 50.13 sm



KEY PLAN



KINGLY CREST WAY



EXISTING DWELLING  
GROUND FLOOR ELEV. 0.000

PROPOSED COVERED SHED  
FF = 1.839m  
(EXISTING GRADES TO REMAIN)

**Received from Planning Dept.**  
October 31, 2023



**Received from Planning Dept.**

October 31, 2023



**Received from Planning Dept.**

October 31, 2023



**Received from Planning Dept.**  
October 31, 2023





**Received from Planning Dept.**

October 31, 2023



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October 31, 2023



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October 31, 2023



**Received from Planning Dept.**  
October 31, 2023



**Received from Planning Dept.**

October 31, 2023



**Received from Planning Dept.**

October 31, 2023



**SCHEDULE B: STAFF & AGENCY COMMENTS**

<b>DEPT/AGENCY</b>	<b>Circulated</b>	<b>Comments Received</b>	<b>Conditions</b>	<b>Nature of Comments</b>
TRCA *Schedule B	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Ministry of Transportation (MTO) *Schedule B	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Region of York *Schedule B	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Alectra *Schedule B	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Bell Canada *Schedule B	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	No Comments Received to Date
YRDSB *Schedule B	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
YCDSB *Schedule B	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
CN Rail *Schedule B	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
CP Rail *Schedule B	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
TransCanada Pipeline *Schedule B	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	No Comments Received to Date
Metrolinx *Schedule B	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Propane Operator *Schedule B	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Development Planning	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Recommend Refusal
Building Standards (Zoning)	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments

**Date:** October 24<sup>th</sup> 2023

**Attention:** **Christine Vigneault**

**RE:** Request for Comments

**File No.:** **A136-23**

**Related Files:**

**Applicant** lou maffeo

**Location** 69 Kingly Crest Way





Discover the possibilities

### COMMENTS:

- We have reviewed the proposed Variance Application and have no comments or objections to its approval.
- We have reviewed the proposed Variance Application and have no objections to its approval, subject to the following comments (attached below).
- We have reviewed the proposed Variance Application and have the following concerns (attached below).

Alectra Utilities (formerly PowerStream) has received and reviewed the proposed Variance Application. This review, however, does not imply any approval of the project or plan.

All proposed billboards, signs, and other structures associated with the project or plan must maintain minimum clearances to the existing overhead or underground electrical distribution system as specified by the applicable standards, codes and acts referenced.

In the event that construction commences, and the clearance between any component of the work/structure and the adjacent existing overhead and underground electrical distribution system violates the Occupational Health and Safety Act, the customer will be responsible for 100% of the costs associated with Alectra making the work area safe. All construction work will be required to stop until the safe limits of approach can be established.

In the event construction is completed, and the clearance between the constructed structure and the adjacent existing overhead and underground electrical distribution system violates the any of applicable standards, acts or codes referenced, the customer will be responsible for 100% of Alectra's cost for any relocation work.

### References:

- Ontario Electrical Safety Code, latest edition (Clearance of Conductors from Buildings)
- Ontario Health and Safety Act, latest edition (Construction Protection)
- Ontario Building Code, latest edition (Clearance to Buildings)
- PowerStream (Construction Standard 03-1, 03-4), attached
- Canadian Standards Association, latest edition (Basic Clearances)

If more information is required, please contact either of the following:

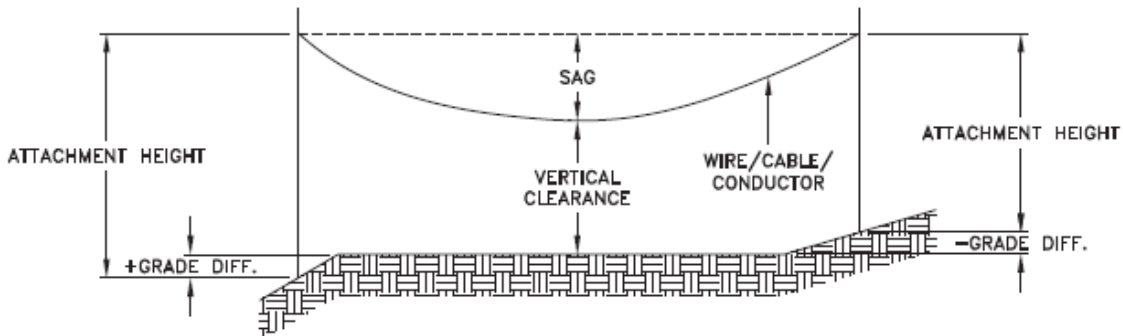
Mr. Stephen Cranley, C.E.T  
Supervisor, Distribution Design, ICI & Layouts (North)  
**Phone:** 1-877-963-6900 ext. 31297

**E-mail:** [stephen.cranley@alectrautilities.com](mailto:stephen.cranley@alectrautilities.com)

Mitchell Penner  
Supervisor, Distribution Design-Subdivisions  
**Phone:** 416-302-6215

**Email:** [Mitchell.Penner@alectrautilities.com](mailto:Mitchell.Penner@alectrautilities.com)

LOCATION OF WIRES, CABLES OR CONDUCTORS	SYSTEM VOLTAGE			
	SPAN GUYS AND COMMUNICATIONS WIRES	UP TO 600V AND NEUTRAL	4.16/2.4kV TO 27.6/16kV (SEE NOTE 1)	44kV
MINIMUM VERTICAL CLEARANCES (SEE NOTE 2)				
OVER OR ALONGSIDE ROADS, DRIVEWAYS OR LANDS ACCESSIBLE TO <u>VEHICLES</u>	442cm	442cm	480cm	520cm
OVER GROUND ACCESSIBLE TO <u>PEDESTRIANS AND BICYCLES ONLY</u>	250cm	310cm	340cm	370cm
ABOVE TOP OF RAIL AT <u>RAILWAY CROSSINGS</u>	730cm	730cm	760cm	810cm



MINIMUM ATTACHMENT HEIGHT = MAXIMUM SAG  
 + MINIMUM VERTICAL CLEARANCE (FROM ABOVE TABLE)  
 ± GRADE DIFFERENCE  
 + 0.3m (VEHICLE OR RAILWAY LOCATION)  
 + SNOW DEPTH (PEDESTRIAN LOCATION, SEE NOTE 3)

**NOTES:**

1. THE MULTIGROUNDED SYSTEM NEUTRAL HAS THE SAME CLEARANCE AS THE 600V SYSTEM.
2. THE VERTICAL CLEARANCES IN THE ABOVE TABLE ARE UNDER MAXIMUM SAG CONDITIONS.
3. REFER TO CSA STANDARD C22.3 No.1, ANNEX D FOR LOCAL SNOW DEPTH VALUES.
4. ALL CLEARANCES ARE IN ACCORDANCE TO CSA STANDARD C22.3.

**CONVERSION TABLE**

METRIC	IMPERIAL (APPROX)
810cm	27'-0"
760cm	25'-4"
730cm	24'-4"
520cm	17'-4"
480cm	16'-0"
442cm	15'-5"
370cm	12'-4"
340cm	11'-4"
310cm	10'-4"
250cm	8'-4"

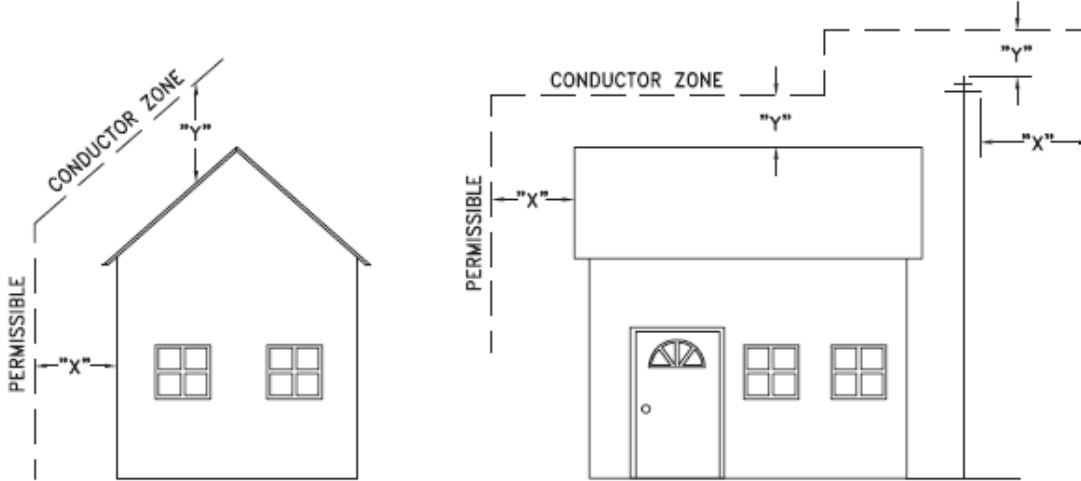
**REFERENCES**

SAGS AND TENSIONS | SECTION 02

**MINIMUM VERTICAL CLEARANCES OF WIRES, CABLES AND CONDUCTORS ABOVE GROUND OR RAILS**

ORIGINAL ISSUE DATE: 2010-DEC-24 REVISION NO: R1 REVISION DATE: 2012-JAN-09

Certificate of Approval	
This construction Standard meets the safety requirements of Section 4 of Regulation 22/04	
Joe Crozier, P.Eng.	2012-JAN-09
Name	Date
P.Eng. Approval By:	Joe Crozier



VOLTAGE	MINIMUM HORIZONTAL CLEARANCE UNDER MAXIMUM SWING CONDITIONS DIMENSION "X" (SEE NOTES 1, 3 & 4)	MINIMUM VERTICAL CLEARANCE UNDER MAXIMUM DESIGN SAG CONDITIONS DIMENSION "Y" (SEE NOTES 1, 2, 4 & 5)
0-600V AND NEUTRAL	100cm	250cm
4.16/2.4 TO 44kV	300cm	480cm

**NOTES**

1. UNDER NO CIRCUMSTANCES SHALL A CONDUCTOR BE PERMITTED TO PENETRATE THE ENVELOPE SHOWN BY THE DOTTED LINE.
2. THE VERTICAL CLEARANCES ARE UNDER CONDITIONS OF MAXIMUM DESIGN SAG.
3. THE HORIZONTAL CLEARANCES ARE UNDER CONDITIONS OF MAXIMUM SWING. WHERE THE CONDUCTOR SWING IS NOT KNOWN A HORIZONTAL CLEARANCE OF 480CM SHALL BE USED.
4. BUILDINGS THAT EXCEED 3 STOREYS OR 15M IN HEIGHT, THE MINIMUM HORIZONTAL CLEARANCE OF THE SECONDARY CONDUCTORS SHOULD BE INCREASED TO 300cm WHERE IT IS NECESSARY TO ALLOW FOR THE RAISING OF LADDERS BY LOCAL FIRE DEPARTMENTS.
5. IN SITUATIONS SUCH AS MULTI-LEVEL GARAGES, WHERE ROOFS ARE NORMALLY USED BY PERSONS AND VEHICLES, THE VERTICAL CLEARANCES OF POWERSTREAM STANDARD 03-1 SHALL APPLY.
6. DISTRIBUTION LINES CONSTRUCTED NEAR BUILDINGS SHALL BE BUILT TO AVOID OVERHANG WHEREVER POSSIBLE. WHERE LINES MUST BE CONSTRUCTED OVER OR ADJACENT TO BUILDINGS THE APPLICABLE HORIZONTAL AND VERTICAL CLEARANCES SHALL BE AT CONDITIONS OF MAXIMUM CONDUCTOR SWING AND MAXIMUM SAG. THE ABOVE CLEARANCES ARE DESIGNED TO PREVENT PERSONS ON OR IN BUILDINGS AS WELL AS EXTERNAL MACHINERY USED IN CONJUNCTION WITH A BUILDING TO COME IN CONTACT WITH CONDUCTORS. EFFORTS SHOULD BE MADE TO INCREASE THESE CLEARANCES WHERE POSSIBLE.
7. ALL CLEARANCES ARE IN ACCORDANCE TO CSA C22.3 NO.1-06 (TABLE-9).

METRIC	IMPERIAL (APPROX)
480cm	16'-0"
300cm	10'-0"
250cm	8'-4"
100cm	3'-4"

**MINIMUM VERTICAL & HORIZONTAL CLEARANCES OF CONDUCTORS FROM BUILDINGS OR OTHER PERMANENT STRUCTURES (CONDUCTORS NOT ATTACHED TO BUILDINGS)**

**Certificate of Approval**  
This construction Standard meets the safety requirements of Section 4 of Regulation 22/04  
Debbie Dadwani, P.Eng. 2010-MAY-05  
Name Date  
P. Eng. Approval By: *D. Dadwani*

ORIGINAL ISSUE DATE: 2010-MAY-05 REVISION NO: REVISION DATE:  
PS:\System Planning and Standards\Standard Design\PowerStream Standards\PowerStream Standards working files\Section 3-1-02\WG 03-4 R0 May 5, 2010.dwg, 5/5/2010 10:22:02 AM, Adobe PDF

**To:** Committee of Adjustment  
**From:** Bernd Paessler, Building Standards Department  
**Date:** November 13, 2023  
**Applicant:** Lou Maffeo  
**Location:** 69 Kingly Crest Way  
 PLAN 65M3423 Lot 50  
**File No.(s):** A136/23

**Zoning Classification:**

The subject lands are zoned R4A(EN) – Fourth Density Residential Zone (Established Neighbourhood) under Zoning By-law 001-2021, as amended.

#	Zoning By-law 001-2021	Variance requested
1	A residential accessory structure with a height greater than 2.8 m shall not be located closer than 2.4 m to any lot line. Section 4.1.2 1b	To permit a residential accessory structure with a height greater and 2.8m to be setback 0.71m from the rear lot line.
2	A residential accessory structure with a height greater than 2.8 m shall not be located closer than 2.4 m to any lot line. Section 4.1.2 1b	To permit the residential accessory structure with a height greater and 2.8m to be setback 1.22m from the north side interior lot line.
3	A residential accessory structure with a height greater than 2.8 m shall not be located closer than 2.4 m to any lot line. Section 4.1.2 1b	To permit the residential accessory structure with a height greater and 2.8m to be setback 0.71m from the south side interior lot line.
4	In any Residential zone, the maximum lot coverage of all accessory buildings and residential accessory structures shall be 10% or 67m <sup>2</sup> whichever is less. 10% of the lot area is 35.6m <sup>2</sup> Section 4.1.3 1	To permit a maximum lot coverage of 14% for the residential accessory structure.
5	The maximum permitted encroachment for eaves is 0.50m into the required interior side yard. Section 4.13 Table 4-1	To permit a maximum encroachment of 0.61m into the required interior side yard on the north lot line.
6	Unless otherwise expressly permitted by this By-law, a minimum distance of 0.6 m shall be required from any permitted encroachment to the nearest lot line. Section 4.13 3	To permit a minimum distance of 0.30m to any permitted encroachment to the nearest lot line.

**Staff Comments:**

**Stop Work Order(s) and Order(s) to Comply:**

There are no outstanding Orders on file

**Building Permit(s) Issued:**

**Other comments:**

General Comments	
1	The applicant shall be advised that additional variances may be required upon review of detailed drawing for building permit/site plan approval.
2	The height of a residential accessory structure shall be measured in accordance with the definitions in section 3.0 DEFINITIONS of bylaw 001-2021 as amended. Established grade shall be the approved grading at the time of the original building permit for the principal dwelling.

**Conditions of Approval:**

If the committee finds merit in the application, the following conditions of approval are recommended.

\* Comments are based on the review of documentation supplied with this application.

**To:** Christine Vigneault, Committee of Adjustment Secretary Treasurer  
**From:** Nancy Tuckett, Director of Development Planning  
**Date:** November 29, 2023  
**Name of Owners:** Louie and Consolata Maffeo  
**Location:** 69 Kingly Crest Way  
**File No.(s):** A136/23

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**Proposed Variances (By-law 001-2021):**

1. To permit a residential accessory structure with a height greater and 2.8 m to be setback 0.71 m from the rear lot line.
2. To permit the residential accessory structure with a height greater and 2.8 m to be setback 1.22 m from the north side interior lot line.
3. To permit the residential accessory structure with a height greater and 2.8 m to be setback 0.71 m from the south side interior lot line.
4. To permit a maximum lot coverage of 14% for the residential accessory structure.
5. To permit a maximum encroachment of 0.61 m into the required interior side yard on the north lot line.
6. To permit a minimum distance of 0.30 m to any permitted encroachment to the nearest lot line.

**By-Law Requirements (By-law 001-2021):**

1. A residential accessory structure with a height greater than 2.8 m shall not be located closer than 2.4 m to any lot line.
2. A residential accessory structure with a height greater than 2.8 m shall not be located closer than 2.4 m to any lot line.
3. A residential accessory structure with a height greater than 2.8 m shall not be located closer than 2.4 m to any lot line.
4. In any Residential zone, the maximum lot coverage of all accessory buildings and residential accessory structures shall be 10% or 67m<sup>2</sup> whichever is less. 10% of the lot area is 35.6 m<sup>2</sup>.
5. The maximum permitted encroachment for eaves is 0.50 m into the required interior side yard.
6. Unless otherwise expressly permitted by this By-law, a minimum distance of 0.6 m shall be required from any permitted encroachment to the nearest lot line.

**Official Plan:**

Vaughan Official Plan 2010 ('VOP 2010'): "Low-Density Residential".

**Comments:**

The Owners are requesting relief to permit an existing accessory structure within the rear yard of 69 Kingly Crest Way with the above noted variances.

The Development Planning Department is not in a position to support Variances 1, 2, 3, 5, and 6 for the reduced rear and interior side yard setbacks to the accessory structure. The 2.9 m high structure has a flat roof design and contains an enclosed storage room and a covered outdoor lounge area. The 9.49 m length of the structure, inclusive of eaves, occupies about 91% of the rear yard's 10.4 m width. The rear yard of the subject property abuts neighbouring rear yards to the north and south and an interior side yard to the east. The structure's length relative to the length of the rear lot line, in combination with its proximity to the side and rear lot lines and its overall footprint results in a significant amount of massing and privacy impacts.

A function of an interior side and rear yard setback is to restrict the built form's proximity to said lot line to address items such as massing and privacy. Setback Variances 1, 2, & 3 are measured from the walls/support posts of the structure rather than the eaves. Therefore, while relief is being sought for 1.22 m and 0.71 m interior side yard setbacks and a 0.71 m rear yard setback, the spatial separation between the eave and lot lines are 0.61 m, 0.305, and 0.305 m respectively. The eaves (Variances 5 and 6), due to the roofline proposed, contribute towards the mass and coverage impacts generated, and

further hinder the establishment of adequate vegetative buffering by providing added shade and rain shadow. There is insufficient spatial separation to buffer the structure and the use it would generate from the abutting yards nor is there sufficient space to establish buffering vegetation to mitigate its mass and privacy impacts anticipated from the placement of such a large building to abutting yards.

The structure, in its existing state, occupies most of the rear yard. The rear yard depth (without the structure) is measured at 9.49 m from the dwelling to the rear lot line. The structure, inclusive of eaves, is setback approximately 4.09 m from the dwelling (occupying approximately 54% of the rear yard's depth) and as previously mentioned above, it spans about 91% of the rear yard's width. The existing structure generates substantial mass via its built form into the rear yard, impacting the overall function of the rear yard as a landscaped outdoor amenity space. A function of a rear yard is to provide sufficient outdoor landscaped (hard and vegetative surface) space to provide open air amenity and recreational opportunities. It is anticipated that a structure of such scale will also generate use impacts greater than a structure of a smaller size. These more intensive uses also cannot be adequately buffered for reasons identified above. It is of Staff's opinion that this structure impedes the functionality of the rear yard of the subject property as well as neighbouring rear yards. Adverse massing and privacy impacts are anticipated to the abutting rear yards to the north and south and the abutting side yard to the east.

Therefore, for Variances 1-3, and 5-6, it is staff's opinion that permitting a structure of this scale where only a 1.22 m and 0.71 m interior side yard setback as well as a 0.71 m rear yard setback remains, even if eaves were not to be considered, is a substantial deviation from the zoning by-law's intent, is not minor in nature, nor desirable and appropriate for the use of the land.

The Development Planning Department is not in position to support Variance 4 to permit a maximum lot coverage of 14%. Urban Design staff reviewed the proposal and are of the opinion that the existing structure is too close to the three surrounding neighbours and that unsuitable space is provided to accommodate any vegetative screening. Both Development Planning and Urban Design staff agree that the structure cannot be supported in its existing state. A function of accessory structure lot coverage is to maintain a modest (i.e., accessory) scale in relation to the dwelling and yard. As identified in the analysis above, the structure occupies a majority of the rear yard and as such it becomes a primary building due to its visual prominence. There are no nearby structures of this size or mass and there is no vegetation present on the subject property or neighbouring properties to mitigate the potential adverse impacts the structure poses on not just the properties within the immediate context, but also on properties further to the north and the streetscape to the south (Royal Pine Avenue).

The increase in maximum lot coverage and reduction in lot line setbacks is not consistent with previous approvals, nor compatible and consistent with other residential lots within the existing subdivision and therefore, cannot be deemed minor in nature, nor desirable and appropriate for the development of the land as it is anticipated to adversely impact the character of the neighbourhood due to its massing and coverage prominence.

Accordingly, the Development Planning Department cannot support Variances 1 through 6 and is of the opinion that the proposal is not minor in nature, does not maintain the general intent and purpose of the Zoning By-law, and is not desirable and appropriate for the development of the land.

**Recommendation:**

The Development Planning Department recommends **refusal** of the application.

**Conditions of Approval:**

If the Committee finds merit in the application, the following conditions of approval are recommended:

None.

**Comments Prepared by:**

Nicholas Del Prete, Planner I  
David Harding, Senior Planner

Attachments: Site Visit Photos







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## Prabhdeep Kaur

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**From:** Kristen Regier <Kristen.Regier@trca.ca>  
**Sent:** Wednesday, October 18, 2023 11:33 AM  
**To:** Committee of Adjustment  
**Cc:** Prabhdeep Kaur  
**Subject:** [External] RE: A136/23 - REQUEST FOR COMMENTS, CITY OF VAUGHAN

Hello,

The subject property at 69 Kingly Crest Way, Vaughan is not located within TRCA's Regulated Area. TRCA's Planning and Regulatory policy interests are not impacted.

As such, TRCA has no comments on the application.

Best,

**Kristen Regier, MA (she / her)**

Planner I

Development Planning and Permits | Development and Engineering Services

Toronto and Region Conservation Authority (TRCA)

T: [437-880-2129](tel:437-880-2129)

E: [kristen.regier@trca.ca](mailto:kristen.regier@trca.ca)

A: [101 Exchange Avenue, Vaughan, ON, L4K 5R6](https://www.trca.ca) | [trca.ca](https://www.trca.ca)



## Lenore Providence

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**Subject:** FW: [External] Re: A136/23 - REQUEST FOR COMMENTS, CITY OF VAUGHAN

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**From:** Monika Sadler <monika.sadler@ycdsb.ca>

**Sent:** October-18-23 2:32 PM

**To:** Committee of Adjustment <CofA@vaughan.ca>

**Subject:** [External] Re: A136/23 - REQUEST FOR COMMENTS, CITY OF VAUGHAN

Hello,

Thank you for the opportunity to review the above referenced Minor Variance application. YCDSB staff have reviewed the material provided and have no comments or objections to their approval.

Thank you,

Monika Sadler  
Planner and Project Analyst, Planning Services  
York Catholic District School Board  
T. 905.713.1211 ext. 12377  
[monika.sadler@ycdsb.ca](mailto:monika.sadler@ycdsb.ca)

## Lenore Providence

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**Subject:** FW: [External] RE: A136/23 - REQUEST FOR COMMENTS, CITY OF VAUGHAN

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**From:** Development Services <developmentsservices@york.ca>  
**Sent:** October-20-23 4:13 PM  
**To:** Prabhdeep Kaur <Prabhdeep.Kaur@vaughan.ca>  
**Cc:** Committee of Adjustment <CofA@vaughan.ca>  
**Subject:** [External] RE: A136/23 - REQUEST FOR COMMENTS, CITY OF VAUGHAN

Hi Prabhdeep,

The Regional Municipality of York has completed its review of the updated minor variance application – A136/23 (69 Kingly Crest Way) and has no comment.

Please provide us with a copy of the notice of decision for our records.

Many thanks,

*Our working hours may be different. Please do not feel obligated to reply outside of your scheduled working hours. Let's work together to help foster healthy work-life boundaries.*

**Niranjan Rajevan, M.Pl.** | Associate Planner, Development Services, Planning and Economic Development, Corporate Services

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The Regional Municipality of York | 17250 Yonge Street | Newmarket, ON L3Y 6Z1  
1-877-464-9675 ext. 71521 | [niranjan.rajevan@york.ca](mailto:niranjan.rajevan@york.ca) | [www.york.ca](http://www.york.ca)  
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**SCHEDULE C: PUBLIC & APPLICANT CORRESPONDENCE**

<b>Correspondence Type</b>	<b>Name</b>	<b>Address</b>	<b>Date Received</b> <small>(mm/dd/yyyy)</small>	<b>Summary</b>
Public	Rose Vella	73 Kingly Crest Way	09/22/2023	No flooding or drainage issues
Public	Frank Paguapas	10 Royal Pine Avenue	09/22/2023	No flooding or drainage issues

September 20, 2023.

Frank ~~PAGLIARAS~~

10 Royal Pine Avenue


Woodbridge Ontario

L4H 1T1

Frank,

This letter is to inform the City Of Vaughan that you have not experienced any unusual drainage or flooding caused by the construction of the existing concrete pad in back yard of 69 Kingly Crest Way.

Best Regards,

A handwritten signature in blue ink, consisting of several overlapping loops and a long horizontal stroke at the bottom.

Frank

September 20, 2023.

Rose Vella  
73 Kingly Crest Way  
Woodbridge Ontario  
L4H 1T1

Rose Vella,

This letter is to inform the City Of Vaughan that you have not experienced any unusual drainage or flooding caused by the construction of the existing concrete pad in back yard of 69 Kingly Crest Way.

Best Regards,

A handwritten signature in blue ink, appearing to read 'Rose Vella', with a long horizontal flourish extending to the right.

Rose Vella



**SCHEDULE D: PREVIOUS COA DECISIONS ON THE SUBJECT LAND**

None