

CITY OF VAUGHAN

EXTRACT FROM COUNCIL MEETING MINUTES OF MAY 14, 2019

Item 21, Report No. 17, of the Committee of the Whole, which was adopted without amendment by the Council of the City of Vaughan on May 14, 2019.

21. NEW CONSTRUCTION -TWO STOREY ADDITION 169 CLARENCE STREET WOODBRIDGE HERITAGE CONSERVATION DISTRICT

The Committee of the Whole recommends approval of the recommendation contained in the following report of the Deputy City Manager, Corporate Services, dated May 7, 2019:

Recommendations

The Deputy City Manager, Corporate Services, on behalf of Heritage Vaughan forwards the following recommendation from its meeting of March 20, 2019 (Item 4, Report No. 1, for consideration:

1. That the recommendation contained in the following report of the Deputy City Manager, Planning and Growth Management, dated March 20, 2019, be approved;
2. That the style and proportion of the window over the garage of the proposed extension be match the dormer of the original structure;
and
3. That the colour of the garage door be more reflective of the colours of the original building.

Committee of the Whole Report

DATE: Tuesday, May 07, 2019

WARD(S): 2

**TITLE: NEW CONSTRUCTION -TWO STOREY
ADDITION 169 CLARENCE STREET
WOODBIDGE HERITAGE CONSERVATION DISTRICT**

FROM:

Nick Spensieri, Deputy City Manager, Corporate Services

ACTION: DECISION

Purpose

To forward a recommendation from Heritage Vaughan Committee for the demolition of a detached garage and the construction of a two-storey addition to the existing one and a half-storey detached dwelling located at 169 Clarence Street, a property located in the Woodbridge Heritage Conservation District Plan and designated under Part V of the *Ontario Heritage Act*.

Report Highlights

- The Owner is proposing to demolish an existing detached garage and construct a two-storey addition to an existing one and a half-storey dwelling located at 169 Clarence Street.
- The proposal is consistent with the relevant policies of the Woodbridge Heritage Conservation District Plan ("WHCD Plan").
- Heritage Vaughan approval is required under the Ontario Heritage Act ("OHA").
- Staff are recommending approval of the proposal as it conforms with the WHCD Plan.

Recommendations

The Deputy City Manager, Corporate Services, on behalf of Heritage Vaughan forwards the following recommendation from its meeting of March 20, 2019 (Item 4, Report No. 1, for consideration:

1. That the recommendation contained in the following report of the Deputy City Manager, Planning and Growth Management, dated March 20, 2019, be approved;
2. That the style and proportion of the window over the garage of the proposed extension be match the dormer of the original structure; and
3. That the colour of the garage door be more reflective of the colours of the original building.

Background

Recommendation and Report of the Deputy City Manager, Planning and Growth Management, dated March 20, 2019:

1. THAT Heritage Vaughan Committee recommend approval to Council for the proposed demolition of the existing garage and a two-storey addition to the existing dwelling at 169 Clarence Street under Section 42 of Ontario Heritage Act, subject to the following conditions:
2.
 - a) Any significant changes to the proposal by the Owner, may require reconsideration by the Heritage Vaughan Committee, which shall be determined at the discretion of the Director of Development Planning and Manager of Urban Design and Cultural Heritage;
 - c) That Heritage Vaughan Committee recommendations to Council do not constitute specific support for any Development Application under the *Ontario Planning Act* or permits currently under review or to be submitted in the future by the Owner as it relates to the subject application; and
 - d) That the applicant submit Building Permit stage architectural drawings and building material specifications to the satisfaction of the Vaughan Development Planning Department, Urban Design and Cultural Heritage Division.

The subject property is municipally known as 169 Clarence Street and is located on the east side of Clarence Street and north of Meeting House Road, as shown in Attachment 1.

The existing building is identified as a contributing property in the Woodbridge Heritage Conservation District ('WHCD') Plan. The WHCD Plan Inventory identifies the existing building as being built from 1925-1950, in the 1940's Cottage Style, as shown in Attachment 3, and notes that it is "largely original".

Previous Reports/Authority

Not applicable.

Analysis and Options

The Owner is proposing to demolish the existing detached one-storey garage and build a new two-storey addition containing a garage.

Currently, the garage is a small detached one-storey garage as shown in Attachment 4. The applicant is proposing a tandem garage with a new second-storey level connected to the existing dwelling as shown in Attachment 8. The proposal requires the demolition of the existing storage shed at the north east corner of the back yard. The proposed rooflines and second-storey dormers have been designed to complement the existing building form and features. The new addition is proposed to be set back from the front of house, roughly in line where the setback of the existing garage and with the existing architectural elements of the house being left intact and viewed in its current state. The proposed materiality for the addition utilizes shingles and brick that will be matched as close as possible to the existing material, creating a smooth transition from the existing dwelling to the proposed addition.

The proposed new construction is consistent with the following relevant sections of the WHCD Plan, with justification provided where the proposal does not meet certain policies in their entirety.

6.2.1 Standards and Guidelines for Conservation

“Rehabilitation: The action or process of ensuring a continuing use or a compatible contemporary use of a heritage resource (or an individual component) through repair, alterations, or additions, while protecting its heritage value. This can include replacing missing historic features either as an accurate replica of the feature or may be a new design that is compatible with the style, era, and character of the heritage resource”.

- The proposed addition will allow for the continued use of the contributing building while protecting its built heritage value of the original home in the 1940's Cottage Style.

Section 6.1.5 – Clarence Street and Park Drive Heritage Attributes

“1. The Street should retain the existing residential character with a single family detached building type and be designed to support a pedestrian streetscape”.

- The proposed addition will not impact the existing residential character, as the existing single family detached building type will remain and the two-storey addition is set to the side and behind the existing building and appears secondary to the main dwelling as shown in Attachments 6 and 7. The additional building height is mitigated by the placement of this addition at the rear of the property, with the

proposed preservation of existing trees that will buffer this proposed addition from Clarence Street.

“4. New buildings should be a minimum of 2 floors (8.5 m) high and a maximum of 3 floors (11 m)”.

- The proposed addition is 2 floors and 8.3 m in height. This proposed height is consistent with the allowable building height as per the above policy. It is noted that the proposed addition is greater in height than the existing building, however Cultural Heritage staff are satisfied that this additional height is mitigated by the rear/side placement of the proposed addition.
- A streetscape rendering (Attachment 10) illustrating the impact of the proposed addition on the overall streetscape has been submitted by the applicant. Cultural Heritage staff are satisfied that this streetscape rendering demonstrates that the side/rear addition contextually fits with the adjacent scale and building massing and will not negatively impact the existing contributing building or the residential character of Clarence Street.

“5. Detached residential units must provide a side yard as per zoning with open east-west views”.

- The Building Standards Department had the following comment, *“A minimum interior side yard setback of 0.6 metres is required to the shed of 2.4 m high, 5.98m², whereas a setback of 0.14 metres is shown on Drawing A3”*. However, the applicant will be relocating the shed indicated on the site plan to another location in consultation with the Building Standards Department. Therefore, no minor variances will be required to the side yard setback. Further, the shed itself has no bearing on the design or function of the proposed garage addition.

Section 6.4.2 – Street Wall Height and Scale

“The height of a building is measured from the average elevation of the finished grade at the front of the building to the highest point of the roof surface for a flat roof and a mansard roof; and to the mean height between the eaves and the highest point of a gable, hip, or a gambrel roof”.

- The proposed roof height has been correctly measured as per the above definition, resulting in a proposed height of 8.3 m.

Section 5.3.2.5 – Circulation, Vehicular Access and Parking

“On-site parking, garages, and parking structures are generally concealed behind or below inhabited buildings.”

- The proposed garage forms part of the ground level of the 2-storey addition proposed to be located to the rear side of the existing contributing building that fronts onto Clarence Street. The garage will be visible from Clarence Street, with the additional building height being mitigated by the placement of the addition, and the preservation of existing trees that will buffer this proposed addition from Clarence Street. Further, the existing garage is also currently visible from Clarence Street.

Materials

Section 6.3.3 Architectural Guidelines – Material Pallet

“There is a very broad range of materials in today’s design palette, but materials proposed for new buildings in the district should include those drawn from ones historically in use in Woodbridge. This includes brick, stone, traditional stucco; wood siding and trim, glass windows and storefronts, and various metals”.

- The proposed architectural style and materials as shown on Attachments 7 and 8 are consistent with those of the original building. The placement of the proposed stone foundation line and brick materials are consistent with the material placement of the existing house.

Section 6.2.8 – Appropriate Materials

Doors: Wood doors and frames, panel construction, may be glazed; transom windows and paired sidelights with real glazing bars; wood french doors for porch entrances; single-bay, wood panelled garage doors.

- The proposed garage door will be wood paneled.

All exterior materials are shown in Attachments 7 and 8.

Landscaping

Section 6.6.3 – Tree Canopy and Vegetation – Guidelines

“3. Trees on public and private property, having a tree diameter of twenty (20) centimeters or more or having a base diameter of twenty (20) centimetres or more, must be conserved, and the requirements of the City of Vaughan Tree Bylaw 052-2018 must be adhered to.”

- The submitted Arborist Report as shown on Attachment 9 confirms that the existing trees on the property will not be removed and will be protected in accordance with City of Vaughan requirements.

Timeline

This Application is subject to the 90-day review under the *Ontario Heritage Act*. This Application was declared complete on February 27, 2019, and must be deliberated upon by May 28, 2019, to meet the 90-day timeline.

Financial Impact

There are no requirements for new funding associated with this report.

Broader Regional Impacts/Considerations

There are no broader Regional impacts or considerations.

Conclusion

The Urban Design and Cultural Heritage Division has reviewed the Application to permit a two-storey addition to the existing dwelling on the property municipally known as 169 Clarence Street and is satisfied that the proposal is consistent with the WHCD Plan. Accordingly, the Urban Design and Cultural Heritage Division of the Development Planning Department can support the approval of the proposed alterations under Section 42 of the *Ontario Heritage Act*, subject to the recommendations in this report.

For more information, please contact: Katrina Guy, Cultural Heritage Coordinator, ext. 8115

Attachments

1. Location Map
2. Subject Property
3. Site Photos
4. Existing Level Plans
 - a. Ground Floor
 - b. Main Floor
 - c. Second Floor
5. Proposed Site Plan
6. Proposed Level Plans
 - a. Ground Floor
 - b. Main Floor
 - c. Second Floor
 - d. Roof Plan
7. Elevation Drawings
 - a. Existing and Proposed Western Elevation
 - b. Existing and Proposed Southern Elevation
 - c. Existing and Proposed Eastern Elevation
 - d. Existing and Proposed Northern Elevation

- e.
- 8. Materials
- 9. Arborist Report
- 10. Proposed Addition Streetscape

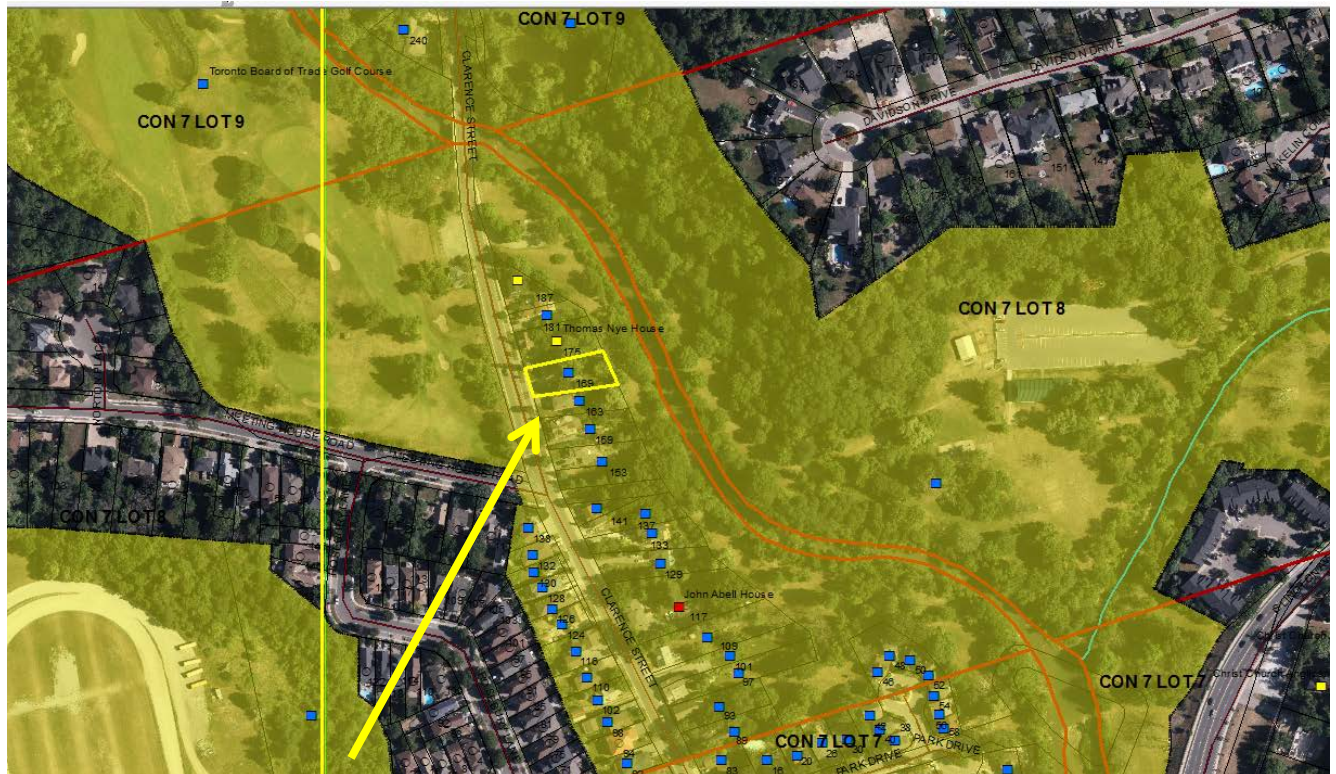
Prepared by

Katrina Guy, Cultural Heritage Coordinator, ext. 8115

Shahrzad Davoudi-Strike, Senior Urban Designer, ext. 8653

Rob Bayley, Manager of Urban Design & Cultural Heritage, ext. 8254

Location Map



169 Clarence Street

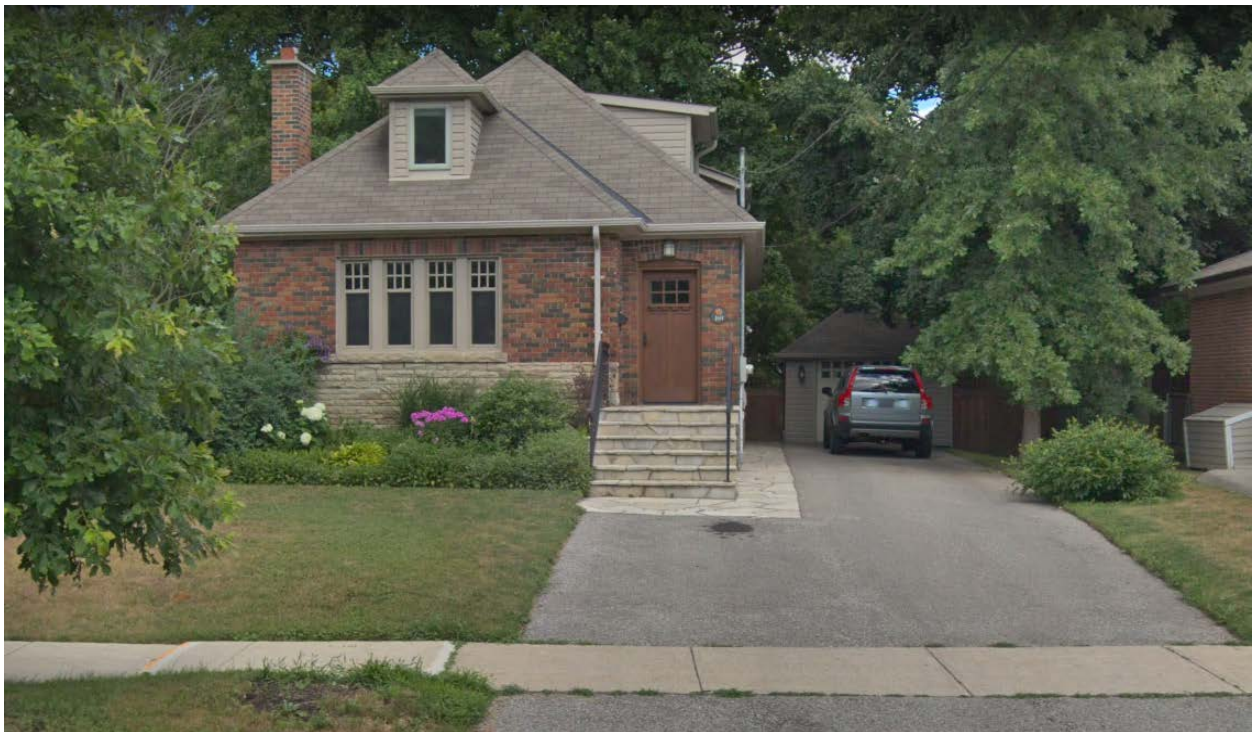
Attachment 2

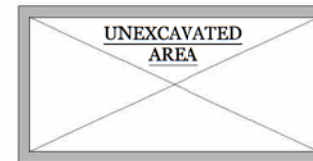
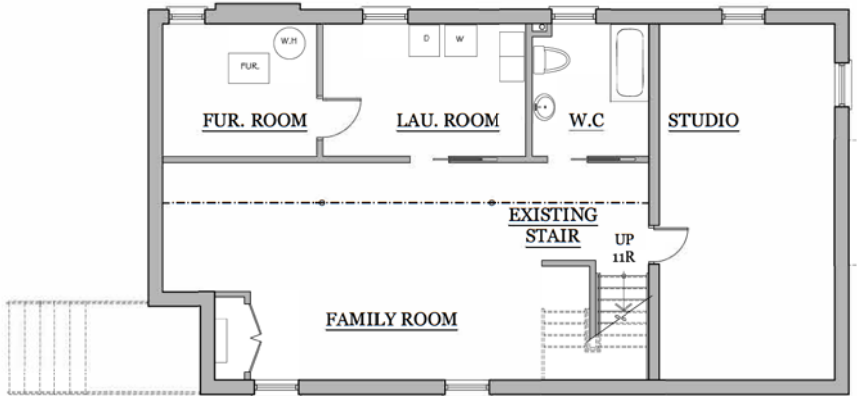
Subject Property



Attachment 3

Streetview of 169 Clarence Street





PROPOSED RENOVATION & ADDITION

Project:
169 Clarence Street,
Vaughan, ON., L4L1L4

Project #:
13021
(DA. 14.098)

No.	Description	Date	By
4.	REVISION AS PER CITY COMMENTS (4.4)	NOV 01, 2018	RR
3.	ISSUED FOR SITE DEVELOPMENT APP (4.3)	AUG. 2, 2018	GF
2.	ISSUED FOR SITE DEVELOPMENT APP (4.2)	JUL. 27, 2016	GS
1.	ISSUED FOR SITE DEVELOPMENT APP.	DEC. 23, 2014	GS



EXISTING BASEMENT FLOOR PLAN SCALE 1/8"=1'-0"

1
A4.1



PROPOSED RENOVATION & ADDITION

Project:
169 Clarence Street,
Vaughan, ON., L4L1L4

Project #:
13021
(DA. 14.098)

No.	Description	Date	By
4.	REVISION AS PER CITY COMMENTS (4.4)	NOV 01, 2018	RR
3.	ISSUED FOR SITE DEVELOPMENT APP (4.3)	AUG. 2, 2018	GF
2.	ISSUED FOR SITE DEVELOPMENT APP (4.2)	JUL. 27, 2016	GS
1.	ISSUED FOR SITE DEVELOPMENT APP.	DEC. 23, 2014	GS



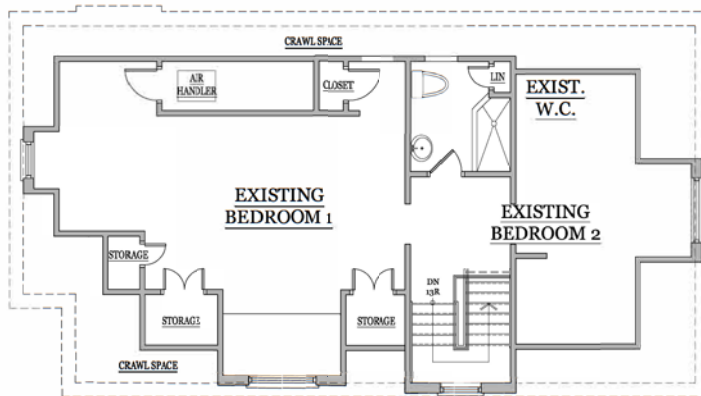
EXISTING GROUND FLOOR PLAN SCALE 1/8"=1'-0"

1
A5.1



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giorgio@frascadesignandplanning.com



**PROPOSED
RENOVATION & ADDITION**

Project:
169 Clarence Street,
Vaughan, ON., L4L1L4

Project #:
13021
(DA 14.098)

No.	Description	Date	By
4.	REVISION AS PER CITY COMMENTS (4.4)	NOV 01, 2018	RR
3.	ISSUED FOR SITE DEVELOPMENT APP (4.3)	AUG. 2, 2018	GF
2.	ISSUED FOR SITE DEVELOPMENT APP (4.2)	JUL. 27, 2016	GS
1.	ISSUED FOR SITE DEVELOPMENT APP.	DEC. 23, 2014	GS



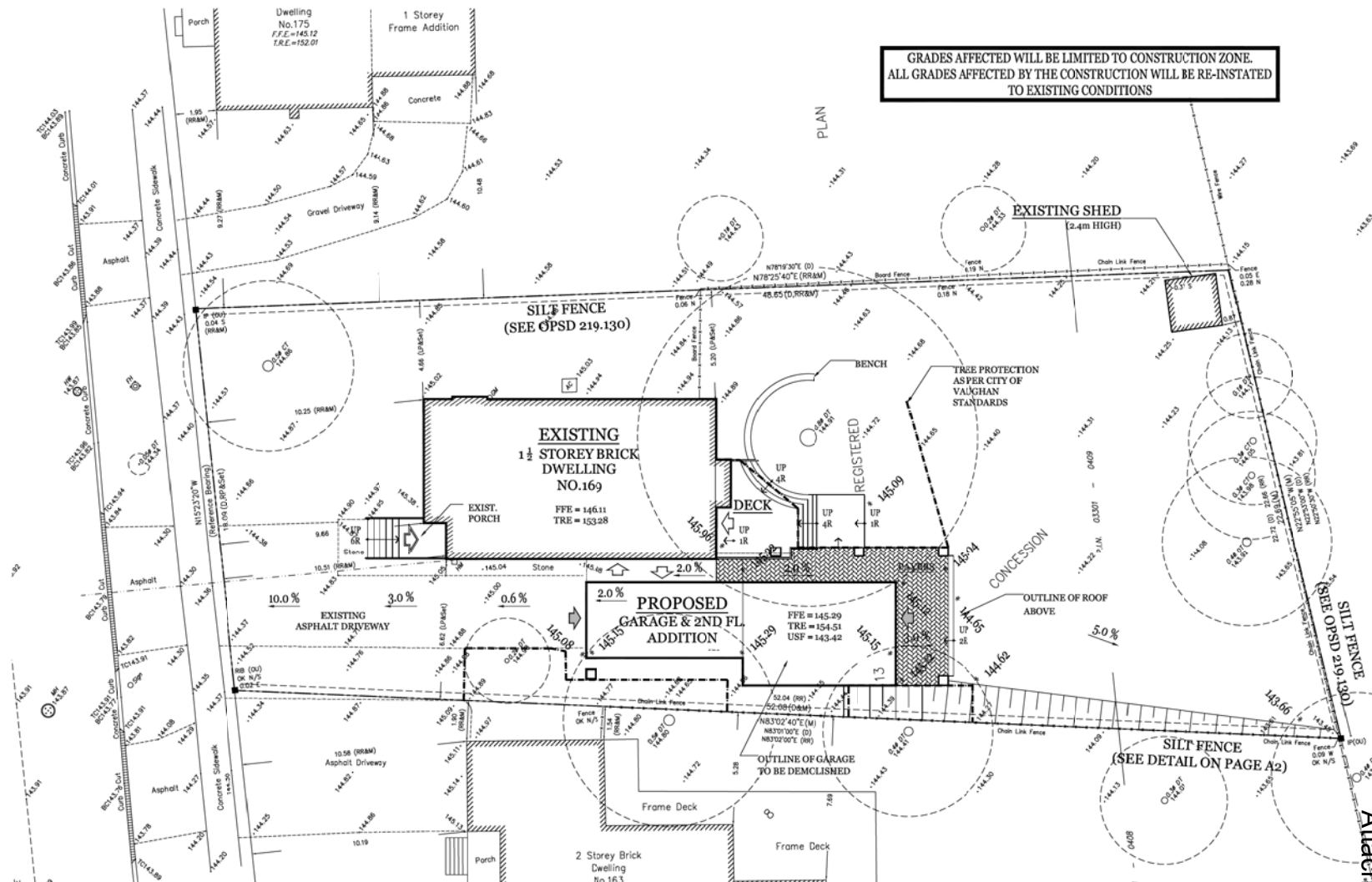
**EXISTING SECOND
FLOOR PLAN**
SCALE 1/8"=1'-0"

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A6.1

SITE DATA	
SINGLE FAMILY DETACHED DWELLING	
ZONE	R3
LOT NUMBER	PART OF LOT 13
LOT AREA	1,006.36SM (10,832SF)
BLDG. AREA	239.86 (2,581SF) (INCL. NEW & EXISTING)
NO. 0 STOREYS	2 (NEW) - 1 1/2 (EXISTING)

<h1>PROPOSED RENOVATION & ADDITION</h1>	
<p>Project:</p> <p>169 Clarence Street, Vaughan, ON., L4L1L4</p>	<p>Project #:</p> <p>13021</p> <p>(DA. 14.098)</p>

Δ	REVISION AS PER CITY COMMENTS (4.4)	NOV 01, 2018	RR
3.	ISSUED FOR SITE DEVELOPMENT APP (4.3)	AUG. 2, 2018	GF
2.	ISSUED FOR SITE DEVELOPMENT APP (4.2)	JUL. 27, 2018	GS
1.	ISSUED FOR SITE DEVELOPMENT APP.	DEC. 23, 2014	GS
No.	Description	Date	By



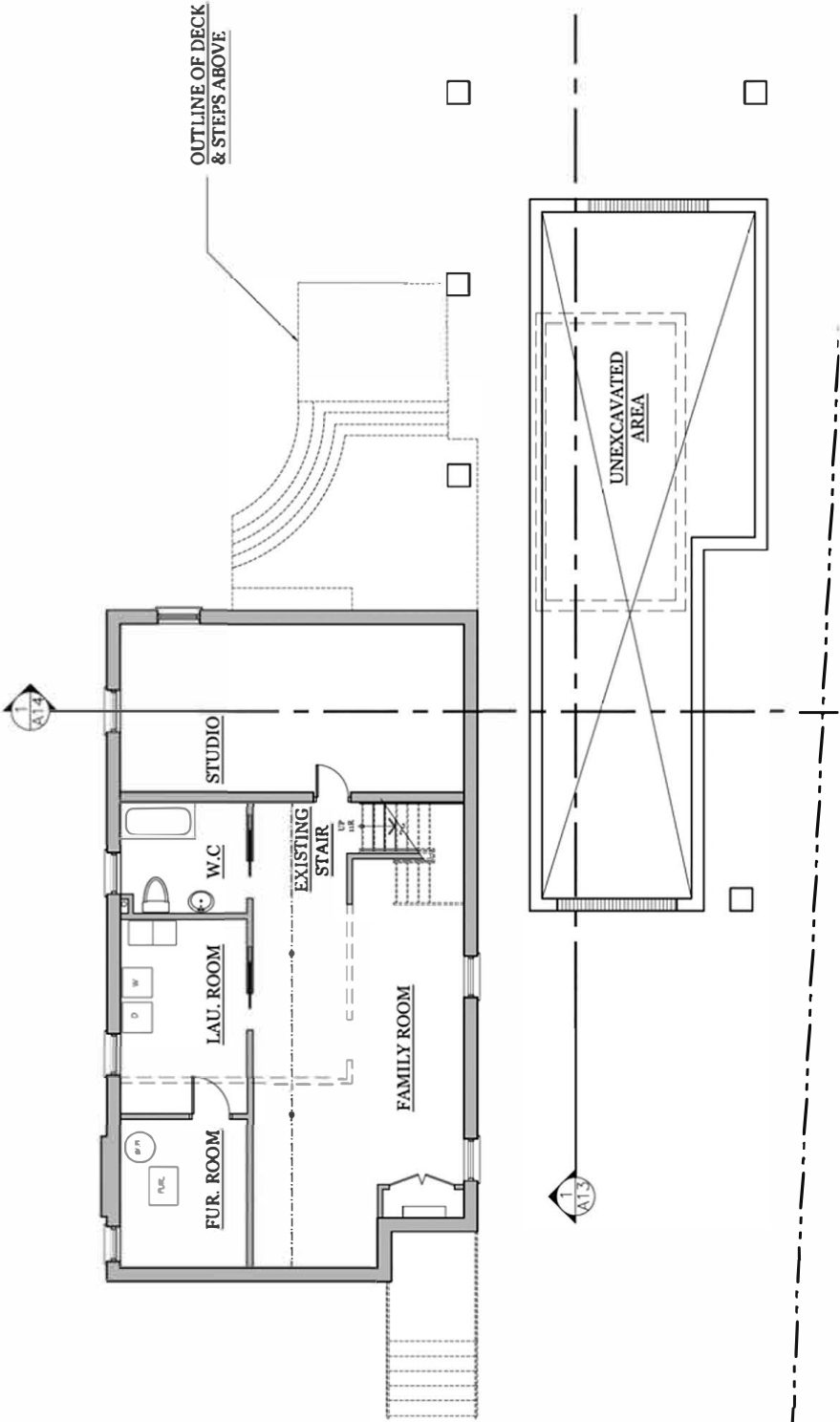
GRADES AFFECTED WILL BE LIMITED TO CONSTRUCTION ZONE.
ALL GRADES AFFECTED BY THE CONSTRUCTION WILL BE RE-INSTATED
TO EXISTING CONDITIONS

SITE GRADING PLAN
SCALE 1/16"=1'-0"

1
A3.1



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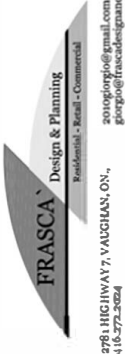


BASEMENT FLOOR PLAN 1
SCALE 1/8"=1'-0"



No.	Description	Date	By
1.	ISSUED FOR SITE DEVELOPMENT APP.	DEC. 23, 2014	GS
2.	ISSUED FOR SITE DEVELOPMENT APP.	JUL. 27, 2016	GS
3.	ISSUED FOR SITE DEVELOPMENT APP.	AUG. 2, 2018	RR
4.	REVISION AS PER CITY COMMENTS (4.4)	NOV. 01, 2018	RR

Project #:	13021
Project:	169 Clarence Street, Vaughan, ON. L4L 1L4
(DA: 14 098)	

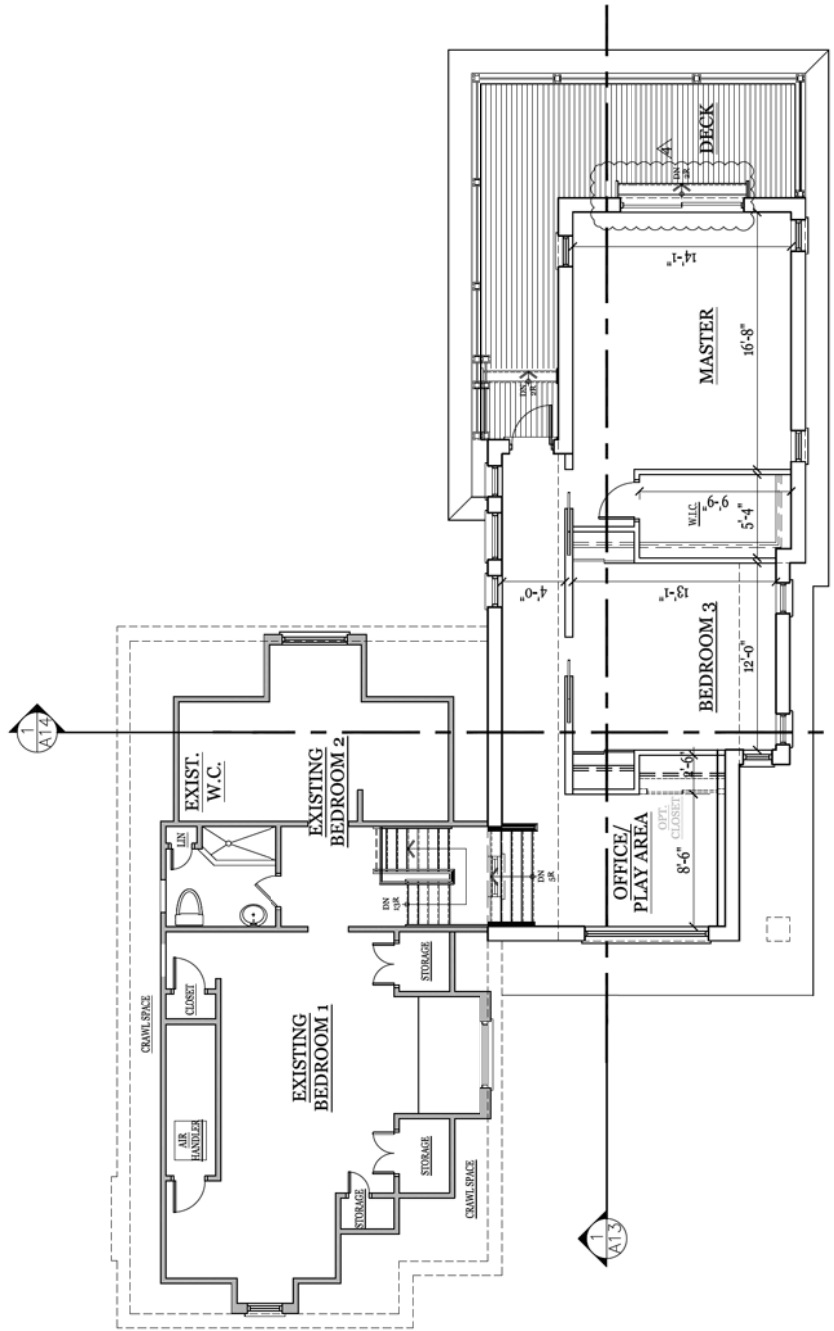


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gongjorgio@gmail.com
Residential - Retail - Commercial
Design & Planning



Project #:
13021
(DA. 14.098)

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A6

SECOND FLOOR PLAN
SCALE 1/8" = 1'-0"



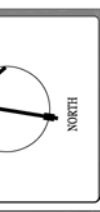
No.	Description	Date	By
1.	ISSUED FOR SITE DEVELOPMENT APP.	DEC. 23, 2014	GS
2.	ISSUED FOR SITE DEVELOPMENT APP.	JUL. 27, 2016	GS
3.	ISSUED FOR SITE DEVELOPMENT APP.	AUG. 2, 2018	GF
4.	REVISION AS PER CITY COMMENTS	NOV. 01, 2018	RR

PROPOSED RENOVATION & ADDITION	Project #: 13021 (DA-14-098)
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Project:
169 Clarence Street,
Vaughan, ON, L4L 1L4

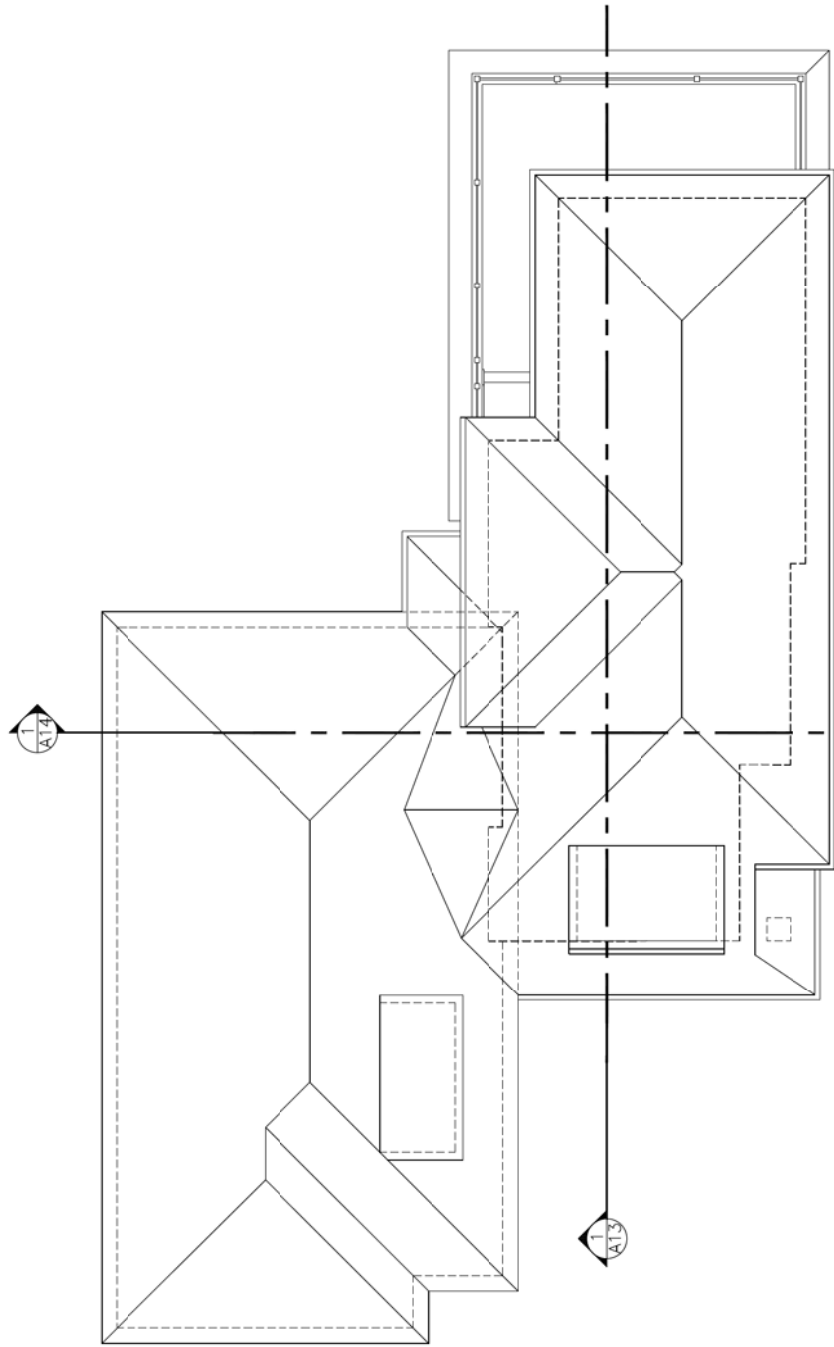
Project #:
13021
(DA-14-098)

Project:
169 Clarence Street,
Vaughan, ON, L4L 1L4



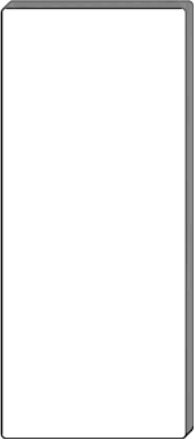
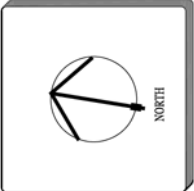
FRASCA
Design & Planning
Residential - Retail - Commercial

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1
A7

ROOF PLAN
SCALE 1/8"=1'-0"



No.	Description	Date	By
4	REVISION AS PER CITY COMMENTS (4.4)	NOV 01, 2018	RR
3	ISSUED FOR SITE DEVELOPMENT APP (4.3)	AUG. 2, 2018	GF
2	ISSUED FOR SITE DEVELOPMENT APP (4.2)	JUL. 27, 2016	GS
1	ISSUED FOR SITE DEVELOPMENT APP.	DEC. 23, 2014	GS

**PROPOSED
RENOVATION & ADDITION**

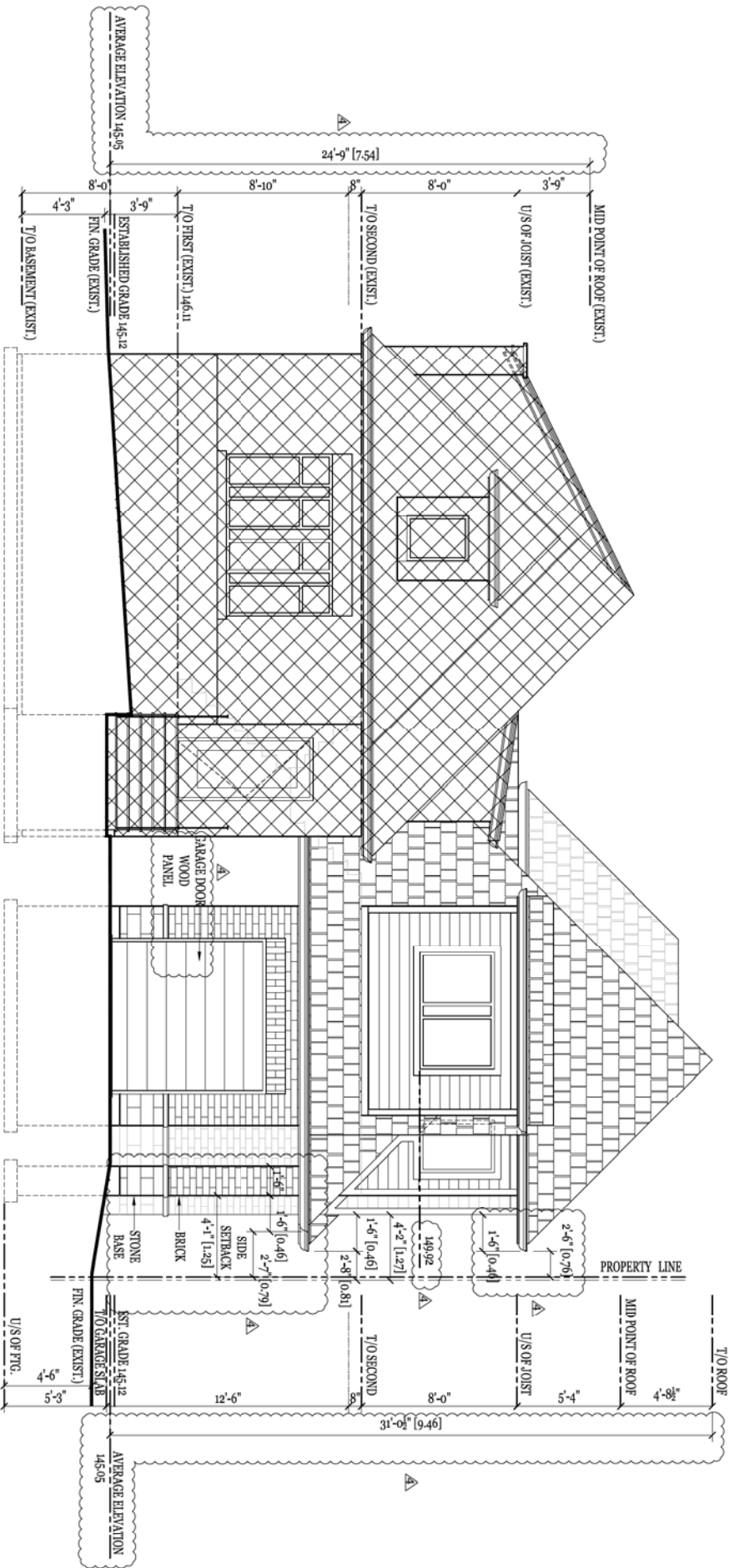
Project:
169 Clarence Street,
Vaughan, ON. L4L1L4

Project #:
13021
(DA 14.098)

FRASCA

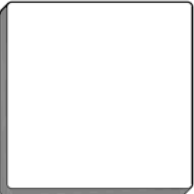
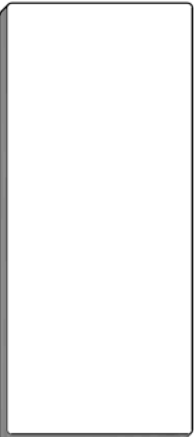
Design & Planning
Residential - Retail - Commercial

2781 HIGHWAY 7, VAUGHAN, ON,
416-272-2024
2019giorgio@gmail.com
giorgio@frascadesignandplanning.com

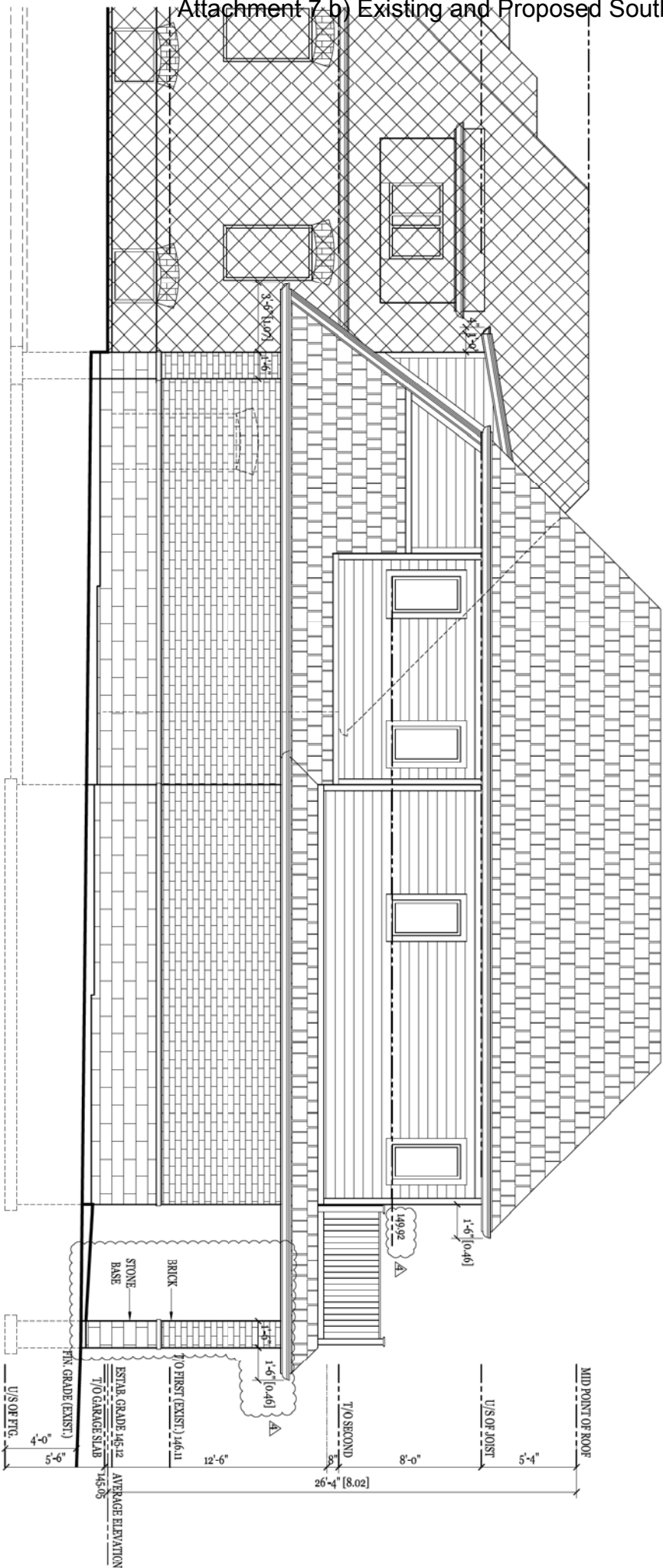


PROPOSED RENOVATION & ADDITION	
Project:	Project #:
169 Clarence Street, Vaughan, ON, L4L1L4	19021
(DL 14-098)	

No.	Description	Date	By
1.	REVISION AS PER CITY COMMENTS (4.4)	OCT. 25, 2018	RR
2.	ISSUED FOR SITE DEVELOPMENT APP (4.3)	AUG. 2, 2018	GS
3.	ISSUED FOR SITE DEVELOPMENT APP (4.3)	JUL. 27, 2016	GS
4.	ISSUED FOR SITE DEVELOPMENT APP.	DEC. 23, 2014	GS



EXISTING & PROPOSED
WEST ELEVATION
SCALE 3/16" = 1'-0"



See Sheet A11 for Continuation

PROPOSED
RENOVATION & ADDITION

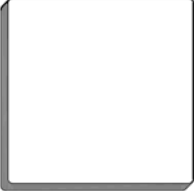
Project:

169 Clarence Street,
Vaughan, ON, L4L1L4

Project #:

19021
(DA-14-098)

NO.	DESCRIPTION	DATE	BY
1.	ISSUED FOR SITE DEVELOPMENT APP.	DEC. 23, 2014	CS
2.	ISSUED FOR SITE DEVELOPMENT APP.	JUL. 27, 2016	CS
3.	ISSUED FOR SITE DEVELOPMENT APP.	AUG. 2, 2018	CS
4.	REVISION AS PER CITY COMMENTS.	OCT. 25, 2018	RR



EXISTING & PROPOSED
SOUTH ELEVATION
SCALE 3/16"=1'-0"

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A9

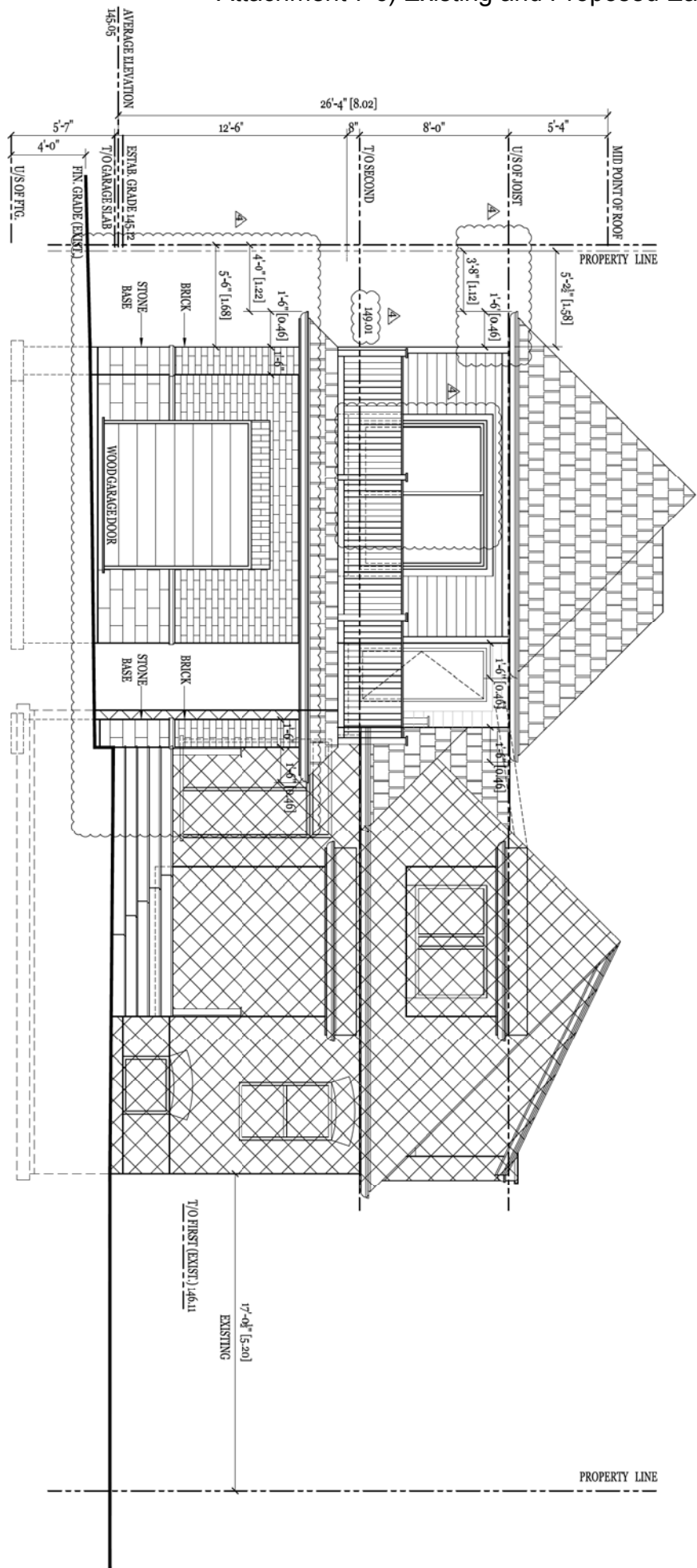
FRASCA

Design & Planning

Kendall - Retail - Commercial

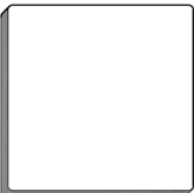
2781 HIGHWAY 7, VAUGHAN, ON,
416-272-2024

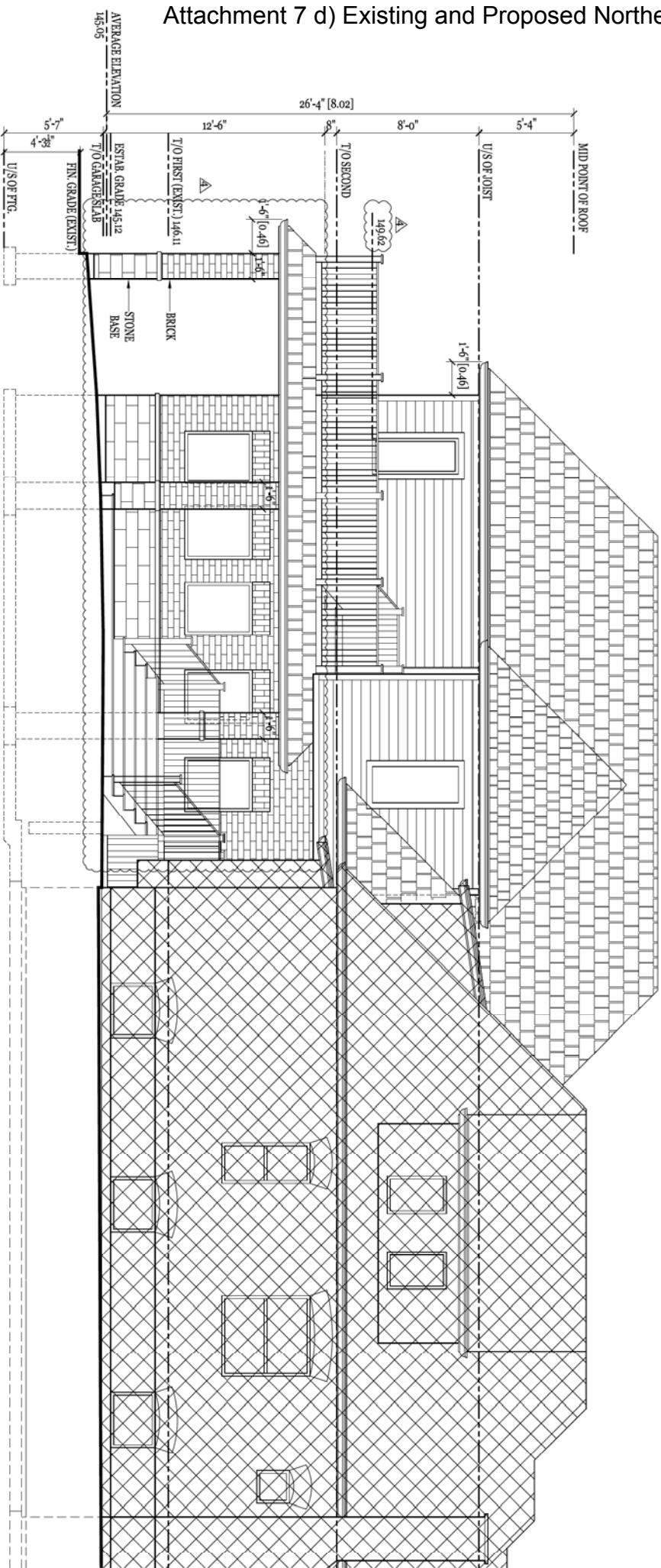
2019orgp@gmail.com
gorgp@frascdesignandplanning.com



PROPOSED RENOVATION & ADDITION	
Project:	Project #:
169 Clarence Street, Vaughan, ON, L4L1L4	19021
(D.A. 14-098)	

No.	Description	Date	By
1.	REVISION AS PER CITY COMMENTS (4.4)	OCT. 25, 2018	RR
2.	ISSUED FOR SITE DEVELOPMENT APP (4.3)	AUG. 2, 2018	CS
3.	ISSUED FOR SITE DEVELOPMENT APP (4.2)	JUL. 27, 2016	CS
4.	ISSUED FOR SITE DEVELOPMENT APP	DEC. 23, 2014	CS





See Sheet A11 for Continuation

**PROPOSED
RENOVATION & ADDITION**

Project:
169 Clarence Street,
Vaughan, ON, L4L1L4

Project #:
19021
(DA 14-098)

No.	Description	Date	By
1.	ISSUED FOR SITE DEVELOPMENT APP.	DEC. 23, 2014	CS
2.	ISSUED FOR SITE DEVELOPMENT APP.	JUL. 27, 2016	CS
3.	ISSUED FOR SITE DEVELOPMENT APP.	AUG. 2, 2018	CS
4.	REVISION AS PER CITY COMMENTS.	OCT. 25, 2018	RR



**EXISTING & PROPOSED
NORTH ELEVATION**

SCALE 3/16"=1'-0"

FRASCA
Design & Planning
Residential - Retail - Commercial

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1 EXISTING ASPHALT SHINGLES



2 WOOD GARAGE DOOR



3 EXISTING BRICK



4 EXISTING BLOCK



5 EXISTING FLAGSTONE



6 ASPHALT SHINGLES IKO - SHADOW SLATE



7 JAMES HARDIE KHAKI BROWN



8 THE BOWERSTON SHALE COMPANY HOOSIER BELND V/M



9 ARRISCRAFT RENAISSANCE SANDRIFT ROCKED



10 1" x 4" CEADER PLANKS

**PROPOSED
RENOVATION & ADDITION**

Project: 169 Clarence Street,
Vaughan, ON., L4L1L4

Project #: 13021
(DA. 14.098)

No.	Description	Date	By
4.	ISSUED FOR SITE DEVELOPMENT APP (44) OPTION 04	NOV. 01, 2018	RR
3.	ISSUED FOR SITE DEVELOPMENT APP (43) OPTION 03	AUG. 02, 2018	GF
2.	ISSUED FOR SITE DEVELOPMENT APP (42)	JUL. 27, 2016	GS
1.	ISSUED FOR SITE DEVELOPMENT APP.	DEC. 23, 2014	GS

**ARCHITECTURAL
MATERIAL BOARD**
SCALE NTS

1
R1



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January 26th, 2018

ARBORIST REPORT

Costa Residence

169 Clarence Street, Vaughan, Ontario

BACKGROUND

Nick Miele was retained to conduct an inventory of the existing trees located at 169 Clarence Street, as they pertain to the City of Vaughan Tree By-laws. Field work was completed on January 11th, 2018, this report relates to the condition of the trees as observed on that date.

PROCEDURE

The on-site inventory of existing trees was carried out using the current survey of the property and relies on the accuracy of this survey. The inventory includes by-law trees within the site boundary, all trees within adjacent public boulevards, and all private trees within 6.0 metres of the site boundary as per City of Vaughan's guidelines.

This inventory is summarized graphically in the Tree Inventory Plan TI-1, which shall always be read in conjunction with this report and shall form part of this report. For the purposes of this report, trees and groupings of trees are identified in terms of species, size, condition, and recommendations.

The following rating system was used in describing the general condition of the trees inventoried:

Good (G): Indicates a condition of vigour and no major concerns;

Fair (F): Indicates an adequate tree, which may have some minor issues;

Poor (P): Indicates declining health, bad form, or other more serious issues;

Dead (D): Indicates a dead tree that should be removed.

ASSUMPTIONS AND LIMITING CONDITIONS

- Care has been taken to obtain all information from reliable sources. All data has been verified insofar as possible and is assumed to be correct; however Nick Miele can neither guarantee nor be responsible for the accuracy of information provided by others.
- It is assumed that the properties are not in violation of any applicable codes, ordinances, statutes, or other governmental regulations.
- Unless otherwise required by law, possession of this report or a copy thereof does not imply right of publication or use for any purpose in whole or in part by any other than the person or company by whom it was commissioned.
- The use of excerpts from this report or alterations to this report, without the authorization of Nick Miele will invalidate the entire report. This report may not be used for any purpose other than its intended purpose as outlined.

- Unless expressed otherwise: 1) information contained in this report covers only those items that were examined and reflect the condition of those items at the time of inspection; and 2) the inspection is limited to visual examination or accessible items without dissection, excavation, probing, or coring. There is no warranty or guarantee, expressed or implied, that problems or deficiencies in the plants inventoried may not arise in the future.
- The determination of ownership of any subject tree(s) is the responsibility of the owner and any civil or common-law issues, which may exist between property owners with respect to trees, must be resolved by the owner. The recommendation to remove or maintain any tree(s) does not grant authority to encroach in any manner onto adjacent private properties.

SUMMARY OF TREES INVENTORIED

The following table summarizes the on-site trees.

Tree #	Common Name	Botanical Name	DBH (CM)	Condition	Comments	Recommendation
1	Oak	Quercus sp.	5	Fair		Retain
2	Spruce	Picea sp.	53	Fair		Retain
3	Crabapple	Malus sp.	11	Fair		Retain
4	Apple	Malus sp.	22	Fair		Retain
5	Maple	Acer sp.	81	Fair		Retain
6	Birch	Betula sp.	12	Fair		Retain
7	Spruce	Picea sp.	31	Fair		Retain
8	Spruce	Picea sp.	30	Fair		Retain
9	Maple	Acer sp.	42	Fair		Retain
10	Maple	Acer sp.	32	Fair		Retain
11	Maple	Acer sp.	31	Fair		Retain
12	Maple	Acer sp.	42	Fair		Retain
13	Maple	Acer sp.	51	Fair		Retain
14	Oak	Quercus sp.	22	Fair		Retain

TREE PROTECTION RECOMMENDATIONS

The following standards shall apply to any trees that are identified to be retained. Where the municipality enforces its own standards, those of the governing municipality shall supersede the recommendations contained herein. In all other instances, the following recommendations shall be treated as minimum standards for tree protection and retention.

1.0 ESTABLISH A TREE PROTECTION ZONE

The purpose of the tree protection zone is to prevent root damage, soil compaction and soil contamination during construction activities. Workers and machinery shall not disturb the tree protection zone in any way. In order to prevent access, the following recommendations are offered.

- Install tree protection hoarding as per City of Vaughan detail TP1.
- Allow no fill, equipment, supplies, or waste within the tree protection zone.
- Maintain the tree protection hoarding in good condition for the duration of construction.
- Tree protection hoarding is not to be removed until all construction activities have been completed.

2.0 ROOT PRUNING

Where possible, hand dig areas closest to each tree to prevent any unnecessary tearing or pulling of roots. Removal of roots that are greater than 2.5 centimetres in diameter or roots that are injured or diseased should be performed as follows:

- Preserve the root bark ridge (similar in structure to the branch bark ridge). Directional Root Pruning (DRP) is the recommended technique and should be employed during hand excavation around tree roots. Roots are similar to branches in their response to pruning practices. With DRP, objectionable and severely injured roots are properly cut to a lateral root that is growing downward or in a favorable direction.
- All roots needing to be pruned or removed shall be cut cleanly with sharp hand tools, by a Certified Arborist.
- No wound dressings or pruning paint shall be used to cover the ends of each cut.
- All roots requiring pruning shall be cut using any of the following tools:
Large or small loppers, Hand pruners, Small hand saws, Woundscribers
- Avoid prolonged exposure of tree roots during construction - keep exposed roots moist and dampened with mulching materials, irrigation or wrap in burlap if exposed for longer than 4 hours.

3.0 FERTILIZATION AND IRRIGATION

The following measures are recommended:

- Aeration and deep root fertilize to ensure that all trees receive the appropriate nutrients for healthy growth.
- Fertilizer must be a low nitrogen formula such as 5-30-30 to promote root growth rather than shoot growth.
- If construction occurs during July and / or August, roots must be irrigated during conditions of drought.

4.0 ESTABLISH MAINTENANCE PROGRAM

Pre-Construction:

- Prune all trees to remove any deadwood and obstruction prune as required.

During Construction:

- Irrigate tree preservation zones during drought conditions (June through September), in an attempt to reduce the effects of drought stress.
- Inspect the site every month to ensure that all tree protection fence / hoarding is in place and in good condition, inspect the trees to monitor condition.

Post-Construction:

- Prune crowns to remove any newly developed deadwood only. Do not remove any live growth.
- Inspect the trees three times per year (May, July, and September) to monitor condition for a minimum period of 2 additional years.

5.0 LANDSCAPING

Any landscaping completed within the tree preservation zones, after construction is completed and tree protection fencing / hoarding has been removed, is to be carried out in such a way that it will not cause damage to any of the trees or their roots. The trees must be protected to the same standards listed earlier in this report, but without the use of tree protection fence or hoarding.

The following guidelines are recommended:

- **No grade changes** are permitted which include adding and/or removing soil.
- **No excavation** is permitted that can cause damage to the roots of the tree.
- **No heavy equipment** can be used to compact the soil within the tree preservation zone.
- Where possible, hard surface paving around trees to be protected should be constructed using permeable products such as interlocking stone. Areas to be paved must be hand dug when encroaching within the tree protection zone.

CONCLUSIONS

It is our opinion that the trees identified for retention can be successfully retained if the recommendations contained herein are followed.

Kindly direct any questions regarding this report to the undersigned.

Respectfully submitted,



Nick A. Miele BLA, OALA, CSLA, ISA
ISA Certified Arborist No. ON-1251A



**PROPOSED
RENOVATION & ADDITION**

Project:
169 Clarence Street,
Vaughan, ON., L4L1L4

Project #:
13021
(DA. 14.098)

1.	ISSUED FOR SITE DEVELOPMENT APP (44)	NOV. 01, 2018	RR
No.	Description	Date	By

**STREETSCAPE VIEW
(CLARENCE STREET)**
SCALE NTS

1
R2



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