

October 31, 2023

Office of the City Clerk  
City of Vaughan  
2141 Major Mackenzie Dr.  
Vaughan, ON L6A 1T1

SENT VIA EMAIL

**Attn: Mayor and Members of Council**

**Re: November 1 Committee of the Whole Public Meeting Agenda Item 4 – Weston 7 Secondary Plan File No. 26.2**

Dear Mayor and Members of Council,

Urban Strategies Inc., are the land use planners for RioCan REIT (“**RioCan**”) with respect to their lands at 7501-7621 Weston Road, also known as the Colossus Centre, a 25-hectare site south and east of Highway 7 and Weston Road (the “**Colossus Centre Site**”). The Colossus Centre Site is currently the subject of active official plan amendment applications (OP.22.002 and OP.22.005) which were submitted March 1, 2022 and deemed complete on July 23, 2022 (the “**Colossus OPA Applications**”).

### **RioCan’s OPA Applications and Quadrant-Wide Masterplan**

RioCan, along with its adjacent landowners have been working together collaboratively for over two years to develop a comprehensive approach to development in the southeast quadrant of Weston 7 as a transit oriented, mixed-use community that prioritizes intensification and higher densities.

The lands identified as the southeast quadrant in the Weston 7 Secondary Plan (“**W7SP**” or “**the Plan**”) are made up of three land holdings, the RioCan site comprising 25.7 ha, the Costco site of 5.28 ha and the PetroCanada site (SunCor) at .63 ha.



Figure 1. Land Ownership Southeast Quadrant

In March of 2022, RioCan submitted the Colossus OPA Applications which included proposed policy direction related to vision and major objectives, land use and density, character areas, mobility, public realm, parks and open space, built form, community facilities and schools, housing, and implementation and phasing. The proposal sought a gross FSI of 4.0 times the lot area to be realized within 25 development blocks across the site.

To provide context for the OPA applications, a complete masterplan (“**Colossus Masterplan**”) for the entire southeast quadrant with the input of Costco and Suncor was prepared. The OPA Applications included supporting transportation and servicing studies that assessed a quadrant-wide level of detail based on the directions of the master plan.

For reference, the proposed Colossus OPA is appended to this letter as Appendix A.

The Staff Report prepared for the November 1 Committee of the Whole Meeting (“**November 1 Staff Report**”) further describes RioCan’s application on page 153. Council has directed that RioCan’s applications do not proceed in advance of the Secondary Plan being considered by Council. The City’s Official Plan policy 10.1.1.6. establishes where it is determined that a Secondary Plan is required but not yet completed, no amendments to this Plan or the zoning by-law will be permitted without prior or concurrent adoption of the Secondary Plan for that area.

## **Weston 7 Southeast Quadrant and Ongoing Landowner Coordination**

In addition to the extensive quadrant-wide study and collaboration related to the Colossus OPA applications, RioCan, Costco and SunCor met collectively with City Staff on October 17, 2023 to discuss their quadrant-wide commentary on the first draft of the Weston 7 Secondary Plan and to affirm their desire to work collaboratively towards the creation of a planning framework that supports the City’s and the landowner’s objectives for the quadrant; taking into consideration the extensive, comprehensive study that has been led by RioCan to date.

We are pleased to see that a number of the policy directions of the W7SP have been refined in the second draft and that the urban structure of the Colossus Masterplan and the W7SP are achieving greater alignment. While progress has been made, it is our opinion that the Plan would benefit from additional modifications to achieve greater clarity within the development framework. The goal of the preliminary comments in this letter is to support RioCan’s ongoing collaboration with the City on a comprehensive planning approach to Weston 7 as a whole, with an emphasis on the southeast quadrant and a focus on implementing a shared vision through the appropriate planning instruments.

## **Commentary on Draft 2 Weston 7 Secondary Plan**

### **Part A: Preamble**

In our opinion, the Preamble to the Plan could benefit from refinement to clarify expectations for how development should respond to non statutory plans or guidelines. For example, we continue to have concerns with the notion that applications are to be consistent with all relevant Manuals, Master Plans, Guidelines and Strategies approved by council. Such documents are not statutory policy and not subject to the same consistency or conformity tests. We encourage the language in this section to be modified to reflect the appropriate role of non statutory instruments in the consideration of development applications.

### **Part B: The Secondary Plan**

#### **Section 1.0 Vision and Principles**

Section 1.0 includes the Plan’s updated Vision and Principles. In general, the vision and principles outlined in the Plan reinforce a similar vision and major objectives as proposed by RioCan in their OPA Applications of 2022. However, we note that, among other changes, “Principle 1: Recognize WESTON 7 as a Vibrant Urban Community” has been updated to state “WESTON 7 will be clearly subordinate to the function and scale of the VMC.” We question if this additional language is necessary or desirable.

Describing Weston 7 as “subordinate in function and scale” is to suggest that Weston 7 should be less important in all aspects. We do not agree with the notion that Weston and 7 should be subordinate to the VMC, and further we do not believe it is the City’s intent to encourage that Weston 7 be less vibrant, less attractive, less worthy of investment. As noted in the November 1 Staff Report from the Deputy City Manager at page 2, which states “Weston 7 is intended to complement and not compete with the VMC”, it is more accurate to describe Weston 7 as complementary to the VMC.

## Section 2.0 Growth Management

### Population and Employment Projections

Population and employment projections are provided in Section 2. An allocation of growth by quadrant continues to be included, which are noted in in 2.2.c) as subject to the available capacity to accommodate growth.

Population and employment projections are a useful tool for high-level, long-range planning, and RioCan understands the need to align growth with capacity, however, tightly tethering growth to absolute population and employment estimates as suggested in 2.2. c) does not provide the needed flexibility required to respond to changes in the market over time. To improve the flexibility within 2.2.c) we suggest the policy language be adjusted to recognize that the population and jobs numbers given in the Plan are projections and therefore subject to a degree of uncertainty. A complementary shift in policy would be to focus on monitoring growth within these ranges rather than allocating it.

In addition, to understand the projected people and jobs per quadrant and how the gross density is expected to result in the projected people and job numbers, we respectfully request the City's methodology describing how the projections were derived and what assumptions were utilized in their development. We also request the associated capacity modelling done through the related transportation study (the Weston 7 Transportation Master Plan Study) and servicing study (the City of Vaughan's Integrated Urban Water Plan Functional Servicing Strategy Reports) which have an influence on the approach to people and employment projections for Weston 7.

### Phasing

While RioCan agrees that logical, coordinated, and predictable phasing is critical to the delivery of complete communities in Weston 7, the City's proposed approach to phasing policies introduces unnecessary complexity and ambiguity to how capacity will be allocated and ties the delivery of affordable housing and public servicing facilities to servicing allocation, which is not appropriate.

For example, the policies in 2.3 d) introduce subjective tests which frustrate, not clarify, how and if development density can be realized. Further to the legal requirements of the *Planning Act* and provincial policy, approvals should be granted on the basis of good planning. There are other tools, such as holding by-laws and subsequent zoning and site plan approval processes that can and should be utilized to phase development along with the availability of necessary infrastructure, rather than introducing a subjective ranking of arbitrarily selected criteria. Furthermore, the delivery of affordable housing and public service facilities should not be governed by servicing allocation as suggested in 2.3 d) but through appropriate planning and growth-related legislation (*Planning Act*, *DC Act*) and planning policy including Inclusionary Zoning.

To improve the approach to phasing, we suggest the City better utilize the concept of the suggested Development Concept Report. The parent Official Plan provides considerable direction regarding matters of phasing, the consideration of capacity within the transportation and municipal servicing networks, commitments to infrastructure elements and the utilization of an "H" among other matters in policies such as 10.1.1.7 – 10.1.1.11. It is our opinion that the proposed Weston 7 policy directions related to phasing are superfluous to the existing policy, which is sufficient to guide these matters.

Phasing of logical, coordinated development has taken in place in Vaughan over time through collaborative relationships between landowners and the City. Distinct from all other quadrants in Weston 7, RioCan, through its OPA Applications, provided a quadrant-wide level of study, conceptual development program and vision for the southeast quadrant as a whole. There are distinct benefits to a coordinated process such as the one that RioCan led through its OPA application. The Weston 7 Secondary Plan should encourage this type of coordination in its policies; to this end, we are encouraged

that the November 1 Staff Report states that City staff are considering the introduction of implementation tools that promote quadrant and/or area wide coordination, including Block Plans and/or Landowner Group Agreements.

## **Section 3.0 Building A Successful Community**

Section 3 includes policy direction related to housing options, supporting a strong economy, supporting a healthy community, encouraging high quality urban design, sustainability and new to this draft of the Plan is a section regarding the provision of Public Service Facilities. We offer the following initial commentary below on various portions of Section 3.

### **Providing Housing Options**

While we support and acknowledge the importance of affordable housing as part of a complete community, we continue to question how policy 3.1. b) could be implemented. This policy establishes a 35% attainable/assisted target and requires all developments to demonstrate their contribution to that target. It is unclear how this direction would be calculated within Weston 7, for instance on a site by site, quadrant or area-wide basis; nor is it clear how this direction can be enforced in the context of a potential future inclusionary zoning regime which has yet to be defined. The City's ability to require affordable housing is limited by the Planning Act to PMTSAs and will be subject to the future Inclusionary Zoning By law, which will establish the required attainable/assisted component.

### **Providing Public Service Facilities**

RioCan is encouraged by the acknowledgement in 3.4 b) that it will be the responsibility of the City and/or other service providers to work with the development industry to secure space for public service facilities. We suggest that the acknowledgement of the inter-relationship of responsibility for the delivery of public services and the limitations on what GFA can be delivered through community benefit charges be referenced as well.

In addition, policy 3.4 d) suggests a new "community hub" be identified in the southeast quadrant. The Colossus OPA proposed priority locations within RioCan's lands for community facilities such as schools, libraries and/ or community centers. Given the scale of the Weston 7 community as a whole, it would benefit the City to be more flexible for the location of such a hub and consider multiple locations for public facilities co-located with parks and open spaces. As high-density communities such as Weston 7 evolve, multiple public service destinations spread across broad geographic areas like Weston 7 may be necessary to maximize access to services for growing populations, and not solely anticipated within the southeast quadrant.

### **Ensuring High Quality Urban Design**

We acknowledge that the second draft of the Plan has removed several of the prescriptive standards for building design that were present in the previous draft. While progress has been made, additional modifications can be made to further improve this section of the Plan. For example, a concept that is largely absent from the Plan but needed is the notion of compatibility with not only the surrounding community but also the planned context. Proposals should not be evaluated in terms of their compatibility with a low-scale existing use that is planned for significant change, for instance.

In addition, a number of proposed policies remain in this section that are more appropriate for a guideline level of detail. We suggest the policies do not duplicate the directions found in specific guidelines including for example the York Region Transit-Oriented Development Guidelines and the Provincial Transit-Supportive Land Use Guidelines, but rather that the City points applicants to consideration of these guidelines on an application by application basis.

## Section 4.0 Land Use and Built Form Policies

Section 4 includes land use specific policies across all land use designations, as well as built form specific policies. While progress has been made to improve the clarity of policy included in this section, it is our opinion that further refinements can be made in this section of the Plan to streamline the policy direction and reflect what is appropriate at a secondary plan level of detail. We offer the initial commentary below regarding specific changes to the policies.

### Land Use Specific Policies

The Plan continues to outline permitted and prohibited uses, which in our opinion are more appropriate for inclusion in a future zoning by law for the Weston 7 area.

With regards to additional needs housing, we suggest that if it is the intention to utilize this concept of additional needs housing across the city, it would be logical to remove this section from the Weston 7 Plan and define this use within the parent Official plan, if required.

This section could be further refined by redirecting the content related to Live Work Units, Home-based businesses, Short Term Accommodations and Day Care Facilities into a zoning by-law. The level of specific direction related to these uses is more appropriate for a finer grained planning instrument and not a secondary plan level of detail.

Section 4.1.10 includes new direction on Elementary Schools that have now been identified in Schedule 1. RioCan's proposed OPA included policy direction as well as priority locations for future community uses that may be required including schools, community centres and/or libraries; sites were identified in RioCan's proposed north precinct and southwest precinct. To improve upon the City's new policies related to schools, we encourage the review of the VMC's approach to school sites which acknowledges consultation with the affected landowners in the school planning process.

In addition, so as to not preclude its ability to respond to development within the Centre as it emerges, the City may also wish to take a more flexible approach to the identification of school sites and not limit potential sites to the northeast and southeast quadrants, especially when the northwest quadrant has a similar population estimate.

### Built Form Specific Policies

We acknowledge that refinements have been made in this section to remove some of the prescriptive details related to built form in Weston 7. We offer the additional suggested refinements to further clarify the intent of the policies, relocate directions to a guideline or zoning by-law level of detail where appropriate and create a sufficiently flexible framework within which to realize the vision for Weston 7.

### High Quality Development

The concept of the Development concept report/ Plan is first identified in the Plan in 2.3 iii where it is noted that the "The City may require that a Development Concept Plan/Report be prepared to ensure that comprehensive planning and implementation issues are fully considered." Whereas the direction in 2.3. iii suggests this report *may* be required, 4.2.1 a) now indicates that it *will* be a requirement of development. We suggest that the Plan consistently refer to the Development Concept Plan/ Report as a potential requirement of a development application.

As with the commentary provided in Section 3, a considerable number of policies in this section could benefit from refinements that direct development to consider not just surrounding development, but existing and planned context as the concept of transition is considered in the built form. In addition, increased flexibility in the directions provided in 4.2.1 e) would be beneficial, recognizing that each development that comes forward will have its own unique response to its context, or the redirection of these concepts to a design guidelines would be appropriate. For example, in the case of the southeast quadrant, RioCan's quadrant-wide urban design guideline document provides various guidance for built form considerations. A copy of RioCan's quadrant-wide Urban Design Guidelines have been appended to this letter as Appendix B.

## **Mid-Rise Buildings and High Rise Buildings**

The Plan's approach to Mid-Rise and High-Rise buildings is articulated in Section 4.2.3 and 4.2.4 and illustrated on Schedule 2. In our opinion, the rationale for the proposed building height ranges on the RioCan lands remains unclear.

As illustrated in the graphic overlay included as Appendix C-1 to this letter, RioCan's proposed OPA and quadrant wide-master plan proposes a different approach to heights with higher heights and predominantly tower podium form buildings in the northerly precinct with the height range transitioning down in the southerly precinct where there is greater mix of mid and high rise typologies envisioned. We remain of the opinion that the Plan's proposed height regime is not appropriately reflective of the scale of development that should be contemplated in Weston 7.

Further, it is our opinion that greater heights are achievable while still maintaining an appropriate complementary relationship to the scale of development in the VMC. Schedule 3 in the W7SP should be updated to more closely align with the heights proposed in the RioCan OPA and Colossus Masterplan, which, based on our initial high-level calculations, do not result in a greater density output than what is proposed by the W7SP on a gross site basis. RioCan's OPA proposed a gross density of 4.0 FSI.

Further, as noted in the Staff Report, there are currently no maximum heights or densities for the lands within Weston 7. A strict prescription of maximum heights within lands identified as a Strategic Growth Area and a Major Transit Station Area which artificially limit the development potential of these lands runs contrary to the objectives of the Growth Plan and the direction of the Province.

In addition, refinement is required to the proposed calculation of height which, as described by the Plan, would be inclusive of a building's mechanical penthouse. This is contrary to conventional practice and would suppress the realization of the proposed densities of the Plan. Further, as noted in regard to a number of these technical calculations, this is a matter to be determined at the zoning by-law level.

Lastly, as suggested within the High Quality Development section and in Section 3, refinements to policies in this section should direct development to consider not just surrounding development, but existing and planned context as the concept of integration is considered.

## **Section 5.0 Land Use Designations**

Section 5 provides policy direction on the low-rise residential designation, the Mixed-Use I and Mixed-Use II designations, as well as new section the Flood Spill Area Overlay and the Colossus Drive Corridor Protection Area Overlay. While progress has been made to improve the clarity of policy included in this section, it is our opinion that further refinements can be made in this section of the Plan to create more flexible and implementable policy framework. We offer the initial commentary below regarding specific changes to the policies.

## **The Mixed-Use I and Mixed-Use II Designation**

We continue to have concerns with the minimum GFA requirements for non-residential uses in Mixed Use I and Mixed Use II areas as well as prescriptive direction for non-residential uses at grade. While a mix of uses, including non-residential uses, is critical to a successful, vibrant urban centre, the universal expectation for all development to deliver this amount of non-residential GFA does not represent a thoughtful approach to achieving the desired outcome. Rather than a blanket approach to non residential use requirements, we encourage the City to focus an emphasis on non residential uses around key public realm elements such as enhanced streetscapes, to reinforce a mix of uses and vibrancy around key investment areas.

We also encourage the City to reconsider the restriction on stand alone residential buildings in Mixed Use areas I and II. In large sites such as the Colossus Centre Site, a range of building forms and uses can be accommodated to create true, mixed use urban neighborhoods. Stand-alone residential buildings can be an appropriate component of a complete community.

## **The Colossus Drive Corridor Protection Area Overlay**

While RioCan agrees that the Colossus Drive overpass is a key element to the emerging urban structure and transportation network of Weston 7 that should be carefully planned, we are concerned that the language as currently drafted would limit the ability for development outside of this area, but connected to the overlay area, to proceed unimpeded. For example, it is not clear how enabling applications for development, such as severances or road realignments that may tie into a future Colossus Drive overpass, would be impacted by this policy. Further clarification is required to understand the scope of what kinds of development would be precluded from consideration and what kinds of exceptions are necessary to ensure unintended consequences do not arise from this policy direction.

## **Section 6.0 The Pedestrian Realm Network**

Section 6 describes the elements of the pedestrian realm network including defining the overall network, urban park spaces and streetscapes. The proposed Pedestrian Realm Network is illustrated on Schedule 3. Appendix C-2 to this letter shows the W7SP Pedestrian Realm Network overlaid with the RioCan OPA parks system, illustrating the general alignment between the two. We are pleased to see this alignment and optimistic about both parties collaborating on the Plan's ultimate directions regarding parks and open space. While we are encouraged by the general direction of the policies related to the pedestrian realm network, we suggest a number of possible modifications to the policies to enhance the clarity and intent of the approach of the policy, and to remove design guideline-level policies.

### **Defining The Pedestrian Realm Network**

The schedules of the Plan and the intent described in 6.2.1 suggest that it is the objective of the City to secure the full range of sizes of open spaces, including large scale parks appropriate for a range of recreational programming. That objective, however, is at odds with the direction found in 6.1 b) where site by site land contributions are required. We suggest these policy directions be reviewed and harmonized to ensure they are not at cross purposes.

### **Urban Park Spaces**

In general, the policies proposed for Urban Park Spaces including those that apply to all urban park spaces, promenades, and pocket parks could benefit from increased flexibility to acknowledge that each park space that comes through a development process will have its own unique context to respond to and may require flexible implementation of these policy directions. More detailed direction related to the envisioned park spaces is more appropriate for incorporation into design guidelines. For example, in the

case of the southeast quadrant, RioCan's quadrant-wide urban design guideline document, provides specific guidance for each of the elements of RioCan's proposed public realm network. A copy of RioCan's quadrant-wide Urban Design Guidelines have been appended to this letter as Appendix B

## **Streetscapes**

We share the Plan's enthusiasm for vibrant streetscapes that will frame the activity of day-to-day life in Weston 7. In general, the Streetscape section continues to contain policy direction that is better suited for a guideline-level of detail and additional clarity is still needed with regards to the requirement for non-residential land uses at grade.

We encourage a further refinement of the streetscape policies to focus on the necessary direction to define new concepts such as enhanced urban streetscapes and typical urban streetscapes, and remove extensive qualitative description of streetscapes, which may vary to some degree in implementation, into an accompanying urban design guideline. For example, in the case of the southeast quadrant, RioCan's quadrant-wide urban design guideline document provides specific guidance for streetscapes and pedestrian connections including conceptual rights of way and description of character. A copy of RioCan's quadrant-wide Urban Design Guidelines have been appended to this letter as Appendix B.

In addition, the direction in 6.3.2 related to where non-residential land uses are required at grade or encouraged at grade lacks coordination with the approach to non-residential ground floor uses for Mixed Use Areas I found in draft policies 5.3.3 c). It is our opinion that a more focused direction, but not prescription, should be considered for non-residential uses at grade that is coordinated with an emphasis on areas of enhanced public realm.

## **Section 7.0 Transportation, Service Infrastructure and Utilities**

Section 7 describes the components of the transportation system and service infrastructure and utilities, including the active transportation network, public transit, the road network, parking, water and wastewater, storm water management, and utilities and telecommunications facilities. The proposed Transportation system is illustrated on Schedule 4 of the W7SP. The City's proposed transportation network and RioCan's proposed street network that was included in the OPA applications of March 2022 are generally aligned, and we appreciate the Plan's direction that minor adjustments to the transportation system may be considered without an Official Plan Amendment. We offer the following comments to enhance clarity and provide sufficient flexibility to facilitate the redevelopment of the southeast quadrant and the Colossus site.

### **A Multi-Modal Transportation System**

RioCan supports a complete active transportation (AT) network, however, the current policy direction would have the result of providing excessive AT infrastructure. For instance, the proposed requirements for cycle tracks on both sides of local roads in our opinion are not necessary. We support wording that encourages major bike routes to include cycle tracks, but minor streets (or those with parallel active transportation corridors) be constructed with a narrower right-of-way which still achieving the multi-modal transportation goals within Weston 7.

Direction related to parking found in 7.1.5 b) should be reconsidered. As it is currently worded, it may prevent adoption of reduced parking standards in order to achieve the transportation goals of the community and the objectives of Provincial policy and Official Plan policy related to environmental sustainability and climate change. We encourage the City to explore wording that permits adoption of reduced minimum parking supplies where appropriate.

In addition, the design standard applied to potential above-grade parking structures has the potential to make the delivery of above-grade parking challenging and possibly impractical. Not only will less parking be provided in podium of a specific height, inter-floor ramp lengths will increase and may make them impossible to implement.

Further clarification is also required around 7.1.5 g) where the policy as stated today could be read to require parking areas need to be designed to accommodate large trucks, which we believe is not the intention of the City. Clarification is encouraged to refine the intent of this policy.

## Service Infrastructure and Utilities

RioCan supports the orderly phasing of development and servicing infrastructure upgrades, including municipal water, wastewater, and stormwater management infrastructure, as well as utilities and telecommunications infrastructure. Nevertheless, this section of the Plan could be adjusted to both provide a framework for orderly development and facilitate responsive, timely processes for the review and approval of development applications.

For example, policy 7.2.1 f) states that the *processing* and approval of development applications shall be contingent upon the availability of servicing capacity. While the approval of applications should certainly consider infrastructure capacity, the process of identifying servicing needs, capacities, and potential gaps is typically addressed *through* application processing. As such, infrastructure constraints are not a sufficient reason to delay application processing and the language of this policy should be updated to reflect this.

Furthermore, the policies of this section suggest that 'overall system capacity' be available or be efficiently and economically provided to support the proposal as a condition of approval. It is unclear what would be necessary to demonstrate overall system capacity; additional clarification is required.

## Section 8.0 Implementation and Interpretation

### Implementation

We are pleased to see that this iteration of the W7SP has removed significant portions of this section that previously may have led to unnecessary duplication or repetitive direction regarding matters that are more appropriately addressed in the parent Official Plan.

While we agree in principle that implementation direction is critical to the long-term effectiveness of a Plan such as the W7SP, there are a number of matters of detail within this section of the Plan that we encourage the City to revisit such as:

- Eliminate language suggesting that applications must "conform to" or be "consistent with" non-statutory plans or guidelines as an application requirement.
- Policies guiding the development of implementing zoning by-laws should consider both the existing and *planned* context. As it stands, the policies may inadvertently privilege the existing context.
- Ensure that the terms of reference for the requested additional Municipal Financial Impact Assessment are clear and achievable. As it stands, the tests within this policy are impossible to achieve as part of an application requirement before the City considers the application.
- Consider the burden for proponents to meet the extensive application requirements while not having certainty through the proposed "capacity allocation criteria" that their application will be evaluated on its merits and achievement of good planning.
- Consider indicating that Development Charge credits for the build out of the road network within the Secondary Plan area will be considered.

- Reconsider the direction of 8.1.7 f), which requires parkland contribution and appears to be contrary to the direction of 8.1.7 d) which allows for parkland dedication or cash-in-lieu equivalent.

We thank you for the opportunity to provide our commentary on the secondary draft of the Secondary Plan. To enable expedited review of future drafts, we request that the City produce a redline to allow for easier comparison and understanding of where changes have been made.

In addition, we request that we be provided notice when new drafts of the Plan are posted on the City's website. Due to a delay in receiving statutory notice regarding the posting of this draft, our review was limited in time. As a matter of good communication with the landowner and community, we encourage the City to provide prompt notice via email when new content is available for review.

We remain open to collaborating with the City on defining a planning framework for RioCan's lands and the southeast quadrant as a whole, as directed by Council in January 2023 and by staff through comments on the March 2022 OPA applications. At this time, we believe it would be advantageous to work directly with the City to establish a pathway for the concurrent adoption of the W7SP and RioCan's OPA Applications, which is enabled by the City's Official Plan policy 10.1.1.6. RioCan has prepared extensive quadrant-wide study to support its applications and there is considerable alignment on the approach to the southeast quadrant urban structure between the City and the Colossus Masterplan. Working together with the City, we can advance an implementable planning framework that supports the directions of the W7SP as a whole, while providing site specific policies that will frame the transformation of the southeast quadrant into a vibrant, transit-oriented, mixed-use community that will be part of Vaughan's long term growth and evolution.

Should you have questions or wish to discuss these comments in detail we would be more than happy to meet with you at your convenience.

Sincerely,



Leigh McGrath  
Partner, Urban Strategies  
RPP, MCIP

**Appendix A – RioCan OPA (submitted separately)**

**THE CITY OF VAUGHAN**

***By-law***

**BY-LAW No. ~ -2022**

**A By-law to adopt Amendment Number ~ to the Vaughan Official Plan 2010 for the Vaughan Planning Area.**

NOW THEREFORE the Council of the Corporation of the City of Vaughan ENACTS AS FOLLOWS:

1. THAT the attached Amendment Number ~ to the Vaughan Official Plan 2010 of the Vaughan Planning Area, consisting of the attached text and Schedule(s) 1-7" is hereby adopted.
2. AND THAT this By-law shall come into force and take effect on the day after the last day for filing a notice of appeal.

Enacted by the City of Vaughan Council this ~ day of Month, 2022.

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Hon. Maurizio Bevilacqua, Mayor

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Todd Coles, City Clerk

**AMENDMENT NUMBER ~**  
**TO THE VAUGHAN OFFICIAL PLAN 2010**  
**OF THE VAUGHAN PLANNING AREA**

The following text and Schedules “1”, “2”, “3”, “4”, “5”, “6”, and “7” constitute Amendment Number ~~ to the Official Plan of the Vaughan Planning Area.

## I PURPOSE

To amend the Vaughan Official Plan (VOP 2010) to facilitate a mixed- use development.

## II LOCATION

The lands subject to this amendment (hereinafter referred to as the “Subject Lands”), are shown on Schedule “1” attached hereto as “Area Subject to Amendment No. ~”. The Subject Lands are located on the east side of Weston Road south of Regional Road (Highway) 7 and are municipally known as 7501-7621 Weston Road.

## III BASIS

The decision to amend the City of Vaughan Official Plan 2010 is based on the following considerations:

1. The Amendment provides appropriate intensification on a site well-served by existing and planned transportation infrastructure. The Subject Lands are currently underutilized, and the Amendment will facilitate comprehensive intensification and transformation of the lands in accordance with the vision for Vaughan’s Primary Centres, including new compact, mixed-use development and new public streets, parks and open spaces.
2. The Provincial Policy Statement 2020 (“PPS”) provides direction for matters of provincial interest regarding land use planning and growth. The PPS promotes:

Efficient development and land use patterns;

Appropriate and efficient use of infrastructure and public service facilities that are planned or available; and

Land use patterns that support active transportation and transit-supportive density.

The proposed Amendment is consistent with the PPS and promotes its goals and objectives.

3. A Place to Grow: Growth Plan for the Greater Golden Horseshoe 2019 (“Growth Plan”) is intended to guide decisions on provincial issues including economic development, transportation, housing, and land use planning. The Growth Plan supports intensification within built-up areas, with a focus on areas designated for growth, including lands around existing and planned transit investment. The Subject Lands are located within a ‘Major Transit Station Area’ identified in the Growth Plan for mixed-use growth with the aim of creating complete communities with a range of housing types. The proposed Amendment conforms with the policy framework of the Growth Plan by optimizing the existing investment in rapid transit present along Highway 7 and supporting the achievement of a complete community that is compact and mixed-use in its form, making efficient use of the Subject Lands. The proposed Amendment also reinforces the Growth Plan direction to plan and design Major Transit Station areas to be transit supportive. The proposed Amendment establishes a new public street network and active transportation infrastructure that will contribute to achieving multi-modal access to transit services within the Major Transit Station Area.
4. The York Region Official Plan (“YROP”) identifies the Subject Lands as being within an Urban Area, along a Regional Corridor. Regional Corridors are identified as urban main streets that have a compact, mixed-use form that is transit-oriented. The proposed Amendment conforms with the York Region Official Plan by introducing a site specific policy framework that will introduce a fine-grained street grid that supports active transportation and connections to transit, encourages an urban form and mix of uses that can meet the needs of the future Weston 7 community and locates the greatest proposed density in walking distance of rapid transit.
5. The Vaughan Official Plan (“VOP 2010”) is intended to guide planning for intensification, transit-supportive development, urban growth and complete communities in the City of Vaughan. The Subject Lands are located in the Weston 7 Primary Centre along a Regional Intensification Corridor. The Official Plan directs that Primary Centres will be locations for intensification in the form of mixed-use

development. Regional Intensification Corridors are expected to be a major focus for intensification, with transit-supportive densities and built form.

The Weston 7 Primary Centre is identified on Schedule 14A of the Official Plan as an area subject to a Secondary Plan. Since the time of the adoption of the VOP 2010, a Secondary Plan for this Primary Centre has not been completed. Policy 10.1.1.13 enables Council to proceed with the processing of a development application in advance of a Secondary Plan, subject to meeting the requirements for various studies established by staff, to the satisfaction of the City. The scope and scale of the studies that have informed the content of the proposed Amendment were established in consultation with City staff and represent a quadrant-wide analysis of the proposal and its impacts and contributions to the Primary Centre as a whole. Given the comprehensive nature of the study that informed the proposed Amendment, and the ability of this portion of the Weston 7 Primary Centre to develop independently of other areas within the Weston 7 Primary Centre, the enactment of proposed Amendment in advance of the Weston 7 Secondary Plan is appropriate.

#### IV DETAILS OF THE AMENDMENT AND POLICES RELATIVE THERETO

The Vaughan Official Plan 2010 is hereby amended by:

1. Amending Volume 1, Schedule 14-C “Areas subject to Site Specific Plans” of VOP 2010 by adding the Subject Lands on Schedule “1” to this Amendment, attached hereto as “Subject Lands”.
2. Amending Volume 2, Section 13.36 Colossus Centre by removing the Subject Lands from Map 13.36.A
3. Amending Volume 2, Section 13.1 – “Areas subject to Site-Specific Policies” by adding the following policy to be renumbered in sequential order:

“OPA # ~ 13.~                    The lands municipally known as 7501-7621 Weston Road and identified on Schedule 14-C (as item #~) are subject to the policies set out in Section 13.~ of this Plan”

4. Adding the following policies to Volume 2, Section 13 – “Site Specific Policies”, and renumbered in sequential order including a location map of the subject lands as per Schedule “1”:

OPA #~ 13.~ 7501-7621 Weston

Road 13.~.1    General

13.~.1.1                    Notwithstanding the policies within Volume 1 of VOP 2010, the following policies and development criteria shall apply to the lands identified on Map 13.~.A:

##### **13.~.1.2 VISION AND MAJOR OBJECTIVES**

- a) The Subject Lands are the current site of the Colossus Centre shopping centre, with an overall size of 24.1 hectares. The Subject Lands are generally bound by Regional Road (Highway) 7 to the north, Highway 400 to the east, Highway 407 to the south and Weston Road to the west and are depicted in the heavy black line on Schedule 1.
- b) The Subject Lands are envisioned to become a vibrant and animated mix-use community that will be transit-oriented and supported by new streets, parks and open space. Current auto-dominated retail and entertainment uses will be transformed into a modern, urban-format retail and entertainment destination that will become a local social and commercial hub in the Weston 7 area. Development will be framed by high quality parks and open spaces that connect future residents to active and passive recreational opportunities and places to play, rest, and connect with nature and their community. The evolution of the Subject Site will be a model for the transformation of underutilized suburban sites into mixed-use, complete communities.

The following major objectives will guide the short and long-term vision for the Subject Lands:

c) **Create Distinct Precincts**

The Northern Precinct and the Southwestern Precinct will be realized as the basic premise for site organization within the development quadrant bounded by Weston Road to the west, Highway 7 to the north, Highway 400 to the east and Highway 407 to the south. The Northern Precinct will feature the highest density of development closest to transit infrastructure and will include a strong residential and commercial character—becoming the focus of a future retail and entertainment destination. The Southwestern Precinct will evolve into a high and medium density community that has a strong neighbourhood character, supported by community-servicing retail and commercial uses and a network of parks and open spaces.

d) **Reinforce a Sense of Arrival**

A strong sense of arrival and place within the new community will contribute to its distinct identity. Key points of arrival will be defined through development at key entrances from Regional Road (Highway) 7, Weston Road and the future Colossus Drive Overpass.

e) **Create Place-Defining Open Spaces**

A connected network of open spaces framed by animated uses and mixed-use development will be a central element of the Subject Site's transformation. The Northern Precinct and Southwestern Precinct will accommodate significant community-servicing parks and open spaces that have the ability for a diversity of recreational programming to meet the needs of various users, providing opportunities for informal play, recreation and social interaction.

f) **Plan for Multi-modal movement**

A connected street network that allows for balanced pedestrian, vehicular, and cycling movement will encourage transit use and safe, convenient movement for all modes. The street network will provide well-designed and comfortable streets that can support a variety of movement choices, which will encourage alternative transportation modes, reducing the need for personal automobiles for all trips.

g) **Develop a Range of Destinations**

A range of destinations, including commercial and entertainment uses as well as community-based uses and facilities will be provided. Varied destinations will promote vibrancy and activity in the community at different times of day for a diversity of users.

### 13.~.1.3 **LAND USE and DENSITY**

a) The Subject Lands are designated as a High-Rise Mixed-Use area, as shown on Schedule 2.

b) The following building types are permitted within the Subject Lands:

- a. High-rise buildings
- b. Mid-rise buildings
- c. The following building types may be permitted on streets that are not Major Streets
  - i. Low-rise buildings
  - ii. Stacked townhouses
  - iii. Townhouses

c) Notwithstanding policy 9.2.2.6.c, in areas designated as High-Rise Mixed-Use the ground floor frontage of buildings facing Famous Avenue and buildings fronting Connector Roads will predominantly consist of retail uses or other active uses that animate the street.

d) Proposed residential and other sensitive land uses close to existing industrial uses shall demonstrate compatibility in general accordance with all Provincial and municipal guidelines.

e) Development will consider the implementation of district energy systems

and explore partnerships with locally-serving energy utility companies to facilitate the design and implementation of district energy systems.

- f) Development should achieve a long-term overall maximum density of 4.0 FSI blended across the entirety of the Subject Lands.
- g) It is anticipated that on a development block basis, individual sites may include a site density greater than or less than blended density.
- h) Maximum building heights, as depicted on Schedule 7, will work in conjunction with the blended maximum density to direct and shape the intensity of development on the Subject Lands.
- i) While the ultimate redevelopment of the entirety of the Subject Lands is anticipated to realize the vision and major objectives set out in Section 13.~.1.2, existing land uses throughout the Subject Lands and on adjacent lands within the quadrant are expected to continue to exist in the near term, and some may remain for the foreseeable future.
- j) Uses and Buildings legally existing on the Subject Lands as of the date of the adoption of this Amendment and their associated existing gross density are permitted.
- k) Expansions of previously approved uses that are not consistent with this amendment shall be permitted without amendment to the plan, provided that the intent of the plan, as it applies to adjacent properties within the quadrant, is not compromised.
- l) Notwithstanding 9.2.2.6.d, where expansions of previously approved uses may take place, retail uses may exceed 50% of the total gross floor area of all uses on the lot.

#### 13.~.1.4 CHARACTER AREAS

##### 1.4.1 The Northern Precinct

The Northern Precinct identified on Schedule 3 is located north of Colossus Drive. With prominent frontage on Weston Road and Regional Road (Highway) 7, this Character Area will be a high density, mixed-use, transit-oriented environment characterized by destination retail and entertainment uses and landmark open spaces that are activated throughout the day and evening by a diverse mix of horizontal and vertical uses. A pedestrian mews will be a focal point for the Northern Precinct as a feature of the public realm that will provide east to west connectivity, encourage walking as a preferred mode of movement and frame future urbanized retail and entertainment uses.

Development in the Northern Precinct should reflect:

- a) Where appropriate, the vision for a compact, mixed-use, urban-form, retail and entertainment destination within the Precinct.
- b) Famous Avenue as the central spine of the Precinct and a priority area for enhanced landscaping and active uses.
- c) The realization of the east-west pedestrian mews to maximize its benefit to the public realm and contribution to the planned character of the Northern Precinct, including framing the mews, where appropriate, with active, animating uses.

##### 1.4.1 The Southwestern Precinct

The Southwestern Precinct is located south of Colossus Drive and generally west of the realigned Famous Avenue and is intended to evolve with a predominantly neighbourhood character including moderate and high-density development. Identified on Schedule 3, the Southwestern Precinct will include residential uses, neighborhood serving retail and commercial uses and community facilities.

Development in the Southwestern Precinct should reflect:

- a) A moderate and high-density area predominantly comprised of residential uses with retail, commercial and community uses that support the needs of the population.
- b) Famous Avenue as the central spine of the Precinct and a priority area for enhanced landscaping and active uses.
- c) Coordination with existing and interim uses on the Subject Lands and adjacent lands within the quadrant to preserve their existing function and future development potential.

## 13.~1.5 MOBILITY

### Streets

- a) The street network in the Northern and Southwestern Precincts should include a fine-grained network of public and private streets that are designed to support all modes of transportation including walking, cycling and public transit, identified on Schedule 4.
- b) Local and Private Local Streets are envisioned as streets that:
  - a. Enhance connectivity within the Subject Lands.
  - b. Accommodate one travel lane in both directions within a right-of-way of generally 17-20 metres.
  - c. Where Private Local streets are realized, they will be designed to meet public street standards.
- c) Connector Streets are envisioned as streets that:
  - a. Provide additional connection into the Subject Lands.
  - b. Have a dedicated right-of-way of approximately 20 metres, with variable width to accommodate turn lanes and medians, as needed.
- d) Major Streets are envisioned as streets that:
  - a. Serve as significant components of the street network to support local and regional travel through the Subject Lands, and accommodate higher traffic volumes.
  - b. Have a variable right-of-way width of between 28 and 33 metres with variable width to accommodate turn lanes and medians, as needed.

### Active Transportation

- e) An active transportation network which considers the movement of pedestrians, cyclists and other non-auto modes is shown on Schedule 5.
- f) The active transportation network should be comprised of sidewalks, a multi-use trail, off-street cycle tracks, and pedestrian connections.
- g) Streets should provide pedestrian infrastructure appropriate for the planned intensity of development including appropriately scaled sidewalks.
- h) A multi-use trail, conceptually illustrated on Schedule 5, is envisioned to accommodate cycling, pedestrian and other non-auto movement as well as provide outdoor amenity and facilitate connections north to south within the Subject Lands.
- i) Collaboration with the Ministry of Transportation, the City of Vaughan and landowners within the Subject Lands is encouraged to realize the multi-use trail and maximize its benefit to the public realm and active transportation network.
- j) Development is encouraged to contribute to realizing convenient pedestrian connections to various destinations, including to transit infrastructure, through the provision of mid-block connections, where appropriate.

### Colossus Drive Overpass

- k) A conceptual alignment of the Colossus Drive Overpass is illustrated on Schedule 4. The ultimate alignment of the Colossus Drive Overpass will be determined through an Environmental Assessment process.
- l) The future alignment of the Colossus Drive Overpass should:
  - a. Accommodate access to future redevelopment blocks or existing and interim uses within the Subject Lands through controlled intersections into the Northern Precinct and the Southwestern Precinct, east of Famous Avenue.
  - b. Not unduly impact the economic viability of existing and interim uses or preclude the redevelopment of the Subject Lands or adjacent lands within the quadrant in accordance with the vision and objectives of this Plan.

- c. Facilitate a fine-grained connected street network.
  - d. Accommodate pedestrian and cyclist movement and enable an at-grade intersection with Famous Avenue.
- m) The Colossus Drive Overpass Conceptual Area is illustrated on Schedule 4. The private local, local and connector streets and associated development blocks within this area are conceptual in nature and changes to these streets and associated blocks may be required to respond to the future orientation of the Colossus Drive Overpass. Should changes in the street network and associated block structure be required in response to the ultimate alignment of the Colossus Drive Overpass, such changes will not require an Official Plan Amendment.

#### **Parking**

- n) Parking will meet the needs of future residents and businesses and support the vision and objectives of this Amendment.
- o) Development is encouraged to provide parking underground wherever possible.
- p) Parking under a new local street, private local street, collector street, pedestrian mews or pedestrian connection shall be permitted provided the intended purpose, function and character of the street/mews are not materially or qualitatively compromised.
- q) Parking shall be permitted under public parks and private open spaces provided the intended purpose, function and character of the public park or private open space are not materially or qualitatively compromised.
- r) Structured, above-grade parking is permitted. To minimize the impact of parking structures on the public realm the following design strategies should be considered:
  - a. Integrate structured parking within the base of new buildings.
  - b. Wrap portions of the lower levels of parking structures with commercial, retail, residential or community uses to integrate the structure into the public realm.
  - c. Design structured parking to incorporate fenestration and well-articulated openings and high-quality materials.
  - d. Enable conversion of parking structures through adaptive re-use to other uses in the future.
- s) Surface parking is discouraged, however, surface parking is permitted to support existing and interim uses.

#### **13.~.1.6 PUBLIC REALM, PARKLAND AND OPEN SPACE**

- a) Development should contribute to a network of parks and open spaces, shown on Schedule 6. The exact location and size of each park will be determined through future planning processes. Should changes to the locations of parks shown on Schedule 6 be required, such changes will not require an Official Plan Amendment.
- b) Where conceptual park locations on Schedule 6 abut land under separate ownership, coordination between affected landowners will be required to enable the development of the park.
- c) As part of any phasing strategy, the conceptual park locations on the Subject Lands that are dependent on redevelopment of adjacent lands within the quadrant, interim private open space uses are permitted.
- d) Development should contribute to achieving a minimum of 10% of developable area as new public parks, net of any conveyances for public roads. Public parks may include unencumbered parkland or strata parkland.
- e) In addition to parks, development should contribute to achieving a minimum of 5% of the developable area, net of any conveyances for public roads, as additional private open space, which may be delivered through privately owned, publicly accessible open spaces (POPS), the multi-use trail, or other private open spaces.
- f) Parking and utilities will be permitted under a public park where it has been demonstrated that the proposed underground parking will not

materially or qualitatively compromise the intended purpose, function and character of the park or square.

### 13.~.1.7 BUILT FORM

- a) Development is encouraged to include a diverse range of building typologies that contribute to the long term vision and objectives for the lands.
- b) Development should contribute to a diverse range of heights and configurations that will contribute to an interesting skyline and diverse urban character.
- c) Development should seek to achieve a comfortable, well scaled pedestrian experience by including streetwall heights of generally 7-9 storeys in the Northern Precinct and 2-6 storeys in the Southwestern Precinct.
- d) Streetwall heights may be distinguished through the use of variations in materiality, projections or recessions in the façade or stepbacks.
- e) The tallest building heights are expected in the Northern Precinct and along the Highway 400 edge of the Subject Lands, generally transitioning down in height in the Southwestern Precinct, as illustrated in Schedule 7.
- f) Consideration for additional height above the maximum height provided shall be given for a landmark development in the Northern Precinct located generally north of Colossus Park and east of Famous Avenue, as generally indicated on Schedule 7, provided that the landmark development:
  - i. Includes distinct architectural features and massing characteristics that help to distinguish the Subject Lands as a landmark within the surrounding context;
  - ii. Incorporates site plan elements that support a more comfortable pedestrian environment, such as wider sidewalks, street furniture, landscaping and opportunities for public art;
  - iii. Generally responds to the other built form policies of this Plan and the Colossus Urban Design and Sustainability Guidelines; and
  - iv. The prescribed overall maximum density for the Subject Lands is maintained.
- g) At the time of future Zoning By-law amendment applications, standards shall be developed to address built form matters such as the size of tower floorplates, setbacks and stepbacks.

### 13.~.1.8 COMMUNITY FACILITIES AND SCHOOLS

- a) Community facilities should be accessible from streets as well as pedestrian and cycling routes and be highly visible within the community.
- b) The location and design of community facilities is encouraged to be accommodated in an urban form, including co-location of facilities where possible.
- c) Priority locations for community facilities that are important destinations, which may include schools, libraries and/or community centers, are indicated on Schedule 7. In addition, development should consider in-kind contribution towards community facilities elsewhere within the Subject Lands.
- d) The appropriate number of schools required will be determined in consultation with the school boards and will be dependent on the pace and composition of development.
- e) Future schools are encouraged to be built to an urban standard that optimizes the use of land including through strategies such as the utilization of urban-sized school sites and/or shared use of public parks for school use.
- f) The site size, site layout and built form of schools shall be compatible with the planned vision and objectives of this Amendment.

### 13.~1.9 HOUSING

- a) Development should enhance housing choice and affordability in the City of Vaughan by providing a diverse range of housing types and tenures that can meet a range of housing needs including family housing and accommodating people at all stages of life.
- b) At the time of rezoning processes, proponents will work with the City of Vaughan, the Region of York and other levels of government to explore opportunities to realize a range of housing affordability within the Subject Lands.

### 13.~1.10 IMPLEMENTATION and PHASING

- a) Development should be guided by the Colossus Urban Design and Sustainability Guidelines, as may be amended from time to time.
- b) Development is expected to unfold through a number of phases over time. Development phases should be coordinated with the delivery of municipal services and community amenities to serve development such as streets, servicing and storm water infrastructure, community amenities and parks and open space.
- c) Development on the Subject Lands should be compatible with existing land uses within the Subject Lands and on adjacent lands within the quadrant. Development should demonstrate, through future rezoning, site plan and/or plan of subdivision applications that ongoing operational needs of existing uses are adequately considered and that future development potential is not negatively impacted.

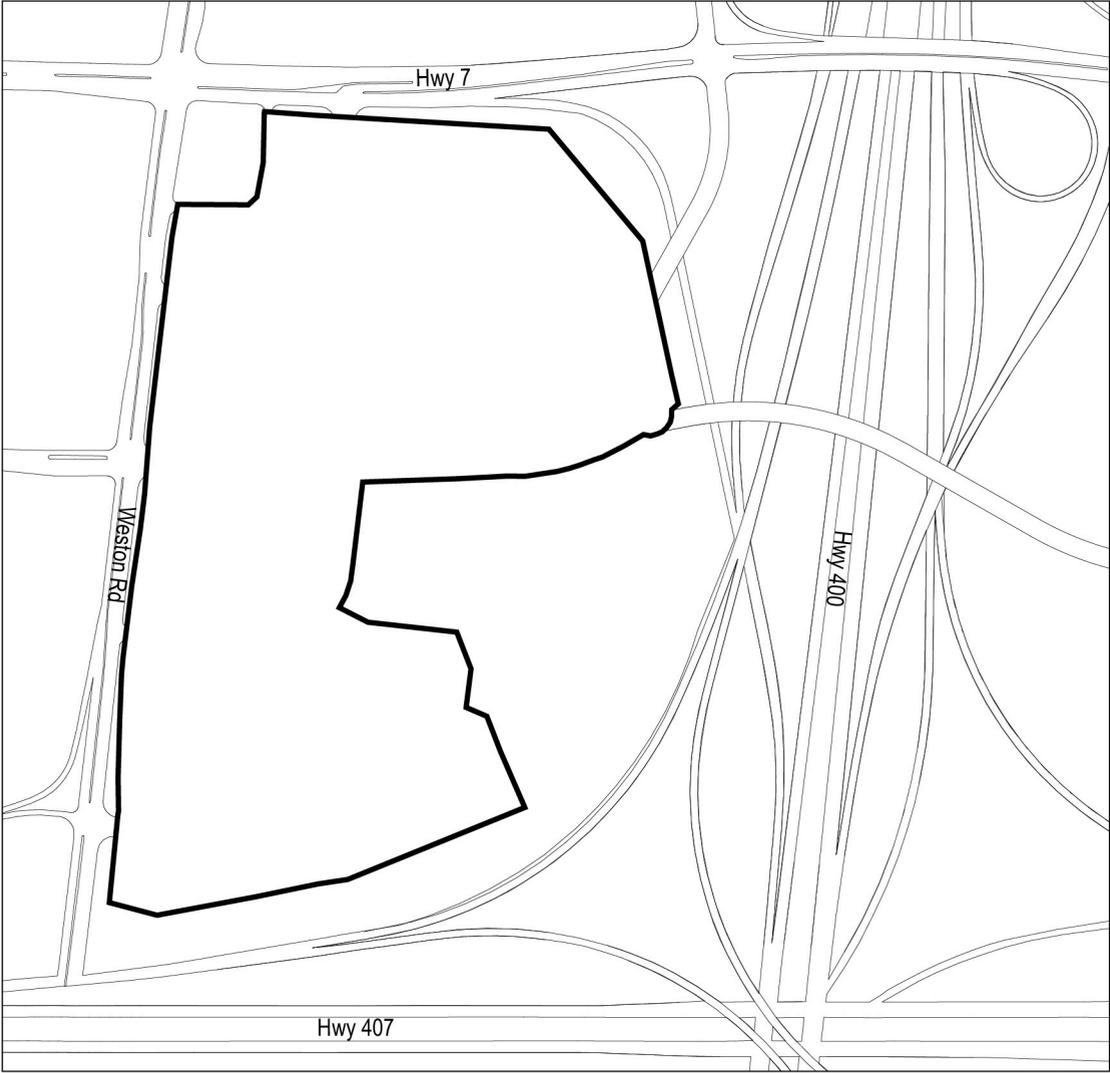
#### V Implementation

It is intended that the polices of the Official Plan of the Vaughan Planning Area pertaining to the Subject Lands shall be implemented by way of an amendment to the City of Vaughan Official Plan pursuant to the *Planning Act*, R.S.O. 1990, c. P.13.

#### VI Interpretation

The provisions of the Official Plan of the Vaughan Planning Area as amended from time to time regarding the interpretation of that Plan shall apply with respect to this Amendment.

# Schedule 1

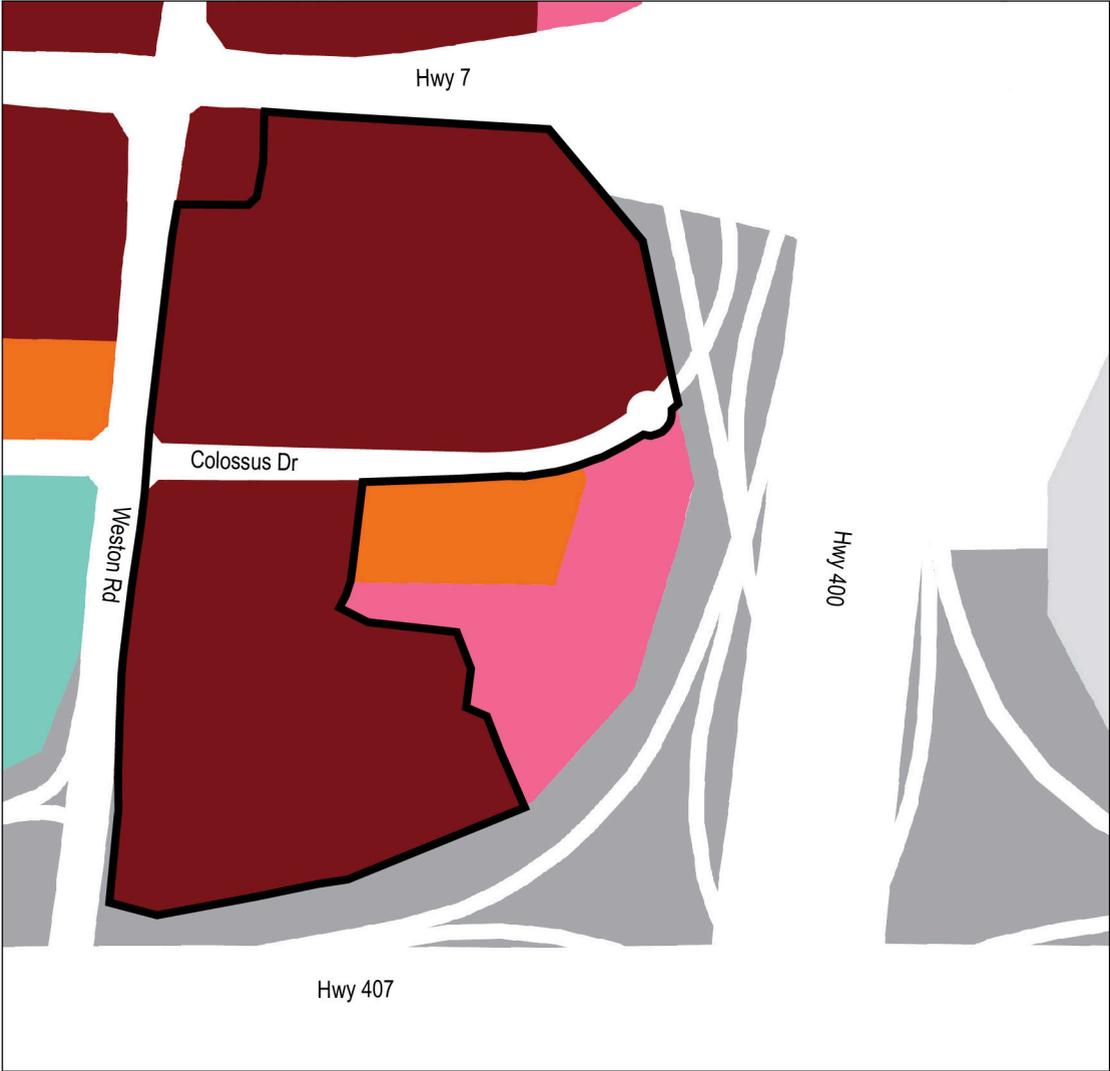


7501 - 7621 Weston Road

 Subject Lands  
*Area Subject to Amendment No.~*

  
Not to Scale

# Schedule 2



7501 - 7621 Weston Road

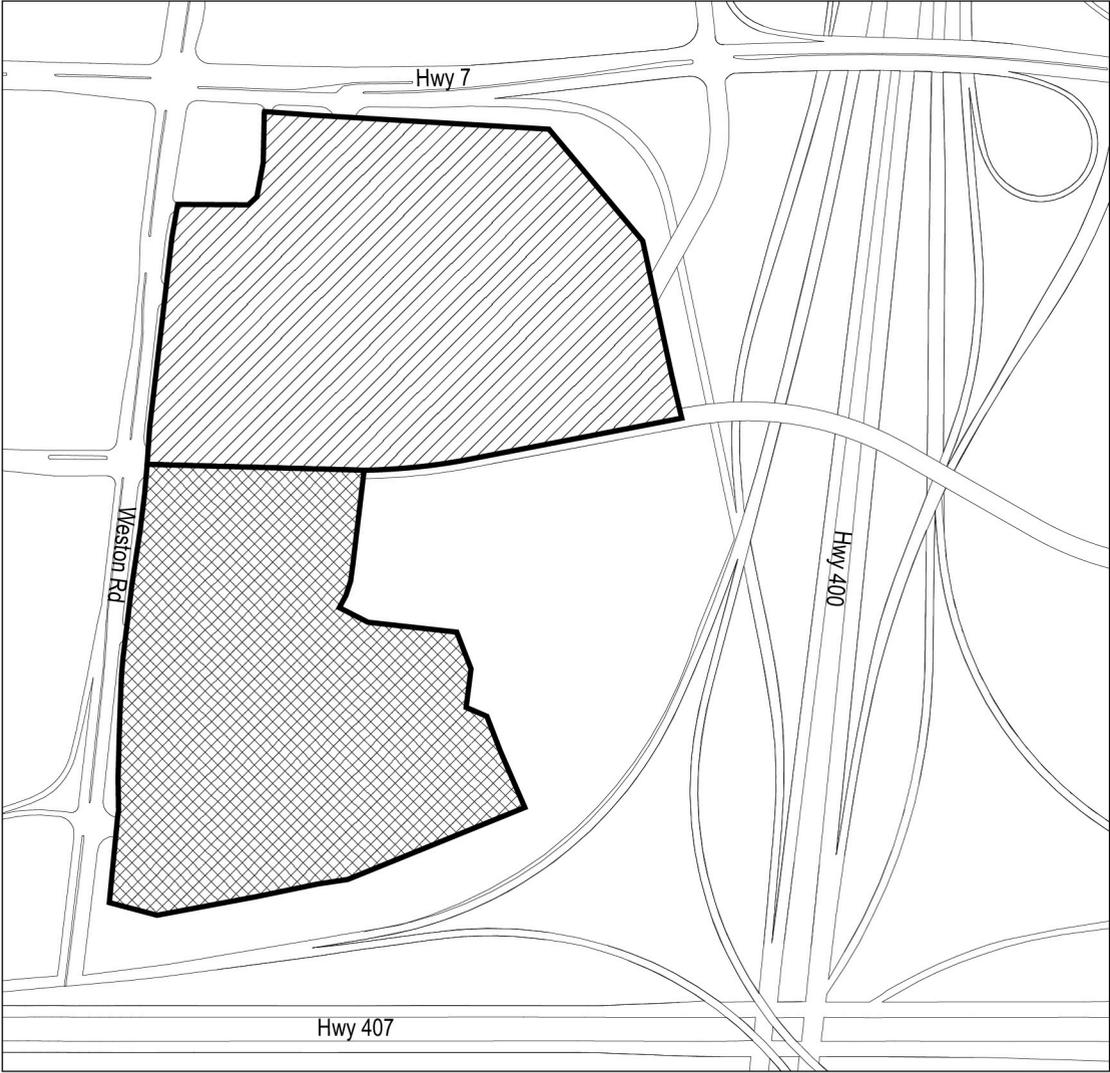
## Land Use

-  Subject Lands
-  High-Rise Mixed-Use



Not to Scale

# Schedule 3



7501 - 7621 Weston Road

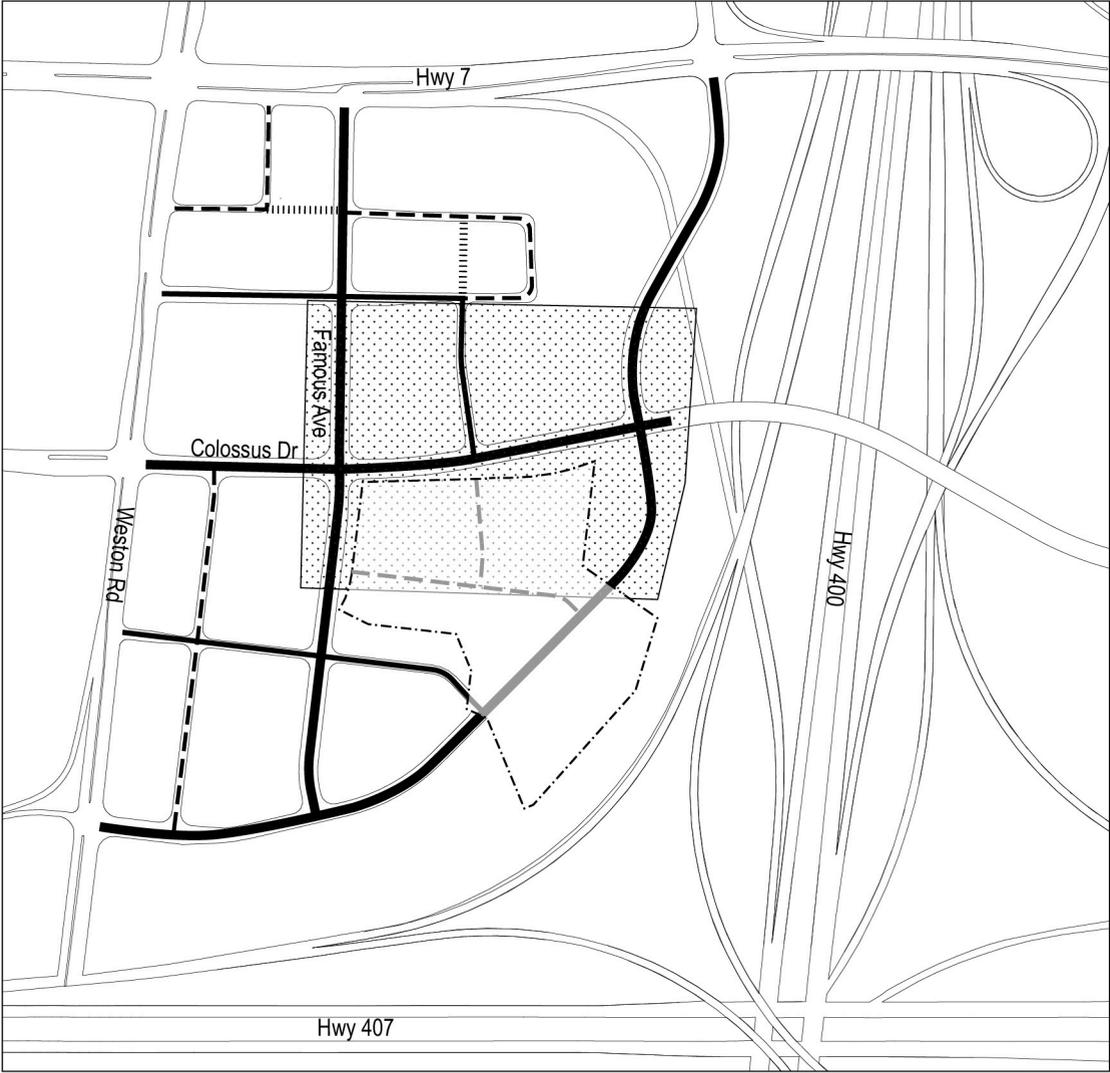
## Precincts

-  Northern Precinct
-  Southwestern Precinct



Not to Scale

# Schedule 4



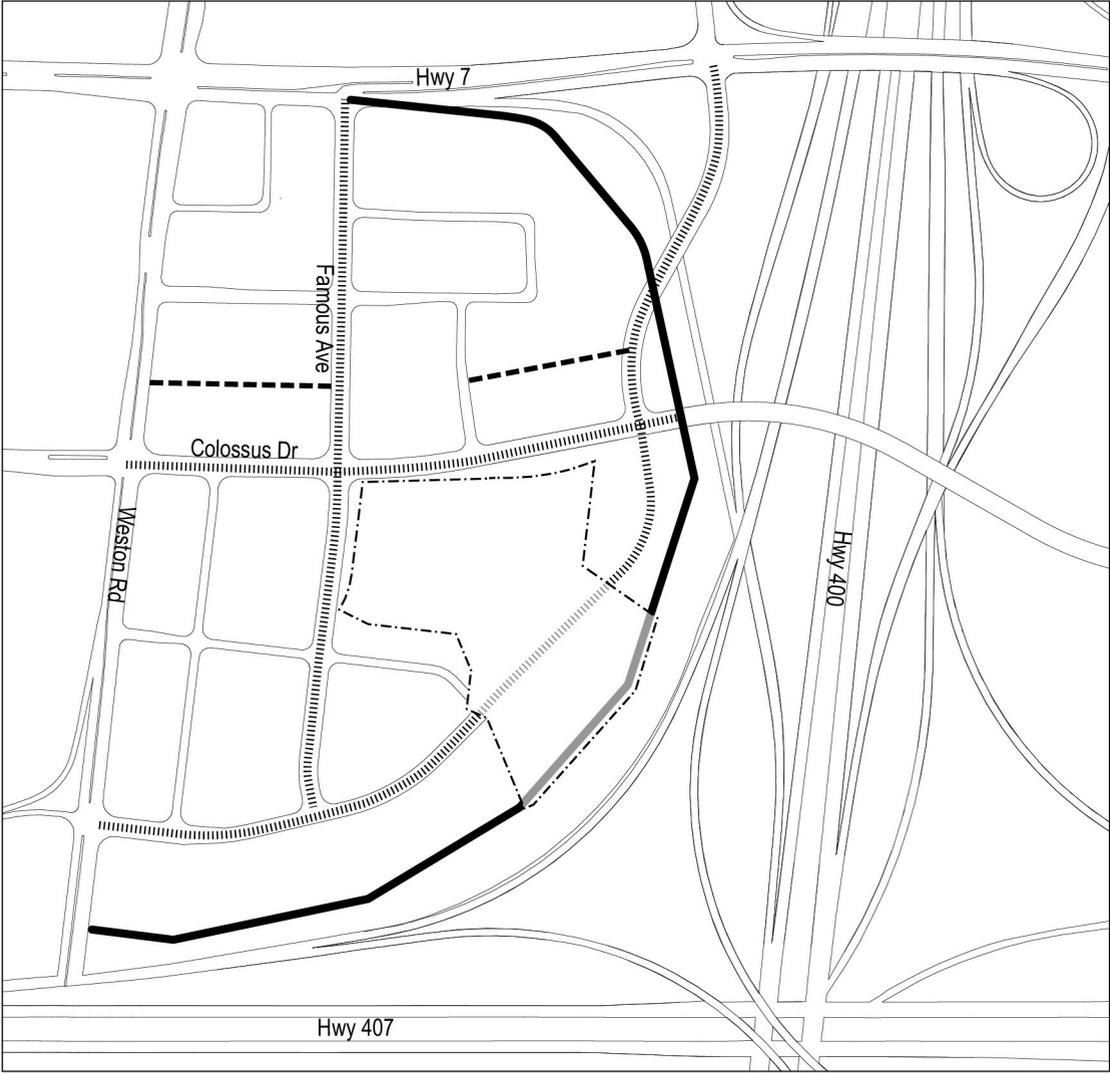
7501 - 7621 Weston Road

## Street Network

-  Major Road
-  Connector Road
-  Local Road
-  Private Local Road
-  Colossus Overpass Conceptual Area
-  Area identified for conceptual purposes only. Does not form part of this amendment



# Schedule 5



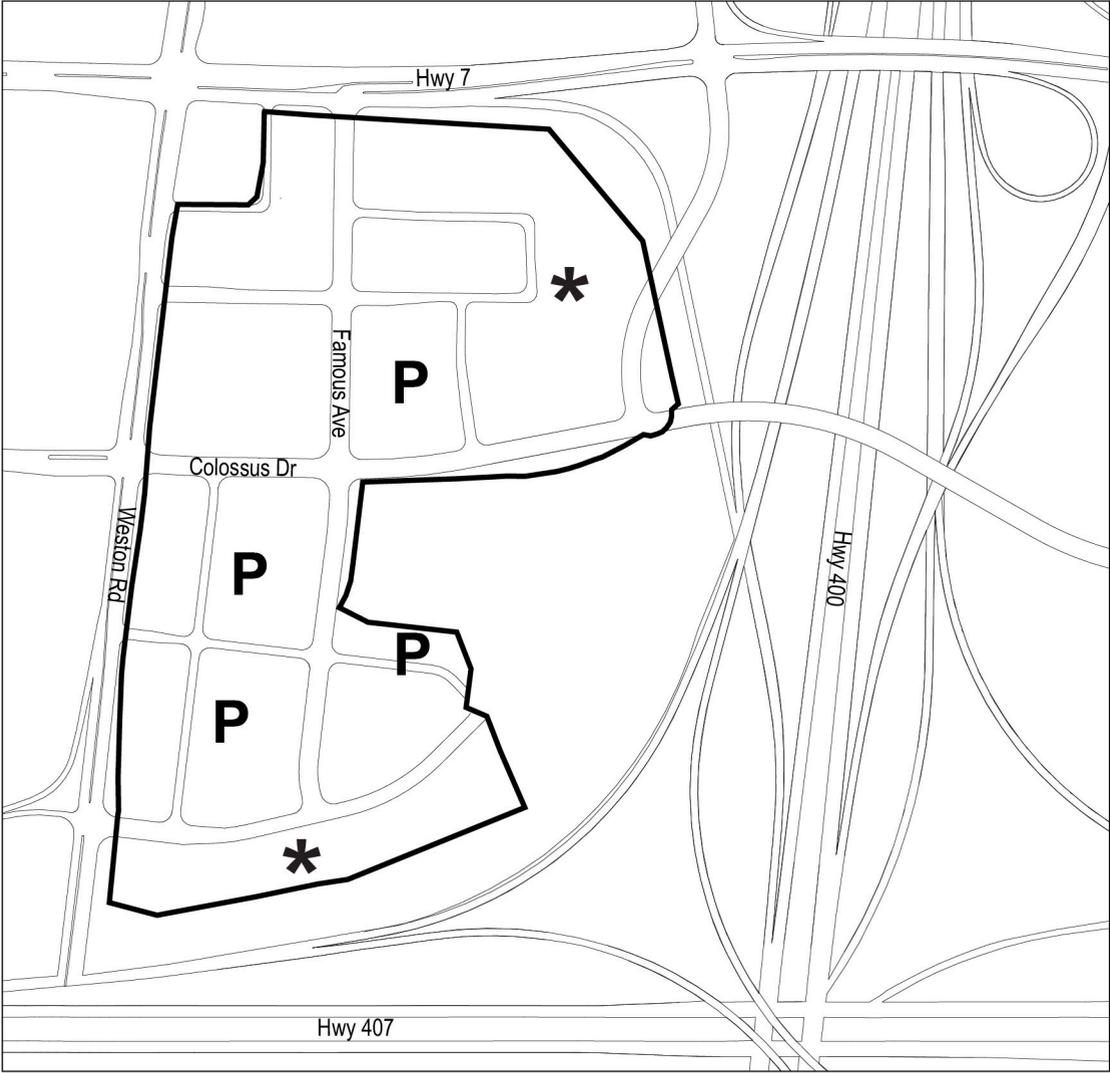
7501 - 7621 Weston Road

## Active Transportation Network

- Multi Use Trail
- ▨ Cycling Network
- - - Pedestrian Mews
- ⋯ Area identified for conceptual purposes only. Does not form part of this amendment



# Schedule 6



7501 - 7621 Weston Road

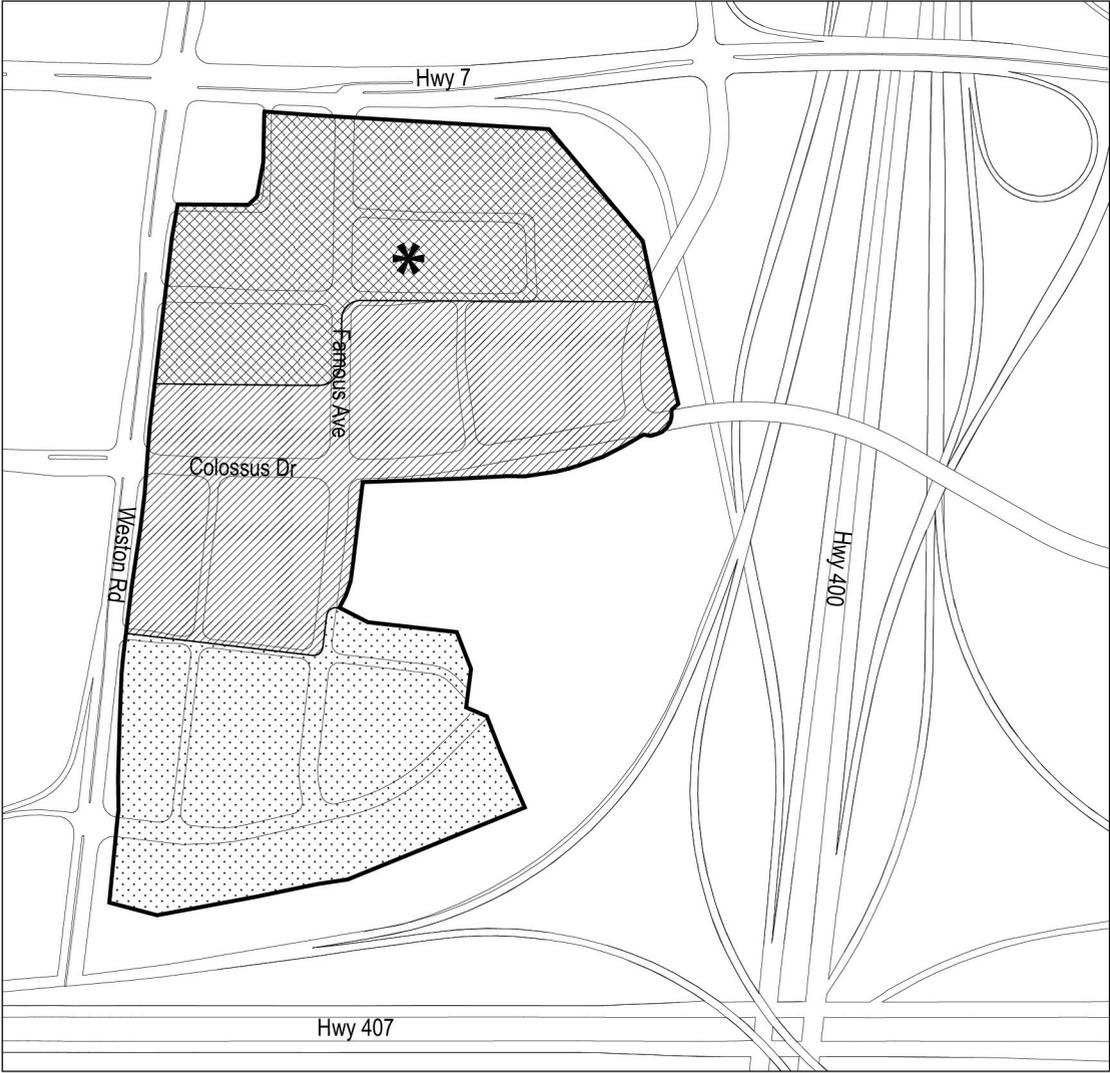
## Parks and Community Facilities

-  Subject Lands
-  Public Parks
-  Priority Locations for Community Facilities



Not to Scale

# Schedule 7



7501 - 7621 Weston Road

## Building Heights

-  Subject Lands
-  Maximum Height 25 ST
-  Maximum Height 40 ST
-  Maximum Height 55 ST
-  \* Landmark Development over 55 ST

*Conceptual heights, not representative of the development*



**Appendix B – RioCan Urban Design Guidelines (submitted separately)**

RIO CAN

# COLOSSUS

Urban Design and  
Sustainability Guidelines

February 2022



URBAN  
STRATEGIES  
INC

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# PURPOSE OF THE DOCUMENT

These Urban Design Guidelines have been prepared by Urban Strategies on behalf of RioCan Management Inc (“RioCan”) in support of an Official Plan Amendment (OPA”) application for the RioCan Colossus site known municipally as 7501-7621 Weston Road (**“the Subject Site” or “the RioCan Colossus Site”**).

As directed by City of Vaughan staff and to satisfy the requirements of the Official Plan Amendment (OPA) application on the Subject Site, these Urban Design Guidelines put forward a master plan-based framework that addresses the whole of the southeast quadrant of the Weston and 7 Primary Centre area (**“the Development Quadrant”**). These Urban Design Guidelines have been prepared in collaboration with Costco, the adjacent landowner in the Development Quadrant, who are aligned with the recommendations of this document.

While the Design Guidelines encompass the full Development Quadrant, the proposal for an Official Plan Amendment that accompanies these Urban Design Guidelines applies only to the RioCan Colossus Site.

Growth within the Master Plan area is expected to be realized through a series of phased development applications over time. The Urban Design Guidelines are intended to function as a high level framework to inform future site-specific applications for redevelopment, providing direction for how to realize a complete, cohesive community in the Development Quadrant as a whole.



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# 1. INTRODUCTION

# 1.1 BACKGROUND



Figure 1. The Development Quadrant and Weston 7 Area

The RioCan Proposal for an Official Plan Amendment seeks to establish a long term development framework for the Subject Site that can contribute to Vaughan’s vision for the Weston 7 Primary Centre and the City’s long term intensification objectives.

This application is being made at time when the City of Vaughan is undertaking a Transportation Master Plan and Secondary Plan study for the Weston 7 Primary Centre. Phase 1 of the Weston 7 Secondary Plan Study process began in 2018. Phases 2 and 3 of the process, including the initiation of the Transportation Master Plan, began in 2020.

Given a comprehensive Secondary Plan is not yet in place for the Weston 7 Primary Centre and to enable the City to comprehensively assess the application prior to the completion of the Secondary Plan, the City of Vaughan has directed the applicant to prepare these guidelines on a quadrant wide basis. The Urban Design Guidelines are based on a comprehensive Master Plan for the south east quadrant of the Weston 7 Primary Centre area which includes:

- The RioCan Colossus Site (“the Subject Site” or “RioCan Colossus Site”)
- The site municipally known as 71 Colossus Drive (“The Costco Site”)
- The site municipally known as 3733 Highway 7 West (“the PetroCanada Site”)
- A stormwater pond owned by the City of Vaughan (“the stormwater pond”)

The RioCan Colossus Site, the Costco Site and the PetroCanada Site are known collectively as “the Development Quadrant”.

While the Official Plan Amendment (OPA) that these Urban Design Guidelines accompany pertains only to the RioCan Colossus site, this document will demonstrate how an urban design framework can be extended across the Development Quadrant to create a cohesive, urban and transit-supportive community. While these guidelines have been developed on a quadrant basis at the request of City Staff, RioCan acknowledges that landowners within the Development Quadrant may seek to make individual applications that may deviate from the concepts put forward within this document.

## INTERPRETATION

These Guidelines are intended to further elaborate on the City's urban design policies found in the City of Vaughan's Official Plan as well as the City's existing guidelines including:

- City-wide Urban Design Guidelines
- Technical Reference Manual
- Streetscape Implementation Manual
- City-wide Public Art Program

All of the detailed images, drawings and renderings contained in these Urban Design Guidelines are provided to conceptually illustrate the application of the proposed Guidelines, and should not be interpreted as the only design solution possible to realize the directions of this document.

The Colossus Urban Design and Sustainability Guidelines is a living document that shall evolve and be reviewed as the Development Quadrant develops over time.

# 1.2 THE DEVELOPMENT QUADRANT

## EXISTING CONDITIONS

The Development Quadrant contains the RioCan Colossus Site, the Costco Site and the PetroCanada Site. The Development Quadrant currently accommodates two gas stations and significant retail and entertainment uses, and functions as a major commercial and entertainment destination for local residents and regional visitors.

Highway 7 and Weston Road bound the Development Quadrant to the north and to the west. These Major Arterial roads carry large volumes of traffic and create harsh edge conditions for the sites within the Development Quadrant. Currently, there are limited access points to the Development Quadrant from Highway 7 and Weston Road comprised of 5 signalized intersections. Highway 400 and 407 define the east and south boundary of the Development Quadrant. An approximately 35-50m landscape buffer area lines the eastern most edge of the Development Quadrant. These buffer lands are owned by RioCan and Costco but managed by the Ministry of Transportation.

Famous Avenue is the central north-south spine within the Development Quadrant and is a private street. Colossus Drive, which bisects the Development Quadrant from west to east, is the only public street within the Quadrant today. Colossus Drive is accessed from Weston Road at Rowntree Dairy Road to the west, from Highway 7 to the north east and directly from a southbound off-ramp from the 400 originating just north of Highway 7.

A new proposed overpass crossing of Highway 400 at the location of Colossus Drive has been identified in the City's Official Plan and the Weston and 7 Transportation Master Plan. The Colossus Drive Overpass is currently being studied through the Weston 7 Transportation Master Plan process which will determine an area of influence within the Development Quadrant for the Overpass, with the final alignment to be studied and determined through an EA process. When constructed, the Colossus Drive Overpass will improve connectivity for the Weston 7 area and serve as a link into the VMC.

An existing stormwater pond is located at the eastern edge of the Development Quadrant which accommodates stormwater and contributes to a landscape buffer between Highway 400 and the retail uses within the Development Quadrant. This pond is owned by the City of Vaughan. The Development Quadrant is surrounded by elevated highway on-ramps to the east and south, and embankments slope from the Quadrant down to meet Highway 400 to the east.

The current built form on the Development Quadrant is typical of a suburban retail format, defined by 1-2 storey low rise retail buildings, internally facing retail pads and extensive surface parking areas.

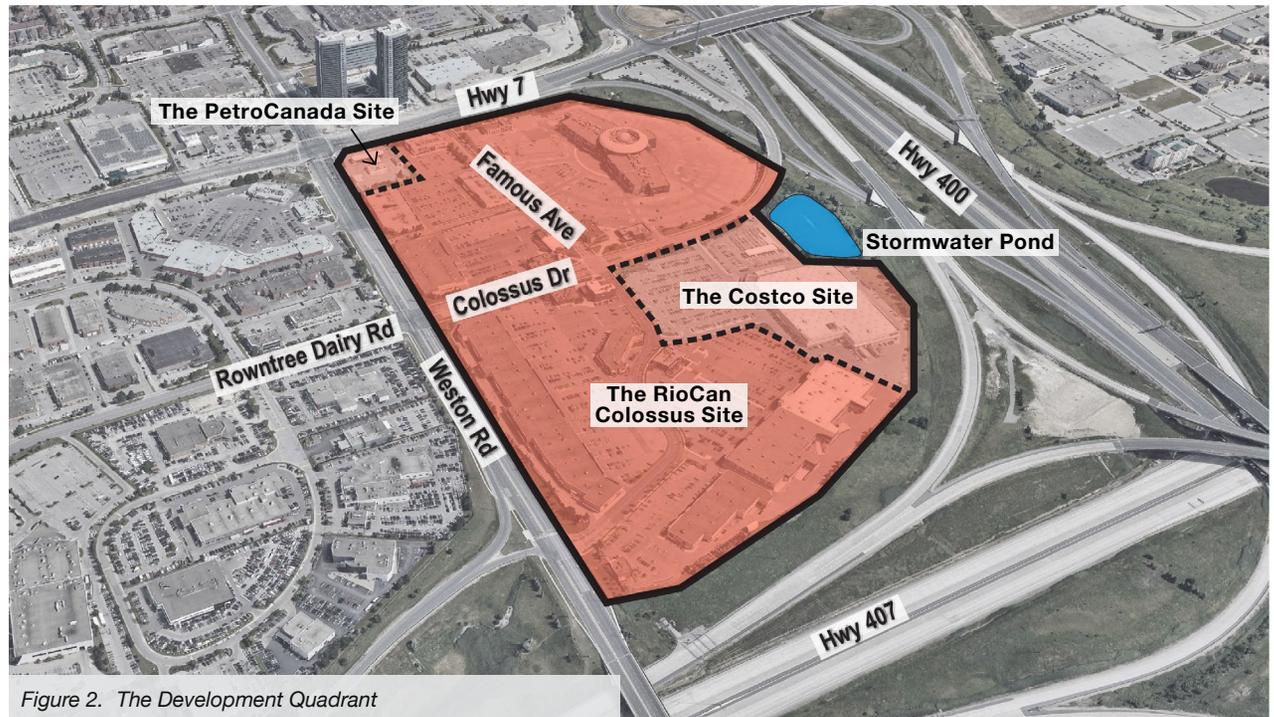


Figure 2. The Development Quadrant

## THE RIOCAN COLOSSUS SITE

The RioCan Colossus Site is one of Vaughan's major retail destinations, with 702,228 sq. ft of leaseable space. At 25 hectares, the site includes anchor tenants including Colossus Cineplex, Bed Bath and Beyond, Buy Buy Baby, HomeSense, and Winners.



*The RioCan Site*

## THE COSTCO SITE

The Costco site accommodates a warehouse membership club which has been in place for 20 years and has an area of 135,500 ft<sup>2</sup>. The site also includes a gas bar with 24 fueling stations and 640 surface parking spaces.



*The Costco Site*

## THE PETROCANADA SITE

The PetroCanada site is located at the intersection of Weston Road and Highway 7. The site accommodates a gas station and car wash facility.



*The Petro Canada Site*

## A COORDINATED QUADRANT APPROACH

Reflecting the importance of a comprehensive approach to planning the southeast quadrant of the Weston 7 Primary Centre, RioCan has worked with Petro Canada and Costco - the neighbouring landowners - to collaboratively establish a Master Plan for the Development Quadrant. The resulting Master Plan reflects a coordinated approach to developing a complete community by establishing a fine grained and connected network of streets and blocks, realizing a network of parks and

open spaces and responding to the unique requirements of each landowner with regards to phasing and interim uses.

While the Master Plan encompasses the full Development Quadrant, the proposal for an Official Plan Amendment that accompanies these Urban Design Guidelines applies only to the RioCan Colossus Site.

# 1.3 THE RIOCAN PROPOSAL

## SUMMARY

The RioCan Colossus Proposal (the “Proposal”) seeks an Official Plan Amendment on the RioCan Colossus Site to realize the vision of the Master Plan for the Development Quadrant. The Proposal is intended to transform the RioCan Colossus Site from a suburban, auto-oriented retail and entertainment hub dominated by surface parking into a vibrant, transit-supportive complete community in the heart of the Weston 7 Primary Centre.

The Proposal seeks to realize a density of 4.0 FSI on the RioCan Colossus Site, which could enable approximately 13,000 units and 1,788 people and jobs per hectare. The Proposal includes an Official Plan Amendment and associated schedules that establish a site-specific street and block network, park and open space network and height distribution strategy. Further details about the RioCan Colossus Proposal can be found in the Planning Rationale Report.

## DEVELOPMENT PHASING

At 25 hectares, the RioCan Colossus Site is large, and it is expected that redevelopment as illustrated in the Proposal will evolve over the long term. The Proposal and its accompanying Official Plan Amendment include a flexible phasing strategy that will guide the evolution of the Subject Site over time.

Development is anticipated to begin in the northwest corner of the RioCan Colossus Site, incrementally realizing development blocks as market demand allows. Development south of Colossus Drive on the Subject Site is expected to take place over the longer term and will need to appropriately consider site access and delivery of open space in relation to the existing, interim and potential future long-term conditions of the adjacent Costco Site. Coordination will be required amongst leaseholders to enable some existing retail uses to remain on the RioCan Site while redevelopment occurs.

The Colossus Drive Overpass is an important piece of infrastructure that will unlock connectivity across Highway 400 for the RioCan Colossus Site, the Development Quadrant and surrounding areas. Future detailed planning applications will consider the design and phasing of the Colossus Drive Overpass construction.



Figure 3. The RioCan Proposal





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## 2. CONTEXT

# 2.1 URBAN CONTEXT

The Development Quadrant is located at the southeast corner of Highway 7 and Weston Road and is part of the Weston 7 Primary Centre, one of the City of Vaughan’s planned locations for intensification and urbanization. The Development Quadrant is situated approximately 1.6 km from the western boundary of the Vaughan Metropolitan Centre (VMC). The Development Quadrant is in close proximity to significant employment areas in Vaughan and is currently an important retail and entertainment hub in the region.

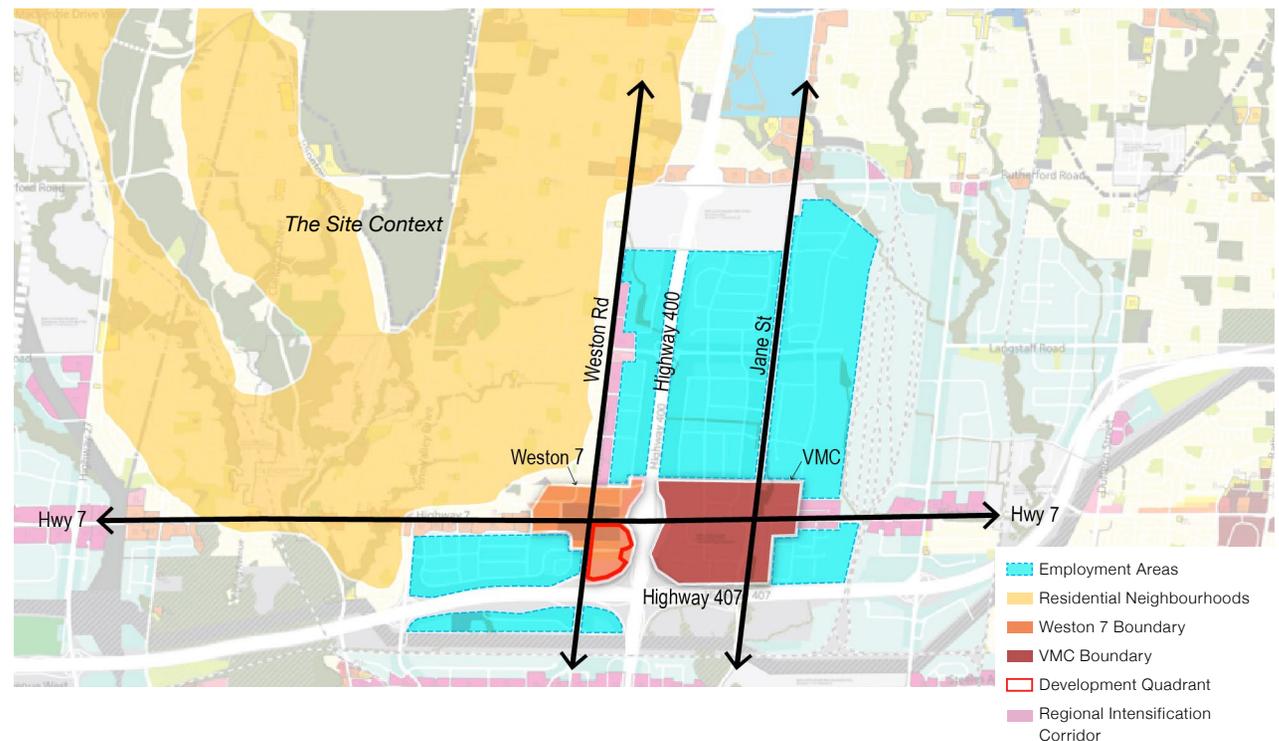
The Development Quadrant functions as a gateway to the western side of Vaughan and forms a critical link to the VMC, the City’s growing downtown and Regional Centre. Unlike the VMC, the Development Quadrant has an immediate relationship with the stable residential neighbourhoods to the northwest, providing retail, service and entertainment uses for that population today.

Highway 7 bounds the Development Quadrant to the north and is an important Regional Intensification Corridor. Tremendous growth and urban transformation along this corridor has been taking place, supported in part by the presence of the Highway 7 Rapid Bus Transit route (Highway 7 Rapidway). The Highway 7 Rapidway currently spans from Bruce Street in Vaughan to Birchmount Road in Markham, connecting to TTC subway service at the VMC as well as connecting to other interregional transit services along its planned 30+ km of dedicated rapid transit service.

The Development Quadrant represents 32% of the total area of the Weston 7 Primary Centre and has a large role to play in realizing the vision for Primary Centres to accommodate mixed-use, transit-supportive growth and intensification.

The Development Quadrant is 1.4 km from the VMC Toronto Transit Commission (TTC) station to the east. In 2017, the VMC subway station opened as part of the TTC Line 1 subway extension, providing a direct connection to Toronto’s downtown.

In addition to the subway station, higher order transit serves the Development Quadrant through the Highway 7 Rapidway Bus Rapid Transit route. The Highway 7 Rapidway has steadily expanded since opening in 2011, now providing connections from Bruce Street in Vaughan to Birchmount Road in Markham. The investment in public transit has increased connectivity throughout the City of Vaughan and supported considerable mixed-use development along this critical spine.



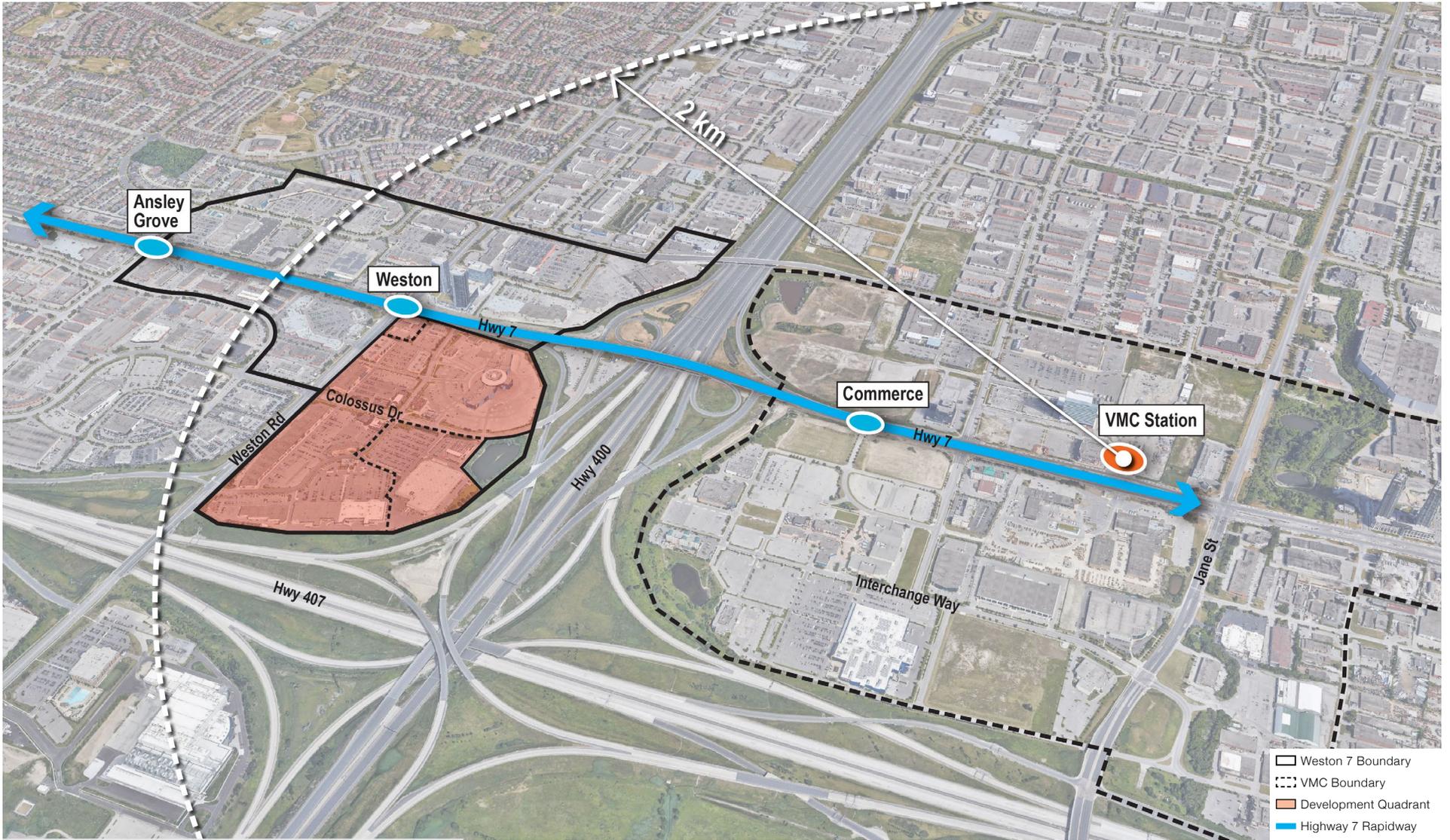


Figure 4. The Urban Context

## 2.2 DEVELOPMENT CONTEXT

There are a number of proposed and recently approved developments in the area surrounding the Development Quadrant. The recent development activity is predominantly residential and/or mixed use with a podium tower built form. The developments closest to the Development Quadrant are described below.

### 1. 7887 Weston Road

A Zoning By-law Amendment and Official Plan Amendment application for four residential towers with height ranging from 40 to 49 storeys including retail uses in the proposed podium. The proposal includes 2,003 dwelling units and 1,801 parking spaces, and an FSI of 9.61.

### 2. 137 Chrislea Road

A Zoning By-law and Official Plan Amendment application to permit eight new residential blocks and two mixed use blocks accommodating a total of 3,962 dwelling units and 3,284 parking spaces for a total FSI of 5.84.

### 3. 7520, 7540, 7560 Weston Road

A Zoning By-law and Official Plan Amendment application to permit two high-rise residential towers of 42 and 45 storeys accommodating a total of 952 residential units and 818 parking spaces.

	Address	Ownership	Status	Height(s) in storeys	FSI
1	7887 Weston Rd	Sorbara	Under Review	40-49	9.61
2	137 Chrislea Rd	Smart Centres	Under Review	8-45, with 2 @ 45	5.8
3	175 Millway Ave	Smart Centres	Approved	35, 45, 50	7.08
4	2920 Highway 7	Royal 7 Developments	Approved	60	10
5	3300 Highway 7	1042710 Ontario Ltd	Under Review	15, 55, 58	11.6
6	3201 Highway 7	Gupta Group	Approved	55, 55, 46, 29, 17 + townhouses	11.3
7	2951 Highway 7	Liberty Developments	OPA/ZBL Approved, SPA Under Review	35, 35, 36, 9	6.3
8	2901 Highway 7	Liberty Developments	Approved	33, 39	5.72
9	2851 Highway 7	GB Vaughan Seven	Under Review	37, 27	5
10	Part Lot 4, Con 5	Mobilio Developments	Approved	12, 15, 18 +townhouses	2.65
11	7520, 7540 and 7560 Weston Road	237193 Ontario Inc.	Under Review	45, 42	7.38
12	15 Jevlan Drive, 156 Chrislea Road	FDF Investments Ltd. and Playacor Holdings	Under Review	N/A	N/A
13	Regional Road 7 – 3301 Highway 7	RP B3N Holdings Inc.	Under Review	40, 46, 55 & 59	7.8
14	Part 16, Part 18, Parts 37-41, Parts 66-68 of Plan 65R-20291	RP B3S Holdings Inc.	Under Review	36, 41, 45	6.6
15	189, 185 Millway Avenue, 996, 1000, 1004 Portage Parkway7894, 7890, 7886 Jane Street	Penguin-Calloway (Vaughan) Inc.	Under Review, Council Approved on May 14, 2019, Site Plan Agreement Pending Review	35, 40, 50 storeys	7.08



Figure 5. Development activity surrounding the Development Quadrant

- Development Quadrant
- Under Review
- Approved / Under Construction
- VMC Proposed Buildings





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# 3. THE MASTER PLAN

# 3.1 GUIDING PRINCIPLES

The following eight guiding principles have shaped the approach to the Master Plan for the Development Quadrant.

## Create a complete community



Realize a complete community that includes housing choices at various affordability levels, opportunities for retail, commercial and entertainment uses as well as community amenities that support and enhance the social fabric of the place.

## Function as a regional destination for retail and entertainment



Reinforce and enhance the area's existing role as a retail and entertainment destination for local residents and visitors.

## Establish a fine grained network of streets and blocks



Improve circulation throughout the Development Quadrant, provide additional access points and connectivity, all uses and elements within the Quadrant and strengthen connections between the Quadrant and the surrounding area.

## Create a pedestrian oriented and walkable urban environment with a number of mobility choices



Encourage active transportation modes by offering convenient pedestrian and cycling routes and amenities, and explore enhanced connections with existing transit infrastructure.

**Establish a connected network of parks and open spaces**



Accommodate a wide range of well designed parks and open spaces to provide formal and informal outdoor amenity, provide a context for social interaction, and contribute to sustainability and resilience.

**Encourage a diverse and high quality built form**



Feature high-quality architectural design to create a distinct and interesting identity and enhance the physical and visual character of the area.

**Promote sustainable development**



Employ a comprehensive approach to sustainable design that encompasses a range of innovative solutions to promote environmental, social, cultural and economic sustainability.

**Establish a flexible plan for phased development**



Enable a flexible approach to implementation, allowing for adaptation and flexibility over time while delivering the overall intent of the Master Plan vision.

## 3.2 THE MASTER PLAN

The Master Plan contemplates a comprehensive redevelopment of the Development Quadrant to create a vibrant, mixed-use, urban environment. The Plan incorporates retail, commercial, residential and community uses anchored by a network of streets and blocks and complemented by a series of new parks and open spaces.

New development will be supported by a robust and highly connected network of public and private streets, including the transition of one private street into a public street. The current internal network of private driveways will be expanded into a formalized street network to increase connectivity throughout the Development Quadrant and to surrounding arterial roads. Development within the Development Quadrant will be anchored by two key linkages: Famous Avenue, a north-south spine, and Colossus Drive, an east-west spine that includes the future Colossus Drive Overpass which will connect the Weston 7 area east to the VMC. The planned Colossus Drive Overpass is an important piece of infrastructure that will connect the Development Quadrant with its surrounding area, including enhancing access to key amenities such as the VMC TTC subway station.

A series of interconnected new parks and open spaces are envisioned as part of the Master Plan, which will enhance the outdoor amenity in the Weston Road and Highway 7 area. New parks are sized to accommodate a range of programming

and design approaches, provide opportunities for active and passive recreation and contribute to environmental sustainability and resilience. A generous range of other private open spaces will supplement the parks, enhancing pedestrian connectivity throughout the Development Quadrant.

The north and west boundaries of the Development Quadrant, Highway 7 and Weston Road, are Major Arterial roads with a harsh, automobile focused character. The Master Plan seeks to respond to these the exterior edges by establishing a minimum 3 metre setback along both Highway 7 and Weston Road, softening the experience of these wide vehicular corridors. In addition, the retail, entertainment and pedestrian focus of the Master Plan is focused towards the interior of the Development Quadrant, creating an intimate development pattern that is more pedestrian oriented and friendly in comparison to the edges of the Development Quadrant.

The Master Plan creates distinct precincts: a Northern Precinct which will evolve as a vibrant high-density destination adjacent to the existing higher order transit infrastructure; the Southwestern Precinct, which will have a strong residential identity that can accommodate a wide range of built form typologies and densities; and the Southeastern Precinct, which will include mixed use high density development that frames the highway edge.



Figure 6. The Master Plan

# 3.3 BIG MOVES

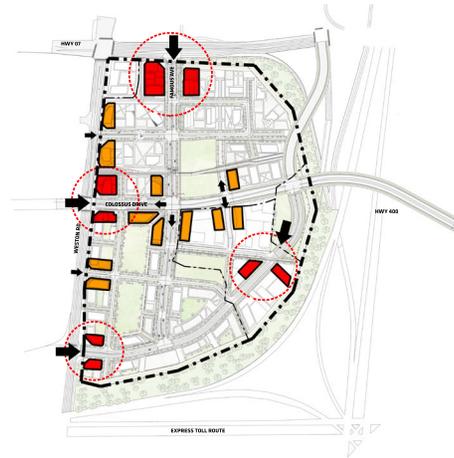
The essence of the Master Plan can be captured in a series of six 'Big Moves' which are further described below. These Big Moves will bring about coordinated, cohesive change in the Development Quadrant.



## Create distinct precincts

The Master Plan seeks to deliver a more intimate, fine-grained development pattern toward the interior of the Development Quadrant while addressing the Highway 7 and Weston Road edges. The Master Plan provides three precincts as a basic premise of site organization: the Northern Precinct, the Southwestern Precinct and the Southeastern Precinct.

The Master Plan locates the highest density and greatest mix of uses in the Northern Precinct, closest to transit access. The Northern Precinct will have a strong residential and commercial character, and will be the focus of entertainment uses. The Southwestern Precinct will include high, medium and lower density uses and evolve with a strong neighbourhood character, complemented by the Southeastern Precinct which will include higher density uses that frame the highway edge.



## Reinforce a sense of arrival

The Master Plan recognizes the importance of establishing a strong sense of arrival at key points of entry to the community.

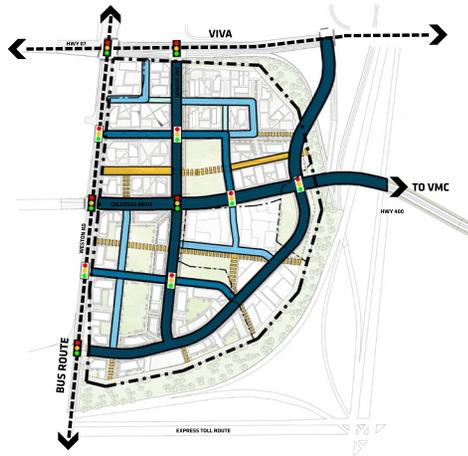
Future development will define gateways through landscape moves or architectural expression at key entrances from Highway 7, Weston Road and at the future Colossus Drive Overpass.



## Create place-defining open spaces

The Master Plan envisions a connected network of signature open spaces that will be framed by community-building uses, animated retail uses, and built form. In the Northern Precinct, the Master Plan contemplates a central public park which will be an animated gathering space, well-defined by buildings and commercial retail uses. In addition to the central public park, a pedestrian mews and a network of other open spaces will define the Precinct and contribute to connectivity.

In the Southwestern and Southeastern Precincts, a series of public parks will be the focus of neighbourhood development and provide opportunities for informal play and recreation. A new park, located adjacent to commercial uses on Famous Avenue will provide a destination for gathering and become a key street linking the public parks in both Precincts. Another park east of Famous Avenue will bridge the RioCan Colossus and Costco Sites and function as a passive space for day-to-day recreational use.



**Plan for multi-modal movement**

The Master Plan proposes a connected street network that allows for balanced pedestrian, vehicular, transit and cycling movement. This well-developed network of streets will support city-building goals focusing on sustainability, streetscape beautification, and pedestrian and cyclist movement.



**Activate parks and mews frontages**

The Master Plan strategically locates animated uses along the public parks and major pedestrian corridors to ensure 'eyes on the street' and encourage 24/7 vibrancy within the Development Quadrant.



**Develop a range of destinations**

The Master Plan includes a land use strategy that will encourage the realization of a range of destinations including commercial entertainment uses, such as a theatre, restaurants and hotel uses, as well as community serving uses to support the neighborhood as a complete community.





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## 4. SUSTAINABILITY GUIDELINES

# 4.1 GENERAL SUSTAINABILITY GUIDELINES

The City of Vaughan has an extensive Sustainability Guidelines document to encourage optimal environmental performance in new development. The City's Sustainability Guidelines include directions for the development of sustainable communities, including improved building performance, lower ecological footprints, decreased greenhouse gas emissions and better integration with open spaces and parks.

These sustainability guidelines build upon Vaughan's existing direction and provide additional direction specific to the Development Quadrant and the Master Plan.

## GUIDELINE 1

Development in the Development Quadrant is encouraged to achieve a minimum of a Bronze Performance Level at the Block Plan / Draft Plan of Subdivision stage.

## GUIDELINE 2

Explore options to integrate District Energy.

## GUIDELINE 3

Facilitate development designed for flood considerations and enhanced stormwater management controls in line with TRCA Humber River Watershed Requirements.

## GUIDELINE 4

Employ Low Impact Development techniques where possible to reduce stormwater runoff.

## GUIDELINE 5

Establish a mobility network that supports active transportation and the use of public transit to minimize personal auto trips and reduce emissions and congestion.

## GUIDELINE 6

Provide green spaces to decrease the heat island on the Development Quadrant.

## GUIDELINE 7

Select landscape materials that are durable and reduce waste.

## GUIDELINE 8

Plant native species wherever possible to strengthen the biodiversity of the Development Quadrant.

## GUIDELINE 9

Incorporate a varied mix of uses in compact built form to reduce car dependence and decrease greenhouse gas emissions.

## GUIDELINE 10

Consider passive solar design where possible to increase energy efficiency.

## GUIDELINE 11

Generally locate the highest densities in proximity to existing transit along the Highway 7 corridor to encourage transit use.

## GUIDELINE 12

Consider designing for third-party certifications where feasible to achieve high performance in areas of human and environmental health, energy efficiency and environmental impact.







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## 5. MOBILITY GUIDELINES

# 5.1 STREET NETWORK

The Master Plan envisions a fine-grained street network comprised of complete streets that are easily accessible to all road users and contribute improvements to the existing road and transit networks. The following guidelines should be considered in the implementation of the street network.

## GUIDELINE 13

Streets in the Development Quadrant should be designed as a complete network, taking into account the needs of all modes of travel.

## GUIDELINE 14

Major Roads should generally be designed to a ROW of 28-33m, and should accommodate pedestrian, cycling and vehicular traffic and reinforce broader connectivity beyond the Development Quadrant to its surrounding context.

## GUIDELINE 15

Connector Roads should generally be designed to a ROW of 20m and provide a finer grain of connectivity than Major Roads, enabling circulation and access throughout the Development Quadrant.

## GUIDELINE 16

Local Roads should generally be designed to a ROW of 17-20m and provide fine-grained street and block connections to enhance porosity to enable efficient pedestrian and active travel.

## GUIDELINE 17

Additional signalized connections to Weston Road are encouraged where feasible to increase permeability and movement choice.

## GUIDELINE 18

All new public streets connecting to Highway 7 or Weston Road as unsignalized intersections should be right-in/right-out to provide enhanced access to future development blocks.

## GUIDELINE 19

The street network should be planned and implemented to accommodate existing, interim and future land uses.

## GUIDELINE 20

Private streets, where present in the Development Quadrant, should be designed to public standards.

## GUIDELINE 21

The street network will be delivered incrementally, and existing and interim conditions and uses will be considered and coordinated across the Development Quadrant.

## GUIDELINE 22

New development with frontage on the future Colossus Drive Overpass should be designed to respond to the grade changes anticipated with this planned infrastructure.

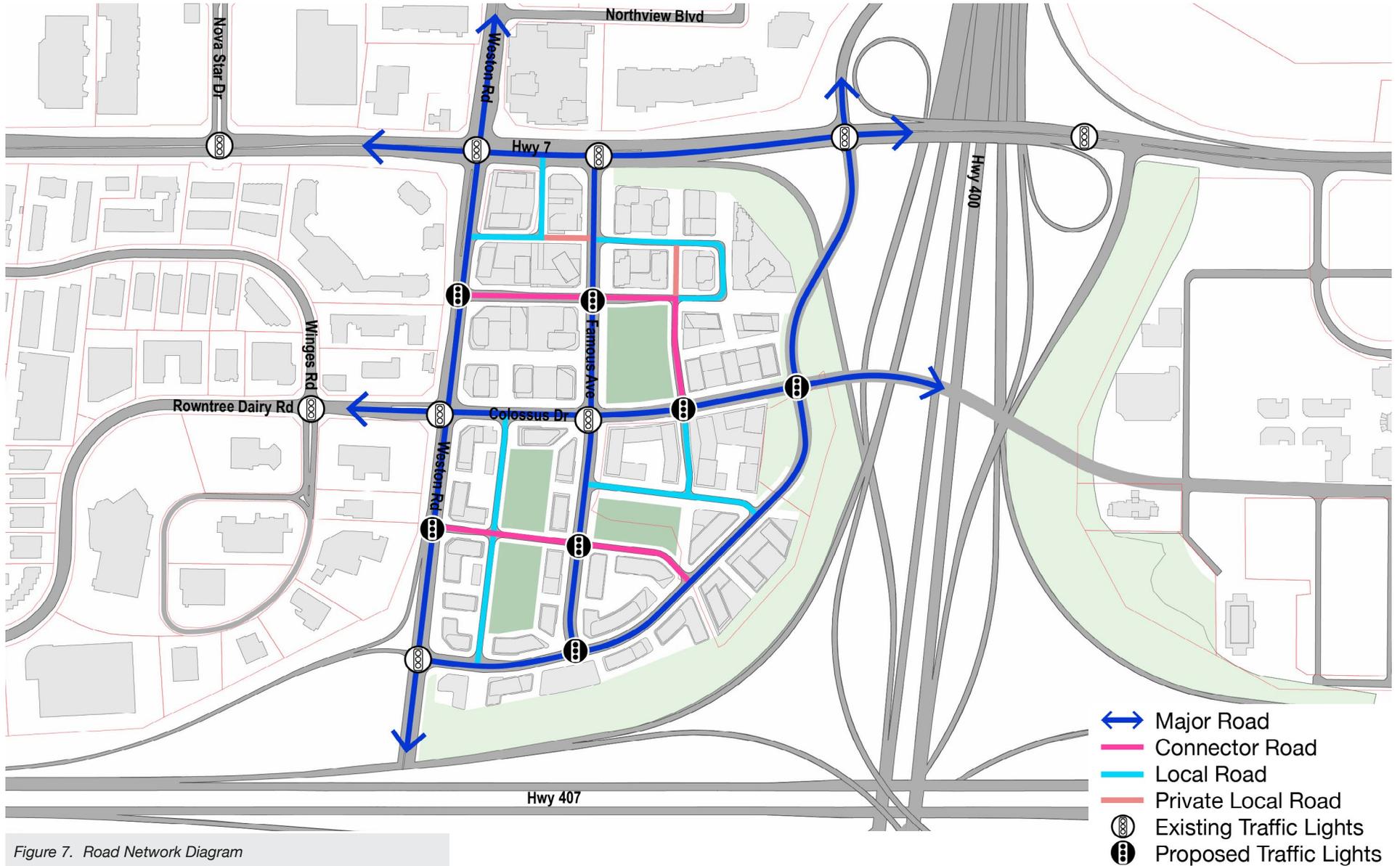


Figure 7. Road Network Diagram

## 5.2 PEDESTRIAN SYSTEM

The delivery of a connected pedestrian and cycling network is essential to the long-term success of mobility within the Development Quadrant and supports the development of a complete and connected community. The following guidelines should be considered in the development of the pedestrian movement systems.

### GUIDELINE 23

All streets within the Development Quadrant, including public and private rights of way, should be designed to accommodate pedestrian movement.

### GUIDELINE 24

Pedestrian comfort should be considered in the size and programming of the public realm, with consideration given for the appropriate size of pedestrian clearways within the right of way that are reflective of the character and role of the street in the Development Quadrant.

### GUIDELINE 25

Public streets should be designed to enhance the pedestrian experience by locating special paving, street furniture and tree planting where appropriate.

### GUIDELINE 26

Streets with frontage on parks should be lined with active, pedestrian-oriented uses to promote animation.

### GUIDELINE 27

Future development should consider pedestrian connectivity in the design and organization of blocks. Mid block connections, interior or exterior to a building, should be encouraged to promote permeability.

### GUIDELINE 28

Parks should be easily and comfortably accessible to pedestrians from public streets.

### GUIDELINE 29

Future wayfinding strategies in the Development Quadrant should provide directional and distance information to encourage walking to fulfill day-to-day trips, including the identification of fitness loops to promote health and wellness.

### GUIDELINE 30

The pedestrian mews should advance pedestrian movement as a priority and should be designed to reflect its special character, including consideration for a curb-less condition which will on occasion accommodate service vehicles.

### GUIDELINE 31

Pedestrian movement may be accommodated outside of the street network through the Multi Use Trail within the MTO setback and potentially through a future naturalized and publicly accessible stormwater pond.

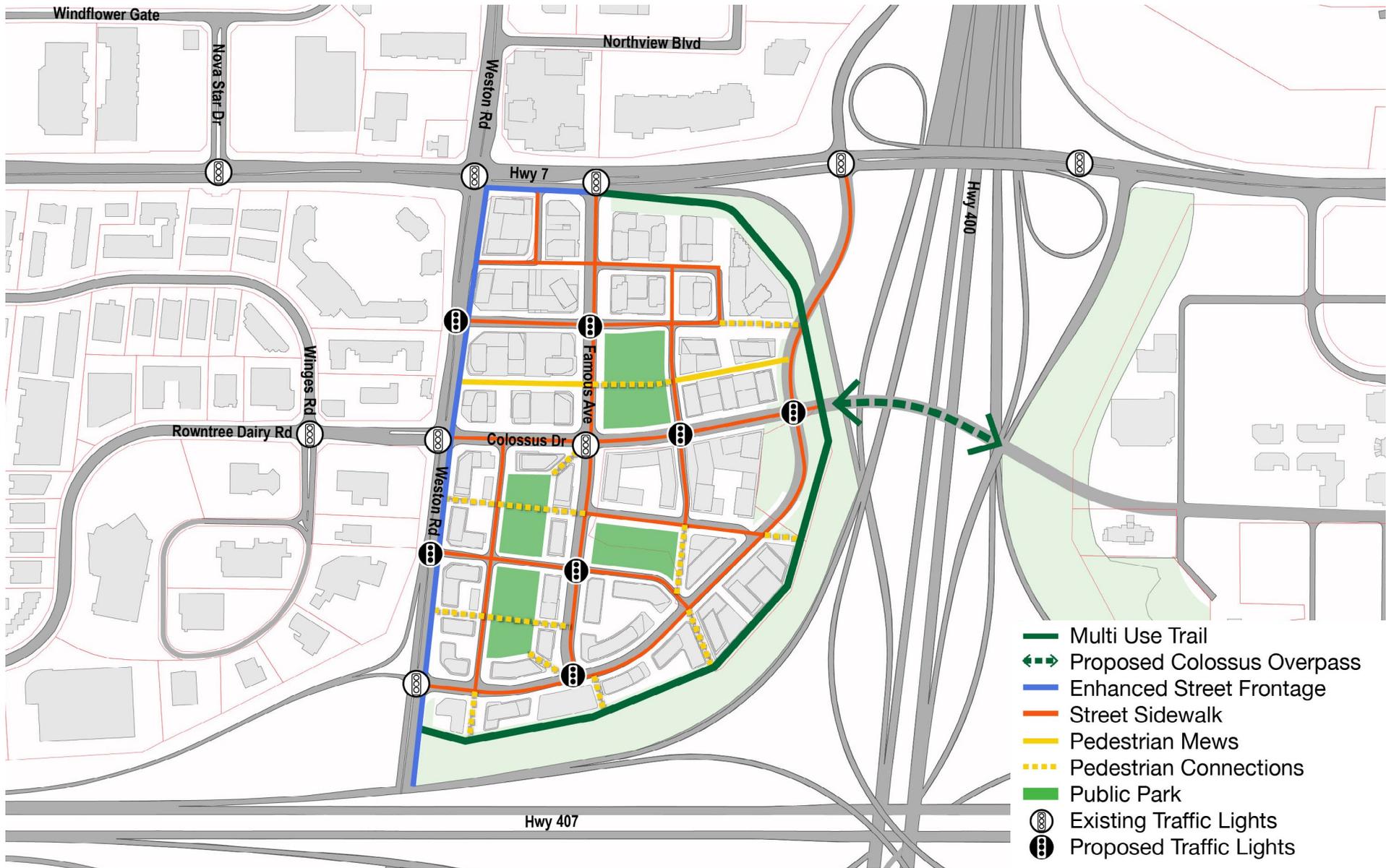


Figure 8. Pedestrian Network Diagram

# 5.3 CYCLING SYSTEM

Sufficient bicycle infrastructure at grade is a key element to foster a bicycle -friendly street scape. The following guidelines address cycling within the Development Quadrant:

## GUIDELINE 32

Major Roads in the Development Quadrant should be designed to accommodate safe cycling routes and consider appropriate cycling infrastructure such as on-street cycling lanes, separated cycling lanes and other design solutions.

## GUIDELINE 33

Cycling may be accommodated outside of the street network through the Multi Use Trail within the MTO setback.

## GUIDELINE 34

Wayfinding strategies for the Development Quadrant should provide directional and distance information to encourage cycling to local destinations. These strategies are expected to be developed as part of more detailed development applications.

## GUIDELINE 35

Safe and convenient cycling infrastructure should be considered in the design process of the future Colossus Drive Overpass.

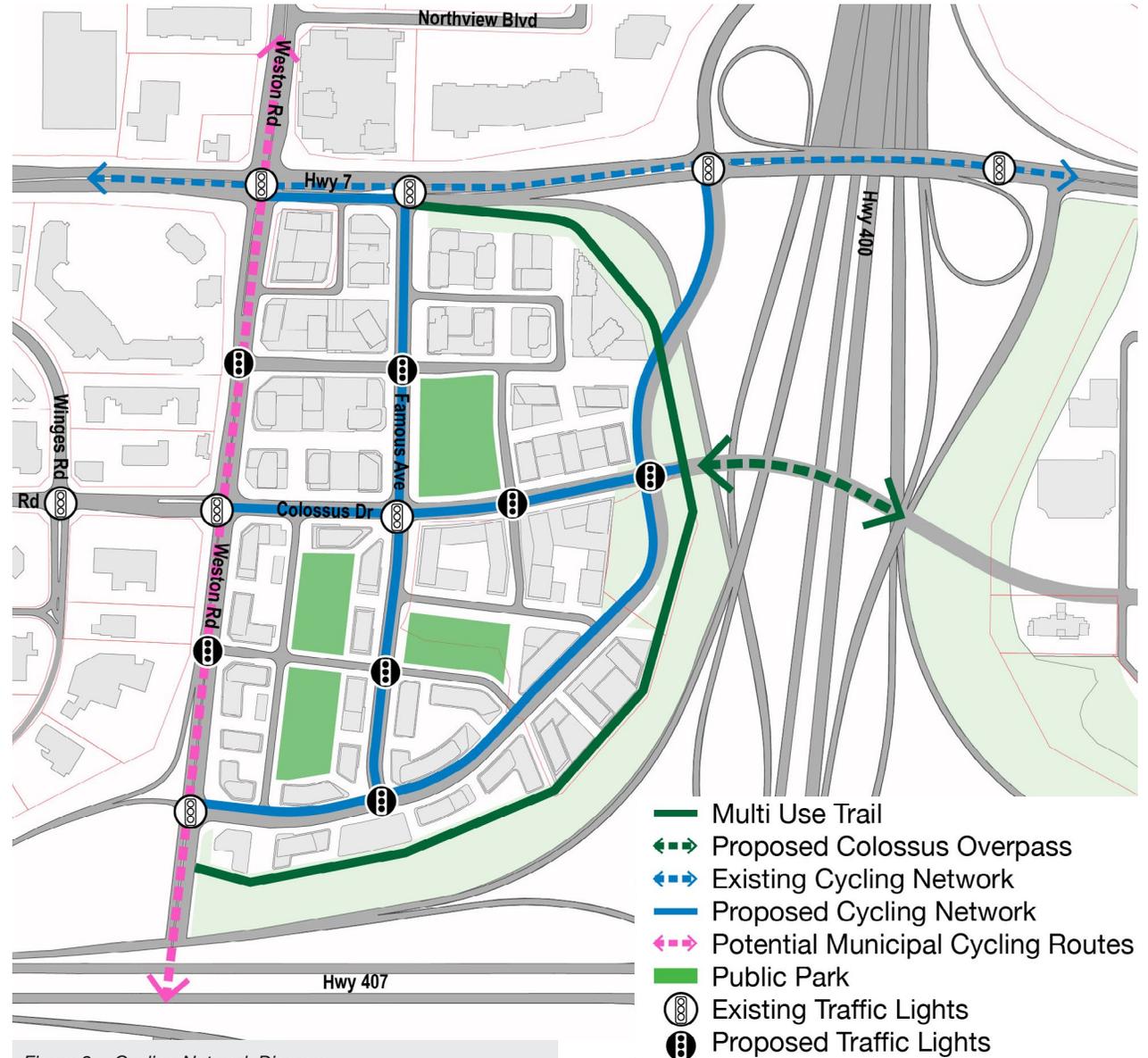


Figure 9. Cycling Network Diagram

# 5.4 TRANSIT

Development should contribute to the promotion of transit use, provide physical connections into the existing network and realize new connections that enhance transit opportunities. The following guidelines describe how development should encourage and enhance transit use.

## GUIDELINE 36

Transit use should be encouraged by providing convenient connections to existing transit.

## GUIDELINE 37

Future wayfinding strategies in the Development Quadrant should provide directional information to connect people to local transit.

## GUIDELINE 38

Major Roads including the future Colossus Drive Overpass should be designed to accommodate potential future transit routes within the Development Quadrant.

## GUIDELINE 39

Last mile transit solutions including local shuttle services routes should be explored to improve transit access for residents, employees and visitors in the Development Quadrant.

## GUIDELINE 40

Future development should consider provision of preloaded transit passes for residents among the Traffic Demand Management strategies that may be implemented.

## GUIDELINE 41

The City of Vaughan should continue to work with regional transit agencies to advocate for an integrated transit fare strategy to encourage transit usage between regions.



*York Region Transit BRT*



*Micro-Transit*

# 5.5 PARKING AND SERVICING

Future development will need to accommodate parking, loading, and all other servicing facilities in an urban and compact built form, reduce adverse impacts on the public realm and encourage pedestrian-oriented activities. The following guidelines address how to accommodate parking and servicing in the quadrant.

## GUIDELINE 42

Given the planned retail and entertainment character of the Development Quadrant, dedicated commercial parking may be required to attract and support these users. Where possible, shared parking among residential and non-residential uses should be encouraged. Where dedicated parking facilities are necessary to support planned uses, they should be located between Weston Road and Famous Avenue to discourage excessive commercial-related traffic infiltration and encourage pedestrian activity.

## GUIDELINE 43

Development should reduce the impact of loading facilities on the public realm by locating servicing facilities within buildings and in consolidated facilities wherever possible.

## GUIDELINE 44

New development should be supported by a Travel Demand Management system to reduce demand for parking.

## GUIDELINE 45

Where possible, development is encouraged to accommodate required parking underground or within consolidated facilities to maximize efficiency. Consolidated commercial parking facilities should be considered and located centrally within the Development Quadrant.

## GUIDELINE 46

Where present, consideration should be given to the design of above grade parking structures including: integrating structured parking within the base of new buildings; wrapping portions of the lower levels of parking structures with commercial, retail, residential or community uses to ensure that the structure is not visible from adjacent streets or open spaces; designing structured parking to incorporate fenestration, well-articulated openings and high-quality materials; consider long term potential adaptive re-use to enable conversion of parking structures to other uses in the future.

## GUIDELINE 47

With the exception of transitional parking, new development blocks should generally be designed to minimize surface parking and on-street parking.

## GUIDELINE 48

At-grade, visible service courts are discouraged.

## GUIDELINE 49

On-street parking is encouraged on local roads to add amenity to the area and provide street friction to slow traffic.

## GUIDELINE 50

As development occurs on the Development Quadrant, the use of surface parking may be required to support development phasing and coordination with existing and interim uses.



*Above-grade parking structure with glazing and commercial uses*



*Structured parking with high-quality materials and clear openings*





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## 6. PUBLIC REALM GUIDELINES

# 6.1 INTRODUCTION

Park, opens spaces and streetscapes are an important part of the livability and success of a community and a strong emphasis has been placed on the public realm in the Master Plan. The future landscape strategy reflects a number of systems that together create a comprehensive public realm approach.

The following section will provide guidelines for each of the landscape systems including:

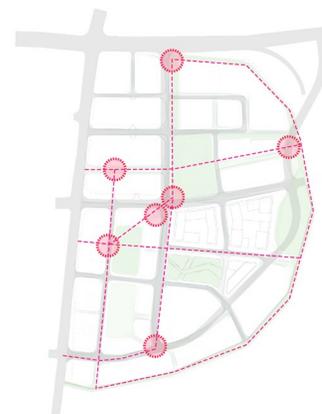
**Parks & Open Space**



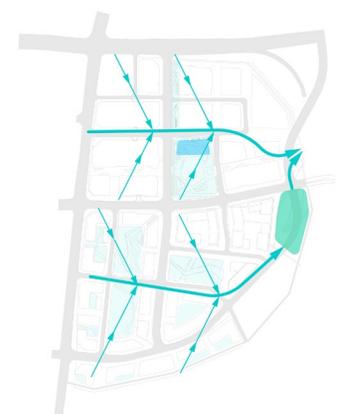
**Streetscape Character**



**Public Realm Focal Points**



**Storm Water System**



- 01 Colossus Park
- 02 Pedestrian Mews
- 03 Central Park West
- 04 Central Park East
- 05 Storm Water Management Area
- 06 MTO setback / Multi-use Trail
- 07 Famous Avenue Promenade



Figure 10. Landscape Demonstration Plan

## 6.2 PARKS AND OPEN SPACES

The following guidelines should be considered to reinforce the role parks and open spaces play in supporting a complete community by providing amenity, enhancing identity and providing a connection to nature.

### **GUIDELINE 51**

Where public parks are provided, they should be accessible by public streets and well framed by building frontages.

### **GUIDELINE 52**

Consideration should be given to the phasing of parks along with development to ensure public amenity is provided as the population grows.

### **GUIDELINE 53**

The design and programming of the public parks should reflect the land use and character of the new development and the adjacent context.

### **GUIDELINE 54**

A variety of programming should be explored in the public parks to accommodate different user groups throughout the day and seasons.

### **GUIDELINE 55**

The existing MTO setback may be transformed into a Multi Use Trail that can enhance future residents' access to open space and recreational opportunities

### **GUIDELINE 56**

Beyond parkland dedication, development is encouraged to include additional open spaces in various forms, including courtyards, Privately Owned Publicly-Accessible Space (POPS) , hardscape plazas or passive landscaped areas where feasible. Clear entrances, access and circulation from the public ROW and consideration for signage, softscape and hardscape materiality should be employed.

### **GUIDELINE 57**

POPS spaces should consider the building edges and respond to at-grade architectural materials, entrances and exits.

### **GUIDELINE 58**

New pedestrian and cycling trails around the stormwater pond should be explored with the City to contribute to the public realm network.

### **GUIDELINE 59**

Parks and open spaces will be provided over time, and the full realization of Central Park East as envisioned will require redevelopment to occur on both the RioCan Colossus Site and the Costco Site.



Figure 11. Parks and Open Space



Grange Park, Toronto



Distillery District, Toronto



Outdoor seating area



Children's play area within a park

## COLOSSUS PARK

### GUIDELINE 60

The public park in the Northern Precinct should be designed and programmed to be an all-season destination and consider elements such as an urban plaza with opportunities to support day to day and larger programmatic capacity.

### GUIDELINE 61

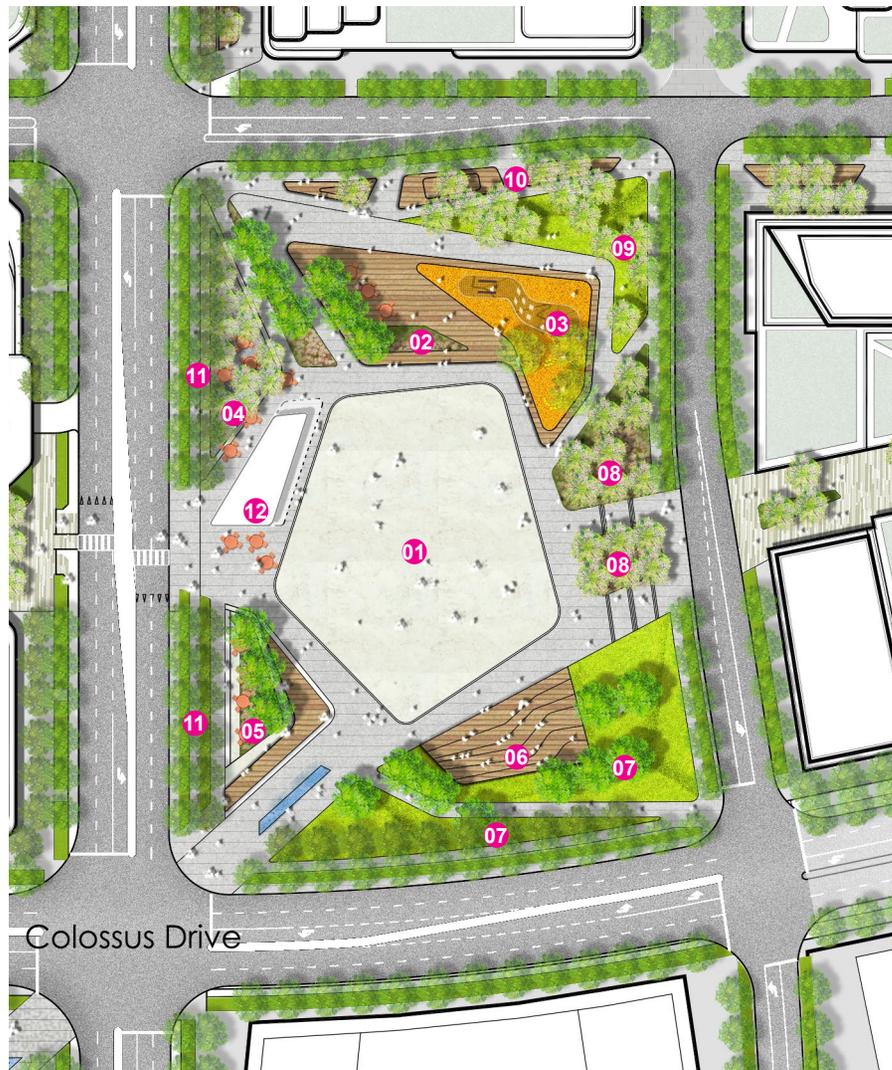
The Colossus Park should be scaled appropriately to accommodate a range of programs from intimate gatherings to larger events such as film screenings and pop-up markets.

### GUIDELINE 62

A modest children's play area should be considered in Colossus Park. The play area should be set back from the street and be internalized adjacent to the active uses such as central plaza and pavilion facilities.

### GUIDELINE 63

The use of expansive fencing should be minimized and replaced with landscape planting and site furnishings to contain edges where necessary.



### Conceptual Program for Colossus Park

- 01 Central Plaza/Ice Rink
- 02 Elevated Platform
- 03 Children's Play North
- 04 Urban Court
- 05 Planted Terrace Seating
- 06 Amphitheatre
- 07 Landform
- 08 Urban Grove
- 09 Orchard Garden
- 10 Wooden Seating Plinth
- 11 High Street Promenade
- 12 Pavilion

Figure 12. Colossus Plaza Demonstration Plan



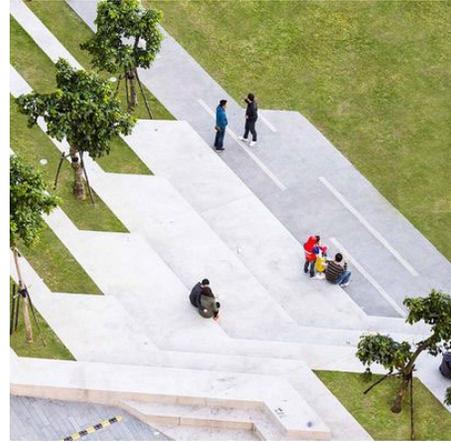
**URBAN EDGE**



**WATER FEATURE**



**AMPHITHEATER SEATING**



**URBAN PLAZA**



**CHILDREN'S PLAY**



**LANDFORM**



*Precedent imagery of possible programmatic elements in Colossus Park*

## CENTRAL PARK WEST

### GUIDELINE 64

The public parks in the Southwestern Precinct should be designed with verdant green space and a strong neighborhood character.

### GUIDELINE 65

The public parks in the Southwestern Precinct should consider an expansive open lawn located to take advantage of the sun aspect while scaled appropriately to support flexible recreational and passive activities.

### GUIDELINE 66

Central Park West may include a larger Children's Play area that is generously set back from streets and designed with sense of playfulness.



### Conceptual Program for Central Park West

- 01 Water Plinth
- 02 Splash Pad
- 03 Wood Bench
- 04 Children's Play - 2-5 yrs
- 05 Children's Play - 6-12 yrs
- 06 Grove
- 07 Spillout Cafe
- 08 Outdoor Patio
- 09 Park Forecourt
- 10 Landform
- 11 The Great Lawn
- 12 Wooden Platform

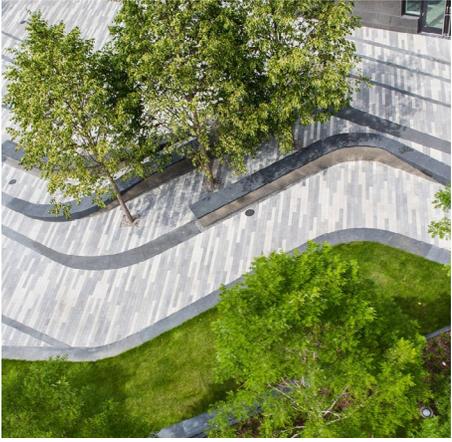
Figure 13. Central Park South Demonstration Plan



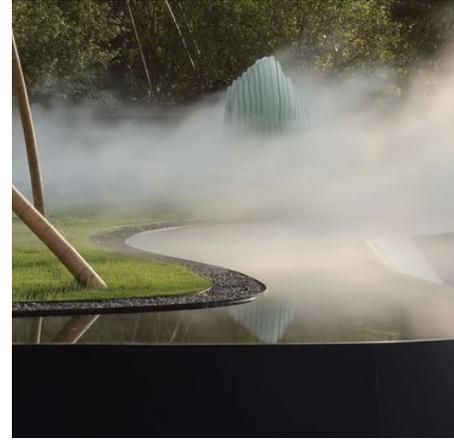
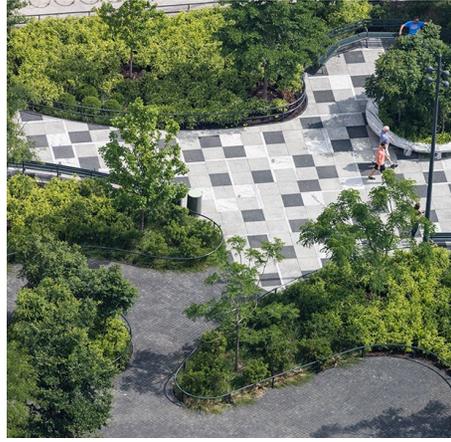
**GREAT LAWN**



**URBAN GROVE**



**GARDEN EDGES**



**WATER FEATURE**



**CHILDREN'S PLAY**



**LANDFORM**



*Precedent imagery of possible programing elements in the Central Park West*

## CENTRAL PARK EAST

### GUIDELINE 67

Central Park East should be developed in collaboration with any future mixed use redevelopment that is contemplated on the Costco lands, considering built form, connections and program. Delivery of Central Park East will be predicated on development occurring on the RioCan Colossus Site and the Costco Site in order to create a seamless park experience.

### GUIDELINE 68

Central Park East should be a passive space for day-to-day use, creating opportunities for seating and meandering pathways through landscape. The park should be complimentary to the broader open space network.

### GUIDELINE 69

Central Park East should respond to the site edges and provide pedestrian connections aligned with adjacent buildings entrances and courtyards. A landscaped forecourt should be considered at the western edge to create a seamless transition into Central Park East.



### Conceptual Program for Central Park East

- 01 Promenade
- 02 Pedestrian Connection
- 03 Landform
- 04 Outdoor Cafe
- 05 Wood Bench
- 06 Feature Seating

Figure 14. Central Park East Demonstration Plan



Pedestrian walkway



Landscaping elements in park



Public seating areas integrated with landscaping



Connected pedestrian walkway in park

## STORM WATER MANAGEMENT AREA

### GUIDELINE 70

The Storm Water Management Area is a City-owned pond and landscaped area intended to collect stormwater from the Development Quadrant which should be considered for naturalization and public access.

### GUIDELINE 71

The storm water management pond area should be considered for use as a passive open space area which includes educational opportunities for visitors to learn about stormwater infrastructure, landscape planting and ecology. Pedestrian pathways, seating and viewing opportunities should be considered.

### GUIDELINE 72

Landscape planting should consider the native plant species that will support the stormwater pond but also provide opportunities for pollinator species and habitat. The planting palette should consider the site conditions, low maintenance and robust species that are layered to create a unique destination within the Development Quadrant.

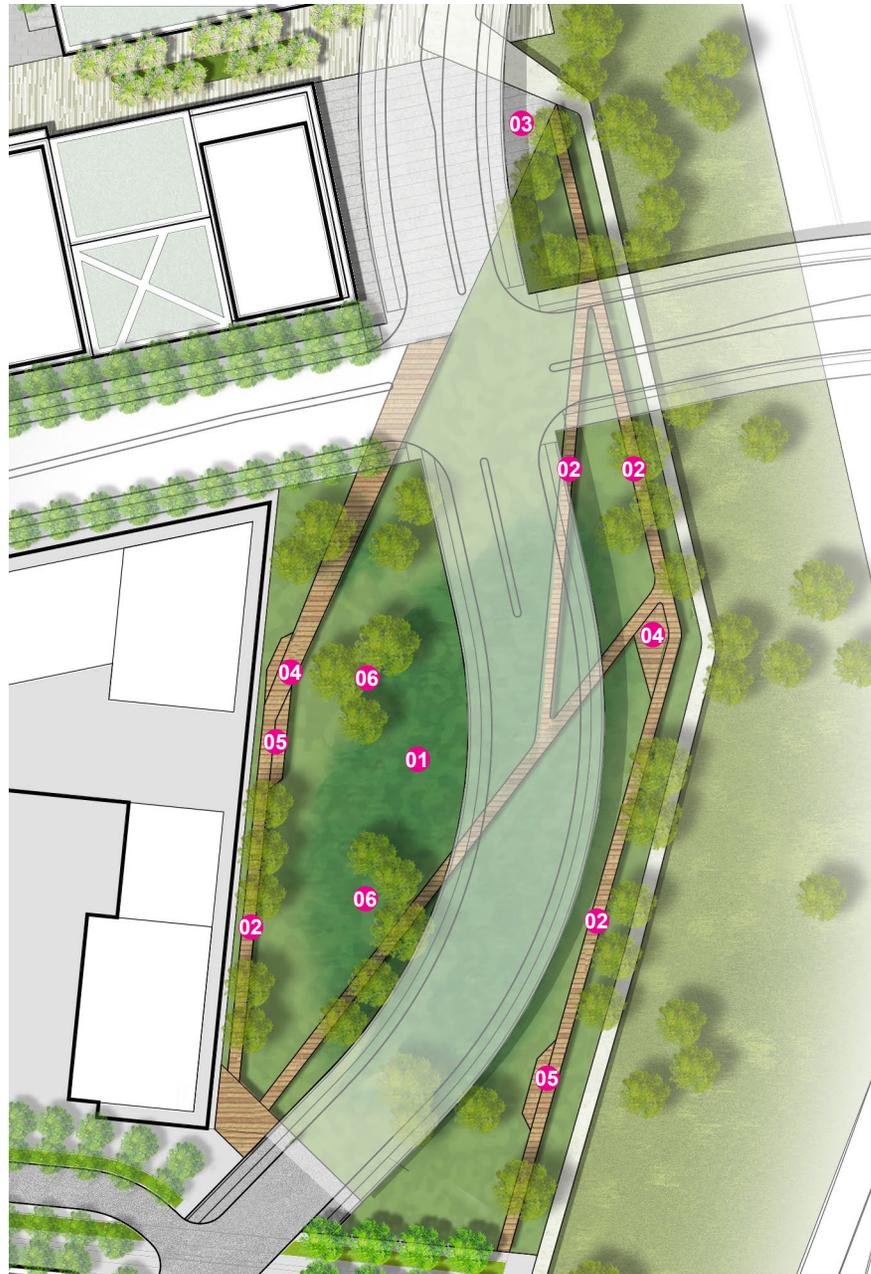


Figure 15. Storm Water Management Area



Landscape surrounding stormwater pond

### Conceptual Program for Storm Water Management Area

- 01 Storm Water Pond
- 02 Boardwalk
- 03 Underpass Plaza
- 04 Viewing Platform
- 05 Wood Bench
- 06 Wetland Planting

## FAMOUS AVE PROMENADE

### GUIDELINE 73

Between high order transit facilities and major open space destinations, a generous building setback shall be explored along Famous Avenue to provide an enhanced streetscaping zone.

### GUIDELINE 74

The Famous Avenue Promenade should be animated and respond to the active uses on the ground floor of surrounding development.

### GUIDELINE 75

A double row of trees should be considered as an integrated part of the streetscape design. Trees should be high canopy and provide adequate microclimate and shade for comfort throughout the season. Tree species should be selected in accordance with the City of Vaughan's guidelines.

### GUIDELINE 76

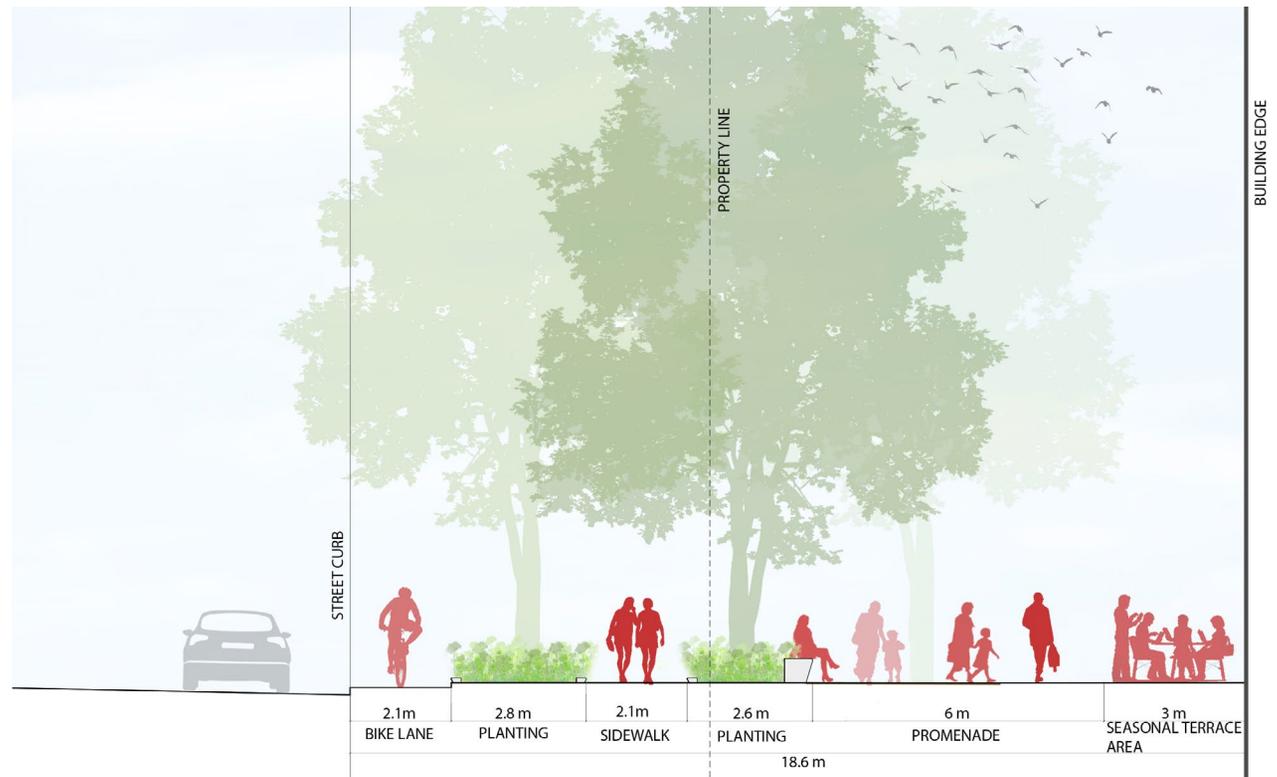
A hierarchy of pedestrian circulation within the Famous Avenue Promenade should be reinforced through the placement of trees, landscape planting, lighting, site furnishings and high quality paving treatments.



*Promenade integrated with active at-grade uses*



*Promenade with landscaping elements*



*Figure 16. Demonstration Section for the Famous Ave Promenade*

## MTO SETBACK / MULTI-USE TRAIL

### GUIDELINE 77

The 14-metre privately-owned MTO setback along the eastern and southern edge of the Development Quadrant may be realized as a multi use path. Intervention within the setback area should be explored to enhance connectivity along the perimeter of the Development Quadrant by facilitating shared pedestrian and cycling circulation paths with informal landscape planting .



*Multi-use trail through park*



*Recreational path in park*



*Multiple trail connections*



*Trail integrated with landscaping elements*

## 6.3 STREETScape & PEDESTRIAN CONNECTIONS

A comprehensive network of pedestrian-friendly linkages creates strong connections to the various Precincts, open spaces and destinations in the Development Quadrant.

### GUIDELINE 78

The pedestrian realm, including landscape, street furniture and paving should be designed to appropriately respond to the street type and surrounding land uses, accommodating desired activity and mitigating the negative impact from traffic and noise from major vehicular movement corridors.

### GUIDELINE 79

The pedestrian network should create opportunities to form various pedestrian loops throughout the Development Quadrant. Distinct theme, character or way-finding strategies may be incorporated in each of the loops to help strengthening the culture and identity of these loops.



Figure 17. Streetscape and Pedestrian Connections

## FAMOUS AVENUE

### GUIDELINE 80

Famous Avenue should be designed as a strong north-south pedestrian spine connecting the Northern, Southwestern and Southeastern Precincts. The public realm on the east and west side of Famous Avenue should have a similar character to create a cohesive boulevard.

### GUIDELINE 81

The pedestrian realm along Famous Avenue should be animated and respond to the active retail, commercial uses and entrance lobbies on the ground floor of the adjacent buildings. Buildings may be strategically set back at appropriate moments along Famous Avenue to support café and restaurant spill outs which will strengthen streetscape animation.

### GUIDELINE 82

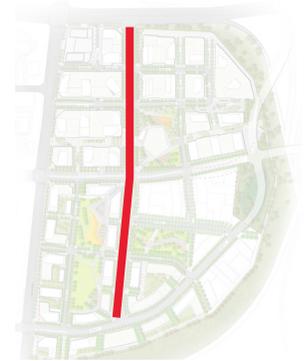
Tree canopies should be provided along the entire length of Famous Avenue. The public realm should include high quality hardscape materials, lighting, and streetscape furnishings subject to City of Vaughan approval.



*Streetscape with generous tree canopy*



*Spill-out space enlivens the streetscape*



*Figure 18. Conceptual section for the pedestrian realm of Famous Ave*

## COLOSSUS DRIVE

### GUIDELINE 83

Colossus Drive should create a strong east-west pedestrian spine which will be the transition streetscape for the Northern, Southwestern and Southeastern Precincts. Clear sight lines from the Pedestrian Mews (west) to Street G should be considered to create strong pedestrian connections to the Southwestern Precinct. A landscape planting strip between the bike lane and sidewalk should be considered with appropriate spacing of breaks to support connections. The public realm should include high quality hardscape materials, lighting and streetscape furnishings subject to the City of Vaughan approval.



*High-quality hardscape materials and tree canopy*



## STREET F

### GUIDELINE 84

Along the southern and eastern edge of the Development Quadrant, Street F is one of the primary roads connecting to Weston Road and Highway 7. The streetscape should include consistent tree planting to create a consistent canopy edge along the street. Landscape planting should provide a buffer for the bike lanes and break at appropriate areas to connect pedestrians and cyclists at significant connection points and building entrances. Landscape planting and trees should not be considered along the ramped portion of Street F.



*Landscape planting provides a buffer for bike lanes*



## WESTON ROAD AND HIGHWAY 7 FRONTAGES

### GUIDELINE 85

An enhanced 3-metre setback along Weston Road should be considered with a single row of trees. The trees should be spaced appropriately along the sidewalk to create a consistent canopied edge while breaking at moments to emphasize connection points such as the Pedestrian Mews.

### GUIDELINE 86

The pedestrian realm along the Highway 7 frontage is unique from the Weston Road to the eastern edge of the Development Quadrant. West of Famous Avenue the Highway 7 frontage should incorporate a generous sidewalk and a single row of trees. East of Famous Avenue along Highway 7 are the MTO buffers lands which will create a more naturalized planting condition.

## NEIGHBORHOOD STREETS

### GUIDELINE 87

The Neighborhood Streets are modestly scaled streetscapes typically found within the neighborhood blocks. These should be framed by a landscape planting edge against the curb with a single row of trees spaced according to the City of Vaughan standards. Where applicable, breaks within the planting strips should accommodate important connections including building, parking and services entrances, and pedestrian connections.



*Generous setback permits planting and street furniture*



*Planting along the edges of quiet streets creates an intimate canopy condition*



## PEDESTRIAN MEWS

### GUIDELINE 88

A pedestrian mews should be provided as an east-west connection from Weston Road through the Colossus Park to the east, terminating at the MTO setback/multi-use trail. The pedestrian mews should be expressed as a curb-less connection which will on occasion accommodate service vehicles.

### GUIDELINE 89

Vertical elements including raised planters, trees, lighting and site furnishings should be appropriately positioned to not interfere with service vehicle access and circulation into the buildings.

### GUIDELINE 90

Trees should be positioned in locations that do not obstruct views for service vehicles and pedestrians.

### GUIDELINE 91

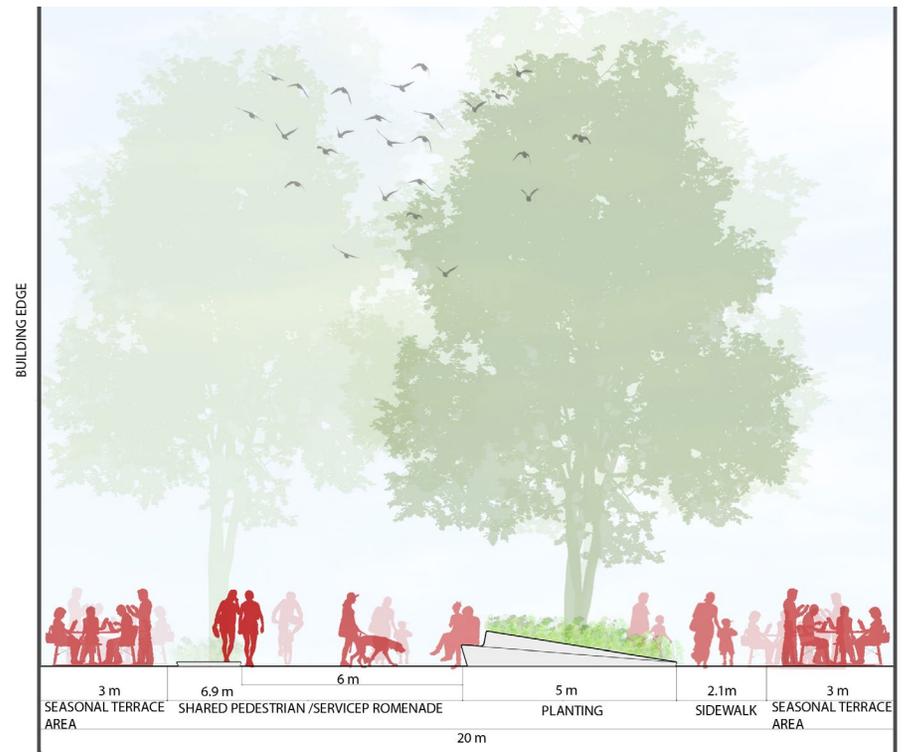
The hardscape surface treatment of the paving within the pedestrian mews should safely identify service vehicular lanes through a number of approaches including textures and banding. Streetscape furnishings and planter materials can also be used to reinforce this delineation.



*Plantings provide shade for pedestrian activity*



*High-quality street furniture invites social activity*



*Figure 19. Conceptual section for the pedestrian mews*

## PEDESTRIAN CONNECTIONS

### GUIDELINE 92

Pedestrian connections provide strong linkages between the open spaces, streets and built form. These connections should consider clear sightlines for legibility and provide focal points to add interest to the pedestrian experience.

### GUIDELINE 93

The scale of the pedestrian connections will vary and should consider the adjacent edges and ground floor programming. In some cases, the pedestrian connections may need to be widened to accommodate café spillout spaces next to food and beverage retailers.

### GUIDELINE 94

Pedestrian connections that are anticipated to support significant pedestrian movement should consider the use of high-quality paving materials, site furnishings and planting to help identify and strengthen linkages.



*Pedestrian connection with clear sightlines*



*Street furnishings provide clear visual pathway*

# 6.4 PUBLIC REALM FOCAL POINTS

Public realm focal points are key places in the public realm that provide anchors and orientation for pedestrians through the Development Quadrant.

## GUIDELINE 95

Public realm focal points should be strategically positioned and considered for key entrances to the Development Quadrant, as connections between major open spaces and the terminus of a view shed.

## GUIDELINE 96

The landscape and architectural design at the public realm focal points should provide identifiable markers that strengthen view sheds along major pedestrian connectors, open spaces and Precincts and cue the entrances into the Development Quadrant.

## GUIDELINE 97

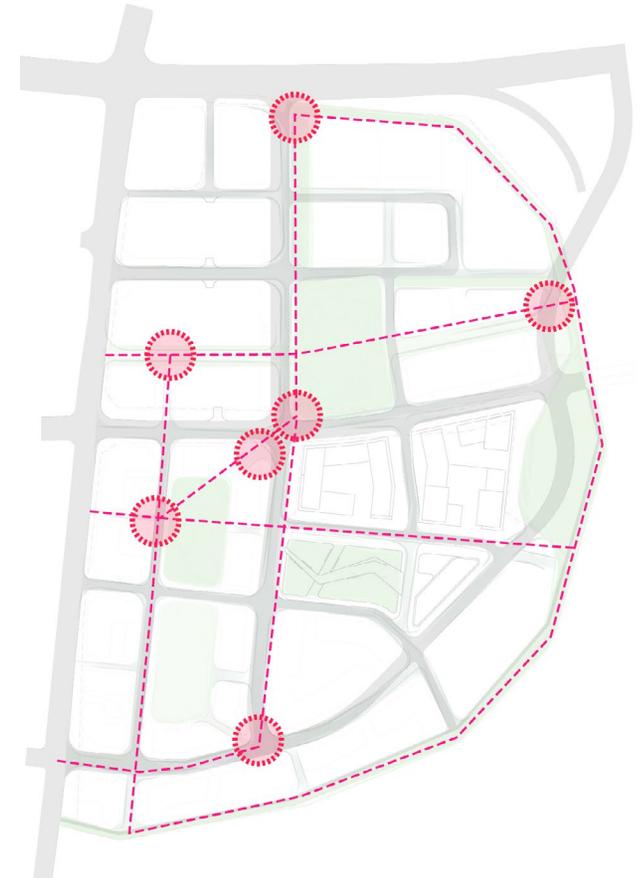
Public realm focal points should be considered as priority locations for public art in the Development Quadrant.



*Lighting enhances prominence of public art*



*Interactive sculpture in open space*



*Figure 20. Potential locations for public realm focal points*

# 6.5 STORM WATER

A comprehensive approach to stormwater management takes advantage of the existing topography. Stormwater will be collected to the centre of the Development Quadrant from the Northern, Southwestern and Southeastern Precincts and conveyed to the stormwater pond to the east.

## GUIDELINE 98

New development should explore a stormwater management system that is well integrated with the open space network.

## GUIDELINE 99

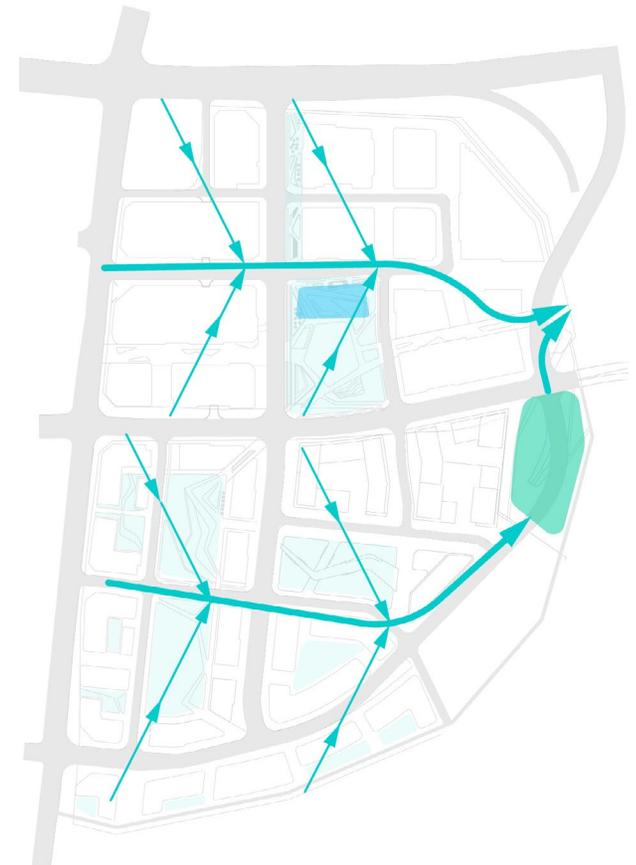
Wherever possible, the design of the storm water management system should incorporate place-making, ecological and public education opportunities.



*Stormwater integrated in landscaping elements*



*Stormwater pond has prominence along a pedestrian path*



*Figure 21. Conceptual Storm Water System*



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# 7. BUILT FORM GUIDELINES

# 7.1 INTRODUCTION

The Master Plan illustrates how to transform the Development Quadrant into a high density mixed use complete community that has strong urban identity, unique places, diverse and pedestrian-oriented built environment.

The following section organizes the built form guidelines around three principles:

- Create a unique built form identity
- Incorporate diverse and environmentally sensitive design at the block level
- Prioritize the pedestrian experience



Figure 22. Conceptual rendering of built form framing the street

## 7.2 UNIQUE BUILT FORM IDENTITY

The Development Quadrant is in a unique location, highly visible at the intersection of two provincial highways.

The Master Plan envisions a high density mixed use community with a unique built form identity. The following guidelines address how future development may achieve this vision.

### GUIDELINE 100

In general the tallest building heights are expected in the Northern Precinct and along the Highway 400 edge of the Development Quadrant.

### GUIDELINE 101

Shadow impacts on public parks from tall buildings should be mitigated where possible through careful building massing and articulation.

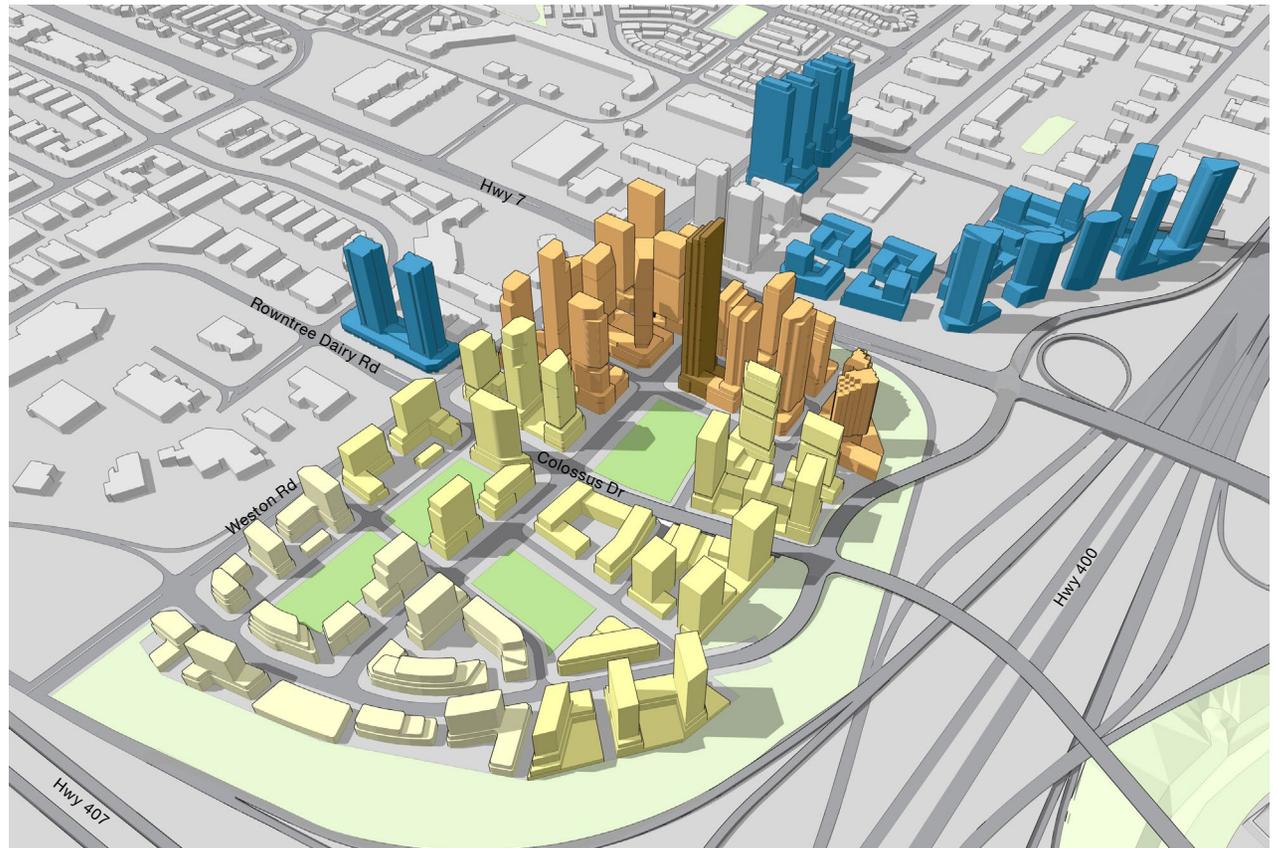


Figure 23. Conceptual Height Map

- Maximum Height 25 ST
- Maximum Height 40 ST
- Maximum Height 55 ST
- Landmark Development over 55 ST
- Proposed Development

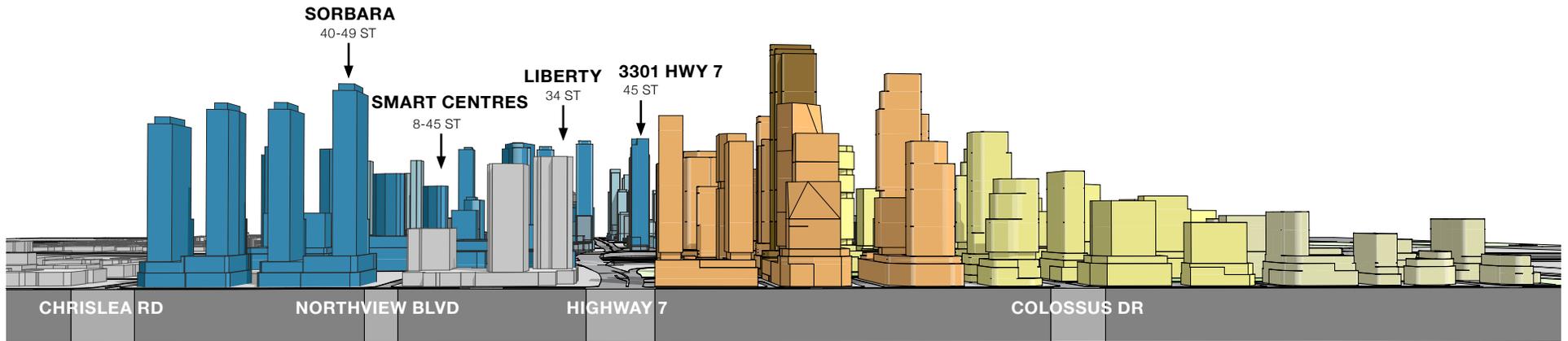


Figure 24. Section Perspective Across Weston Rd Looking East

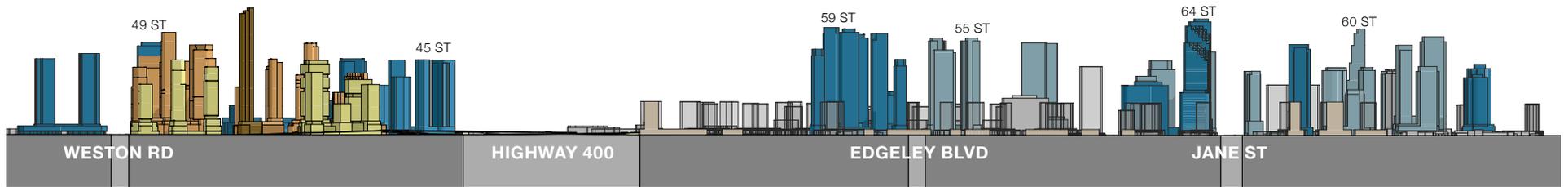


Figure 25. Section Perspective Colossus Drive Looking North

The section perspectives above demonstrate how a varied and distinct skyline can be achieved.

- Maximum Height 25 ST
- Maximum Height 40 ST
- Maximum Height 55 ST
- Landmark Development over 55 ST
- Proposed Development

## GUIDELINE 102

A diversity of tall building heights should be encouraged to create an interesting, dynamic and varied skyline.

## GUIDELINE 103

Consideration should be given for the height peak of the Northern Precinct to occur immediately north of Colossus Park, providing a landmark that denotes it a regional destination.

## GUIDELINE 104

Major vehicular and pedestrian entrances to the Development Quadrant shall be framed and distinguished with iconic architecture to reinforce a sense of arrival.

## GUIDELINE 105

Development at key entrances to the Development Quadrant should consider including site plan elements that distinguish the entrance and support a more comfortable pedestrian environment, such as wider sidewalks, way-finding, street furniture, landscaping and public art.



*Unique architecture can distinguish key points of interest*



*Buildings framing pedestrian walkway*



*Prominent entrance to pedestrian connection*

# 7.3 A MIX OF BUILDING TYPOLOGIES WITHIN BLOCKS

Built form coordination at the block level will be critical in creating desirable urban environments in the Development Quadrant. A creative yet sensitive mix of built form within urban blocks shall be explored in the Development Quadrant. The following guidelines provide direction for how to successfully accommodate a mix of building typologies with a block. Special considerations should be given to the relationship between low, mid and high-rise buildings.

## GUIDELINE 106

New development within the Development Quadrant should explore the opportunity to integrate various building heights and typologies at the block level to support a diverse built form including mid-rise and high-rise elements on the same block.

## GUIDELINE 107

Podium base buildings will be designed to reinforce a pedestrian-scaled environment.

## GUIDELINE 108

Porosity and gaps between buildings should be incorporated to preserve sky views, improve daylight access and to enhance natural ventilation.

## GUIDELINE 109

Mid-block pedestrian connection should be considered where feasible in large blocks to support a fine-grained pedestrian network.

## GUIDELINE 110

Wherever possible, the design of the blocks shall explore opportunity for internal landscaped courtyard or green rooftop amenity spaces.



*Diverse built form with pedestrian-oriented design*



Figure 26. Rendering of Colossus Park

# 7.4 PRIORITIZE THE PEDESTRIAN EXPERIENCE

A positive pedestrian experience is essential to a transit-oriented high density community. Development within the Development Quadrant should demonstrate design excellence with high-quality architecture and urban design that contributes to a pedestrian-oriented public realm that is vibrant, dynamic and comfortable.

## GUIDELINE 111

Buildings should be sited to frame and define the pedestrian realm. Base buildings should be sited close to the street-front property line to form a pedestrian-scaled streetwall, while providing adequate setbacks to support pedestrian movement and the development of a mature tree canopy.

## GUIDELINE 112

More detailed architectural expression and variation should be incorporated in the base building to break down the bulk of a building, provide pedestrian scaled frontages and lend visual interest at the pedestrian scale.

## GUIDELINE 113

Key public realm areas should be animated with active ground level uses that may include a mix of small, medium or large retail frontages, ground level residential units, articulated lobby entrances and community amenities.

## GUIDELINE 114

Development should provide a horizontally and vertically varied streetscape through the use of projections or recessions in the facade or setbacks.

## GUIDELINE 115

The incorporation of mid-block connections, forecourts and increased building setbacks should be encouraged to diversify the pedestrian experience.

## GUIDELINE 116

Weather protection elements such as awnings may be considered at the ground level along key pedestrian routes and at building entrances.

## GUIDELINE 117

Varied building materials and architectural expression is encouraged throughout the Development Quadrant to provide a dynamic pedestrian experience.

## GUIDELINE 118

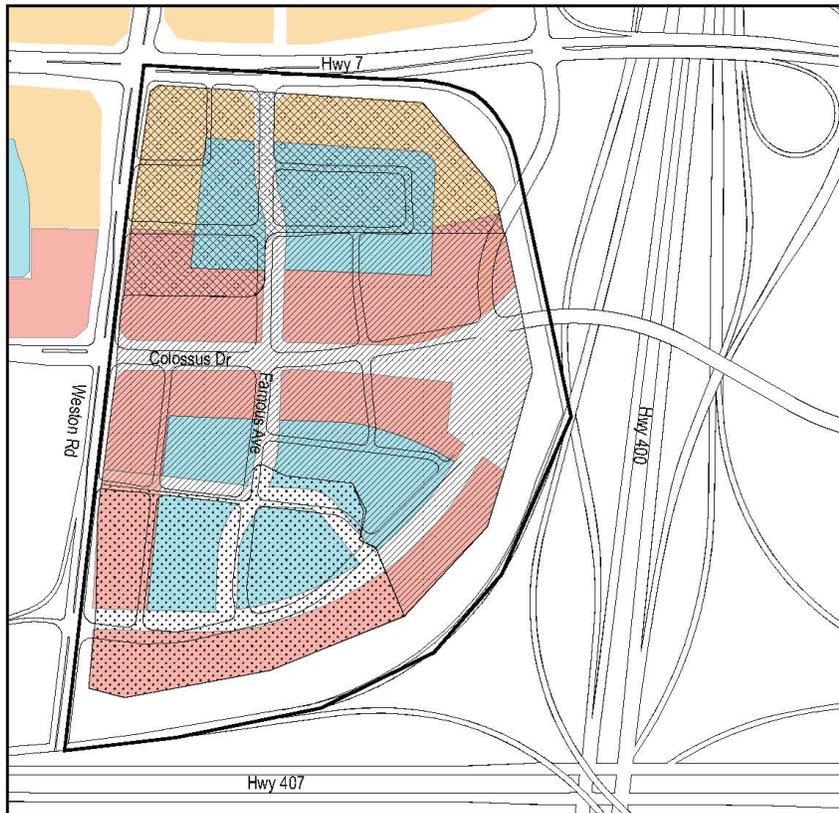
Parking entrances and loading areas should be discretely integrated within the development blocks. Vehicular entrances to new development should be designed to be as integrated as possible by minimizing curb cuts and interruption to the pedestrian realm. Surface parking should generally be discouraged and allocated internal to the development blocks.



Figure 27. Rendering of Pedestrian Mews

**URBAN  
STRATEGIES  
INC .**

## Appendix C - 1



7501 - 7621 Weston Road

### Building Height Comparison

▭ Subject Lands

- RioCan's Proposal**
- ▨ Maximum Height 25 ST
  - ▨ Maximum Height 40 ST
  - ▨ Maximum Height 55 ST

- Weston 7's Proposal**
- Low-Rise - up to 3 storeys
  - Mid-Rise - up to 8 storeys
  - High-Rise I - up to 18 storeys
  - High-Rise II - 19+ storeys



Not to Scale

## Appendix C - 2



7501 - 7621 Weston Road

### Parks and Public Realm Comparison

▭ Subject Lands

#### RioCan's Park System

- Park
- Green Corridor

#### Weston 7's Park System

- Urban Squares
- Promenades
- Enhanced Urban Streetscape
- Existing Stormwater Management Pond



Not to Scale