

	COMMITTEE OF ADJUSTMENT REPORT SUMMARY MINOR VARIANCE APPLICATION FILE NUMBER A063/22
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AGENDA ITEM NUMBER: 22	CITY WARD #: 1
APPLICANT:	2019625 Ontario Inc.
AGENT:	Wes Surdyka - Wes Surdyka Architect Inc.
PROPERTY:	233 Sweetriver Blvd Bldg C, Maple
ZONING DESIGNATION:	See below.
VAUGHAN OFFICIAL PLAN (2010) DESIGNATION:	Vaughan Official Plan 2010 ('VOP2010'): Mid-Rise Mixed-Use
RELATED DEVELOPMENT APPLICATIONS: <small>*May include related applications for minor variance, consent, site plan, zoning amendments etc.</small>	None
PURPOSE OF APPLICATION:	Relief from the Zoning By-law is being requested to permit proposed additions to the existing car dealerships on the subject land and to permit that waste storage not be wholly located within a building.

THIS REPORT CONTAINS COMMENTS FROM THE FOLLOWING DEPARTMENTS & AGENCIES:

*Please see **Schedule B** of this report for a copy of Development Planning and Agency correspondence.

Additional comments from departments and agencies may be received after the publication of the Staff Report. These comments will be processed as an addendum and posted on the City’s Website.

DEPARTMENTS	Circulated	Comments Received	Conditions	Nature of Comments
Committee of Adjustment	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Building Standards (Zoning Review)	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Building Inspection (Septic)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Development Planning	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	Recommend Adjournment
Development Engineering	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	Recommend Approval w/Conditions
Parks, Forestry and Horticulture Operations	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	No Comments no concerns
By-law & Compliance, Licensing & Permits	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Development Finance	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	No Comments no concerns
Real Estate	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Fire Department	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
AGENCIES	Circulated	Comments Received	Conditions	Nature of Comments
TRCA	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	Recommend Approval w/Conditions
Ministry of Transportation (MTO)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Region of York	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Recommend Approval/No Conditions
Alectra	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Recommend Approval/No Conditions
Bell Canada	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	No Comments no concerns
YRDSB	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
YCDSB	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
CN Rail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
CP Rail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
TransCanada Pipeline	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Metrolinx	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Propane Operator	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	

PUBLIC & APPLICANT CORRESPONDENCE				
*Please see Schedule C of this report for a copy of the public & applicant correspondence listed below.				
The deadline to submit public comments is noon on the last business day prior to the scheduled hearing date.				
Comments and written public submissions received after the publication of this Staff Report will be processed as an addendum and posted on the City’s Website.				
All personal information collected because of this public meeting (including both written and oral submissions) is collected under the authority of the Municipal Act, the Municipal Freedom of Information and Protection of Privacy Act (MFIPPA), the Planning Act and all other relevant legislation, and will be used to assist in deciding on this matter. All personal information (as defined by MFIPPA), including (but not limited to) names, addresses, opinions and comments collected will become property of the City of Vaughan, will be made available for public disclosure (including being posted on the internet) and will be used to assist the Committee of Adjustment and staff to process this application.				
Correspondence Type	Name	Address	Date Received (mm/dd/yyyy)	Summary
None.				

PREVIOUS COA DECISIONS ON THE SUBJECT LAND		
*Please see Schedule D for a copy of the Decisions listed below		
File Number	Date of Decision MM/DD/YYYY	Decision Outcome
None		

ADJOURNMENT HISTORY	
* Previous hearing dates where this application was adjourned by the Committee and public notice issued.	
None	



**COMMITTEE OF ADJUSTMENT REPORT
MINOR VARIANCE APPLICATION
A063/22**

FILE MANAGER: Lenore Providence, Administrative Coordinator - Committee of Adjustment

AGENDA ITEM NUMBER: 22	CITY WARD #: 1
APPLICANT:	2019625 Ontario Inc.
AGENT:	Wes Surdyka - Wes Surdyka Architect Inc.
PROPERTY:	233 Sweetriver Blvd Bldg C, Maple
ZONING DESIGNATION:	See below.
VAUGHAN OFFICIAL PLAN (2010) DESIGNATION:	Vaughan Official Plan 2010 ('VOP2010'): Mid-Rise Mixed-Use
RELATED DEVELOPMENT APPLICATIONS: <small>*May include related applications for minor variance, consent, site plan, zoning amendments etc.</small>	None
PURPOSE OF APPLICATION:	Relief from the Zoning By-law is being requested to permit proposed additions to the existing car dealerships on the subject land and to permit that waste storage not be wholly located within a building.

The following variances have been requested from the City's Zoning By-law:

The subject lands are zoned GMU, General Mixed Use Zone and subject to the provisions of Exception 14.700 under Zoning By-law 001-2021.

#	Zoning By-law 001-2021	Variance requested
1	The proposed addition to Building C shall be located within the required 3.5 m to 7 m build-to-zone. [Table 8-3]	To permit a building not to be located within the required 3.5 m to 7 m build-to-zone for a proposed addition to Building C.
2	A minimum ground floor height of 4.5 metres is required for the proposed addition to Building C. [Table 8-3]	To permit a minimum ground floor height of 3.81 m for a proposed addition to Building C.
3	Waste storage shall be wholly located within a building, within a waste storage enclosure or within a private garage in the GMU zone. [4.24]	To permit waste storage not to be wholly located within a building, within a waste storage enclosure or within a private garage in the GMU zone.

The subject lands are zoned C1, Restricted Commercial and subject to the provisions of Exception 9(1031A) under Zoning By-law 1-88, as amended.

	Zoning By-law 1-88	Variance requested
4	A minimum rear yard setback of 15 metres is required to Building C. [Schedule A]	To permit a minimum rear yard setback of 9.04 metres to Building C.
5	A strip of land not less than 6.0 metres in width shall be provided along a lot line which abuts a street line, and shall be used for no other purpose than landscaping. [Subsections 3.13 & 5.1.1]	To permit a minimum landscape strip width of 4.46 metres abutting a street line.

HEARING INFORMATION

DATE & TIME OF HEARING: April 14, 2022 at 6:00 p.m.

As a result of COVID-19, Vaughan City Hall and all other City facilities are closed to the public at this time.

PUBLIC PARTICIPATION

You can watch a live stream of the hearing at [Vaughan.ca/LiveCouncil](https://vaughan.ca/LiveCouncil)

HEARING INFORMATION
<p>If you wish to speak to the Committee of Adjustment on an application please complete and submit a Public Deputation Form to cofa@vaughan.ca by noon on the last business day prior to the hearing.</p> <p>Any person who supports or opposes this application, but is unable to attend the hearing, may make a written submission, together with reasons for support or opposition. Written submissions on an Application must be submitted by noon on the last business day prior to the day of the scheduled hearing. Written submissions can be emailed to cofa@vaughan.ca</p> <p>For more information, please visit the City of Vaughan website.</p>

INTRODUCTION
<p>Staff and Agencies act as advisory bodies to the Committee of Adjustment. The comments contained in this report are presented as recommendations to the Committee.</p> <p>Section 45(1) of the Planning Act sets the criteria for authorizing minor variances to the City of Vaughan’s Zoning By-law. Accordingly, review of the application may consider the following:</p> <p>That the general intent and purpose of the by-law will be maintained. That the general intent and purpose of the official plan will be maintained. That the requested variance(s) is/are acceptable for the appropriate development of the subject lands. That the requested variance(s) is/are minor in nature.</p> <p>Public written and oral submissions relating to this application are taken into consideration by the Committee of Adjustment as part of its deliberations and final decision on this matter.</p>

COMMITTEE OF ADJUSTMENT COMMENTS	
Date Public Notice Mailed:	March 30, 2022
Date Applicant Confirmed Posting of Sign:	March 24, 2022
Applicant Justification for Variances: <small>*As provided by Applicant in Application Form</small>	<ol style="list-style-type: none">1) The rear setback is less than that the minimum required setback from By-law 1-88.2) The landscape strip abutting a street is less than the minimum required strip from By-Law 1-88.3) The proposed ground floor height is less than the required minimum ground floor height from By-law 1-2021
Adjournment Requests (from staff): <small>*Adjournment requests provided to applicant prior to issuance of public notice</small>	Planning has determined the application premature and have requested adjournment.
Was a Zoning Review Waiver (ZRW) Form submitted by Applicant: <small>*ZRW Form may be used by applicant in instances where a revised submission is made, and zoning staff do not have an opportunity to review and confirm variances prior to the issuance of public notice.</small> <small>*A revised submission may be required to address staff / agency comments received as part of the application review process.</small> <small>*Where a zoning review has not been completed on a revised submission, an opportunity is provided to the applicant to adjourn the proposal prior to the issuance of public notice.</small>	No
Adjournment Fees: In accordance with Procedural By-law 069-2019, an Adjournment Fee is applicable to reschedule an application after the issuance of public notice where a request for adjournment has been provided to the applicant prior to the issuance of public notice. An Adjournment Fee can only be waived in instances where adjournment of an application is requested by the Committee or staff after the issuance of public notice.	
Committee of Adjustment Comments:	None
Committee of Adjustment Recommended Conditions of Approval:	None

BUILDING STANDARDS (ZONING) COMMENTS
<p>Stop work orders and orders to comply: There are no outstanding Orders on file</p> <p>Building Permit(s) Issued: A Building Permit has not been issued. The Ontario Building Code requires a building permit for structures that exceed 10m2</p>

BUILDING STANDARDS (ZONING) COMMENTS	
The applicant shall be advised that additional variances may be required upon review of detailed drawing for building permit/site plan approval.	
The subject lands may be subject to Ontario Regulation 166/06 (TRCA - Toronto and Region Conservation Authority).	
The subject lands may be subject to the Oak Ridges Moraine Conservation Act, RSO 2001.	
Please note as of January 1, 2015 Accessible Parking Spaces are subject to the provisions of Ontario Regulation - O.Reg.413/12, Subsections 80.32 through 80.39, which supersede the current by-law requirements in By-law 1-88 as amended.	
Please note Section 4.1.1(b) that states the maximum height of an accessory building or structure measured from the average finished ground level to the highest point of the said building or structure shall be 4.5 metres. The nearest part of the roof shall not be more than three(3) metres above finished grade.	
An A/C unit and/or pool equipment shall be setback a minimum of 1.2 metres from the interior side lot line; and may encroach a maximum of 1.5 metres into the required rear yard or exterior side yard.	
Building Standards Recommended Conditions of Approval:	None

DEVELOPMENT PLANNING COMMENTS	
**See Schedule C for Development Planning Comments.	
Development Planning Recommended Conditions of Approval:	None

DEVELOPMENT ENGINEERING COMMENTS	
The Development Engineering (DE) Department does not object to variance application A063/22 subject to the following condition(s):	
Development Engineering Recommended Conditions of Approval:	The Owner/applicant shall obtain approval for the related Site Development Application (DA.21.042) from the Development Engineering (DE) Department.

PARKS, FORESTRY & HORTICULTURE (PFH) COMMENTS	
No comments received to date.	
PFH Recommended Conditions of Approval:	None

DEVELOPMENT FINANCE COMMENTS	
No comments received to date.	
Development Finance Recommended Conditions of Approval:	None

BY-LAW AND COMPLIANCE, LICENSING AND PERMIT SERVICES COMMENTS	
No comments received to date.	
BCLPS Recommended Conditions of Approval:	None

BUILDING INSPECTION (SEPTIC) COMMENTS	
No comments received to date.	
Building Inspection Recommended Conditions of Approval:	None

FIRE DEPARTMENT COMMENTS	
No comments received to date.	
Fire Department Recommended Conditions of Approval:	None

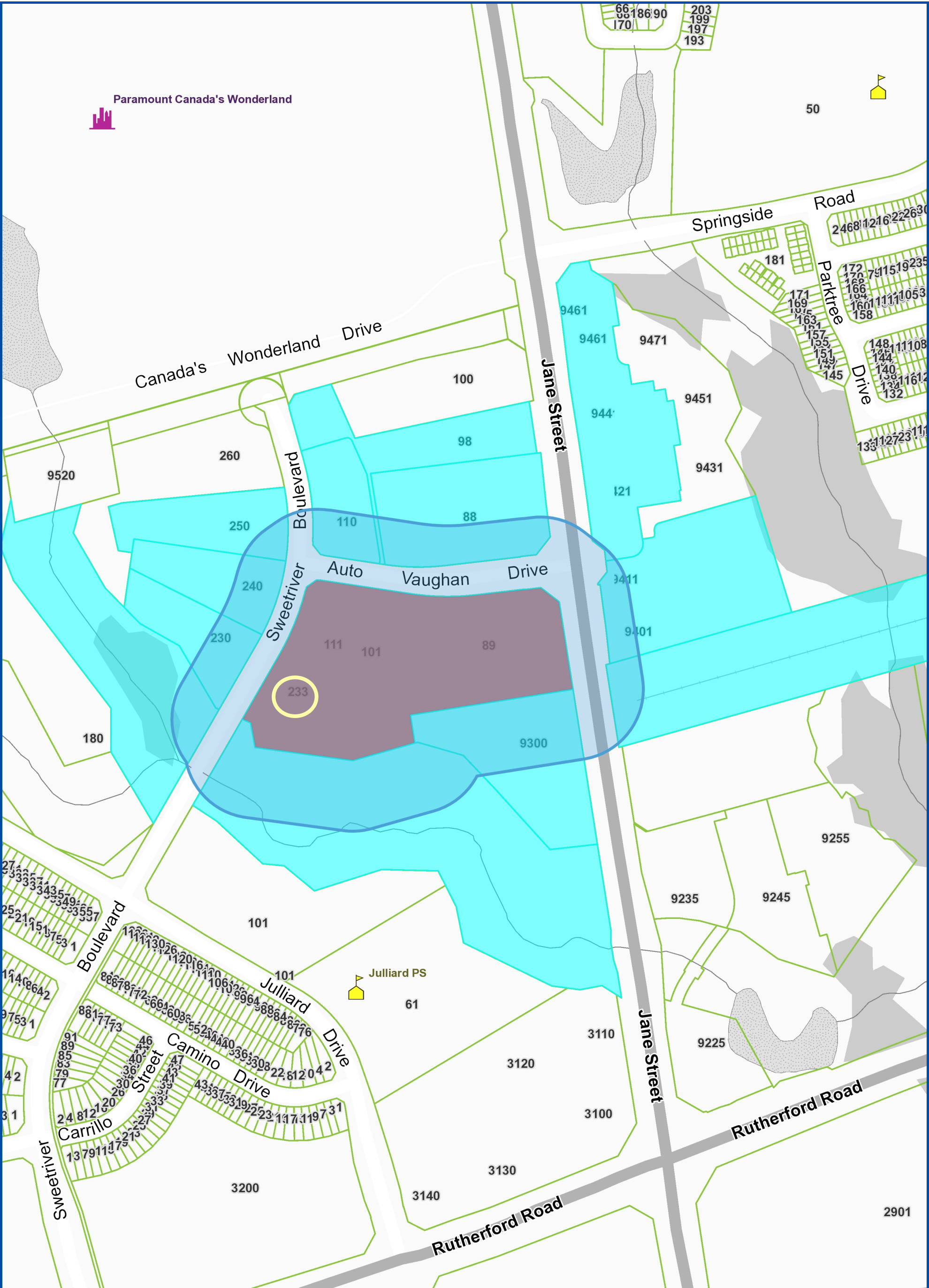
SCHEDULES TO STAFF REPORT	
*See Schedule for list of correspondence	
Schedule A	Drawings & Plans Submitted with the Application
Schedule B	Development Planning & Agency Comments
Schedule C (if required)	Correspondence (Received from Public & Applicant)
Schedule D (if required)	Previous COA Decisions on the Subject Land

Should the Committee find it appropriate to approve this application in accordance with request and the sketch submitted with the application, as required by Ontario Regulation 200/96, the following conditions have been recommended:

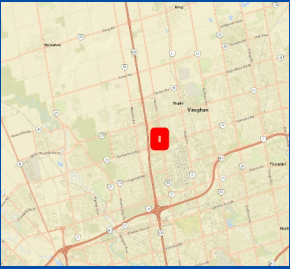
SUMMARY OF RECOMMENDED CONDITIONS OF APPROVAL		
All conditions of approval, unless otherwise stated, are considered to be incorporated into the approval “ if required ”. If a condition is no longer required after an approval is final and binding, the condition may be waived by the respective department or agency requesting conditional approval. A condition cannot be waived without written consent from the respective department or agency.		
#	DEPARTMENT / AGENCY	CONDITION(S) DESCRIPTION
1	Development Engineering farzana.khan@vaughan.ca	The Owner/applicant shall obtain approval for the related Site Development Application (DA.21.042) from the Development Engineering (DE) Department.
2	TRCA hamedeh.razavi@trca.ca mark.howard@trca.ca	The applicant submits the application fee of \$610 payable to the Toronto and Region Conservation Authority.

IMPORTANT INFORMATION – PLEASE READ
<p>CONDITIONS: It is the responsibility of the owner/applicant and/or authorized agent to obtain and provide a clearance letter from respective department and/or agency (see condition chart above for contact). This letter must be provided to the Secretary-Treasurer to be finalized. All conditions must be cleared prior to the issuance of a Building Permit.</p> <p>APPROVALS: Making any changes to your proposal after a decision has been made may impact the validity of the Committee’s decision.</p> <p>An approval obtained from the Committee of Adjustment, where applicable, is tied to the building envelope shown on the plans and drawings submitted with the application and subject to the variance approval.</p> <p>A building envelope is defined by the setbacks of the buildings and/or structures shown on the plans and drawings submitted with the application, as required by Ontario Regulation 200/96. Future development outside of an approved building envelope, where a minor variance was obtained, must comply with the provisions of the City’s Zoning By-law.</p> <p>Elevation drawings are provided to reflect the style of roof (i.e. flat, mansard, gable etc.) to which a building height variance has been applied. Where a height variance is approved, building height is applied to the style of roof (as defined in the City’s Zoning By-law) shown on the elevation plans submitted with the application.</p> <p>Architectural design features that are not regulated by the City’s Zoning By-law are not to be considered part of an approval unless specified in the Committee’s decision.</p> <p>DEVELOPMENT CHARGES: That the payment of the Regional Development Charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the Regional Development Charges By-law in effect at the time of payment.</p> <p>That the payment of the City Development Charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the City's Development Charges By-law in effect at the time of payment.</p> <p>That the payment of the Education Development Charge if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the Boards of Education By-laws in effect at the time of payment</p> <p>That the payment of Special Area Development charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and The City's Development Charge By-law in effect at the time of Building permit issuance to the satisfaction of the Reserves/Capital Department.</p> <p>NOTICE OF DECISION: If you wish to be notified of the decision in respect to this application or a related Ontario Land Tribunal (OLT) hearing you must complete a Request for Decision form and submit to the Secretary Treasurer (ask staff for details). In the absence of a written request to be notified of the Committee’s decision you will not receive notice.</p>

SCHEDULE A: DRAWINGS & PLANS



Map Information:



Title:

233 SWEETRIVER BOULEVARD, MAPLE

NOTIFICATION MAP - A063/22

Disclaimer:

Every reasonable effort has been made to ensure that the information appearing on this map is accurate and current. We believe the information to be reliable, however the City of Vaughan assumes no responsibility or liability due to errors or omissions. Please report any discrepancies to Infrastructure Programming.



Scale: 1: 4,514

0 0.07 km



Created By:
Infrastructure Delivery
Department
March 29, 2022 11:04 AM

Projection:
NAD 83
UTM Zone
17N

ZONING:

Current Zoning: C1(H) Restricted Commercial Zone

Subject to Specific Exception 9(1031A) by Zoning By-law 1-88

Comprehensive Zoning: GMU-700 - General Mixed-Use Zone

Subject to Exception Number 700 by Zoning By-law 001-2021

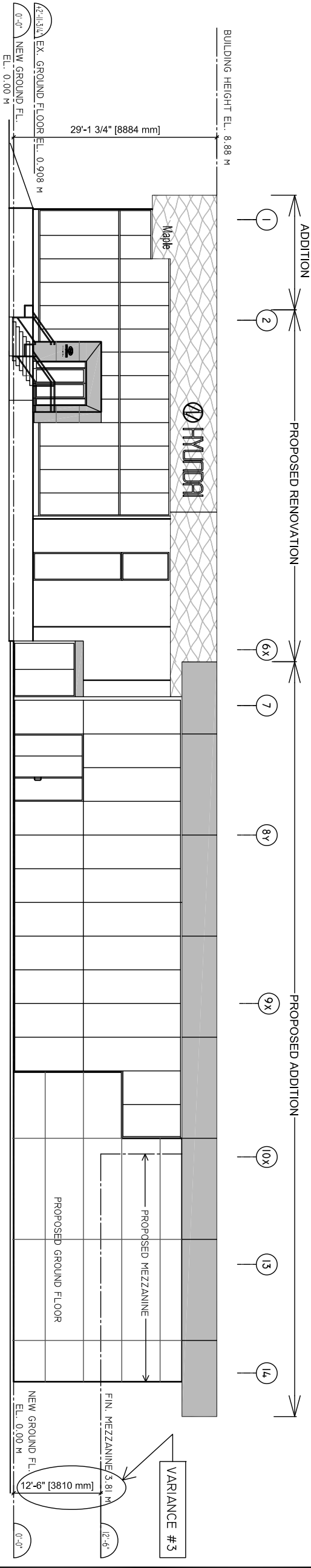
PROPOSED ADDITION & RENOVATION

TO EXISTING CAR DEALERSHIP

233 SWEETRIVER BLVD.

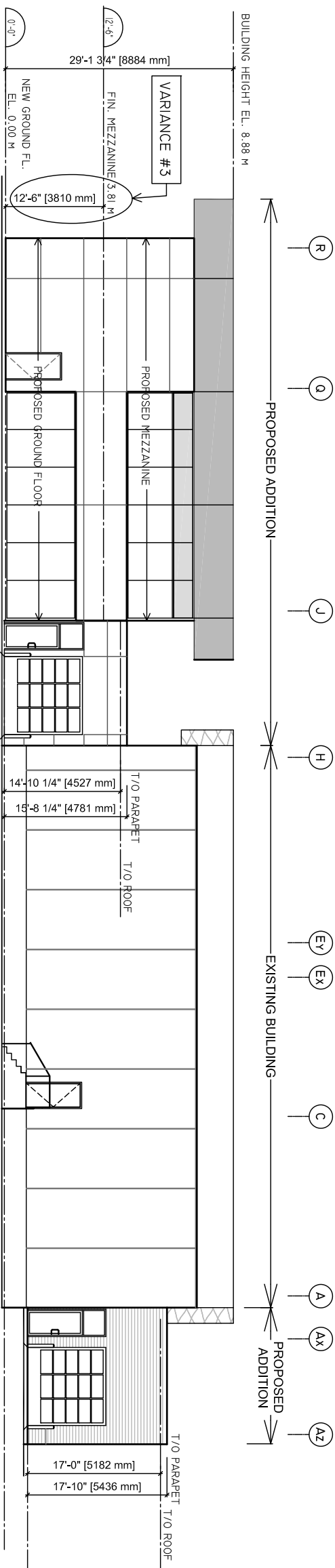
VAUGHAN, ONTARIO

REFERENCES: SP17-004, P.A.C.21.077, D.A.21.042



WEST ELEVATION

SCALE 1:200



SOUTH ELEVATION

SCALE 1:200

A063/22

Revised

March 16, 2022

DRAWING NO.

A3.0

WEB SURDYKA

architect inc

3645 KEELE STREET 2ND FLOOR, STE 108
TORONTO, ONTARIO M3J 1K6
TEL (416) 630-2254 FAX (416) 630-5741
PROJECT NO. 16-44 DATE: FEB 23 2022

SCHEDULE B: DEVELOPMENT PLANNING & AGENCY COMMENTS

AGENCIES	Circulated	Comments Received	Conditions	Nature of Comments
TRCA	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	Recommend Approval w/Conditions
Ministry of Transportation (MTO)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Region of York	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Recommend Approval/No Conditions
Alectra	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Recommend Approval/No Conditions
Bell Canada	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	No Comments no concerns
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YCDSB	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
CN Rail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
CP Rail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
TransCanada Pipeline	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Metrolinx	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Propane Operator	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Development Planning	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Recommend Adjournment

To: Christine Vigneault, Committee of Adjustment Secretary Treasurer

From: Nancy Tuckett, Director of Development Planning

Date: April 14, 2022

Name of Owner: 2019625 Ontario Limited

Location: 233 Sweetriver Boulevard

File No.(s): A063/22

Proposed Variance(s) (By-law 01-2021):

1. To permit a building not to be located within the required 3.5 m to 7 m build-to-zone for a proposed addition to Building C.
2. To permit a minimum ground floor height of 3.81 m for a proposed addition to Building C.
3. To permit waste storage not to be wholly located within a building, within a waste storage enclosure or within a private garage in the GMU zone.

By-Law Requirement(s) (By-law 01-2021):

1. The proposed addition to Building C shall be located within the required 3.5 m to 7 m build-to-zone. [Table 8-3]
2. A minimum ground floor height of 4.5 metres is required for the proposed addition to Building C. [Table 8-3]
3. Waste storage shall be wholly located within a building, within a waste storage enclosure or within a private garage in the GMU zone. [4.24]

Proposed Variance(s) (By-law 1-88):

4. To permit a minimum rear yard setback of 9.04 metres to Building C.
5. To permit a minimum landscape strip width of 4.46 metres abutting a street line.

By-Law Requirement(s) (By-law 1-88):

4. A minimum rear yard setback of 15 metres is required to Building C. [Schedule A]
5. A strip of land not less than 6.0 metres in width shall be provided along a lot line which abuts a street line and shall be used for no other purpose than landscaping. [Subsections 3.13 & 5.1.1]

Official Plan:

Vaughan Official Plan 2010 ('VOP 2010'): "Mid-Rise Mixed-Use"

Comments:

The Owner is requesting permission to construct to facilitate one (1) one-storey 411.74 m² addition to the existing Acura car dealership (Building A), three (3) one-storey additions totaling 655.50 m² to the existing Hyundai car dealership (Building C) with the above noted variances.

The associated Site Development File DA.21.042 (2nd submission) is still being reviewed and the TRCA has yet to complete their technical review to determine whether they will be in support of the southwest addition on Building C. Therefore, the Development Planning Application deems this minor variance application to be premature.

Recommendation:

The Development Planning Department recommends adjournment of the application.

Conditions of Approval:

If the Committee finds merit in the application, the following conditions of approval are recommended:

None

Comments Prepared by:

Roberto Simbana, Planner I
Chris Cosentino, Senior Planner

Date: March 24th , 2022

Attention: **Christine Vigneault**

RE: Request for Comments

File No.: **A063-22**

Related Files:

Applicant 2019625 Ontario Ltd.

Location 233 Sweet River Blvd.

COMMENTS:

- ☐ We have reviewed the proposed Variance Application and have no comments or objections to its approval.
- ☒ We have reviewed the proposed Variance Application and have no objections to its approval, subject to the following comments (attached below).
- ☐ We have reviewed the proposed Variance Application and have the following concerns (attached below).

Alectra Utilities (formerly PowerStream) has received and reviewed the proposed Variance Application. This review, however, does not imply any approval of the project or plan.

All proposed billboards, signs, and other structures associated with the project or plan must maintain minimum clearances to the existing overhead or underground electrical distribution system as specified by the applicable standards, codes and acts referenced.

In the event that construction commences, and the clearance between any component of the work/structure and the adjacent existing overhead and underground electrical distribution system violates the Occupational Health and Safety Act, the customer will be responsible for 100% of the costs associated with Alectra making the work area safe. All construction work will be required to stop until the safe limits of approach can be established.

In the event construction is completed, and the clearance between the constructed structure and the adjacent existing overhead and underground electrical distribution system violates the any of applicable standards, acts or codes referenced, the customer will be responsible for 100% of Alectra's cost for any relocation work.

References:

- Ontario Electrical Safety Code, latest edition (Clearance of Conductors from Buildings)
- Ontario Health and Safety Act, latest edition (Construction Protection)
- Ontario Building Code, latest edition (Clearance to Buildings)
- PowerStream (Construction Standard 03-1, 03-4), attached
- Canadian Standards Association, latest edition (Basic Clearances)

If more information is required, please contact either of the following:

Mr. Stephen Cranley, C.E.T
Supervisor, Distribution Design, ICI & Layouts (North)
Phone: 1-877-963-6900 ext. 31297

E-mail: stephen.cranley@alectrautilities.com

Mitchell Penner
Supervisor, Distribution Design-Subdivisions
Phone: 416-302-6215

Email: Mitchell.Penner@alectrautilities.com

Lenore Providence

Subject: FW: [External] RE: A063/22 - 233 Sweet River Blvd - City of Vaughan Request for Comments

From: Hurst, Gabrielle <Gabrielle.Hurst@york.ca>

Sent: March-28-22 10:18 AM

To: Lenore Providence <Lenore.Providence@vaughan.ca>

Cc: Committee of Adjustment <CofA@vaughan.ca>

Subject: [External] RE: A063/22 - 233 Sweet River Blvd - City of Vaughan Request for Comments

Good morning Lenore,
The Regional Municipality of York has completed its review of the above minor variance and has no comment.

Gabrielle

Gabrielle Hurst mcip rpp | Programs and Process Improvement| Community Planning and Development Services | The Regional Municipality of York| 1-877 464 9675 ext 71538 | gabrielle.hurst@york.ca | www.york.ca

Lenore Providence

Subject: FW: [External] RE: A063/22 - 233 Sweet River Blvd - City of Vaughan Request for Comments

From: Gordon, Carrie <carrie.gordon@bell.ca>

Sent: March-23-22 8:35 AM

To: Lenore Providence <Lenore.Providence@vaughan.ca>

Subject: [External] RE: A063/22 - 233 Sweet River Blvd - City of Vaughan Request for Comments

Hello Lenore,

Bell Canada has no comments for this minor variance.

Kind regards,

Carrie Gordon

Carrie Gordon



Associate, External Liaison
Right of Way Control Centre
140 Bayfield St, Fl 2
Barrie ON, L4M 3B1
T: 705-722-2244/844-857-7942
F :705-726-4600

April 6, 2022

CFN 66448.06
Ex Ref CFN 64180.18

BY E-MAIL: Christine.vigneault@vaughan.ca

Christine Vigneault
Secretary Treasurer
Committee of Adjustment
City of Vaughan
2141 Major Mackenzie Drive
Vaughan, Ontario L6A 1T1

Dear Ms. Vigneault:

**Re: Minor Variance Application A063/22
Part of Lot 15, Concession 5
233 Sweetriver Boulevard
City of Vaughan
2019625 Ontario Ltd. (Wes Surdyka Architect Inc.)**

This letter will acknowledge receipt of the above noted application, received by TRCA on March 22, 2022. Toronto and Region Conservation Authority (TRCA) staff has reviewed the application and offers the following comments for the consideration of the Committee of Adjustment.

Background

It is our understanding that the purpose of the Minor Variance Application is to request the following variances under Zoning By-Law 001-2021:

1. To permit a building not to be located within the required 3.5 metre to 7 metre build-to-zone for a proposed addition to Building C, where the proposed addition to Building C shall be located within the required 3.5 – 7 metre build-to-zone.
2. To permit a minimum ground flood height of 3.81 metres for a proposed addition to Building C, whereas a minimum ground floor height of 4.5 metres is required.
3. To permit waste storage not to be wholly located within a building, within a waste storage enclosure or within a private garage in the GMU zone, whereas waste storage shall be wholly located within a building, within a waste storage enclosure or within a private garage in the GMU zone.

It is our understanding that the following variances are being requested under Zoning By-Law 1-88:

1. To permit a minimum rear yard setback of 9.04 metres to Building C, whereas a minimum rear yard setback of 15 metres is required.
2. To permit a minimum landscape strip width of 4.26 metres abutting a street line, whereas a strip of land not less than 6.0 metres in width shall be provided along a lot line which abuts a street line. And shall be used for no other purpose than landscaping.

It is the understanding of TRCA staff that the variances are required to permit the construction of three additions to Building C and one addition to Building A as well as new parking lot area.

Recommendation

Based upon the comments noted below, TRCA staff provides **conditional approval** to the above noted Minor Variance application, subject to the following condition:

1. The applicant submits the application fee of \$610 payable to the Toronto and Region Conservation Authority.

TRCA's conditional approval does not include any clearance and/or approvals for the technical issues in support of the related Site Plan Application or a future permit application.

Ontario Regulation 166/06

A portion of the subject property is within TRCA's Regulated Area of the Don River Watershed. As such, a TRCA permit pursuant to Ontario Regulation 166/06 will be required for any development or site alteration within the Regulated Area.

Application-Specific Comments

The subject property is regulated by TRCA because of a valley corridor associated with a tributary of the Don River that traverses the southern portion of the site.

TRCA staff have been involved in the review of an associated Site Development application (City File No. DA.21.042). A recirculation of the application, which was received on March 23, 2022, is currently being reviewed by TRCA staff.

Although TRCA staff have no objection to the requested variances, we are currently working with the City and the applicant and their consultants to review technical aspects related to the applicable Site Development Application.

Fees

By copy of this letter, the applicant is advised that the TRCA has implemented a fee schedule for our planning application review services. This application is subject to a \$610 Variance Application (Minor) review fee. The applicant is responsible for fee payment and should forward the application fee to this office as soon as possible. This fee is separate from that requested for TRCA's review of the applicable Site Plan Application.

I trust these comments are of assistance. Should you have any additional questions or comments, please do not hesitate to contact the undersigned.

Sincerely,



Stephen Bohan
Planner

Development Planning and Permits | Development and Engineering Services

SCHEDULE C: PUBLIC & APPLICANT CORRESPONDENCE

Correspondence Type	Name	Address	Date Received (mm/dd/yyyy)	Summary
None.				