

| | |
|------------------|--|
| ITEM: 6.1 | COMMITTEE OF ADJUSTMENT REPORT SUMMARY MINOR VARIANCE APPLICATION FILE NUMBER A016/23 10680 ISLINGTON AVENUE, VAUGHAN |
|------------------|--|

**THIS REPORT CONTAINS COMMENTS FROM THE FOLLOWING
DEPARTMENTS & AGENCIES:**

*Please see **Schedule B** of this report for a copy of Development Planning and Agency correspondence.

Additional comments from departments and agencies may be received after the publication of the Staff Report. These comments will be processed as an addendum and posted on the City's Website.

| DEPARTMENTS | Circulated | Comments Received | Conditions | Nature of Comments |
|---|-------------------------------------|-------------------------------------|-------------------------------------|----------------------------------|
| Committee of Adjustment | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | General Comments |
| Building Standards (Zoning Review) | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | General Comments |
| Building Inspection (Septic) | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | No Comments Received to Date |
| Development Planning | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | Recommend Approval/No Conditions |
| Development Engineering | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | Recommend Approval w/Conditions |
| Parks, Forestry and Horticulture Operations | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | No Comments Received to Date |
| By-law & Compliance, Licensing & Permits | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | No Comments Received to Date |
| Development Finance | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | No Comments Received to Date |
| Real Estate | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | |
| Fire Department | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | No Comments Received to Date |
| AGENCIES | Circulated | Comments Received | Conditions | Nature of Comments |
| TRCA | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | No Comments Received to Date |
| Ministry of Transportation (MTO) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | |
| Region of York | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | General Comments |
| Alectra | <input checked="" type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | General Comments |
| Bell Canada | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | No Comments Received to Date |
| YRDSB | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | |
| YCDSB | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | |
| CN Rail | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | |
| CP Rail | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | |
| TransCanada Pipeline | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | No Comments Received to Date |
| Metrolinx | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | |
| Propane Operator | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | |
| | | | | |

PUBLIC & APPLICANT CORRESPONDENCE

*Please see **Schedule C** of this report for a copy of the public & applicant correspondence listed below.

The deadline to submit public comments is **noon on the last business day prior to the scheduled hearing date.**

Comments and written public submissions received after the publication of this Staff Report will be processed as an addendum and posted on the City's Website.

All personal information collected because of this public meeting (including both written and oral submissions) is collected under the authority of the Municipal Act, the Municipal Freedom of Information and Protection of Privacy Act (MFIPPA), the Planning Act and all other relevant legislation, and will be used to assist in deciding on this matter. All personal information (as defined by MFIPPA), including (but not limited to) names, addresses, opinions and comments collected will become property of the City of Vaughan, will be made available for public disclosure (including being posted on the internet) and will be used to assist the Committee of Adjustment and staff to process this application.

| Correspondence Type | Name | Address | Date Received (mm/dd/yyyy) | Summary |
|---------------------|------|---------|-------------------------------|---------|
| None | | | | |

PREVIOUS COA DECISIONS ON THE SUBJECT LAND

*Please see **Schedule D** for a copy of the Decisions listed below

| File Number | Date of Decision | Decision Outcome |
|-------------|------------------|------------------|
| | | |

PREVIOUS COA DECISIONS ON THE SUBJECT LAND

*Please see **Schedule D** for a copy of the Decisions listed below

| | MM/DD/YYYY | |
|------|------------|--|
| None | | |

ADJOURNMENT HISTORY

* Previous hearing dates where this application was adjourned by the Committee and public notice issued.

| | |
|----------------|--|
| April 20, 2023 | Adjourned to accommodate statutory public notice |
|----------------|--|



**COMMITTEE OF ADJUSTMENT REPORT
MINOR VARIANCE APPLICATION
A016/23
10680 ISLINGTON AVE, VAUGHAN**

| | |
|--|---|
| ITEM NUMBER: 6.1 | CITY WARD #: 1 |
| APPLICANT: | Harmeet Malhans, Gurprit Malhans, Charan Malhans & Alessandra Ocampo |
| AGENT: | Andrew Solari, Charlie Bancheri & Christopher Marchese |
| PROPERTY: | 10680 Islington Ave, Vaughan |
| ZONING DESIGNATION: | See Below |
| VAUGHAN OFFICIAL PLAN (2010) DESIGNATION: | Vaughan Official Plan 2010 ('VOP 2010'): "Low-Rise Residential," Volume 1, and "Village Residential," Volume 2, Section 12.4 Kleinburg Core |
| RELATED DEVELOPMENT APPLICATIONS: | None |
| PURPOSE OF APPLICATION: | Relief from the Zoning By-law is being requested to permit the construction of a proposed cabana, swimming pool, a retaining wall and proposed reduction to landscaping requirements. |

The following variances have been requested from the City's Zoning By-law:

The subject lands are zoned R1B(EN) – First Density Residential Zone (Established Neighbourhood), subject to Exception 14.307, under Zoning By-law 001-2021, as amended.

| # | Zoning By-law 01-2021 | Variance requested |
|---|--|--|
| 1 | In the R1B zone, any portion of a yard in excess of 135.0 m ² shall be comprised of a minimum 60% soft landscape [Section 4.19.1.1]. | To permit the area in excess of 135.0 m ² of the rear yard to be comprised of a minimum of 55.50% soft landscape. |
| 2 | In any residential zone, the maximum height of a residential accessory structure shall be 3.0 metres [Section 4.1.4.1] | To permit a maximum height of 3.82 metres for a residential accessory structure. |
| 3 | A retaining wall that is greater than 1 metre in height shall be setback an equal distance to the height of the highest portion of the retaining wall [Table 4-1]. | To permit a retaining wall with a height of 2.17 metres to be setback a minimum of 0.43 metres. |
| 4 | A maximum driveway width of 9.0 metres is required for a lot with a frontage of 12.0 metres or greater [Table 6-7]. | To permit a maximum driveway width of 9.80 metres. |

The subject lands are zoned R1, Residential Zone, subject to Exception 9(1347), under Zoning By-law 1-88, as amended.

| # | Zoning By-law 1-88 | Variance requested |
|---|--|--|
| 5 | A minimum rear yard of 7.5 metres is required [Schedule A]. | To permit a minimum rear yard of 2.44 metres for an accessory structure. |
| 6 | In the R1 zone, where the area of a rear yard of a lot is greater than 135.0 m ² , a minimum 60% of that portion of the rear yard in excess of 135 sq.m. shall be comprised of soft landscaping [Section 4.1.2 b)]. | To permit the area in excess of 135.0 m ² of the rear yard to be comprised of a minimum of 55.50% soft landscape. |
| 7 | A 9.0m width landscape strip shall be provided along Regional Road 27 in the manner shown on Schedule E-1475 [Exception 9(1347) ii)] | To permit a minimum landscape strip of 1.83 metres along Regional Road 27 as shown on the sketch. |
| 8 | A maximum driveway width of 9.0 metres is required [Section 4.1.4 f)v)]. | To permit a maximum driveway width of 9.8 metres. |

| # | Zoning By-law 1-88 | Variance requested |
|---|--|--|
| 9 | A retaining wall which exceeds 1 metre in height must be setback from the nearest property line a distance equal to its height [Section 4.1.1 j)]. | To permit a retaining wall with a height of 2.17 metres to be setback 0.43 metres to the interior side lot line. |

HEARING INFORMATION

DATE OF MEETING: Monday, April 24, 2023
TIME: 6:00 p.m.
MEETING LOCATION: Vaughan City Hall, **Woodbridge Room**, 2141 Major Mackenzie Drive, Vaughan
LIVE STREAM LINK: Vaughan.ca/LiveCouncil

PUBLIC PARTICIPATION

If you would like to speak to the Committee of Adjustment at the meeting, either remotely or in person, please complete the [Request to Speak Form](#) and submit to cofa@vaughan.ca
If you would like to submit written comments, please quote file number above and submit by mail or email to:

Email: cofa@vaughan.ca

Mail: City of Vaughan, Office of the City Clerk, Committee of Adjustment, 2141 Major Mackenzie Drive, Vaughan, ON, L6A 1T1

THE DEADLINE TO REGISTER TO SPEAK OR SUBMIT WRITTEN COMMENTS ON THE ABOVE NOTED FILE(S) IS NOON ON THE LAST BUSINESS DAY BEFORE THE MEETING.

INTRODUCTION

Staff and Agencies act as advisory bodies to the Committee of Adjustment. The comments contained in this report are presented as recommendations to the Committee.

Section 45(1) of the Planning Act sets the criteria for authorizing minor variances to the City of Vaughan's Zoning By-law. Accordingly, review of the application may consider the following:

- That the general intent and purpose of the by-law will be maintained.
- That the general intent and purpose of the official plan will be maintained.
- That the requested variance(s) is/are acceptable for the appropriate development of the subject lands.
- That the requested variance(s) is/are minor in nature.

Public written and oral submissions relating to this application are taken into consideration by the Committee of Adjustment as part of its deliberations and final decision on this matter.

COMMITTEE OF ADJUSTMENT COMMENTS

| | |
|--|--|
| Date Public Notice Mailed: | April 13, 2023 |
| Date Applicant Confirmed Posting of Sign: | April 5, 2023 |
| Applicant Justification for Variances: <small>*As provided by Applicant in Application Form</small> | By laws restrict the location of the proposed accessory structure and swimming pool |
| Adjournment Requests (from staff): <small>*Adjournment requests provided to applicant prior to issuance of public notice</small> | On April 5, 2023, Development Planning advised: We do recommend you adjourn until June 1 st as that is the next CofA meeting after the May 24 Heritage Vaughan Meeting. However, if you do choose to proceed to either the April 20 th or May 11 th CofA meeting, Development Planning will still be in a position to support the application. We just want to reiterate that there is always a risk to proceeding ahead of the Heritage Vaughan meeting and that if they request any changes that impact the variances, you will need to submit another Minor Variance Application to address them. On April 5, 2023, the applicant confirmed that they wish to proceed to the April 20 Committee of Adjustment hearing. |
| Was a Zoning Review Waiver (ZRW) Form submitted by Applicant: <small>*ZRW Form may be used by applicant in instances where a revised submission is made, and zoning staff do not have an opportunity to review and confirm variances prior to the issuance of public notice. *A revised submission may be required to address staff / agency comments received as part of the application review process.</small> | No |

COMMITTEE OF ADJUSTMENT COMMENTS

*Where a zoning review has not been completed on a revised submission, an opportunity is provided to the applicant to adjourn the proposal prior to the issuance of public notice.

Adjournment Fees:

In accordance with Procedural By-law 069-2019, an Adjournment Fee is applicable to reschedule an application after the issuance of public notice where a request for adjournment has been provided to the applicant prior to the issuance of public notice.

An Adjournment Fee can only be waived in instances where adjournment of an application is requested by the Committee or staff **after** the issuance of public notice.

| | |
|--|------|
| Committee of Adjustment Comments: | None |
|--|------|

| | |
|--|------|
| Committee of Adjustment Recommended Conditions of Approval: | None |
|--|------|

BUILDING STANDARDS (ZONING) COMMENTS

**See Schedule B for Building Standards (Zoning) Comments

| | |
|---|------|
| Building Standards Recommended Conditions of Approval: | None |
|---|------|

DEVELOPMENT PLANNING COMMENTS

**See Schedule B for Development Planning Comments.

| | |
|---|------|
| Development Planning Recommended Conditions of Approval: | None |
|---|------|

DEVELOPMENT ENGINEERING COMMENTS

[Link to Grading Permit](#) [Link to Pool Permit](#) [Link to Curb Curt Permit](#) [Link Culvert Installation](#)

As the proposed Cabana in the subject property is 40.01m², the owner/ applicant needs to obtain a lot grading permit from Development Inspection and Lot Grading division of the City's Development Engineering Department. Please note any in ground structure over 10 m² requires a grading permit. Please contact COA application engineering reviewer after receiving the grading permit to clear the condition. (Condition attached)

The proposed work by the owner/ applicant will increase the rear yard hardscaping area in the subject property. The added hardscape may have impact on City's Storm Water management system. Staff suggests the owner/applicant to introduce Low-impact Development (LID) measures (Bioswales, Permeable pavers, Rain Gardens, Rain Barrels etc.) to reduce the impacts.

The Development Engineering (DE) Department does not object to the variance application A016/23 subject to the following condition.

| | |
|--|--|
| Development Engineering Recommended Conditions of Approval: | The Owner/applicant shall submit a revised Lot Grading and/or Servicing Plan to the Development Inspection and Lot Grading division of the City's Development Engineering Department for final lot grading and/or servicing approval prior to any work being undertaken on the property. Please visit or contact the Development Engineering Department through email at DEPermits@vaughan.ca or visit the grading permit link provided above to learn how to apply for lot grading and/or servicing approval. |
|--|--|

PARKS, FORESTRY & HORTICULTURE (PFH) COMMENTS

No comments received to date.

| | |
|--|------|
| PFH Recommended Conditions of Approval: | None |
|--|------|

DEVELOPMENT FINANCE COMMENTS

No comments received to date.

| | |
|--|------|
| Development Finance Recommended Conditions of Approval: | None |
|--|------|

BY-LAW AND COMPLIANCE, LICENSING AND PERMIT SERVICES COMMENTS

No comments received to date.

| | |
|--|------|
| BCLPS Recommended Conditions of Approval: | None |
|--|------|

BUILDING INSPECTION (SEPTIC) COMMENTS

No comments received to date.

| | |
|--|------|
| Building Inspection Recommended Conditions of Approval: | None |
|--|------|

FIRE DEPARTMENT COMMENTS

No comments received to date.

| | |
|--|------|
| Fire Department Recommended Conditions of Approval: | None |
|--|------|

SCHEDULES TO STAFF REPORT

*See Schedule for list of correspondence

| | |
|---------------------------------|---|
| Schedule A | Drawings & Plans Submitted with the Application |
| Schedule B | Staff & Agency Comments |
| Schedule C (if required) | Correspondence (Received from Public & Applicant) |
| Schedule D (if required) | Previous COA Decisions on the Subject Land |

Should the Committee find it appropriate to approve this application in accordance with request and the sketch submitted with the application, as required by Ontario Regulation 200/96, the following conditions have been recommended:

SUMMARY OF RECOMMENDED CONDITIONS OF APPROVAL

All conditions of approval, unless otherwise stated, are considered to be incorporated into the approval “if required”. If a condition is no longer required after an approval is final and binding, the condition may be waived by the respective department or agency requesting conditional approval. A condition cannot be waived without written consent from the respective department or agency.

| # | DEPARTMENT / AGENCY | CONDITION(S) DESCRIPTION |
|---|---|--|
| 1 | Development Engineering ian.reynolds@vaughan.ca | The Owner/applicant shall submit a revised Lot Grading and/or Servicing Plan to the Development Inspection and Lot Grading division of the City’s Development Engineering Department for final lot grading and/or servicing approval prior to any work being undertaken on the property. Please visit or contact the Development Engineering Department through email at DEPermits@vaughan.ca or visit the grading permit link provided above to learn how to apply for lot grading and/or servicing approval. |

IMPORTANT INFORMATION – PLEASE READ

CONDITIONS: It is the responsibility of the owner/applicant and/or authorized agent to obtain and provide a clearance letter from respective department and/or agency (**see condition chart above for contact**). This letter must be provided to the Secretary-Treasurer to be finalized. All conditions must be cleared prior to the issuance of a Building Permit.

IMPORTANT INFORMATION – PLEASE READ

APPROVALS: Making any changes to your proposal after a decision has been made may impact the validity of the Committee's decision.

An approval obtained from the Committee of Adjustment, where applicable, is tied to the building envelope shown on the plans and drawings submitted with the application and subject to the variance approval.

A building envelope is defined by the setbacks of the buildings and/or structures shown on the plans and drawings submitted with the application, as required by Ontario Regulation 200/96. Future development outside of an approved building envelope, where a minor variance was obtained, must comply with the provisions of the City's Zoning By-law.

Elevation drawings are provided to reflect the style of roof (i.e. flat, mansard, gable etc.) to which a building height variance has been applied. Where a height variance is approved, building height is applied to the style of roof (as defined in the City's Zoning By-law) shown on the elevation plans submitted with the application.

Architectural design features that are not regulated by the City's Zoning By-law are not to be considered part of an approval unless specified in the Committee's decision.

DEVELOPMENT CHARGES: That the payment of the Regional Development Charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the Regional Development Charges By-law in effect at the time of payment.

That the payment of the City Development Charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the City's Development Charges By-law in effect at the time of payment.

That the payment of the Education Development Charge if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the Boards of Education By-laws in effect at the time of payment

That the payment of Special Area Development charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and The City's Development Charge By-law in effect at the time of Building permit issuance to the satisfaction of the Reserves/Capital Department.

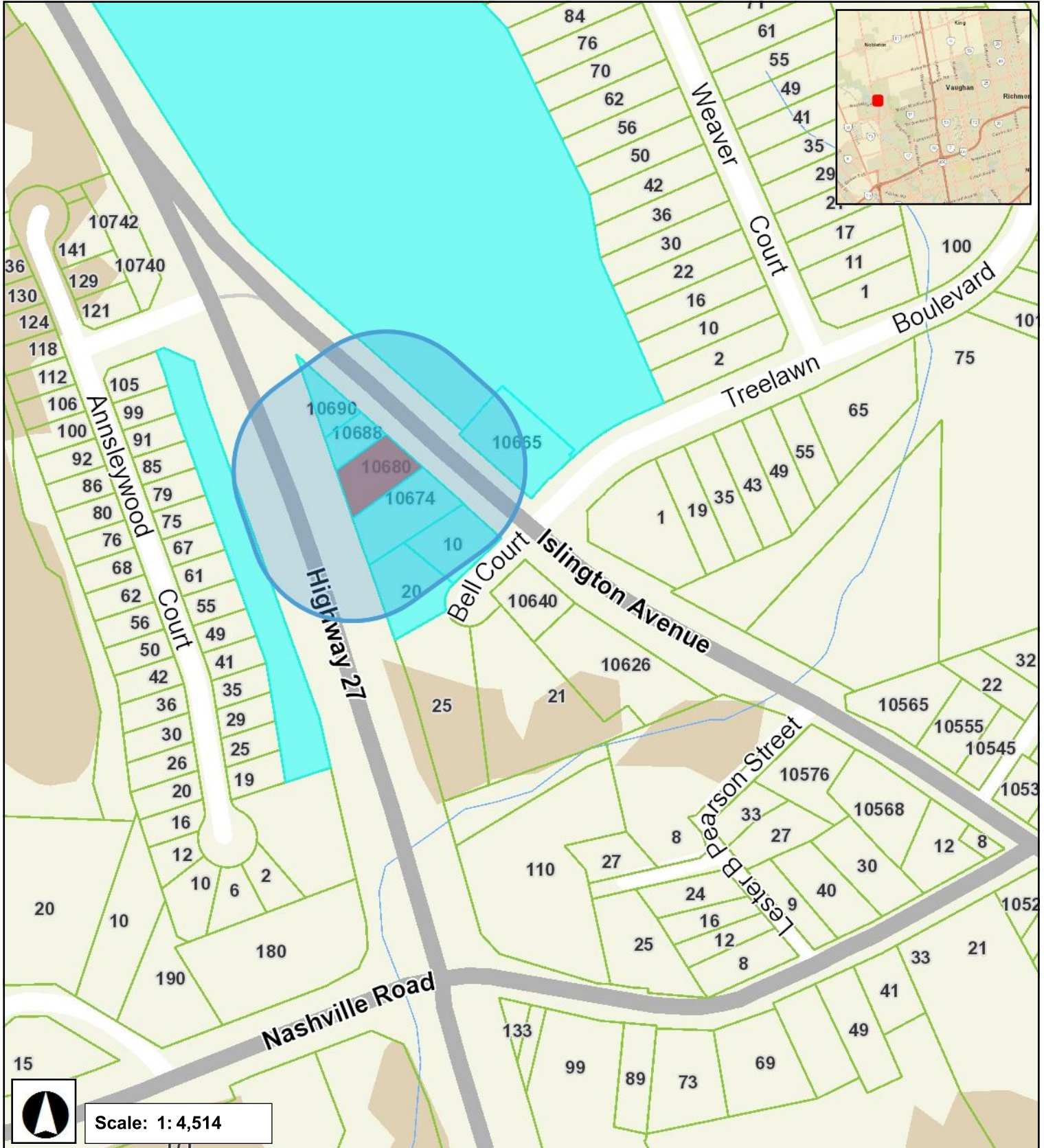
NOTICE OF DECISION: If you wish to be notified of the decision in respect to this application or a related Ontario Land Tribunal (OLT) hearing you must complete a Request for Decision form and submit to the Secretary Treasurer (ask staff for details). In the absence of a written request to be notified of the Committee's decision you will **not** receive notice.

SCHEDULE A: DRAWINGS & PLANS



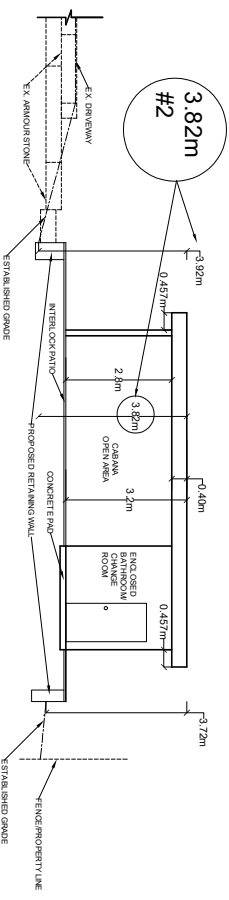
LOCATION MAP - A016/23

10680 ISLINGTON AVENUE, VAUGHAN



RECEIVED

By Christine Vigneault at 12:07 pm, Apr 14, 2023



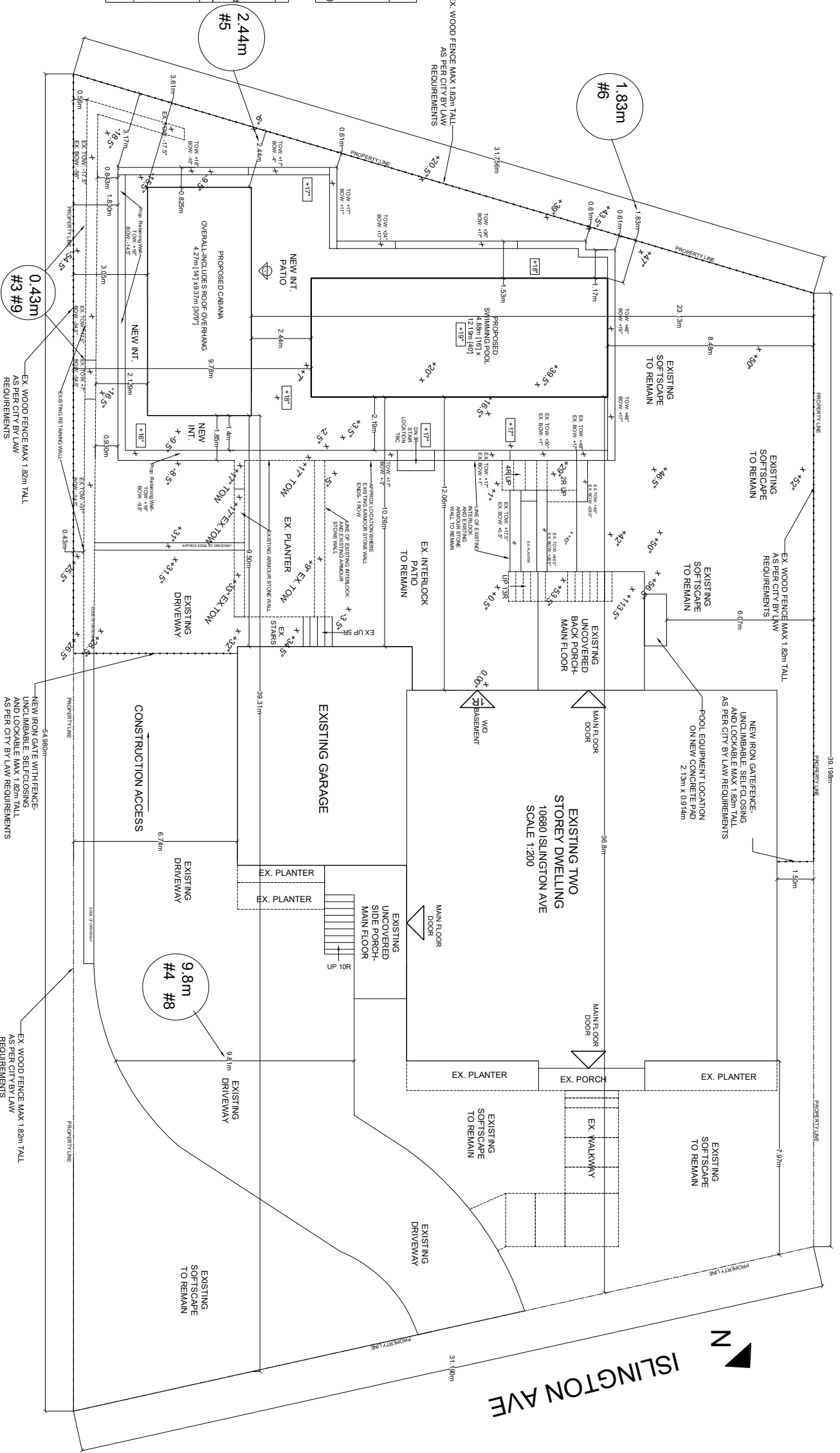
1 CABANA-WEST/FRONT ELEVATION
SCALE 1:200

10680 ISLINGTON AVE
APRIL 14 2023
SCALE 1:200

| LOT COVERAGE | |
|--|--|
| House/Garage/ Front Porch/Planter/ Porch/Steps/Side/ Prop. Cabana: | 313.86m ² 37.69m ² 40.00m ² |
| Lot Coverage (%) | 391.55m ² (27.3%) |

| REAR YARD PERVIOUS AREA | |
|--|-------------------------------|
| Minimum 1.59m ² & Loggia/Steps- | 431.18m ² |
| Softscape 60 % | 258.77m ² required |

| HAND US SOFT SCAPES PROPOSED | |
|------------------------------|---|
| EX Hardscapes: | 115.72m ² |
| Pop. Hardscapes | 111.78m ² |
| Loggia: | 13.49m ² |
| Pool: | 59.49m ² |
| Softscapes | 239.19m ² |
| Pop. Cabana | 40.00m ² |
| Softscape %: | 239.19m ² /431.18m ² = 55.50% |



ISLINGTON AVE
N

#1 #6
55.50%

#3 #9
0.43m

#4 #8
9.8m

#5
2.44m

#6
1.83m

EX WOOD FENCE MAX 1.82m TALL AS PER CITY BY LAW REQUIREMENTS

NEW IRON GATE WITH FENCE UNCLIMBABLE SELF-CLOSING AND LOCKABLE MAX 1.82m TALL AS PER CITY BY LAW REQUIREMENTS

EX WOOD FENCE MAX 1.82m TALL AS PER CITY BY LAW REQUIREMENTS

EXISTING SOFTSCAPE TO REMAIN

EXISTING DRIVEWAY

EXISTING SOFTSCAPE TO REMAIN

EXISTING SOFTSCAPE TO REMAIN

EXISTING SOFTSCAPE TO REMAIN

EXISTING SOFTSCAPE TO REMAIN

EXISTING SOFTSCAPE TO REMAIN

EXISTING SOFTSCAPE TO REMAIN

EXISTING SOFTSCAPE TO REMAIN

EXISTING SOFTSCAPE TO REMAIN

EXISTING SOFTSCAPE TO REMAIN

EXISTING SOFTSCAPE TO REMAIN

EXISTING SOFTSCAPE TO REMAIN

SCHEDULE B: STAFF & AGENCY COMMENTS

| DEPT/AGENCY | Circulated | Comments Received | Conditions | Nature of Comments |
|--|-------------------|--------------------------|-------------------|----------------------------------|
| TRCA *Schedule B | X | | | No Comments Received to Date |
| Ministry of Transportation (MTO) *Schedule B | | | | No Comments Received to Date |
| Region of York *Schedule B | X | X | | General Comments |
| Alectra *Schedule B | X | X | | General Comments |
| Bell Canada *Schedule B | X | | | No Comments Received to Date |
| YRDSB *Schedule B | | | | |
| YCDSB *Schedule B | | | | |
| CN Rail *Schedule B | | | | |
| CP Rail *Schedule B | | | | |
| TransCanada Pipeline *Schedule B | X | | | No Comments Received to Date |
| Metrolinx *Schedule B | | | | |
| Propane Operator *Schedule B | | | | |
| Development Planning | X | X | | Recommend approval no conditions |
| Building Standards (Zoning) | X | X | | General Comments |

Date: April 14th 2023

Attention: **Christine Vigneault**

RE: Request for Comments

File No.: **A016-23**

Related Files:

Applicant Harmeet Malhans, Gurprit Malhans, Charan Malhans & Alessandra Ocampo

Location 10680 Islington Avenue



Discover the possibilities

COMMENTS:

- We have reviewed the proposed Variance Application and have no comments or objections to its approval.
- We have reviewed the proposed Variance Application and have no objections to its approval, subject to the following comments (attached below).
- We have reviewed the proposed Variance Application and have the following concerns (attached below).

Alectra Utilities (formerly PowerStream) has received and reviewed the proposed Variance Application. This review, however, does not imply any approval of the project or plan.

All proposed billboards, signs, and other structures associated with the project or plan must maintain minimum clearances to the existing overhead or underground electrical distribution system as specified by the applicable standards, codes and acts referenced.

In the event that construction commences, and the clearance between any component of the work/structure and the adjacent existing overhead and underground electrical distribution system violates the Occupational Health and Safety Act, the customer will be responsible for 100% of the costs associated with Alectra making the work area safe. All construction work will be required to stop until the safe limits of approach can be established.

In the event construction is completed, and the clearance between the constructed structure and the adjacent existing overhead and underground electrical distribution system violates the any of applicable standards, acts or codes referenced, the customer will be responsible for 100% of Alectra's cost for any relocation work.

References:

- Ontario Electrical Safety Code, latest edition (Clearance of Conductors from Buildings)
- Ontario Health and Safety Act, latest edition (Construction Protection)
- Ontario Building Code, latest edition (Clearance to Buildings)
- PowerStream (Construction Standard 03-1, 03-4), attached
- Canadian Standards Association, latest edition (Basic Clearances)

If more information is required, please contact either of the following:

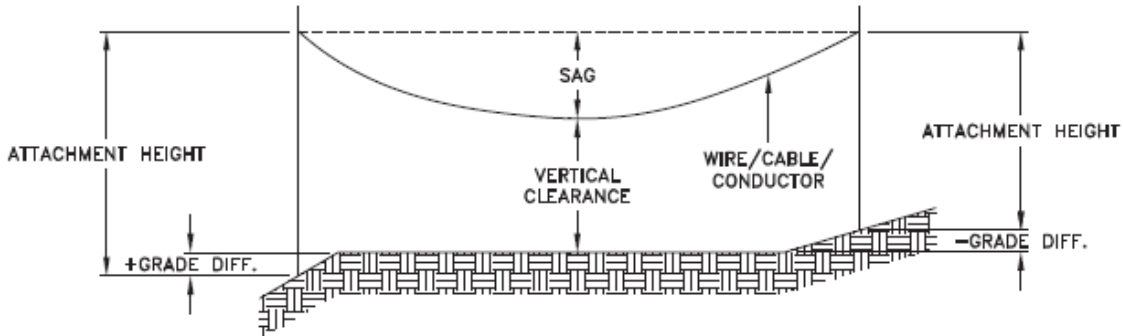
Mr. Stephen Cranley, C.E.T
Supervisor, Distribution Design, ICI & Layouts (North)
Phone: 1-877-963-6900 ext. 31297

E-mail: stephen.cranley@alectrautilities.com

Mitchell Penner
Supervisor, Distribution Design-Subdivisions
Phone: 416-302-6215

Email: Mitchell.Penner@alectrautilities.com

| LOCATION OF WIRES, CABLES OR CONDUCTORS | SYSTEM VOLTAGE | | | |
|---|------------------------------------|------------------------|--------------------------------------|-------|
| | SPAN GUYS AND COMMUNICATIONS WIRES | UP TO 600V AND NEUTRAL | 4.16/2.4kV TO 27.6/16kV (SEE NOTE 1) | 44kV |
| MINIMUM VERTICAL CLEARANCES (SEE NOTE 2) | | | | |
| OVER OR ALONGSIDE ROADS, DRIVEWAYS OR LANDS ACCESSIBLE TO <u>VEHICLES</u> | 442cm | 442cm | 480cm | 520cm |
| OVER GROUND ACCESSIBLE TO <u>PEDESTRIANS AND BICYCLES ONLY</u> | 250cm | 310cm | 340cm | 370cm |
| ABOVE TOP OF RAIL AT <u>RAILWAY CROSSINGS</u> | 730cm | 730cm | 760cm | 810cm |



MINIMUM ATTACHMENT HEIGHT = MAXIMUM SAG
 + MINIMUM VERTICAL CLEARANCE (FROM ABOVE TABLE)
 ± GRADE DIFFERENCE
 + 0.3m (VEHICLE OR RAILWAY LOCATION)
 + SNOW DEPTH (PEDESTRIAN LOCATION, SEE NOTE 3)

NOTES:

1. THE MULTIGROUNDED SYSTEM NEUTRAL HAS THE SAME CLEARANCE AS THE 600V SYSTEM.
2. THE VERTICAL CLEARANCES IN THE ABOVE TABLE ARE UNDER MAXIMUM SAG CONDITIONS.
3. REFER TO CSA STANDARD C22.3 No.1, ANNEX D FOR LOCAL SNOW DEPTH VALUES.
4. ALL CLEARANCES ARE IN ACCORDANCE TO CSA STANDARD C22.3.

CONVERSION TABLE

| METRIC | IMPERIAL (APPROX) |
|--------|-------------------|
| 810cm | 27'-0" |
| 760cm | 25'-4" |
| 730cm | 24'-4" |
| 520cm | 17'-4" |
| 480cm | 16'-0" |
| 442cm | 15'-5" |
| 370cm | 12'-4" |
| 340cm | 11'-4" |
| 310cm | 10'-4" |
| 250cm | 8'-4" |

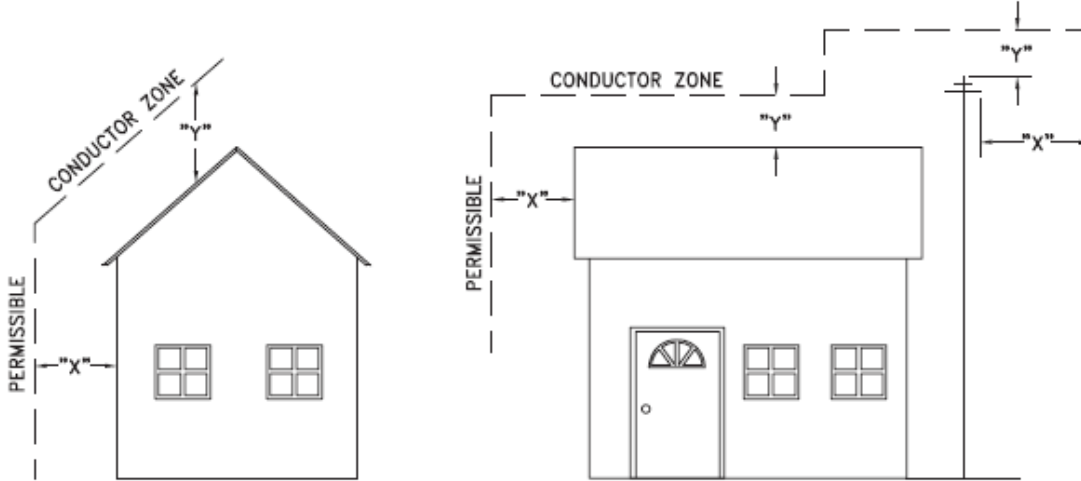
REFERENCES

SAGS AND TENSIONS | SECTION 02

MINIMUM VERTICAL CLEARANCES OF WIRES, CABLES AND CONDUCTORS ABOVE GROUND OR RAILS

ORIGINAL ISSUE DATE: 2010-DEC-24 REVISION NO: R1 REVISION DATE: 2012-JAN-09

| Certificate of Approval | |
|---|-------------|
| This construction Standard meets the safety requirements of Section 4 of Regulation 22/04 | |
| Joe Crozier, P.Eng. | 2012-JAN-09 |
| Name | Date |
| P.Eng. Approval By: | Joe Crozier |



| VOLTAGE | MINIMUM HORIZONTAL CLEARANCE UNDER MAXIMUM SWING CONDITIONS DIMENSION "X" (SEE NOTES 1, 3 & 4) | MINIMUM VERTICAL CLEARANCE UNDER MAXIMUM DESIGN SAG CONDITIONS DIMENSION "Y" (SEE NOTES 1, 2, 4 & 5) |
|--------------------|--|--|
| 0-600V AND NEUTRAL | 100cm | 250cm |
| 4.16/2.4 TO 44kV | 300cm | 480cm |

NOTES

1. UNDER NO CIRCUMSTANCES SHALL A CONDUCTOR BE PERMITTED TO PENETRATE THE ENVELOPE SHOWN BY THE DOTTED LINE.
2. THE VERTICAL CLEARANCES ARE UNDER CONDITIONS OF MAXIMUM DESIGN SAG.
3. THE HORIZONTAL CLEARANCES ARE UNDER CONDITIONS OF MAXIMUM SWING. WHERE THE CONDUCTOR SWING IS NOT KNOWN A HORIZONTAL CLEARANCE OF 480CM SHALL BE USED.
4. BUILDINGS THAT EXCEED 3 STOREYS OR 15M IN HEIGHT, THE MINIMUM HORIZONTAL CLEARANCE OF THE SECONDARY CONDUCTORS SHOULD BE INCREASED TO 300cm WHERE IT IS NECESSARY TO ALLOW FOR THE RAISING OF LADDERS BY LOCAL FIRE DEPARTMENTS.
5. IN SITUATIONS SUCH AS MULTI-LEVEL GARAGES, WHERE ROOFS ARE NORMALLY USED BY PERSONS AND VEHICLES, THE VERTICAL CLEARANCES OF POWERSTREAM STANDARD 03-1 SHALL APPLY.
6. DISTRIBUTION LINES CONSTRUCTED NEAR BUILDINGS SHALL BE BUILT TO AVOID OVERHANG WHEREVER POSSIBLE. WHERE LINES MUST BE CONSTRUCTED OVER OR ADJACENT TO BUILDINGS THE APPLICABLE HORIZONTAL AND VERTICAL CLEARANCES SHALL BE AT CONDITIONS OF MAXIMUM CONDUCTOR SWING AND MAXIMUM SAG. THE ABOVE CLEARANCES ARE DESIGNED TO PREVENT PERSONS ON OR IN BUILDINGS AS WELL AS EXTERNAL MACHINERY USED IN CONJUNCTION WITH A BUILDING TO COME IN CONTACT WITH CONDUCTORS. EFFORTS SHOULD BE MADE TO INCREASE THESE CLEARANCES WHERE POSSIBLE.
7. ALL CLEARANCES ARE IN ACCORDANCE TO CSA C22.3 NO.1-06 (TABLE-9).

| METRIC | IMPERIAL (APPROX) |
|--------|-------------------|
| 480cm | 16'-0" |
| 300cm | 10'-0" |
| 250cm | 8'-4" |
| 100cm | 3'-4" |

MINIMUM VERTICAL & HORIZONTAL CLEARANCES OF CONDUCTORS FROM BUILDINGS OR OTHER PERMANENT STRUCTURES (CONDUCTORS NOT ATTACHED TO BUILDINGS)

Certificate of Approval
This construction Standard meets the safety requirements of Section 4 of Regulation 22/04
Debbie Dadwani, P.Eng. 2010-MAY-05
Name Date
P. Eng. Approval By: *D. Dadwani*

ORIGINAL ISSUE DATE: 2010-MAY-05 REVISION NO: REVISION DATE:
PS:\System Planning and Standards\Standard Design\PowerStream Standards\PowerStream Standards working files\Section 3-1-02\WG 03-4 R0 May 5, 2010.dwg, 5/5/2010 10:22:02 AM, Adobe PDF

To: Committee of Adjustment
From: Lindsay Haviland, Building Standards Department
Date: March 20, 2023
Applicant: Harmeet Malhans, Gurprit Malhans, Charan Malhans & Alessandra Ocampo
Location: 10680 Islington Avenue
 PLAN 210 Part of Lot 1
 PLAN 65R27548 Part 1
 Part of Lot 25, Concession 8
File No.(s): A016/23

Zoning Classification:

The subject lands are zoned R1B(EN) – First Density Residential Zone (Established Neighbourhood), subject to Exception 14.307, under Zoning By-law 001-2021, as amended.

| # | Zoning By-law 01-2021 | Variance requested |
|---|--|--|
| 1 | In the R1B zone, any portion of a yard in excess of 135.0 m ² shall be comprised of a minimum 60% soft landscape [Section 4.19.1.1]. | To permit the area in excess of 135.0 m ² of the rear yard to be comprised of a minimum of 55.50% soft landscape. |
| 2 | In any residential zone, the maximum height of a residential accessory structure shall be 3.0 metres [Section 4.1.4.1] | To permit a maximum height of 3.82 metres for a residential accessory structure. |
| 3 | A retaining wall that is greater than 1 metre in height shall be setback an equal distance to the height of the highest portion of the retaining wall [Table 4-1]. | To permit a retaining wall with a height of 2.17 metres to be setback a minimum of 0.43 metres. |
| 4 | A maximum driveway width of 9.0 metres is required for a lot with a frontage of 12.0 metres or greater [Table 6-7]. | To permit a maximum driveway width of 9.80 metres. |

The subject lands are zoned R1, Residential Zone, subject to Exception 9(1347), under Zoning By-law 1-88, as amended.

| # | Zoning By-law 1-88 | Variance requested |
|---|--|--|
| 5 | A minimum rear yard of 7.5 metres is required [Schedule A]. | To permit a minimum rear yard of 2.44 metres for an accessory structure. |
| 6 | In the R1 zone, where the area of a rear yard of a lot is greater than 135.0 m ² , a minimum 60% of that portion of the rear yard in excess of 135 sq.m. shall be comprised of soft landscaping [Section 4.1.2 b)]. | To permit the area in excess of 135.0 m ² of the rear yard to be comprised of a minimum of 55.50% soft landscape. |
| 7 | A 9.0m width landscape strip shall be provided along Regional Road 27 in the manner shown on Schedule E-1475 [Exception 9(1347) ii)] | To permit a minimum landscape strip of 1.83 metres along Regional Road 27 as shown on the sketch. |
| 8 | A maximum driveway width of 9.0 metres is required [Section 4.1.4 f)v)]. | To permit a maximum driveway width of 9.8 metres. |
| 9 | A retaining wall which exceeds 1 metre in height must be setback from the nearest property line a distance equal to its height [Section 4.1.1 j)]. | To permit a retaining wall with a height of 2.17 metres to be setback 0.43 metres to the interior side lot line. |

Staff Comments:

Stop Work Order(s) and Order(s) to Comply:

There are no outstanding Orders on file

Building Permit(s) Issued:

Building Permit No. 22-135302 for Shed/Gazebo - New, Issue Date: (Not Yet Issued)

Other Comments:

| General Comments | |
|-------------------------|--|
| 1. | The applicant shall be advised that additional variances may be required upon review of detailed drawing for building permit/site plan approval. |
| 2. | Heritage Approval is required for the proposed accessory structure. |

Conditions of Approval:

If the committee finds merit in the application, the following conditions of approval are recommended.

* Comments are based on the review of documentation supplied with this application.

To: Christine Vigneault, Committee of Adjustment Secretary Treasurer
From: Nancy Tuckett, Director of Development Planning
Date: April 14, 2023
Name of Owners: Harmeet, Gurprit, Charan Malhans and Alessandra Ocampo
Location: 10680 Islington Avenue
File No.(s): A016/23

Proposed Variance(s) (By-law 001-2021):

1. To permit the area in excess of 135.0 m² of the rear yard to be comprised of a minimum of 55.50% soft landscape.
2. To permit a maximum height of 3.82 m for a residential accessory structure.
3. To permit a retaining wall with a height of 2.17 m to be set back a minimum of 0.43 m.
4. To permit a maximum driveway width of 9.80 m.

By-Law Requirement(s) (By-law 001-2021):

1. In the R1B zone, any portion of a yard in excess of 135.0 m² shall be comprised of a minimum 60% soft landscape.
2. In any residential zone, the maximum height of a residential accessory structure shall be 3.0 m.
3. A retaining wall that is greater than 1.0 m in height shall be set back an equal distance to the height of the highest portion of the retaining wall.
4. A maximum driveway width of 9.0 m is required for a lot with a frontage of 12.0 m or greater.

Proposed Variance(s) (By-law 1-88):

5. To permit a minimum rear yard of 2.44 m for an accessory structure.
6. To permit the area in excess of 135.0 m² of the rear yard to be comprised of a minimum of 55.50% soft landscape.
7. To permit a minimum landscape strip of 1.83 m along Regional Road 27 as shown on the sketch.
8. To permit a maximum driveway width of 9.8 m.
9. To permit a retaining wall with a height of 2.17 m to be set back 0.43 m to the interior side lot line.

By-Law Requirement(s) (By-law 1-88):

5. A minimum rear yard of 7.5 m is required.
6. In the R1 zone, where the area of a rear yard of a lot is greater than 135.0 m², a minimum 60% of that portion of the rear yard in excess of 135 m² shall be comprised of soft landscaping.
7. A 9.0m width landscape strip shall be provided along Regional Road 27 in the manner shown on Schedule E-1475.
8. A maximum driveway width of 9.0 m is required.
9. A retaining wall which exceeds 1.0 m in height must be set back from the nearest property line a distance equal to its height.

Official Plan:

City of Vaughan Official Plan 2010 ('VOP 2010'): "Low-Rise Residential," Volume 1, and "Village Residential," Volume 2, Section 12.4 Kleinburg Core

Comments:

The Owners are requesting relief to permit the construction of a cabana and hard landscaping and the existing driveway, with the above noted variances.

The Development Planning Department has no objection to Variances 1, 6 and 7 for the proposed rear yard hard landscaping. The reduction in soft landscaping is minor in nature and will maintain an appropriate balance of soft landscaping to facilitate drainage and the establishment of vegetation. The Development Engineering Department has reviewed the proposal and is satisfied that drainage in the rear yard will be maintained.

York Region staff have also reviewed the proposal and are satisfied with the 1.83 m landscape strip along Regional Road 27 (Highway 27).

The Development Planning Department has no objection to Variances 2 and 5 for the proposed cabana. The subject property is situated between two roads, fronting onto Islington Avenue and bordering Highway 27 to the rear. There is a significant change in grade throughout the property. The grade lowers the further west the rear yard travels. The 3.82 m height of the cabana is measured from the top of the roof to the average grade. Due to the grade change, the cabana is proposed to sit atop a retaining wall. The height of the cabana when measured from the top of the retaining wall is 3.0 m. The property backs onto Highway 27 and is screened from view by the existing trees in the Right-of-Way. As such, the increased height and reduction to the rear yard setback will not pose adverse massing impacts to the neighbouring properties or streetscape and an appropriate area for maintenance access is maintained. The proposed cabana has also been reviewed by Cultural Heritage staff and will be heard by the Heritage Vaughan Committee on May 24, 2023. Cultural Heritage staff have no concerns with the application; however, the Owners are aware that should the Heritage Vaughan Committee request any changes an additional Minor Variance Application may be required.

The Development Planning Department has no objection to Variances 3 and 9 for the proposed retaining wall as the Development Engineering Department has reviewed the proposal and is satisfied that drainage in the rear yard will be maintained.

The Development Planning Department has no objection to Variances 4 and 8 for the driveway. The driveway curves around and to the side of the dwelling as the access into the garage is located on the dwelling's south side wall. The 9.8 m driveway width is measured at the widest point, but this measurement is taken where the driveway flares out to provide access to a side entrance and only represents a portion of the driveway. When measured at the front lot line the driveway is 4.5 m wide, and when measured in front of the garage the driveway is 5.9 m wide. There are also trees present between the curved part of the driveway and Islington Avenue. As such, the driveway appropriately facilitates the interior side facing garage and will not cause adverse impacts to the existing streetscape.

Accordingly, the Development Planning Department can support the requested variances and is of the opinion that the proposal is minor in nature, maintains the general intent and purpose of the Official Plan and Zoning By-laws, and is desirable for the appropriate development of the land.

Recommendation:

The Development Planning Department recommends approval of the application.

Conditions of Approval:

If the Committee finds merit in the application, the following conditions of approval are recommended:

None

Comments Prepared by:

Joshua Cipolletta, Planner I

David Harding, Senior Planner

From: [Rajevan, Niranjan](#)
To: [Christine Vigneault](#)
Cc: [Committee of Adjustment](#)
Subject: [External] RE: A016/23 (10680 Islington Avenue) - Agency Circulation
Date: Tuesday, April 11, 2023 6:50:47 PM
Attachments: [image001.png](#)
[image003.png](#)

Hi Christine,

The Regional Municipality of York has completed its review of the above minor variance and has no comment.

Many thanks,

Our working hours may be different. Please do not feel obligated to reply outside of your scheduled working hours. Let's work together to help foster healthy work-life boundaries.

Niranjan Rajevan, M.Pl. | Associate Planner, Programs and Process Improvement, Planning and Economic Development, Corporate Services

The Regional Municipality of York | 17250 Yonge Street | Newmarket, ON L3Y 6Z1
1-877-464-9675 ext. 71521 | niranjan.rajevan@york.ca | www.york.ca

Our Values: Integrity, Commitment, Accountability, Respect, Excellence



Our Mission: **Working together to serve our thriving communities – today and tomorrow**

SCHEDULE C: PUBLIC & APPLICANT CORRESPONDENCE

None