

<b>ITEM: 6.6</b>	<b>COMMITTEE OF ADJUSTMENT REPORT SUMMARY</b> <b>MINOR VARIANCE APPLICATION</b> <b>FILE NUMBER A012/23</b> <b>UNIT 1 ON BLOCK 20, PLAN 65M 4761</b> <b>(HARTINGTON STREET</b>
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**THIS REPORT CONTAINS COMMENTS FROM THE FOLLOWING DEPARTMENTS & AGENCIES:**

\*Please see **Schedule B** of this report for a copy of Development Planning and Agency correspondence.

Additional comments from departments and agencies may be received after the publication of the Staff Report. These comments will be processed as an addendum and posted on the City's Website.

DEPARTMENTS	Circulated	Comments Received	Conditions	Nature of Comments
Committee of Adjustment	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Building Standards (Zoning Review)	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Building Inspection (Septic)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	No Comments Received to Date
Development Planning	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Recommend Approval/No Conditions
Development Engineering	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Recommend Approval/No Conditions
Parks, Forestry and Horticulture Operations	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
By-law & Compliance, Licensing & Permits	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	No Comments Received to Date
Development Finance	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Real Estate	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Fire Department	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	No Comments Received to Date
AGENCIES	Circulated	Comments Received	Conditions	Nature of Comments
TRCA	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	Recommend Approval w/Conditions
Ministry of Transportation (MTO)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Region of York	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Alectra	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Bell Canada	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	No Comments Received to Date
YRDSB	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
YCDSB	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
CN Rail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
CP Rail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
TransCanada Pipeline	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	No Comments Received to Date
Metrolinx	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Propane Operator	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	

**PUBLIC & APPLICANT CORRESPONDENCE**

\*Please see **Schedule C** of this report for a copy of the public & applicant correspondence listed below.

The deadline to submit public comments is **noon on the last business day prior to the scheduled hearing date.**

Comments and written public submissions received after the publication of this Staff Report will be processed as an addendum and posted on the City's Website.

All personal information collected because of this public meeting (including both written and oral submissions) is collected under the authority of the Municipal Act, the Municipal Freedom of Information and Protection of Privacy Act (MFIPPA), the Planning Act and all other relevant legislation, and will be used to assist in deciding on this matter. All personal information (as defined by MFIPPA), including (but not limited to) names, addresses, opinions and comments collected will become property of the City of Vaughan, will be made available for public disclosure (including being posted on the internet) and will be used to assist the Committee of Adjustment and staff to process this application.

Correspondence Type	Name	Address	Date Received (mm/dd/yyyy)	Summary
None				

**PREVIOUS COA DECISIONS ON THE SUBJECT LAND**

\*Please see **Schedule D** for a copy of the Decisions listed below

<b>File Number</b>	<b>Date of Decision</b> MM/DD/YYYY	<b>Decision Outcome</b>
None		

**ADJOURNMENT HISTORY**

\* Previous hearing dates where this application was adjourned by the Committee and public notice issued.

None	
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**COMMITTEE OF ADJUSTMENT REPORT  
MINOR VARIANCE APPLICATION  
A012/23  
UNIT 1 ON BLOCK 20, PLAN 65M 4761  
(HARTINGTON STREET)**

<b>ITEM NUMBER: 6.6</b>	<b>CITY WARD #: 1</b>
<b>APPLICANT:</b>	Nashville Developments (South) Inc.
<b>AGENT:</b>	TACC Developments
<b>PROPERTY:</b>	Unit 1 on Block 20, Plan 65M 4761 (Hartington Street)
<b>ZONING DESIGNATION:</b>	See Below
<b>VAUGHAN OFFICIAL PLAN (2010) DESIGNATION:</b>	Vaughan Official Plan 2010 ('VOP 2010'): "Mid-Rise Mixed-Use," Volume 1, and "Mid-Rise Mixed-Use B," Volume 2, Section 12.7 Block 61 West – Nashville Heights
<b>RELATED DEVELOPMENT APPLICATIONS:</b>	None
<b>PURPOSE OF APPLICATION:</b>	Relief from the Zoning By-law is being requested to permit a reduced exterior side yard setback and a reduced site triangle setback for Unit 1 on Block 20, Plan 65M 4761 to permit the construction of a proposed townhouse.

The following variances have been requested from the City's Zoning By-law:

**The subject lands are zoned RT1 Residential Townhouse, and subject to the provisions of Exception 9(1510) under Zoning By-law 1-88, as amended.**

**The subject lands are zoned RT Residential Townhouse, and subject to the provisions of Exception No. 14.1006 under Zoning By-law 001-2021 as amended.**

<b>Zoning By-law 1-88</b>		<b>Variance requested</b>
1	A minimum exterior side yard of 4.5 metres for Townhouse 1 Schedule "A3"	To permit a minimum exterior side yard of 3.96 metres for Townhouse 1.
2	A minimum of 3.0 metres is required to a site triangle. Schedule "A3"	To permit a minimum of 2.70 metres to the site triangle for Townhouse 1.

<b>Zoning By-law 001-2021</b>		<b>Variance requested</b>
	N/A	

**HEARING INFORMATION**

**DATE OF MEETING:** Thursday, March 16, 2023  
**TIME:** 6:00 p.m.  
**MEETING LOCATION:** Vaughan City Hall, Council Chamber, 2141 Major Mackenzie Drive, Vaughan  
**LIVE STREAM LINK:** [Vaughan.ca/LiveCouncil](http://Vaughan.ca/LiveCouncil)

**PUBLIC PARTICIPATION**

If you would like to speak to the Committee of Adjustment at the meeting, either remotely or in person, please complete the [Request to Speak Form](#) and submit to [cofa@vaughan.ca](mailto:cofa@vaughan.ca)  
 If you would like to submit written comments, please quote file number above and submit by mail or email to:  
**Email:** [cofa@vaughan.ca](mailto:cofa@vaughan.ca)

**Mail:** City of Vaughan, Office of the City Clerk, Committee of Adjustment, 2141 Major Mackenzie Drive, Vaughan, ON, L6A 1T1

**THE DEADLINE TO REGISTER TO SPEAK OR SUBMIT WRITTEN COMMENTS ON THE ABOVE NOTED FILE(S) IS NOON ON THE LAST BUSINESS DAY BEFORE THE MEETING.**

## INTRODUCTION

Staff and Agencies act as advisory bodies to the Committee of Adjustment. The comments contained in this report are presented as recommendations to the Committee.

Section 45(1) of the Planning Act sets the criteria for authorizing minor variances to the City of Vaughan's Zoning By-law. Accordingly, review of the application may consider the following:

- That the general intent and purpose of the by-law will be maintained.
- That the general intent and purpose of the official plan will be maintained.
- That the requested variance(s) is/are acceptable for the appropriate development of the subject lands.
- That the requested variance(s) is/are minor in nature.

Public written and oral submissions relating to this application are taken into consideration by the Committee of Adjustment as part of its deliberations and final decision on this matter.

## COMMITTEE OF ADJUSTMENT COMMENTS

<b>Date Public Notice Mailed:</b>	March 3, 2023
<b>Date Applicant Confirmed Posting of Sign:</b>	February 27, 2023
<b>Applicant Justification for Variances:</b> <small>*As provided by Applicant in Application Form</small>	The reason for the variance request is that the Building code changed to require larger setbacks of overhangs, increasing interior sideyard setback, "pushing" the building envelope into the sight triangle setback.
<b>Adjournment Requests (from staff):</b> <small>*Adjournment requests provided to applicant prior to issuance of public notice</small>	None
<b>Was a Zoning Review Waiver (ZRW) Form submitted by Applicant:</b>  <small>*ZRW Form may be used by applicant in instances where a revised submission is made, and zoning staff do not have an opportunity to review and confirm variances prior to the issuance of public notice.  *A revised submission may be required to address staff / agency comments received as part of the application review process.  *Where a zoning review has not been completed on a revised submission, an opportunity is provided to the applicant to adjourn the proposal prior to the issuance of public notice.</small>	No
<b>Adjournment Fees:</b> In accordance with Procedural By-law 069-2019, an Adjournment Fee is applicable to reschedule an application after the issuance of public notice where a request for adjournment has been provided to the applicant prior to the issuance of public notice.  An Adjournment Fee can only be waived in instances where adjournment of an application is requested by the Committee or staff <b>after</b> the issuance of public notice.	
<b>Committee of Adjustment Comments:</b>	None
<b>Committee of Adjustment Recommended Conditions of Approval:</b>	That the applicant obtain a municipal address from the GIS Mapping Section of the Development Planning Department and that confirmation of address creation be provided to the Secretary Treasurer.

## BUILDING STANDARDS (ZONING) COMMENTS

\*\*See Schedule B for Building Standards (Zoning) Comments

<b>Building Standards Recommended Conditions of Approval:</b>	None
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## DEVELOPMENT PLANNING COMMENTS

\*\*See Schedule B for Development Planning Comments.

<b>Development Planning Recommended Conditions of Approval:</b>	None
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## DEVELOPMENT ENGINEERING COMMENTS

[Link to Grading Permit](#)   [Link to Pool Permit](#)   [Link to Curb Curt Permit](#)   [Link Culvert Installation](#)

The Development Transportation Engineering Division has reviewed the above noted Minor Variance Application and have no objections to Items # 2 on the zoning report dated February 10th, 2023.

### DEVELOPMENT ENGINEERING COMMENTS

[Link to Grading Permit](#)   [Link to Pool Permit](#)   [Link to Curb Curt Permit](#)   [Link Culvert Installation](#)

Development Engineering Department does not object to the Minor Variance application A012/23.

<b>Development Engineering Recommended Conditions of Approval:</b>	None
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### PARKS, FORESTRY & HORTICULTURE (PFH) COMMENTS

Forestry has no comment.

<b>PFH Recommended Conditions of Approval:</b>	None
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### DEVELOPMENT FINANCE COMMENTS

That the payment of the City Development Charge is payable to the City of Vaughan prior to issuance of a building permit in accordance with the Development Charges Act and City-wide Development Charge By-law in effect at time of payment.

That the payment of Region of York Development Charge is payable to the City of Vaughan prior to issuance of a building permit in accordance with the Development Charges Act and Regional Development Charges By-laws in effect at time of payment.

That the payment of Education Development Charge is payable to the City of Vaughan prior to issuance of a building permit in accordance with the Education Act and York Region District School Board and York Catholic District School Board Development Charges By-laws in effect at time of payment.

That the payment of applicable Area Specific Development Charges are payable to the City of Vaughan prior to issuance of a building permit in accordance with the Development Charges Act and Area Specific Development Charge By-laws in effect at time of payment.

<b>Development Finance Recommended Conditions of Approval:</b>	None
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### BY-LAW AND COMPLIANCE, LICENSING AND PERMIT SERVICES COMMENTS

No comments received to date.

<b>BCLPS Recommended Conditions of Approval:</b>	None
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### BUILDING INSPECTION (SEPTIC) COMMENTS

No comments received to date.

<b>Building Inspection Recommended Conditions of Approval:</b>	None
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### FIRE DEPARTMENT COMMENTS

No comments received to date.

<b>Fire Department Recommended Conditions of Approval:</b>	None
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### SCHEDULES TO STAFF REPORT

\*See Schedule for list of correspondence

<b>Schedule A</b>	Drawings & Plans Submitted with the Application
<b>Schedule B</b>	Staff & Agency Comments
<b>Schedule C</b> (if required)	Correspondence (Received from Public & Applicant)
<b>Schedule D</b> (if required)	Previous COA Decisions on the Subject Land

Should the Committee find it appropriate to approve this application in accordance with request and the sketch submitted with the application, as required by Ontario Regulation 200/96, the following conditions have been recommended:

## SUMMARY OF RECOMMENDED CONDITIONS OF APPROVAL

All conditions of approval, unless otherwise stated, are considered to be incorporated into the approval “if required”. If a condition is no longer required after an approval is final and binding, the condition may be waived by the respective department or agency requesting conditional approval. A condition cannot be waived without written consent from the respective department or agency.

#	DEPARTMENT / AGENCY	CONDITION(S) DESCRIPTION
1	Committee of Adjustment <a href="mailto:christine.vigneault@vaughan.ca">christine.vigneault@vaughan.ca</a>	That the applicant obtain a municipal address from the GIS Mapping Section of the Development Planning Department and that confirmation of address creation be provided to the Secretary Treasurer.
2	TRCA <a href="mailto:Kristen.Regier@trca.ca">Kristen.Regier@trca.ca</a>	That the applicant provides the required fee amount of \$1250.00 payable to the Toronto and Region Conservation Authority.

## IMPORTANT INFORMATION – PLEASE READ

**CONDITIONS:** It is the responsibility of the owner/applicant and/or authorized agent to obtain and provide a clearance letter from respective department and/or agency (**see condition chart above for contact**). This letter must be provided to the Secretary-Treasurer to be finalized. All conditions must be cleared prior to the issuance of a Building Permit.

**APPROVALS:** Making any changes to your proposal after a decision has been made may impact the validity of the Committee’s decision.

An approval obtained from the Committee of Adjustment, where applicable, is tied to the building envelope shown on the plans and drawings submitted with the application and subject to the variance approval.

A building envelope is defined by the setbacks of the buildings and/or structures shown on the plans and drawings submitted with the application, as required by Ontario Regulation 200/96. Future development outside of an approved building envelope, where a minor variance was obtained, must comply with the provisions of the City’s Zoning By-law.

Elevation drawings are provided to reflect the style of roof (i.e. flat, mansard, gable etc.) to which a building height variance has been applied. Where a height variance is approved, building height is applied to the style of roof (as defined in the City’s Zoning By-law) shown on the elevation plans submitted with the application.

Architectural design features that are not regulated by the City’s Zoning By-law are not to be considered part of an approval unless specified in the Committee’s decision.

**DEVELOPMENT CHARGES:** That the payment of the Regional Development Charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the Regional Development Charges By-law in effect at the time of payment.

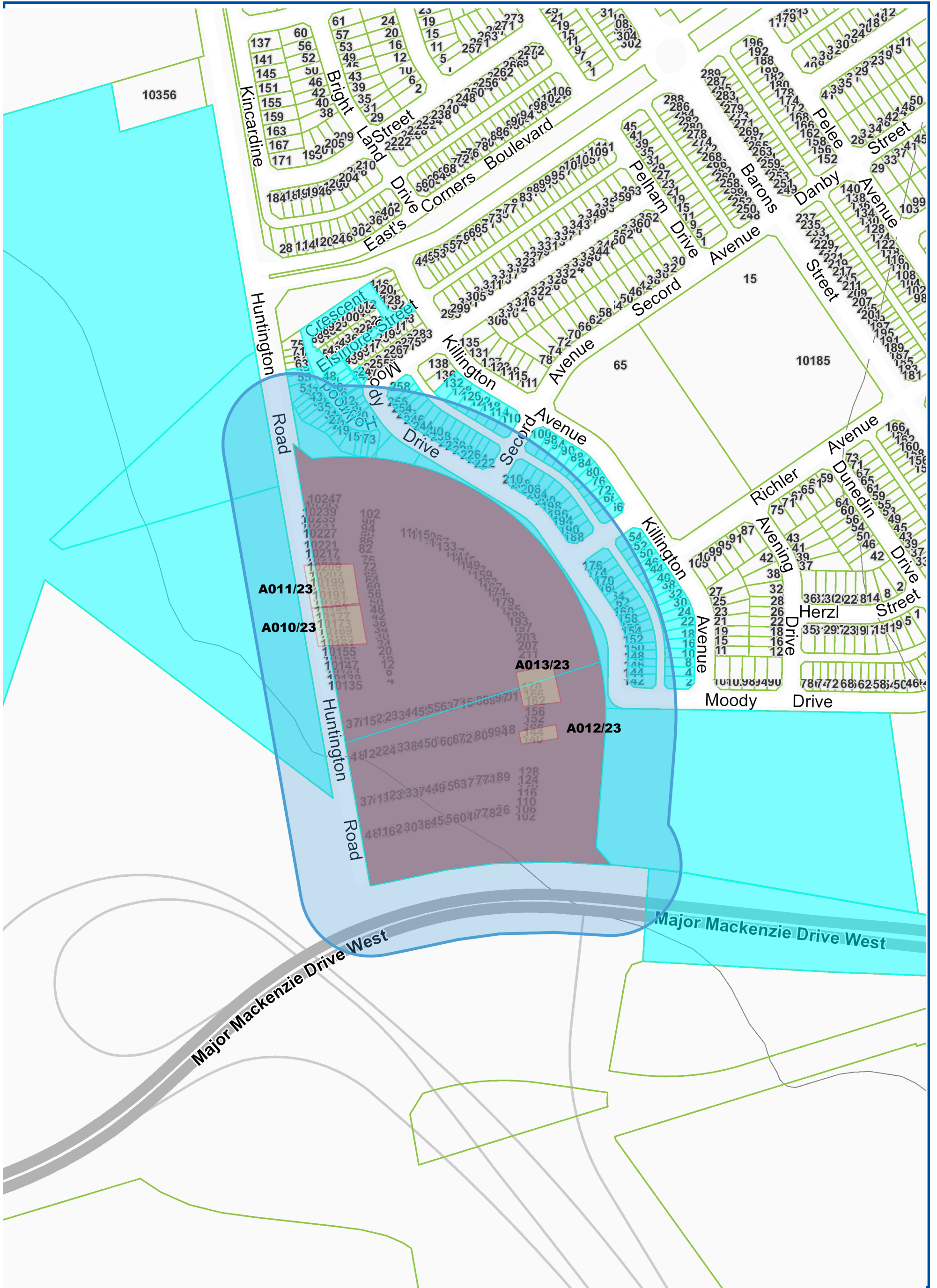
That the payment of the City Development Charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the City’s Development Charges By-law in effect at the time of payment.

That the payment of the Education Development Charge if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the Boards of Education By-laws in effect at the time of payment

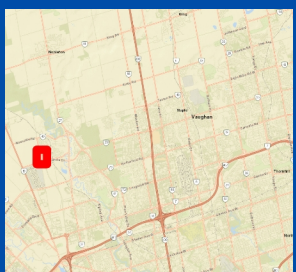
That the payment of Special Area Development charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and The City’s Development Charge By-law in effect at the time of Building permit issuance to the satisfaction of the Reserves/Capital Department.

**NOTICE OF DECISION:** If you wish to be notified of the decision in respect to this application or a related Ontario Land Tribunal (OLT) hearing you must complete a Request for Decision form and submit to the Secretary Treasurer (ask staff for details). In the absence of a written request to be notified of the Committee’s decision you will **not** receive notice.

**SCHEDULE A: DRAWINGS & PLANS**



Map Information:



Title: 10161-10203 HUNTINGTON ROAD,  
140, 162, 166 & 170 HARTINGTON STREET  
NOTIFICATION MAP - A010/23-A013/23

Disclaimer:

Every reasonable effort has been made to ensure that the information appearing on this map is accurate and current. We believe the information to be reliable, however the City of Vaughan assumes no responsibility or liability due to errors or omissions. Please report any discrepancies to Infrastructure Programming.



Scale: 1: 4,514  
0 0.07 km



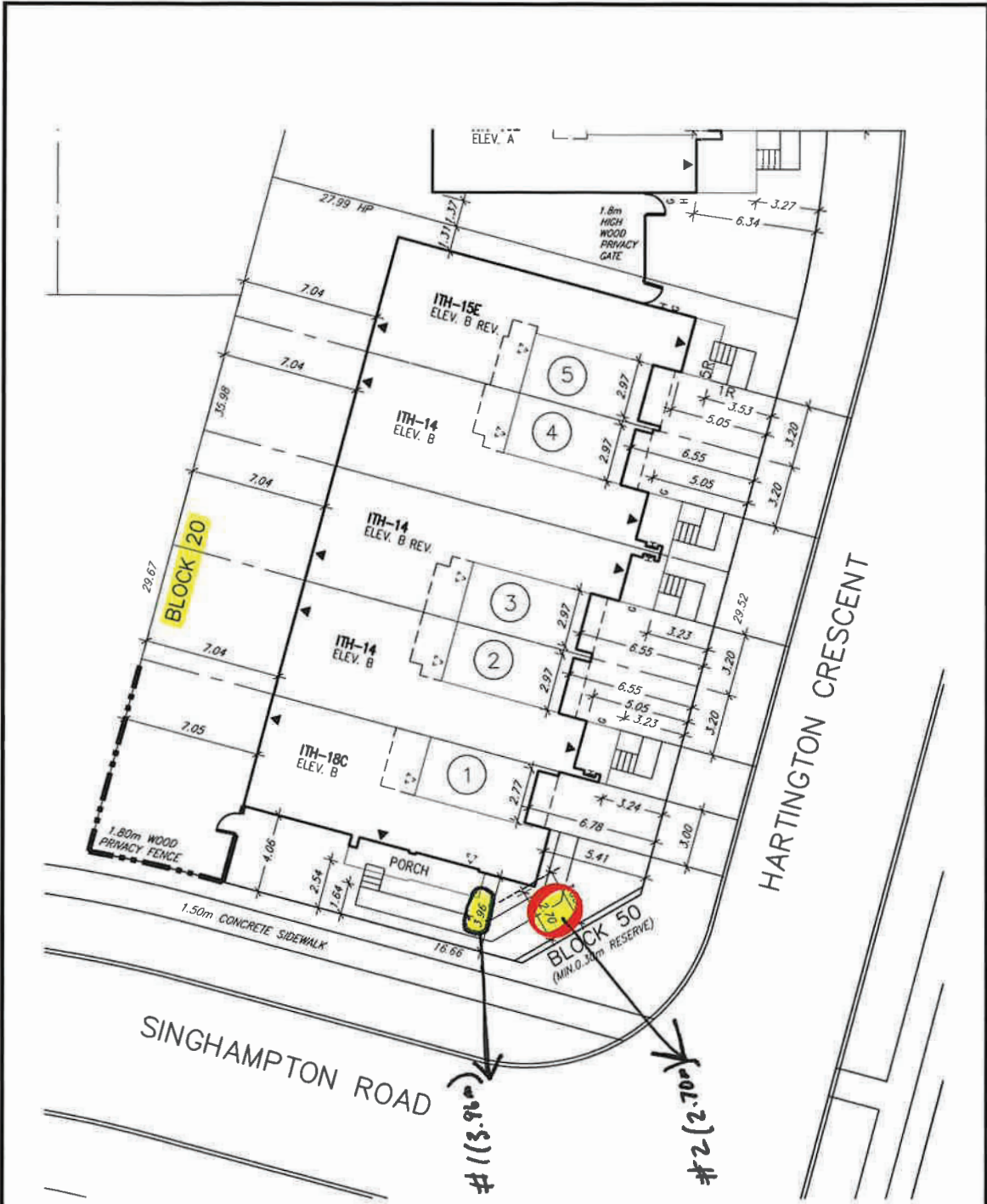
Created By:  
Infrastructure Delivery  
Department  
February 22, 2023 10:51 AM

Projection:  
NAD 83  
UTM Zone  
17N



**PLANS PREPARED AND VARIANCES IDENTIFIED BY APPLICANT**

**A012/23**



**SITE STATISTICS**  
*(STATISTICS BASED ON ENTIRE BUILDING)*

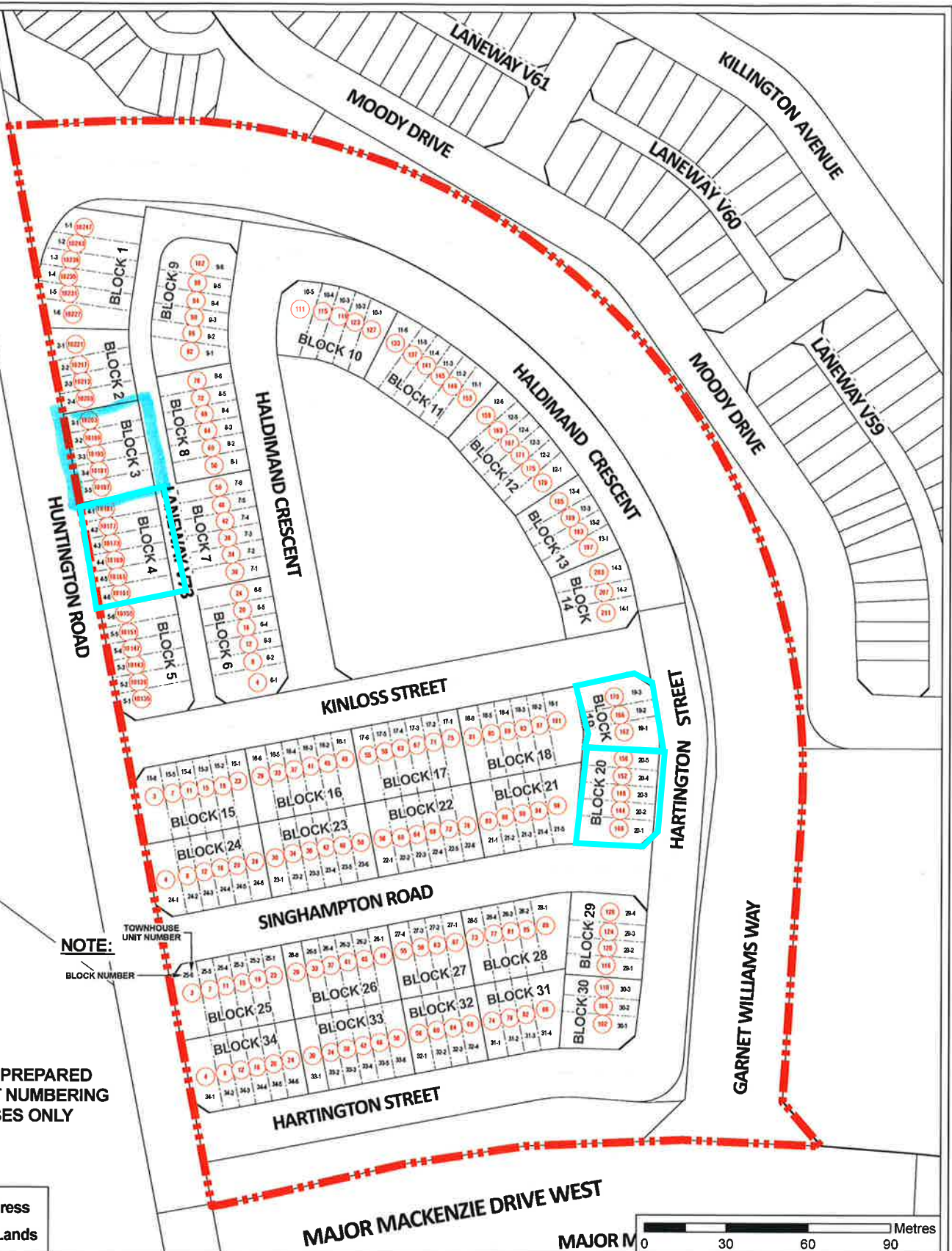
LOT FRONTAGE AT 6.00m SETBACK:	35.70m
LOT DEPTH:	27.99m
LOT AREA:	987.74m <sup>2</sup>
GROUND FLOOR COVERAGE INCLUDING PORCH:	488.30m <sup>2</sup>
LOT COVERAGE:	49.44%



**A1**

 255 Consumers Rd Suite 120 Toronto ON M2J 1R4 t 416.630.2255 f 416.630.4782 vo3design.com	<b>FIELDGATE HOMES</b>		lot/block no. <b>BLOCK 20</b> registered plan no.
	project name <b>NASHVILLE HEIGHTS PHASE 7</b>	municipality <b>KLEINBURG, ONT.</b>	project no. <b>21014</b>
date 2022-12	checked by STEVE SOSTARIC	scale 1:250	drawing no. <b>1</b>
Builder to verify location of all hydrants, street lights, transformers and other services. If minimum dimensions are not maintained, builder is to relocate at his own expense. Builder to verify service connection elevations prior to constructing foundations.			SITE PLAN file name 21014-SP-COA

All drawings, specifications, related documents and design are the copyright property of VA3 DESIGN. Reproduction of this property in whole or in part is strictly prohibited without VA3 DESIGN's written permission.



**NOTE:**  
 TOWNHOUSE UNIT NUMBER  
 BLOCK NUMBER

**THIS PLAN PREPARED FOR STREET NUMBERING PURPOSES ONLY**

 New Address  
 Subject Lands



# New Municipal Addresses

**LOCATION:**  
 Part of Lot 22, Concession 9  
 City Block 61

**APPLICANT:**  
 Nashville Developments Inc.

## PLAN 65M-4761

**DATE:**  
 January 4, 2023

**FILE:**  
 19T-19V001



**SCHEDULE B: STAFF & AGENCY COMMENTS**

<b>DEPT/AGENCY</b>	<b>Circulated</b>	<b>Comments Received</b>	<b>Conditions</b>	<b>Nature of Comments</b>
TRCA *Schedule B	X	X	X	General Comments w/condition
Ministry of Transportation (MTO) *Schedule B				No Comments Received to Date
Region of York *Schedule B	X	X		General Comments
Alectra *Schedule B	X	X		General Comments
Bell Canada *Schedule B	X			No Comments Received to Date
YRDSB *Schedule B				
YCDSB *Schedule B				
CN Rail *Schedule B				
CP Rail *Schedule B				
TransCanada Pipeline *Schedule B	X			No Comments Received to Date
Metrolinx *Schedule B				
Propane Operator *Schedule B				
Development Planning	X	X		Recommend Approval/no conditions
Building Standards (Zoning)	X	X		General Comments

**Date:** February 24<sup>th</sup> 2023

**Attention:** **Christine Vigneault**

**RE:** Request for Comments

**File No.:** **A012-23 & A013-23**

**Related Files:**

**Applicant** Aaron Hershoff

**Location** Block 4, 65M 4761 (Huntington Road)



Discover the possibilities

### COMMENTS:

- We have reviewed the proposed Variance Application and have no comments or objections to its approval.
- We have reviewed the proposed Variance Application and have no objections to its approval, subject to the following comments (attached below).
- We have reviewed the proposed Variance Application and have the following concerns (attached below).

Alectra Utilities (formerly PowerStream) has received and reviewed the proposed Variance Application. This review, however, does not imply any approval of the project or plan.

All proposed billboards, signs, and other structures associated with the project or plan must maintain minimum clearances to the existing overhead or underground electrical distribution system as specified by the applicable standards, codes and acts referenced.

In the event that construction commences, and the clearance between any component of the work/structure and the adjacent existing overhead and underground electrical distribution system violates the Occupational Health and Safety Act, the customer will be responsible for 100% of the costs associated with Alectra making the work area safe. All construction work will be required to stop until the safe limits of approach can be established.

In the event construction is completed, and the clearance between the constructed structure and the adjacent existing overhead and underground electrical distribution system violates the any of applicable standards, acts or codes referenced, the customer will be responsible for 100% of Alectra's cost for any relocation work.

### References:

- Ontario Electrical Safety Code, latest edition (Clearance of Conductors from Buildings)
- Ontario Health and Safety Act, latest edition (Construction Protection)
- Ontario Building Code, latest edition (Clearance to Buildings)
- PowerStream (Construction Standard 03-1, 03-4), attached
- Canadian Standards Association, latest edition (Basic Clearances)

If more information is required, please contact either of the following:

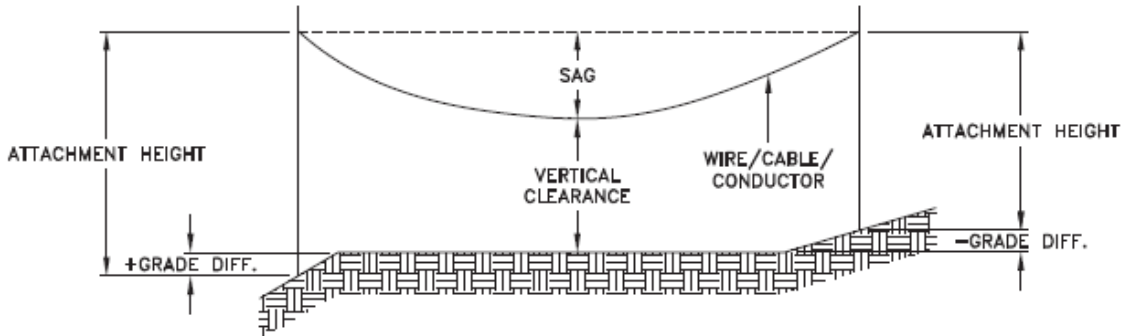
Mr. Stephen Cranley, C.E.T  
Supervisor, Distribution Design, ICI & Layouts (North)  
**Phone:** 1-877-963-6900 ext. 31297

**E-mail:** [stephen.cranley@alectrautilities.com](mailto:stephen.cranley@alectrautilities.com)

Mitchell Penner  
Supervisor, Distribution Design-Subdivisions  
**Phone:** 416-302-6215

**Email:** [Mitchell.Penner@alectrautilities.com](mailto:Mitchell.Penner@alectrautilities.com)

LOCATION OF WIRES, CABLES OR CONDUCTORS	SYSTEM VOLTAGE			
	SPAN GUYS AND COMMUNICATIONS WIRES	UP TO 600V AND NEUTRAL	4.16/2.4kV TO 27.6/16kV (SEE NOTE 1)	44kV
MINIMUM VERTICAL CLEARANCES (SEE NOTE 2)				
OVER OR ALONGSIDE ROADS, DRIVEWAYS OR LANDS ACCESSIBLE TO <u>VEHICLES</u>	442cm	442cm	480cm	520cm
OVER GROUND ACCESSIBLE TO <u>PEDESTRIANS AND BICYCLES ONLY</u>	250cm	310cm	340cm	370cm
ABOVE TOP OF RAIL AT <u>RAILWAY CROSSINGS</u>	730cm	730cm	760cm	810cm



MINIMUM ATTACHMENT HEIGHT = MAXIMUM SAG  
 + MINIMUM VERTICAL CLEARANCE (FROM ABOVE TABLE)  
 ± GRADE DIFFERENCE  
 + 0.3m (VEHICLE OR RAILWAY LOCATION)  
 + SNOW DEPTH (PEDESTRIAN LOCATION, SEE NOTE 3)

**NOTES:**

1. THE MULTIGROUNDED SYSTEM NEUTRAL HAS THE SAME CLEARANCE AS THE 600V SYSTEM.
2. THE VERTICAL CLEARANCES IN THE ABOVE TABLE ARE UNDER MAXIMUM SAG CONDITIONS.
3. REFER TO CSA STANDARD C22.3 No.1, ANNEX D FOR LOCAL SNOW DEPTH VALUES.
4. ALL CLEARANCES ARE IN ACCORDANCE TO CSA STANDARD C22.3.

**CONVERSION TABLE**

METRIC	IMPERIAL (APPROX)
810cm	27'-0"
760cm	25'-4"
730cm	24'-4"
520cm	17'-4"
480cm	16'-0"
442cm	15'-5"
370cm	12'-4"
340cm	11'-4"
310cm	10'-4"
250cm	8'-4"

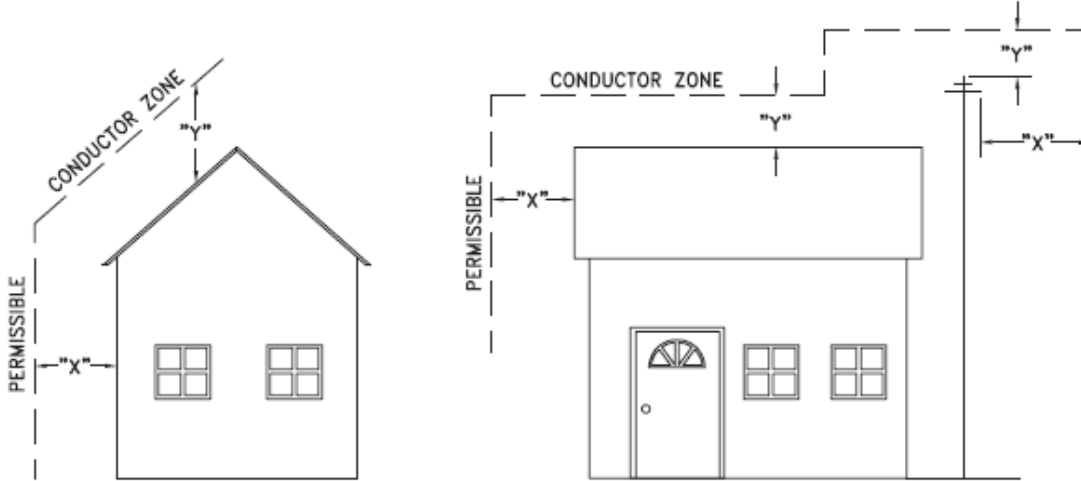
**REFERENCES**

SAGS AND TENSIONS | SECTION 02

**MINIMUM VERTICAL CLEARANCES OF WIRES, CABLES AND CONDUCTORS ABOVE GROUND OR RAILS**

ORIGINAL ISSUE DATE: 2010-DEC-24 REVISION NO: R1 REVISION DATE: 2012-JAN-09

Certificate of Approval	
This construction Standard meets the safety requirements of Section 4 of Regulation 22/04	
Joe Crozier, P.Eng.	2012-JAN-09
Name	Date
P.Eng. Approval By:	Joe Crozier



VOLTAGE	MINIMUM HORIZONTAL CLEARANCE UNDER MAXIMUM SWING CONDITIONS DIMENSION "X" (SEE NOTES 1, 3 & 4)	MINIMUM VERTICAL CLEARANCE UNDER MAXIMUM DESIGN SAG CONDITIONS DIMENSION "Y" (SEE NOTES 1, 2, 4 & 5)
0-600V AND NEUTRAL	100cm	250cm
4.16/2.4 TO 44kV	300cm	480cm

**NOTES**

1. UNDER NO CIRCUMSTANCES SHALL A CONDUCTOR BE PERMITTED TO PENETRATE THE ENVELOPE SHOWN BY THE DOTTED LINE.
2. THE VERTICAL CLEARANCES ARE UNDER CONDITIONS OF MAXIMUM DESIGN SAG.
3. THE HORIZONTAL CLEARANCES ARE UNDER CONDITIONS OF MAXIMUM SWING. WHERE THE CONDUCTOR SWING IS NOT KNOWN A HORIZONTAL CLEARANCE OF 480CM SHALL BE USED.
4. BUILDINGS THAT EXCEED 3 STOREYS OR 15M IN HEIGHT, THE MINIMUM HORIZONTAL CLEARANCE OF THE SECONDARY CONDUCTORS SHOULD BE INCREASED TO 300cm WHERE IT IS NECESSARY TO ALLOW FOR THE RAISING OF LADDERS BY LOCAL FIRE DEPARTMENTS.
5. IN SITUATIONS SUCH AS MULTI-LEVEL GARAGES, WHERE ROOFS ARE NORMALLY USED BY PERSONS AND VEHICLES, THE VERTICAL CLEARANCES OF POWERSTREAM STANDARD 03-1 SHALL APPLY.
6. DISTRIBUTION LINES CONSTRUCTED NEAR BUILDINGS SHALL BE BUILT TO AVOID OVERHANG WHEREVER POSSIBLE. WHERE LINES MUST BE CONSTRUCTED OVER OR ADJACENT TO BUILDINGS THE APPLICABLE HORIZONTAL AND VERTICAL CLEARANCES SHALL BE AT CONDITIONS OF MAXIMUM CONDUCTOR SWING AND MAXIMUM SAG. THE ABOVE CLEARANCES ARE DESIGNED TO PREVENT PERSONS ON OR IN BUILDINGS AS WELL AS EXTERNAL MACHINERY USED IN CONJUNCTION WITH A BUILDING TO COME IN CONTACT WITH CONDUCTORS. EFFORTS SHOULD BE MADE TO INCREASE THESE CLEARANCES WHERE POSSIBLE.
7. ALL CLEARANCES ARE IN ACCORDANCE TO CSA C22.3 NO.1-06 (TABLE-9).

METRIC	IMPERIAL (APPROX)
480cm	16'-0"
300cm	10'-0"
250cm	8'-4"
100cm	3'-4"

**MINIMUM VERTICAL & HORIZONTAL CLEARANCES OF CONDUCTORS FROM BUILDINGS OR OTHER PERMANENT STRUCTURES (CONDUCTORS NOT ATTACHED TO BUILDINGS)**

**Certificate of Approval**  
This construction Standard meets the safety requirements of Section 4 of Regulation 22/04  
Debbie Dadwani, P.Eng. 2010-MAY-05  
Name Date  
P. Eng. Approval By: *D. Dadwani*

ORIGINAL ISSUE DATE: 2010-MAY-05 REVISION NO: REVISION DATE:  
PS:\System Planning and Standards\Standard Design\PowerStream Standards\PowerStream Standards working files\Section 3-1-02\WG 03-4 R0 May 5, 2010.dwg, 5/5/2010 10:22:02 AM, Adobe PDF

**To:** Committee of Adjustment  
**From:** Pia Basilone, Building Standards Department  
**Date:** February 10, 2023  
**Applicant:** Aaron Hershoff  
**Location:** CONC 3 Lot 20 municipally known as Block 20, 65M 4761 (Huntington Road)  
**File No.(s):** A012/23

**Zoning Classification:**

The subject lands are zoned RT1 Residential Townhouse, and subject to the provisions of Exception 9(1510) under Zoning By-law 1-88, as amended.  
 The subject lands are zoned RT Residential Townhouse, and subject to the provisions of Exception No. 14.1006 under Zoning By-law 001-2021 as amended.

Zoning By-law 1-88		Variance requested
1	A minimum exterior side yard of 4.5 metres for Townhouse 1. Schedule "A3"	To permit a minimum exterior side yard of 3.96 metres for Townhouse 1.
2	A minimum of 3.0 metres is required to a site triangle. Schedule "A3"	To permit a minimum of 2.70 metres to the site triangle for Townhouse 1.

Zoning By-law 001-2021		Variance requested
	N/A	

**Staff Comments:**

**Stop Work Order(s) and Order(s) to Comply:**

There are no outstanding Orders on file.

**Other Comments to Applicant:**

General Comments	
1	The applicant shall be advised that additional variances may be required upon review of detailed drawing for building permit/site plan approval.
2	This application has been deemed to be transitioned under Section 1.6.2.6. of By-law 001-2021 as amended.

**Conditions of Approval:**

none

\* Comments are based on the review of documentation supplied with this application.



**To:** Christine Vigneault, Committee of Adjustment Secretary Treasurer  
**From:** Nancy Tuckett, Director of Development Planning  
**Date:** March 8, 2023  
**Name of Owner:** Nashville Developments (South) Inc.  
**Location:** Unit 1, Block 20, 65M-4761 (Hartington Street)  
**File No.(s):** A012/23

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**Proposed Variance(s) (By-law 1-88):**

1. To permit a minimum exterior side yard of 3.96 m for Townhouse Unit 1.
2. To permit a minimum of 2.70 m to the sight triangle for Townhouse Unit 1.

**By-Law Requirement(s) (By-law 1-88):**

1. A minimum exterior side yard of 4.5 m for Townhouse Unit 1.
2. A minimum of 3.0 m is required to a sight triangle.

**Official Plan:**

City of Vaughan Official Plan 2010 ("VOP 2010"): "Mid-Rise Mixed-Use", Volume 1, and "Mid-Rise Mixed-Use B", Volume 2, Section 12.7 Block 61 West – Nashville Heights

**Comments:**

The Owner is requesting relief to permit the construction of a townhouse unit (Unit 1) on Block 20 of Registered Plan of Subdivision 65M-4761, with the above noted variances.

The Development Planning Department has no objection to Variance 1 for the exterior side yard of Townhouse Unit 1 as the reduced exterior side yard will not be perceptible nor have adverse impacts to the streetscape. The setback reduction applies to the southeastern corner of the dwelling. Therefore, it is the edge of the corner that will utilize the full 0.3 m extent of the requested relief. A 0.3 m wide reserve from which the exterior side yard setback is measured is opposite this corner. As the width of the reserve is equal to the maximum length of the projection, it is not anticipated that the variance will be perceptible. The setback has also been reviewed by the Urban Design Division of the Development Planning Department and is considered appropriate to facilitate the development.

The Development Planning Department has no objection to Variance 2 for the proposed sight triangle, given that the proposal has been reviewed by the Transportation Engineering Division of the Development Engineering Department and they have no concerns.

Accordingly, the Development Planning Department can support the required variances and is of the opinion that the proposal is minor in nature, maintains the general intent and purpose of the Official Plan and Zoning By-law, and is desirable for the appropriate development of the land.

**Recommendation:**

The Development Planning Department recommends approval of the application.

**Conditions of Approval:**

If the Committee finds merit in the application, the following conditions of approval are recommended:

None

**Comments Prepared by:**

Joshua Cipolletta, Planner I  
David Harding, Senior Planner

March 7, 2023

X-Ref: 61460, 43665, 66448.14

**SENT BY E-MAIL: Christine.Vigneault@vaughan.ca**

Christine Vigneault  
Secretary Treasurer  
Committee of Adjustment  
City of Vaughan  
2141 Major Mackenzie Drive  
Vaughan, Ontario L6A 1T1

Dear Christine:

**Re: Minor Variance Applications A010/23, A012/23, A013/23  
Concession 3, Lot 20  
Block 4, Block 20, Block 19, 65M 4761 (Huntington Road)  
City of Vaughan, Region of York  
Owner: Aaron Hershoff**

This letter acknowledges receipt of the above-noted applications circulated by the City of Vaughan. The materials were received by Toronto and Region Conservation Authority (TRCA) on February 21, 2023. TRCA staff have reviewed the applications and offer the following comments for the consideration of the Committee of Adjustment.

**Purpose of the Application**

It is our understanding that the purpose of the above noted applications is to request the following variances under By-Law 1-88:

By-Law 1-88:

- To permit a minimum rear yard of 1.0 m to the garage in Block 4, whereas a minimum rear yard of 7.5 metres is required to the garage.
- To permit a maximum garage width of 5.54 metres for Townhouse 1 and 5 in Block 4, whereas a maximum garage width of 3.048 metres is permitted for lots less than a lot frontage of 11.0 m and for corner lots greater than 14.0 m.
- To permit a minimum exterior side yard of 3.96 m for Townhouse 1 in Block 20, whereas a minimum exterior side yard of 4.5 m is permitted.
- To permit a minimum of 2.70 m to the site triangle for Townhouse 1 in Block 20, whereas a minimum of 3.0 m is required to a site triangle.
- To permit a minimum exterior side yard of 3.96 m for Townhouse 3 in Block 19, whereas a minimum exterior side yard of 4.5 m is required.
- To permit a minimum of 2.40 m to the site triangle for Townhouse 3 in Block 19, whereas a minimum of 3.0 m is required to a site triangle.

The noted variances are being requested to facilitate the construction of 6 townhouse units in Block 4, 1 townhouse unit in Block 20, and 3 townhouse units in Block 19.

### **Ontario Regulation 166/06**

The lands subject to the above noted applications are not located within TRCA's Regulated Area. As such a permit pursuant to Ontario Regulation 166/06 would not be required for any development or site alteration.

### **Application-Specific Comments**

TRCA staff were previously involved in the review of *Planning Act* applications for the subject lands including Draft Plan of Subdivision 19T-19V001, Zoning By-law Amendment Z.19.004, and Official Plan Amendment OP.19.002. TRCA provided conditions of approval for draft plan of subdivision 19T-19V001 on July 22, 2021. A permit pursuant to Ontario Regulation 166/06 was subsequently approved and issued addressing those conditions. The permit was issued for watercourse alteration/enclosure and final grading/servicing works associated with the subdivision project (TRCA Permit No. C-211017, issued on August 27, 2021, revised on September 29, 2021 & October 8, 2021). A clearance letter was also sent to the City of Vaughan on September 15, 2022, confirming staff had no objection to the clearance of the Draft Approved Plan of Subdivision.

Based on a review of the materials circulated with the current minor variance applications, staff are satisfied that the proposed variances do not impact TRCA's regulatory or planning policy interests. As such, TRCA staff has no concern with the proposed variances.

### **Fees**

By copy of this letter, the applicant is advised that the TRCA has implemented a fee schedule for our planning application review services. This application is subject to a \$1250.00 (Minor Variance – Industrial/Commercial/Subdivision/Institutional - Minor) review fee. The applicant is responsible for fee payment and should forward the application fee to this office as soon as possible.

### **Recommendations**

Based on the comments noted above, TRCA provides conditional approval to the approval of Minor Variance Applications A010.23, A012.23, and A013.23 subject to the following conditions:

1. That the applicant provides the required fee amount of \$1250.00 payable to the Toronto and Region Conservation Authority.

We trust these comments are of assistance. Should you have any questions, please contact me at 437-880-2129 or at [Kristen.regier@trca.ca](mailto:Kristen.regier@trca.ca)

Sincerely,

*Kristen Regier*

Kristen Regier  
Planner I  
Development Planning and Permits

KR/sb

**From:** [Development Services](#)  
**To:** [Lenore Providence](#)  
**Cc:** [Committee of Adjustment](#)  
**Subject:** [External] RE: A012/23 Unit 1, Block 20 65M 4761 (140 Hartington Road) - REQUEST FOR COMMENTS, CITY OF VAUGHAN  
**Date:** Wednesday, March 1, 2023 5:01:05 PM  
**Attachments:** [image002.png](#)  
[image004.png](#)

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Hi Lenore,

The Regional Municipality of York has completed its review of the above minor variance and has no comment.

Please note regional comments would be sent through the associated Registered Plan of Subdivision (SUBR.22.V.0040) and Site Plan (SP.22.V.0267).

Many thanks,

*Our working hours may be different. Please do not feel obligated to reply outside of your scheduled working hours. Let's work together to help foster healthy work-life boundaries.*

**Niranjan Rajevan, M.Pl.** | Associate Planner, Programs and Process Improvement, Planning and Economic Development, Corporate Services

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The Regional Municipality of York | 17250 Yonge Street | Newmarket, ON L3Y 6Z1  
1-877-464-9675 ext. 71521 | [niranjan.rajevan@york.ca](mailto:niranjan.rajevan@york.ca) | [www.york.ca](http://www.york.ca)

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**SCHEDULE C: PUBLIC & APPLICANT CORRESPONDENCE**

None