

ITEM: 6.2	COMMITTEE OF ADJUSTMENT REPORT SUMMARY MINOR VARIANCE APPLICATION FILE NUMBER A001/23 320 MILANI BLVD WOODBRIDGE
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**THIS REPORT CONTAINS COMMENTS FROM THE FOLLOWING
DEPARTMENTS & AGENCIES:**

*Please see **Schedule B** of this report for a copy of Development Planning and Agency correspondence.

Additional comments from departments and agencies may be received after the publication of the Staff Report. These comments will be processed as an addendum and posted on the City's Website.

DEPARTMENTS	Circulated	Comments Received	Conditions	Nature of Comments
Committee of Adjustment	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Building Standards (Zoning Review)	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Building Inspection (Septic)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	No Comments Received to Date
Development Planning	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Recommend Approval/No Conditions
Development Engineering	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	Recommend Approval w/Conditions
Parks, Forestry and Horticulture Operations	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	General Comments w/condition
By-law & Compliance, Licensing & Permits	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	No Comments Received to Date
Development Finance	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Real Estate	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Fire Department	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	No Comments Received to Date
AGENCIES	Circulated	Comments Received	Conditions	Nature of Comments
TRCA	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	No Comments Received to Date
Ministry of Transportation (MTO)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Region of York	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Alectra	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Bell Canada	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	No Comments Received to Date
YRDSB	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
YCDSB	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
CN Rail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
CP Rail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
TransCanada Pipeline	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	No Comments Received to Date
Metrolinx	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Propane Operator	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	

PUBLIC & APPLICANT CORRESPONDENCE

*Please see **Schedule C** of this report for a copy of the public & applicant correspondence listed below.

The deadline to submit public comments is **noon on the last business day prior to the scheduled hearing date.**

Comments and written public submissions received after the publication of this Staff Report will be processed as an addendum and posted on the City's Website.

All personal information collected because of this public meeting (including both written and oral submissions) is collected under the authority of the Municipal Act, the Municipal Freedom of Information and Protection of Privacy Act (MFIPPA), the Planning Act and all other relevant legislation, and will be used to assist in deciding on this matter. All personal information (as defined by MFIPPA), including (but not limited to) names, addresses, opinions and comments collected will become property of the City of Vaughan, will be made available for public disclosure (including being posted on the internet) and will be used to assist the Committee of Adjustment and staff to process this application.

Correspondence Type	Name	Address	Date Received (mm/dd/yyyy)	Summary
Applicant/Agent	Goodman's		01/03/23	Application Cover Letter

PREVIOUS COA DECISIONS ON THE SUBJECT LAND

*Please see **Schedule D** for a copy of the Decisions listed below

File Number	Date of Decision	Decision Outcome
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PREVIOUS COA DECISIONS ON THE SUBJECT LAND

*Please see **Schedule D** for a copy of the Decisions listed below

	MM/DD/YYYY	
None		

ADJOURNMENT HISTORY

* Previous hearing dates where this application was adjourned by the Committee and public notice issued.

None	
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**COMMITTEE OF ADJUSTMENT REPORT
MINOR VARIANCE APPLICATION
A001/23
320 MILANI BLVD WOODBRIDGE**

ITEM NUMBER: 6.2	CITY WARD #:
APPLICANT:	MRJ Properties Inc.
AGENT:	Ian Andres Goodmans LLP
PROPERTY:	320 Milani Blvd Woodbridge ON L4H 4E3
ZONING DESIGNATION:	See Below
VAUGHAN OFFICIAL PLAN (2010) DESIGNATION:	Vaughan Official Plan 2010 ('VOP 2010'): "Employment Commercial Mixed-Use"
RELATED DEVELOPMENT APPLICATIONS:	None
PURPOSE OF APPLICATION:	Relief from the Zoning By-law is being requested to permit a reduced number of parking spaces as well as parking space length to accommodate the construction of a new corporate headquarters for the Mircom Group of Companies.

The following variances have been requested from the City's Zoning By-law:

The subject lands are zoned **GC- General Commercial Zone** and subject to the provisions of **Exception 14.988** under **Zoning By-law 001-2021**, as amended. This application is deemed to be **Transitioned** under section 1.6.2.6 of **Zoning By-law 001-2021** as amended.

#	Zoning By-law 001-2021	Variance requested
	None.	

The subject lands are zoned **EM3 – Retail Warehouse Employment Area Zone** and subject to the provisions of **Exception 9(1356)** under **Zoning By-law 1-88**, as amended.

	Zoning By-law 1-88	Variance requested
1	A minimum of 485 parking spaces are required [Exception 9(1356)].	To permit a minimum of 126 parking spaces.
2	A minimum parking space length of 6.0 metres is required [Section 2.0, definition of parking space]	To permit a minimum parking space length of 5.7 metres.

HEARING INFORMATION

DATE OF MEETING: Thursday, March 16, 2023
TIME: 6:00 p.m.
MEETING LOCATION: Vaughan City Hall, Council Chamber, 2141 Major Mackenzie Drive, Vaughan
LIVE STREAM LINK: Vaughan.ca/LiveCouncil

PUBLIC PARTICIPATION

If you would like to speak to the Committee of Adjustment at the meeting, either remotely or in person, please complete the [Request to Speak Form](#) and submit to cofa@vaughan.ca
 If you would like to submit written comments, please quote file number above and submit by mail or email to:

Email: cofa@vaughan.ca

Mail: City of Vaughan, Office of the City Clerk, Committee of Adjustment, 2141 Major Mackenzie Drive, Vaughan, ON, L6A 1T1

THE DEADLINE TO REGISTER TO SPEAK OR SUBMIT WRITTEN COMMENTS ON THE ABOVE NOTED FILE(S) IS NOON ON THE LAST BUSINESS DAY BEFORE THE MEETING.

INTRODUCTION

Staff and Agencies act as advisory bodies to the Committee of Adjustment. The comments contained in this report are presented as recommendations to the Committee.

INTRODUCTION

Section 45(1) of the Planning Act sets the criteria for authorizing minor variances to the City of Vaughan's Zoning By-law. Accordingly, review of the application may consider the following:

- That the general intent and purpose of the by-law will be maintained.
- That the general intent and purpose of the official plan will be maintained.
- That the requested variance(s) is/are acceptable for the appropriate development of the subject lands.
- That the requested variance(s) is/are minor in nature.

Public written and oral submissions relating to this application are taken into consideration by the Committee of Adjustment as part of its deliberations and final decision on this matter.

COMMITTEE OF ADJUSTMENT COMMENTS

Date Public Notice Mailed:	March 3, 2023	
Date Applicant Confirmed Posting of Sign:	February 27, 2023	
Applicant Justification for Variances: <small>*As provided by Applicant in Application Form</small>	<p>The parking ratio of 3.9 spaces per 100 square metres is excessive for an industrial and office building of the nature proposed by Mircom. This parking rate appears to have been imposed when the property and surrounding lands were rezoned to permit retail employment uses, which have much higher parking demand. The only parking required for the proposed Mircom facility is for employees and visitors, and Mircom believes that 127 spaces are more than sufficient for these purposes. The proposed building footprint is required to meet Mircom's operational needs. In order for additional parking to be provided, it would have to be located either on the rear portion of the property within the hydro easement (which we understand Hydro One is not currently willing to support) or in an underground garage (which is very expensive to build). While Mircom might be open to considering one level of underground parking, the additional parking spaces are not required from an operational standpoint and therefore the additional construction cost is not justified. It is also noted that By-law 001-2021 imposes a much lower parking rate in the comparable EM2 zone for office uses (1.4 spaces per 100 square metres) and no requirement for manufacturing and warehousing uses. If these requirements were applied, the proposed 127 parking spaces would be compliant. The proposed building height is necessary to accommodate racking and associated clearance space within the industrial portion of the building.</p>	
Adjournment Requests (from staff): <small>*Adjournment requests provided to applicant prior to issuance of public notice</small>	None	
Was a Zoning Review Waiver (ZRW) Form submitted by Applicant: <small>*ZRW Form may be used by applicant in instances where a revised submission is made, and zoning staff do not have an opportunity to review and confirm variances prior to the issuance of public notice. *A revised submission may be required to address staff / agency comments received as part of the application review process. *Where a zoning review has not been completed on a revised submission, an opportunity is provided to the applicant to adjourn the proposal prior to the issuance of public notice.</small>	No	
Adjournment Fees: In accordance with Procedural By-law 069-2019, an Adjournment Fee is applicable to reschedule an application after the issuance of public notice where a request for adjournment has been provided to the applicant prior to the issuance of public notice. An Adjournment Fee can only be waived in instances where adjournment of an application is requested by the Committee or staff after the issuance of public notice.		
Committee of Adjustment Comments:	None	
Committee of Adjustment Recommended Conditions of Approval:	None	

BUILDING STANDARDS (ZONING) COMMENTS

**See Schedule B for Building Standards (Zoning) Comments

Building Standards Recommended Conditions of Approval:	None
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DEVELOPMENT PLANNING COMMENTS

**See Schedule B for Development Planning Comments.

Development Planning Recommended Conditions of Approval:	None
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DEVELOPMENT ENGINEERING COMMENTS

[Link to Grading Permit](#) [Link to Pool Permit](#) [Link to Curb Curt Permit](#) [Link Culvert Installation](#)

Staff concur with the findings of the Parking Justification Study prepared by LEA Consulting on February 2023, supplemented by the additional clarification provided by LEA Consulting through email correspondence on February 23, 2023.

The Parking Justification Study utilized the parking observed at the proxy site of the applicant's existing Headquarter building located at 25 Interchange Way, which has the same use as the proposed site of Manufacturing or Processing Facility and Accessory Office. The use of an adjustment factor to compare the results from the proxy site to the ratio for required parking for the two uses in the Comprehensive Zoning By-Law 001-2021 provides adequate justification to the reduction in parking from the site-specific By-Law.

The proposed 126 parking spaces are deemed sufficient for the proposed use of a manufacturing facility of 7,286 m² GFA including an accessory office of 5,140 m² GFA at this site located at 320 Milani Boulevard.

The Development Engineering (DE) Department does not object to the variance application A001/23 subject to the following condition(s).

Development Engineering Recommended Conditions of Approval:	<p>The Owner/Applicant shall submit a grading/servicing plan and storm water management report for the proposed site, to the satisfaction of the Development Engineering Department, prior to final clearance of this condition.</p> <p>Staff have confirmed that the property is located within an unassumed subdivision. The Owner/Applicant shall obtain a signed and stamped letter from the Developer's Civil Engineering Consultant stating that the proposed service connection and stormwater management report have been reviewed and approved for the proposed site. The signed letter shall be provided to the satisfaction of the Development Engineering Department, prior to final clearance of this condition.</p>
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PARKS, FORESTRY & HORTICULTURE (PFH) COMMENTS

Recommended condition of approval:

PFH Recommended Conditions of Approval:	<p>Applicant/owner shall obtain a "Private Property Tree Removal & Protection" permit through the forestry division prior to any construction works on the subject property.</p>
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DEVELOPMENT FINANCE COMMENTS

That the payment of the City Development Charge is payable to the City of Vaughan prior to issuance of a building permit in accordance with the Development Charges Act and City-wide Development Charge By-law in effect at time of payment.

That the payment of Region of York Development Charge is payable to the City of Vaughan prior to issuance of a building permit in accordance with the Development Charges Act and Regional Development Charges By-laws in effect at time of payment.

That the payment of Education Development Charge is payable to the City of Vaughan prior to issuance of a building permit in accordance with the Education Act and York Region District School Board and York Catholic District School Board Development Charges By-laws in effect at time of payment.

DEVELOPMENT FINANCE COMMENTS

That the payment of applicable Area Specific Development Charges are payable to the City of Vaughan prior to issuance of a building permit in accordance with the Development Charges Act and Area Specific Development Charge By-laws in effect at time of payment.

Development Finance Recommended Conditions of Approval:	None
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BY-LAW AND COMPLIANCE, LICENSING AND PERMIT SERVICES COMMENTS

No comments

BCLPS Recommended Conditions of Approval:	None
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BUILDING INSPECTION (SEPTIC) COMMENTS

No comments received to date.

Building Inspection Recommended Conditions of Approval:	None
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FIRE DEPARTMENT COMMENTS

Within the scope of this application ensure that the fire route is not compromised and meets the requirements of the code. Hydrants be it public or private are in place as pre code.

Fire Department Recommended Conditions of Approval:	None
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SCHEDULES TO STAFF REPORT

*See Schedule for list of correspondence

Schedule A	Drawings & Plans Submitted with the Application
Schedule B	Staff & Agency Comments
Schedule C (if required)	Correspondence (Received from Public & Applicant)
Schedule D (if required)	Previous COA Decisions on the Subject Land

Should the Committee find it appropriate to approve this application in accordance with request and the sketch submitted with the application, as required by Ontario Regulation 200/96, the following conditions have been recommended:

SUMMARY OF RECOMMENDED CONDITIONS OF APPROVAL

All conditions of approval, unless otherwise stated, are considered to be incorporated into the approval “if required”. If a condition is no longer required after an approval is final and binding, the condition may be waived by the respective department or agency requesting conditional approval. A condition cannot be waived without written consent from the respective department or agency.

#	DEPARTMENT / AGENCY	CONDITION(S) DESCRIPTION
1	Development Engineering ian.reynolds@vaughan.ca	<ol style="list-style-type: none"> 1. The Owner/Applicant shall submit a grading/servicing plan and storm water management report for the proposed site, to the satisfaction of the Development Engineering Department, prior to final clearance of this condition. 2. Staff have confirmed that the property is located within an unassumed subdivision. The Owner/Applicant shall obtain a signed and stamped letter from the Developer's Civil Engineering Consultant stating that the proposed service connection and stormwater management report have been reviewed and approved for the proposed site. The signed letter shall be provided to the satisfaction of the Development Engineering Department, prior to final clearance of this condition.
2	Parks, Forestry and Horticulture Operations zachary.quizzetti@vaughan.ca	Applicant/owner shall obtain a “Private Property Tree Removal & Protection” permit through the forestry division prior to any construction works on the subject property.

IMPORTANT INFORMATION – PLEASE READ

CONDITIONS: It is the responsibility of the owner/applicant and/or authorized agent to obtain and provide a clearance letter from respective department and/or agency (**see condition chart above for contact**). This letter must be provided to the Secretary-Treasurer to be finalized. All conditions must be cleared prior to the issuance of a Building Permit.

APPROVALS: Making any changes to your proposal after a decision has been made may impact the validity of the Committee's decision.

An approval obtained from the Committee of Adjustment, where applicable, is tied to the building envelope shown on the plans and drawings submitted with the application and subject to the variance approval.

A building envelope is defined by the setbacks of the buildings and/or structures shown on the plans and drawings submitted with the application, as required by Ontario Regulation 200/96. Future development outside of an approved building envelope, where a minor variance was obtained, must comply with the provisions of the City's Zoning By-law.

Elevation drawings are provided to reflect the style of roof (i.e. flat, mansard, gable etc.) to which a building height variance has been applied. Where a height variance is approved, building height is applied to the style of roof (as defined in the City's Zoning By-law) shown on the elevation plans submitted with the application.

Architectural design features that are not regulated by the City's Zoning By-law are not to be considered part of an approval unless specified in the Committee's decision.

DEVELOPMENT CHARGES: That the payment of the Regional Development Charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the Regional Development Charges By-law in effect at the time of payment.

That the payment of the City Development Charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the City's Development Charges By-law in effect at the time of payment.

That the payment of the Education Development Charge if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the Boards of Education By-laws in effect at the time of payment

That the payment of Special Area Development charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and The City's Development Charge By-law in effect at the time of Building permit issuance to the satisfaction of the Reserves/Capital Department.

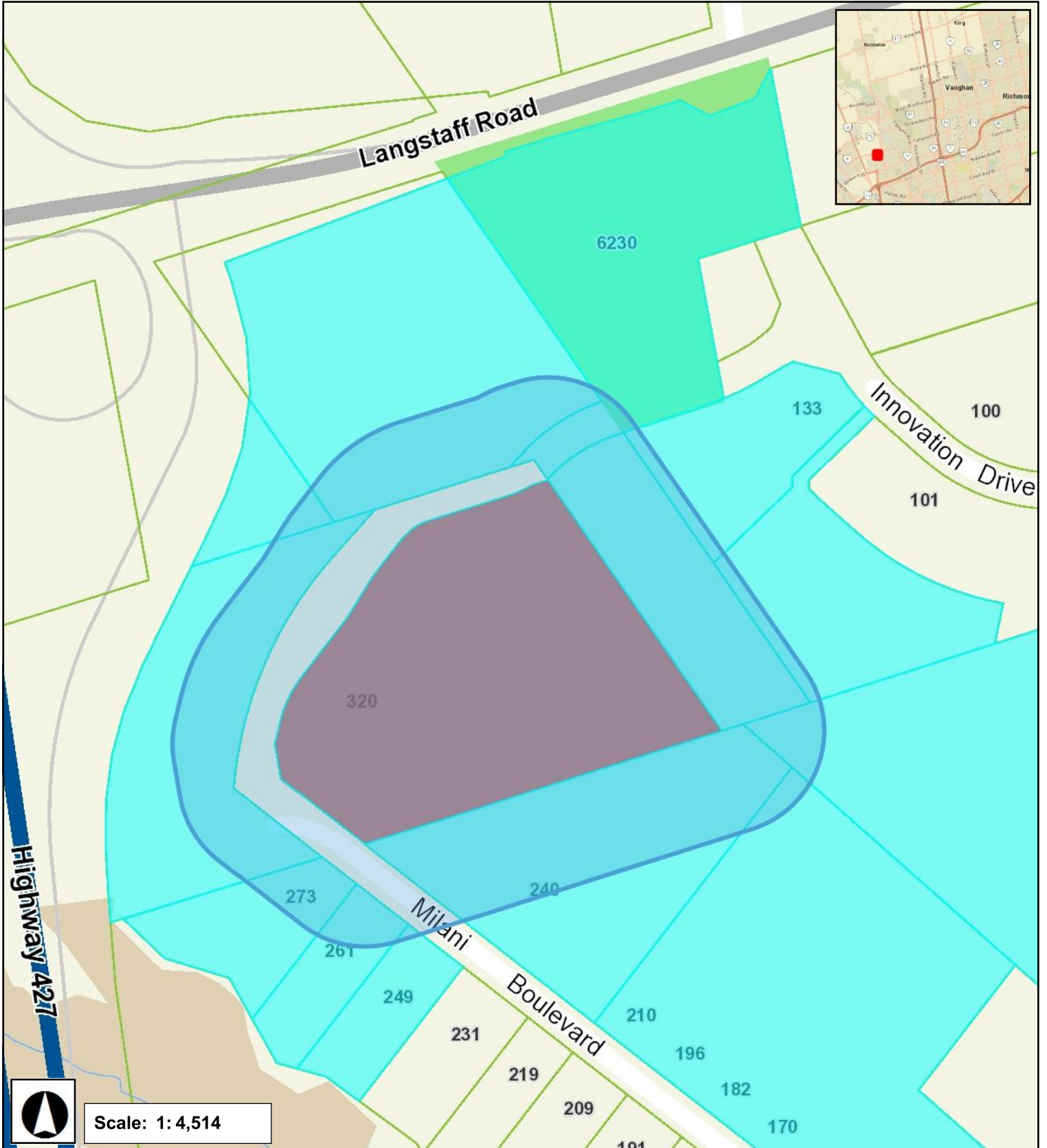
NOTICE OF DECISION: If you wish to be notified of the decision in respect to this application or a related Ontario Land Tribunal (OLT) hearing you must complete a Request for Decision form and submit to the Secretary Treasurer (ask staff for details). In the absence of a written request to be notified of the Committee's decision you will **not** receive notice.

SCHEDULE A: DRAWINGS & PLANS



LOCATION MAP - A001/23

320 MILANI BLVD. WOODBRIDGE





ARTIST RENDERING - FOR REFERENCE ONLY

NORR

NORR Architects & Engineers Limited
 175 Bloor Street East
 North Tower, 15th Floor
 Toronto, Ontario M4W 3R8
 PHONE: 416 929 0200 FAX: 416 929 3635
 norr.com

MIRCOM HQ AND INDUSTRIAL DEVELOPMENT

320 Milani Blvd. Vaughan, ON.

ISSUED FOR MINOR VARIANCE APPLICATION R2
 JANUARY 27, 2023

NORR JOB NO: ONBL22-0181



MIRCOM GROUP OF COMPANIES
 25 Interchange Way,
 Concord, ON, L4K 5W3
 mircom.com

ARCHITECTURE	STRUCTURAL	MECHANICAL	ELECTRICAL	CIVIL
NORR ARCHITECTS & ENGINEERS LIMITED 175 Bloor Street East north tower, 15th Floor Toronto, Toronto, Ontario M4W 3R8 PHONE: 416 929 0200 FAX: 416 929 3635	NORR ARCHITECTS & ENGINEERS LIMITED 175 Bloor Street East North Tower, 15th Floor Toronto, Toronto, Ontario M4W 3R8 PHONE: 416 929 0200 FAX: 416 929 3635	NORR ARCHITECTS & ENGINEERS LIMITED 175 Bloor Street East North Tower, 15th Floor Toronto, Toronto, Ontario M4W 3R8 PHONE: 416 929 0200 FAX: 416 929 3635	NORR ARCHITECTS & ENGINEERS LIMITED 175 Bloor Street East North Tower, 15th Floor Toronto, Toronto, Ontario M4W 3R8 PHONE: 416 929 0200 FAX: 416 929 3635	TRANS ENGINEERING SOLUTION (TES) 6589 Skipper Way Mississauga, Ontario, L5W 1P8 PHONE: 647 204 7435

PRELIMINARY - NOT FOR CONSTRUCTION

DATE	ISSUED FOR	REV
2022-12-02	CLIENT REVIEW	A
2022-12-05	CLIENT REVIEW	B
2022-12-22	SCHEMATIC DESIGN	C
2023-01-03	MINOR VARIANCE APPLICATION	D
2023-01-09	DEVELOPMENT	E
2023-01-13	CLIENT REVIEW	F
2023-01-17	DD PROGRESS	G
2023-01-20	MINOR VARIANCE APPLICATION	H
2023-01-27	MINOR VARIANCE APPLICATION	I

Zoning By-law 1-88		Variance requested
1	A minimum of 485 parking spaces are required [Exception 9(1356)].	To permit a minimum of 126 parking spaces.
2	A minimum parking space length of 6.0 metres is required [Section 2.0, definition of parking space]	To permit a minimum parking space length of 5.7 metres.

BUILDING GFA:

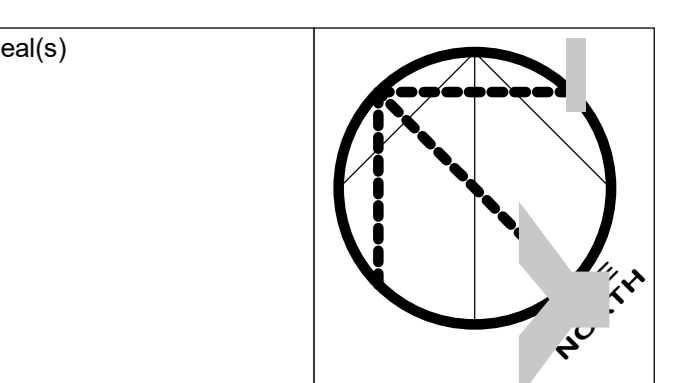
LEVEL 1	8,641 sm	93,009.6 sft
LEVEL 2	3,784.98 sm	40,741.18 sft
TOTAL	12,425 sm	133,749.12 sft

This drawing has been prepared solely for the use of MIRCOM GROUP OF COMPANIES and there are no representations of any kind made by NORR ARCHITECTS & ENGINEERS LIMITED to any party with whom NORR ARCHITECTS & ENGINEERS LIMITED has not entered into a contract.

This drawing shall not be used for construction purposes until the seal appearing hereon is signed and dated by the Architect or Engineer.

Project Component
Key Plan

Consultants
Civil: NORR ARCHITECTS & ENGINEERS LIMITED
Landscape: TBD
Architecture: NORR ARCHITECTS & ENGINEERS LIMITED
Structural: NORR ARCHITECTS & ENGINEERS LIMITED
Mechanical: NORR ARCHITECTS & ENGINEERS LIMITED
Electrical: TRANS ENGINEERING SOLUTIONS



NORR
NORR Architects & Engineers Limited
175 Bloor St. E. 15th Floor
Toronto, ON Canada M4W 3R8
norr.com

Project Manager: P. RANCHAL
Project Leader: M. KANU
Client: MIRCOM GROUP OF COMPANIES

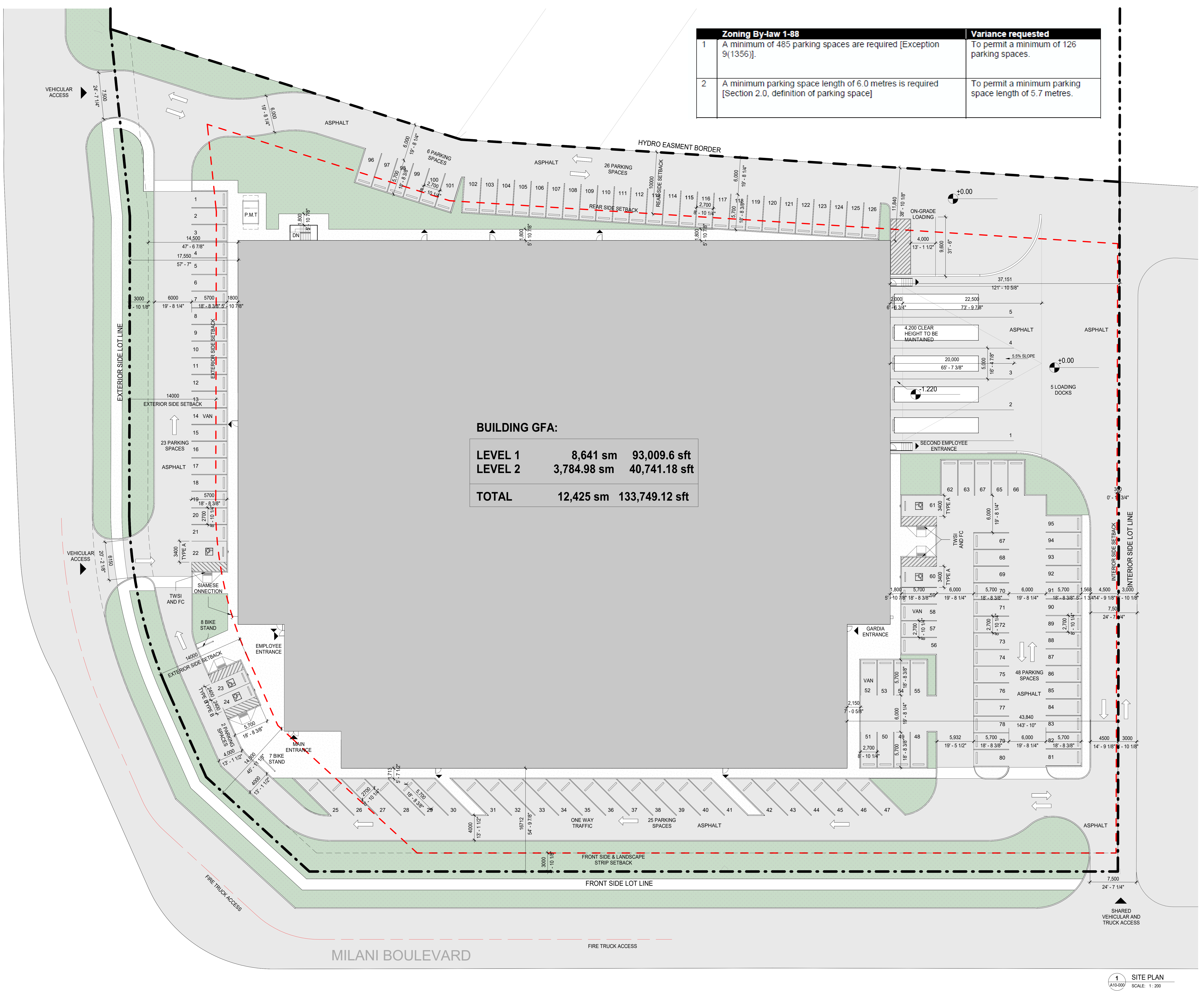
MIRCOM GROUP OF COMPANIES
MIRCOM GROUP OF COMPANIES

Project: MIRCOM HQ AND INDUSTRIAL DEVELOPMENT
320 Milani Blvd. Vaughan, ON

Drawing Title: SITE PLAN

Scale: 1 : 200
Project No.: ONBL22-0181
Drawing No.: A10-000

PRELIMINARY NOT FOR CONSTRUCTION



1 SITE PLAN
SCALE: 1 : 200

MIRCOM GROUP OF COMPANIES | ARCHITECTURE | 2023-01-27 | 10:00 AM | 10:00 AM | 10:00 AM

DATE	ISSUED FOR	REV
2022-12-22	SCHEMATIC DESIGN	A
2023-01-03	MINOR VARIANCE APPLICATION	B
2023-01-20	MINOR VARIANCE APPLICATION	C
2023-01-27	MINOR VARIANCE APPLICATION	D

Zoning By-law 1-88	Variance requested
1 A minimum of 485 parking spaces are required [Exception 9(1356)].	To permit a minimum of 126 parking spaces.
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LEGAL DESCRIPTION	
MUNICIPAL ADDRESS	320 Milani Blvd. Vaughan, ON.
	m ² sq.ft.
SITE AREA WITH HYDRO EASEMENT	36,477.56 392,641.18
SITE AREA WITHOUT HYDRO EASEMENT	18,599.35 200,201.8

ZONING BY-LAW 1-88 (EM3)			
	REQUIRED	PROVIDED	
LOT COVERAGE	60% (max.)	8,641 sm	WITH HYDRO EASEMENT 23.68% WITHOUT HYDRO EASEMENT 46.45%
BUILDING HEIGHT	15 m	15 m	
REQUIRED LANDSCAPE STRIP ABUTTING STREET (m)	3 m	3 m	
MINIMUM LANDSCAPE OPEN SPACE (%)	5%	1,857 sm	WITH HYDRO EASEMENT 5% WITHOUT HYDRO EASEMENT 10%
MINIMUM PARKING SPACES: BY-LAW 1-88 EM3 EXCEPTION NO. 9(1356)	PARKING STALL (2.7 m x 6 m)	PARKING STALL (2.7 m x 5.7 m)	
INDUSTRIAL USE	284		
OFFICE USE	201	126 ON-GRADE PARKING	
TOTAL	485	126 ON-GRADE PARKING	
MINIMUM ACCESSIBLE PARKING SPACES			
	TYPE A (3.4 m x 6 m)	TYPE B (2.4 m x 6 m)	TYPE A (3.4 m x 5.7 m)
NUMBER OF PARKING REQUIRED - BY TYPE	3	2	3
1+ 3% OF THE TOTAL PROVIDED PARKING=5	5		2
TOTAL			

SETBACKS (BY-LAW 1-88 EM3)		
	REQUIRED	PROVIDED
FRONT YARD SETBACK (m)	3.0	16.71
REAR YARD SETBACK (m)	10.0	11.84
INTERIOR YARD SETBACK (m)	0.3	37.15
EXTERIOR YARD SETBACK (m)	14.0	17.50

GROSS FLOOR AREA		
LEVEL	m ²	ft ²
LEVEL 1:		
INDUSTRIAL	7,285.51	78,420.57
OFFICES	1,355.21	14,587.35
LEVEL 1 TOTAL	8,640.72	93,009.3
LEVEL 2:		
OFFICE	3,784.98	40,741.18
LEVEL 2 TOTAL	3,784.98	40,741.18
TOTAL OFFICE AREA	5,140.19	55,328.54
TOTAL GFA	12,425.7	133,749.12

PROJECT STATISTICS
SCALE: 1:1

Variance #2 to permit a minimum parking space length of 5.7 meters for all parking spaces.

Variance #1 to permit a minimum of 126 parking spaces



1 CONTEXT PLAN
SCALE: 1:400

This drawing has been prepared solely for the use of MIRCOM GROUP OF COMPANIES and there are no representations of any kind made by NORR ARCHITECTS & ENGINEERS LIMITED to any party with whom NORR ARCHITECTS & ENGINEERS LIMITED has not entered into a contract.
This drawing shall not be used for construction purposes until the seal appearing hereon is signed and dated by the Architect or Engineer.

Project Component
Key Plan

Consultants
Civil: NORR ARCHITECTS & ENGINEERS LIMITED
Landscape: TBD
Architecture: NORR ARCHITECTS & ENGINEERS LIMITED
Structural: NORR ARCHITECTS & ENGINEERS LIMITED
Mechanical: NORR ARCHITECTS & ENGINEERS LIMITED
Electrical: TRANS ENGINEERING SOLUTIONS

Seal(s)

NORR
NORR Architects & Engineers Limited
175 Bloor St. E. 15th Floor
Toronto, ON Canada M4W 3R8
norr.com

Project Manager: P. RANCHAL
Project Leader: M. KANU
Client: MIRCOM GROUP OF COMPANIES

MIRCOM GROUP OF COMPANIES
GROUP OF COMPANIES

Project: MIRCOM HQ AND INDUSTRIAL DEVELOPMENT
320 Milani Blvd. Vaughan, ON.

Drawing Title: SITE CONTEXT

Scale: As indicated
Project No.: ONBL22-0181
Drawing No.: A10-010

PRELIMINARY - NOT FOR CONSTRUCTION

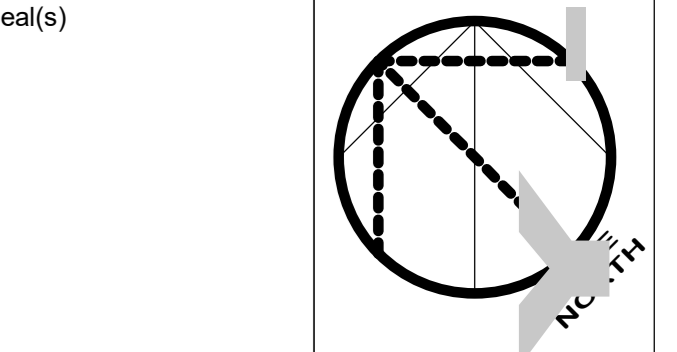
MIRCOM GROUP OF COMPANIES - 2023-01-27 10:40:00 AM

DATE	ISSUED FOR	REV
2022-12-22	SCHEMATIC DESIGN	A
2023-01-03	MINOR VARIANCE APPLICATION	B
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2023-01-30	MINOR VARIANCE APPLICATION	D
2023-01-27	MINOR VARIANCE APPLICATION	E

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Project Component	
Key Plan	
Consultants	
Civil:	NORR ARCHITECTS & ENGINEERS LIMITED
Landscape:	TBD
Architecture:	NORR ARCHITECTS & ENGINEERS LIMITED
Structural:	NORR ARCHITECTS & ENGINEERS LIMITED
Mechanical:	NORR ARCHITECTS & ENGINEERS LIMITED
Electrical:	TRANS ENGINEERING SOLUTIONS



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 Toronto, ON Canada M4W 3R8
 norr.com

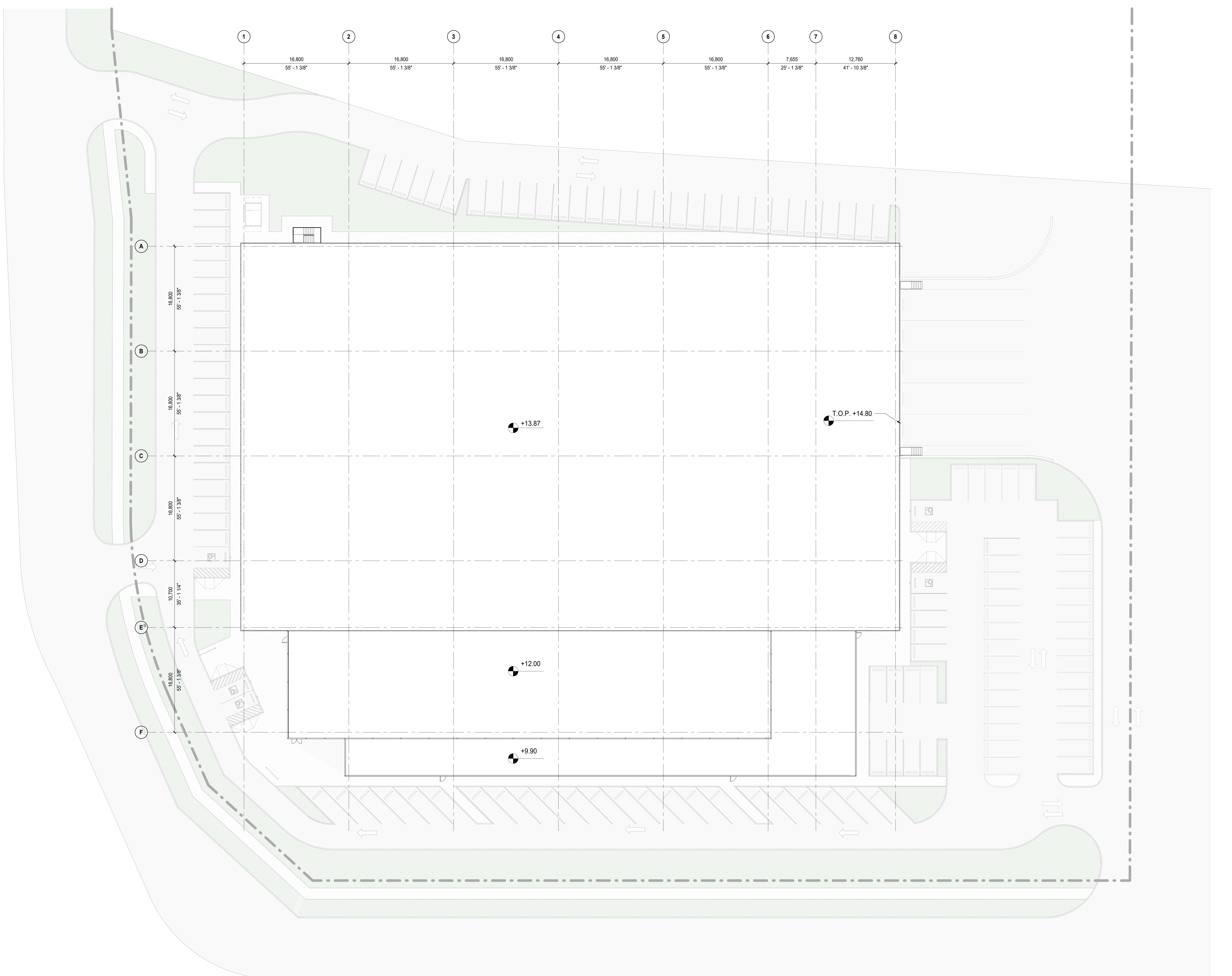
Project Manager	Drawn
P. RANCHAL	P. FAR
Project Leader	Checked
M. KANU	M. KANU



Project
 MIRCOM HQ AND INDUSTRIAL DEVELOPMENT
 320 Midland Blvd. Vaughan, ON

Drawing Title
ROOF PLAN

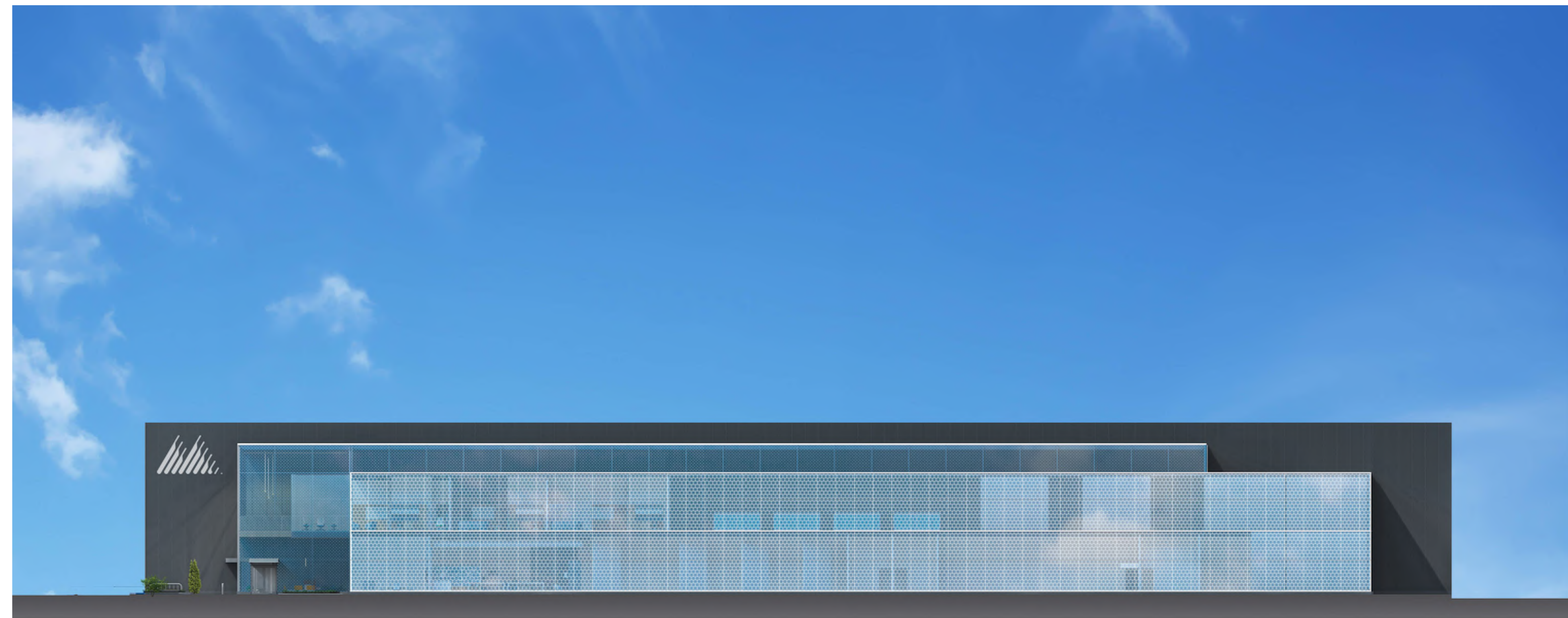
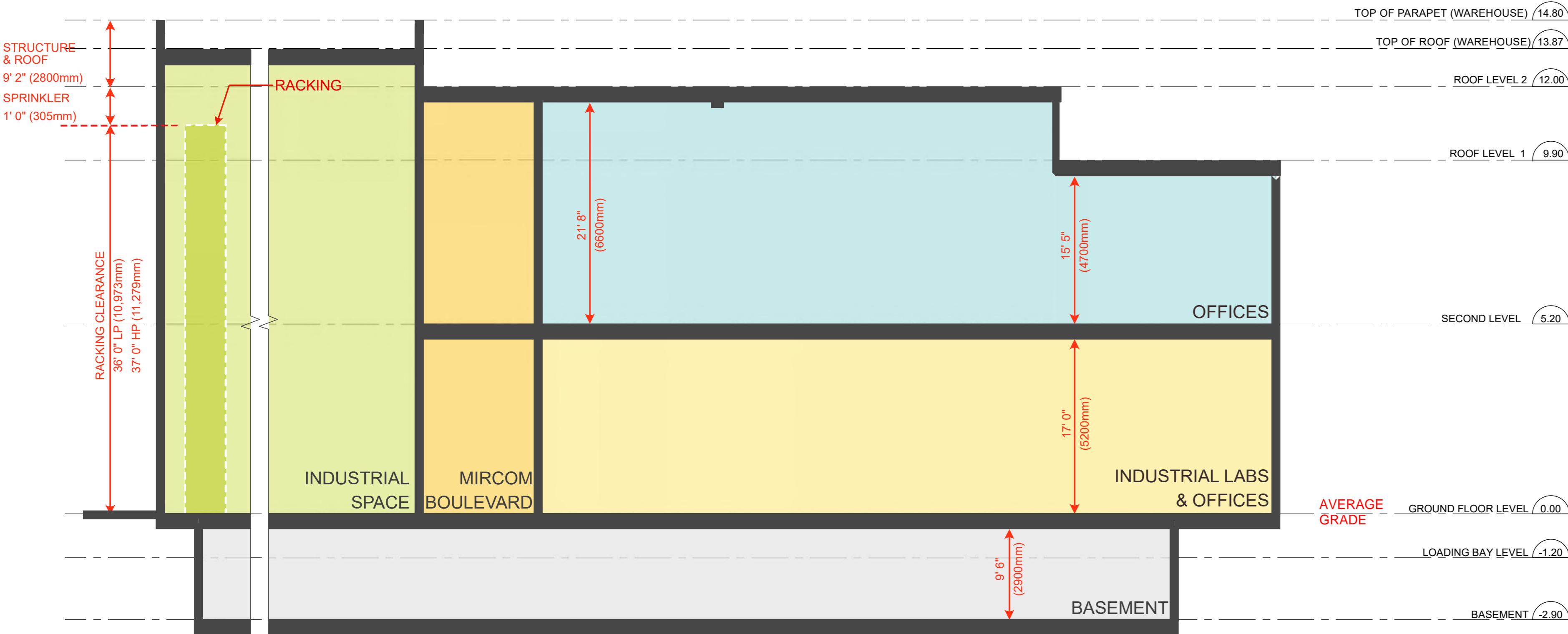
Scale	1 : 200
Project No.	ONBL22-0181
Drawing No.	A20-040



1 TOP OF PARAPET
 SCALE: 1 : 200

PRELIMINARY - NOT FOR CONSTRUCTION

2023-07-26 11:17 AM



DATE	ISSUED FOR	REV
2022-12-22	SCHEMATIC DESIGN	A
2023-01-03	MINOR VARIANCE APPLICATION	B
2023-01-20	MINOR VARIANCE APPLICATION	C
2023-01-27	MINOR VARIANCE APPLICATION	D

This drawing has been prepared solely for the use of MIRCOM GROUP OF COMPANIES and there are no representations of any kind made by NORR ARCHITECTS & ENGINEERS LIMITED to any party with whom NORR ARCHITECTS & ENGINEERS LIMITED has not entered into a contract.

This drawing shall not be used for construction purposes until the seal appearing hereon is signed and dated by the Architect or Engineer.

Project Component	
Key Plan	
Consultants	
Civil:	NORR ARCHITECTS & ENGINEERS LIMITED
Landscape:	TBD
Architecture:	NORR ARCHITECTS & ENGINEERS LIMITED
Structural:	NORR ARCHITECTS & ENGINEERS LIMITED
Mechanical:	NORR ARCHITECTS & ENGINEERS LIMITED
Electrical:	TRANS ENGINEERING SOLUTIONS

Seal(s)	
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NORR
Architects & Engineers Limited

175 Bloor St. E., 15th Floor
Toronto, ON Canada M4W 3R8
norrcan.com

Project Manager	Drawn
P. RANCHAL	P. FAR
Project Leader	Checked
M. KANU	M. KANU

Client
MIRCOM GROUP OF COMPANIES

Project
MIRCOM HQ AND INDUSTRIAL DEVELOPMENT
320 Miami Blvd., Vaughan, ON

Drawing Title
PERSPECTIVE VIEW

Scale	
Project No.	ONBL22-0181
Drawing No.	A30-031

PRELIMINARY - NOT FOR CONSTRUCTION

NORR_2023-01-27_11:17 AM_Rev_02 (Sheet 19) Copyright © 2019

SCHEDULE B: STAFF & AGENCY COMMENTS

DEPT/AGENCY	Circulated	Comments Received	Conditions	Nature of Comments
TRCA *Schedule B	X			No Comments Received to Date
Ministry of Transportation (MTO) *Schedule B				No Comments Received to Date
Region of York *Schedule B	X	X		General Comments
Alectra *Schedule B	X	X		General Comments
Bell Canada *Schedule B	X			No Comments Received to Date
YRDSB *Schedule B				
YCDSB *Schedule B				
CN Rail *Schedule B				
CP Rail *Schedule B				
TransCanada Pipeline *Schedule B	X			No Comments Received to Date
Metrolinx *Schedule B				
Propane Operator *Schedule B				
Development Planning	X	X		Recommend Approval/no conditions
Building Standards (Zoning)	X	X		General Comments

Date: January 31st 2023

Attention: **Christine Vigneault**

RE: Request for Comments

File No.: **A001-23**

Related Files:

Applicant MRJ Properties Inc.

Location 320 Milani Blvd



Discover the possibilities

COMMENTS:

- We have reviewed the proposed Variance Application and have no comments or objections to its approval.
- We have reviewed the proposed Variance Application and have no objections to its approval, subject to the following comments (attached below).
- We have reviewed the proposed Variance Application and have the following concerns (attached below).

Alectra Utilities (formerly PowerStream) has received and reviewed the proposed Variance Application. This review, however, does not imply any approval of the project or plan.

All proposed billboards, signs, and other structures associated with the project or plan must maintain minimum clearances to the existing overhead or underground electrical distribution system as specified by the applicable standards, codes and acts referenced.

In the event that construction commences, and the clearance between any component of the work/structure and the adjacent existing overhead and underground electrical distribution system violates the Occupational Health and Safety Act, the customer will be responsible for 100% of the costs associated with Alectra making the work area safe. All construction work will be required to stop until the safe limits of approach can be established.

In the event construction is completed, and the clearance between the constructed structure and the adjacent existing overhead and underground electrical distribution system violates the any of applicable standards, acts or codes referenced, the customer will be responsible for 100% of Alectra's cost for any relocation work.

References:

- Ontario Electrical Safety Code, latest edition (Clearance of Conductors from Buildings)
- Ontario Health and Safety Act, latest edition (Construction Protection)
- Ontario Building Code, latest edition (Clearance to Buildings)
- PowerStream (Construction Standard 03-1, 03-4), attached
- Canadian Standards Association, latest edition (Basic Clearances)

If more information is required, please contact either of the following:

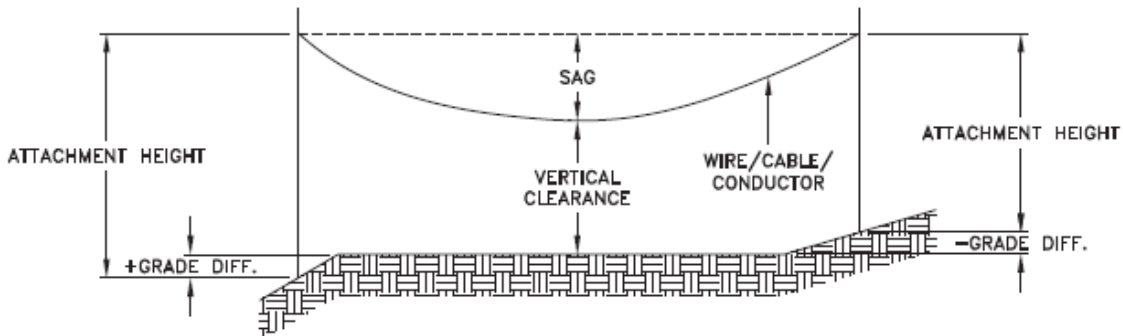
Mr. Stephen Cranley, C.E.T
Supervisor, Distribution Design, ICI & Layouts (North)
Phone: 1-877-963-6900 ext. 31297

E-mail: stephen.cranley@alectrautilities.com

Mitchell Penner
Supervisor, Distribution Design-Subdivisions
Phone: 416-302-6215

Email: Mitchell.Penner@alectrautilities.com

LOCATION OF WIRES, CABLES OR CONDUCTORS	SYSTEM VOLTAGE			
	SPAN GUYS AND COMMUNICATIONS WIRES	UP TO 600V AND NEUTRAL	4.16/2.4kV TO 27.6/16kV (SEE NOTE 1)	44kV
MINIMUM VERTICAL CLEARANCES (SEE NOTE 2)				
OVER OR ALONGSIDE ROADS, DRIVEWAYS OR LANDS ACCESSIBLE TO <u>VEHICLES</u>	442cm	442cm	480cm	520cm
OVER GROUND ACCESSIBLE TO <u>PEDESTRIANS AND BICYCLES ONLY</u>	250cm	310cm	340cm	370cm
ABOVE TOP OF RAIL AT <u>RAILWAY CROSSINGS</u>	730cm	730cm	760cm	810cm



MINIMUM ATTACHMENT HEIGHT = MAXIMUM SAG
 + MINIMUM VERTICAL CLEARANCE (FROM ABOVE TABLE)
 ± GRADE DIFFERENCE
 + 0.3m (VEHICLE OR RAILWAY LOCATION)
 + SNOW DEPTH (PEDESTRIAN LOCATION, SEE NOTE 3)

NOTES:

1. THE MULTIGROUNDED SYSTEM NEUTRAL HAS THE SAME CLEARANCE AS THE 600V SYSTEM.
2. THE VERTICAL CLEARANCES IN THE ABOVE TABLE ARE UNDER MAXIMUM SAG CONDITIONS.
3. REFER TO CSA STANDARD C22.3 No.1, ANNEX D FOR LOCAL SNOW DEPTH VALUES.
4. ALL CLEARANCES ARE IN ACCORDANCE TO CSA STANDARD C22.3.

CONVERSION TABLE

METRIC	IMPERIAL (APPROX)
810cm	27'-0"
760cm	25'-4"
730cm	24'-4"
520cm	17'-4"
480cm	16'-0"
442cm	15'-5"
370cm	12'-4"
340cm	11'-4"
310cm	10'-4"
250cm	8'-4"

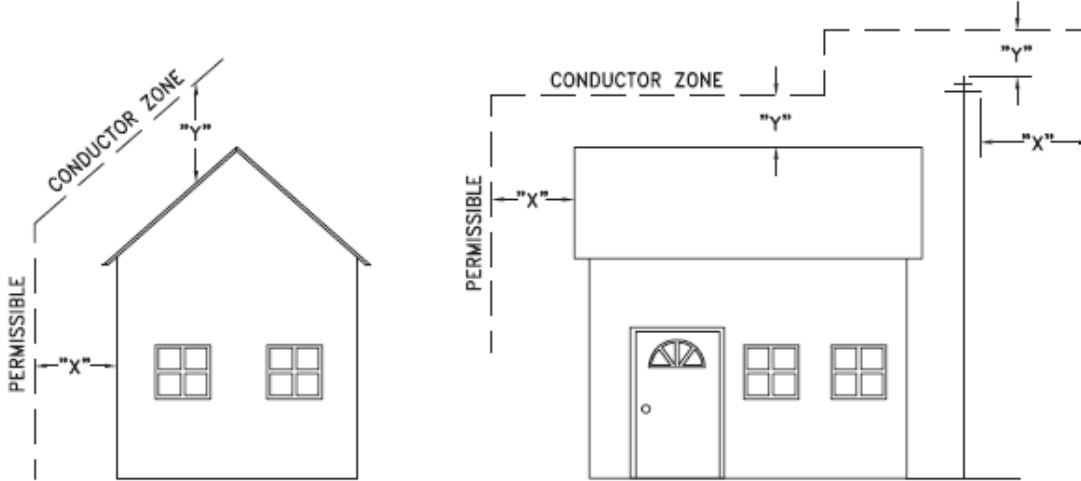
REFERENCES

SAGS AND TENSIONS | SECTION 02

MINIMUM VERTICAL CLEARANCES OF WIRES, CABLES AND CONDUCTORS ABOVE GROUND OR RAILS

ORIGINAL ISSUE DATE: 2010-DEC-24 REVISION NO: R1 REVISION DATE: 2012-JAN-09

Certificate of Approval	
This construction Standard meets the safety requirements of Section 4 of Regulation 22/04	
Joe Crozier, P.Eng.	2012-JAN-09
Name	Date
P.Eng. Approval By:	Joe Crozier



VOLTAGE	MINIMUM HORIZONTAL CLEARANCE UNDER MAXIMUM SWING CONDITIONS DIMENSION "X" (SEE NOTES 1, 3 & 4)	MINIMUM VERTICAL CLEARANCE UNDER MAXIMUM DESIGN SAG CONDITIONS DIMENSION "Y" (SEE NOTES 1, 2, 4 & 5)
0-600V AND NEUTRAL	100cm	250cm
4.16/2.4 TO 44kV	300cm	480cm

NOTES

1. UNDER NO CIRCUMSTANCES SHALL A CONDUCTOR BE PERMITTED TO PENETRATE THE ENVELOPE SHOWN BY THE DOTTED LINE.
2. THE VERTICAL CLEARANCES ARE UNDER CONDITIONS OF MAXIMUM DESIGN SAG.
3. THE HORIZONTAL CLEARANCES ARE UNDER CONDITIONS OF MAXIMUM SWING. WHERE THE CONDUCTOR SWING IS NOT KNOWN A HORIZONTAL CLEARANCE OF 480CM SHALL BE USED.
4. BUILDINGS THAT EXCEED 3 STOREYS OR 15M IN HEIGHT, THE MINIMUM HORIZONTAL CLEARANCE OF THE SECONDARY CONDUCTORS SHOULD BE INCREASED TO 300cm WHERE IT IS NECESSARY TO ALLOW FOR THE RAISING OF LADDERS BY LOCAL FIRE DEPARTMENTS.
5. IN SITUATIONS SUCH AS MULTI-LEVEL GARAGES, WHERE ROOFS ARE NORMALLY USED BY PERSONS AND VEHICLES, THE VERTICAL CLEARANCES OF POWERSTREAM STANDARD 03-1 SHALL APPLY.
6. DISTRIBUTION LINES CONSTRUCTED NEAR BUILDINGS SHALL BE BUILT TO AVOID OVERHANG WHEREVER POSSIBLE. WHERE LINES MUST BE CONSTRUCTED OVER OR ADJACENT TO BUILDINGS THE APPLICABLE HORIZONTAL AND VERTICAL CLEARANCES SHALL BE AT CONDITIONS OF MAXIMUM CONDUCTOR SWING AND MAXIMUM SAG. THE ABOVE CLEARANCES ARE DESIGNED TO PREVENT PERSONS ON OR IN BUILDINGS AS WELL AS EXTERNAL MACHINERY USED IN CONJUNCTION WITH A BUILDING TO COME IN CONTACT WITH CONDUCTORS. EFFORTS SHOULD BE MADE TO INCREASE THESE CLEARANCES WHERE POSSIBLE.
7. ALL CLEARANCES ARE IN ACCORDANCE TO CSA C22.3 NO.1-06 (TABLE-9).

METRIC	IMPERIAL (APPROX)
480cm	16'-0"
300cm	10'-0"
250cm	8'-4"
100cm	3'-4"

MINIMUM VERTICAL & HORIZONTAL CLEARANCES OF CONDUCTORS FROM BUILDINGS OR OTHER PERMANENT STRUCTURES (CONDUCTORS NOT ATTACHED TO BUILDINGS)

Certificate of Approval
This construction Standard meets the safety requirements of Section 4 of Regulation 22/04
Debbie Dadwani, P.Eng. 2010-MAY-05
Name Date
P. Eng. Approval By: *D. Dadwani*

ORIGINAL ISSUE DATE: 2010-MAY-05 REVISION NO: REVISION DATE:
PS:\System Planning and Standards\Standard Design\PowerStream Standards\PowerStream Standards working files\Section 3-1-02\WG 03-4 R0 May 5, 2010.dwg, 5/5/2010 10:22:02 AM, Adobe PDF

To: Committee of Adjustment
From: Lindsay Haviland, Building Standards Department
Date: January 31, 2023
Applicant: MRJ Properties Inc.
Location: 320 Milani Blvd
 PLAN 65M4386 Part of Block 1
 CONC 9 Part of Lot 10
File No.(s): A001/23

Zoning Classification:

The subject lands are zoned GC- General Commercial Zone and subject to the provisions of Exception 14.988 under Zoning By-law 001-2021, as amended. This application is deemed to be Transitioned under section 1.6.2.6 of Zoning By-law 001-2021 as amended.

#	Zoning By-law 001-2021	Variance requested
	None.	

The subject lands are zoned EM3 – Retail Warehouse Employment Area Zone and subject to the provisions of Exception 9(1356) under Zoning By-law 1-88, as amended.

#	Zoning By-law 1-88	Variance requested
1	A minimum of 485 parking spaces are required [Exception 9(1356)].	To permit a minimum of 126 parking spaces.
2	A minimum parking space length of 6.0 metres is required [Section 2.0, definition of parking space]	To permit a minimum parking space length of 5.7 metres.

Staff Comments:

Stop Work Order(s) and Order(s) to Comply:

There are no outstanding Orders on file

Building Permit(s) Issued:

A Building Permit has not been issued. The Ontario Building Code requires a building permit for structures that exceed 10m²

Other Comments:

General Comments	
1	The applicant shall be advised that additional variances may be required upon review of detailed drawing for building permit/site plan approval.

Conditions of Approval:

If the committee finds merit in the application, the following conditions of approval are recommended.

* Comments are based on the review of documentation supplied with this application.

To: Christine Vigneault, Committee of Adjustment Secretary Treasurer
From: Nancy Tuckett, Director of Development Planning
Date: March 8, 2023
Name of Owner: MRJ Properties Inc.
Location: 320 Milani Boulevard
File No.(s): A001/23

Proposed Variance(s) (By-law 1-88):

1. To permit a minimum of 126 parking spaces.
2. To permit a minimum parking space length of 5.7 m.

By-Law Requirement(s) (By-law 1-88):

1. A minimum of 485 parking spaces are required.
2. A minimum parking space length of 6.0 m is required.

Official Plan:

City of Vaughan Official Plan 2010 ('VOP 2010'): "Employment Commercial Mixed-Use", subject to Site-Specific Policy 13.14, 2107638/RioCan/SRF Vaughan, as amended.

Comments:

The Owner is requesting relief to permit the construction of a two-storey employment building, with the above noted variances. The proposed building will contain a mix of industrial and office uses to facilitate research and development, manufacturing, applications, finance, marketing, customer service and technical support. The office use will be accessory to the industrial use.

The Development Planning Department has no objection to Variance 1 given that the proposed parking ratio has been reviewed by the Transportation Engineering Division of the Development Engineering Department and is considered sufficient to serve the proposed industrial and accessory office uses.

The Development Planning Department has no objection to Variance 2 given that the proposed parking space length complies with Zoning By-law 001-2021, which was enacted by Vaughan Council on October 20, 2021. Transportation Engineering staff have also reviewed the minor variance application and has no objection.

Accordingly, the Development Planning Department can support the requested variances and is of the opinion that the proposal is minor in nature, maintains the general intent and purpose of the Official Plan and Zoning By-law, and is desirable for the appropriate development of the land.

Recommendation:

The Development Planning Department recommends approval of the application.

Conditions of Approval:

If the Committee finds merit in the application, the following conditions of approval are recommended:

None

Comments Prepared by:

Joshua Cipolletta, Planner I
David Harding, Senior Planner

From: [Development Services](#)
To: [Christine Vigneault](#)
Cc: [Committee of Adjustment](#)
Subject: [External] RE: REVISED A001/23 (320 Milani Blvd) - REQUEST FOR COMMENTS, CITY OF VAUGHAN
Date: Friday, February 3, 2023 3:53:00 PM
Attachments: [image002.png](#)
[image004.png](#)
[image005.png](#)

Hi Christine,

The Regional Municipality of York has completed its review of the above minor variance and has no comment.

Please note regional comments are sent through the associated Site Plan and related files.

Many thanks,

Our working hours may be different. Please do not feel obligated to reply outside of your scheduled working hours. Let's work together to help foster healthy work-life boundaries.

Niranjan Rajevan, M.Pl. | Associate Planner, Programs and Process Improvement, Planning and Economic Development, Corporate Services

The Regional Municipality of York | 17250 Yonge Street | Newmarket, ON L3Y 6Z1
1-877-464-9675 ext. 71521 | niranjan.rajevan@york.ca | www.york.ca

Our Values: Integrity, Commitment, Accountability, Respect, Excellence



Our Mission: **Working together to serve our thriving communities – today and tomorrow**

Please consider the environment before printing this email.

From: Christine Vigneault <Christine.Vigneault@vaughan.ca>
Sent: Wednesday, January 25, 2023 5:47 PM
To: Development Services <developmentsservices@york.ca>
Subject: FW: REVISED A001/23 (320 Milani Blvd) - REQUEST FOR COMMENTS, CITY OF VAUGHAN

CAUTION! This is an external email. Verify the sender's email address and carefully examine any links or attachments before clicking. If you believe this may be a phishing email, forward it to isitsafe@york.ca then delete it from your inbox. If you think you may have clicked on a phishing link, report it to the IT Service Desk, ext. 71111, and notify your supervisor immediately.

SCHEDULE C: PUBLIC & APPLICANT CORRESPONDENCE

Correspondence Type	Name	Address	Date Received (mm/dd/yyyy)	Summary
Applicant/Agent	Goodman's		01/03/23	Application Cover Letter

January 3, 2023

Our File No.: 223403

Submitted Online

City of Vaughan
Committee of Adjustment
Vaughan City Hall, Level 100
2141 Major Mackenzie Dr.
Vaughan, ON L6A 1T1

Dear Sirs/Mesdames:

Re: 320 Milani Boulevard – Minor Variance Application

We are solicitors for MRJ Properties Inc. (“**MRJ**”), the owner of the property known municipally as 320 Milani Boulevard (the “**Subject Property**”).

MRJ is part of the Mircom Group of Companies (“**Mircom**”), a privately held family-run global designer, manufacturer and distributor of Intelligent Building Solutions, proudly headquartered in the City of Vaughan since 1991. Since that time, Mircom has been a significant contributor to the City’s economic development and sustainability initiatives. It currently employs over 500 employees, over half of whom work in Mircom’s existing headquarters located at 25 Interchange Way within the Vaughan Metropolitan Centre.

Given the imminent redevelopment and intensification of the VMC, Mircom must relocate to a new headquarters as soon as possible. After searching across the GTA for a suitable replacement site, Mircom is pleased to have acquired the Subject Property, as it is an ideal location for Mircom to construct its new state-of-the-art industrial and office facility. One key to Mircom’s success is the complete vertical integration of its operations, and the Subject Property will comfortably accommodate a new two-storey building including various critical business functions such as research and development, manufacturing, applications, finance, marketing, customer service and technical support. Based on Mircom’s projected growth, it expects to increase its local employee base by a further 20-30% by 2025, and the proposed new facility will provide the necessary space for it to transition seamlessly and to continue to grow in the future.

Accordingly, we are pleased to submit this minor variance application seeking the necessary zoning relief to enable Mircom to obtain its building permit and to commence construction by May 31, 2023. We have confirmed with the City’s Development Planning staff that the project is exempt from site plan approval. We respectfully request that this application be placed on the earliest possible Committee of Adjustment agenda to ensure this timeline can be achieved.

The Subject Property

The Subject Property is located just south of Langstaff Road, east of Highway 427 and west of Highway 27, at the end of the Milani Boulevard road allowance. It is a large 3.65 hectare site, although the rear portion of the site (comprising approximately half of the overall site area) is encumbered by an easement in favour of Hydro One and contains two large hydro poles and associated conductors and transmission lines. No construction is proposed within this area.

The developable portion of the Subject Property is approximately 1.86 hectares, and forms somewhat of a corner lot condition at the existing terminus of Milani Boulevard, with the future road extension wrapping around the north side of the Subject Property. The Subject Property was originally included within two plans of subdivision and is effectively a remnant vacant parcel following the development of the majority of the subdivision with retail uses along Highway 27 and other employment uses along Milani Boulevard. The extensive hydro corridor at the rear of the site provides a significant buffer between the Subject Property and the large surface parking lots and retail stores comprising the RioCan Centre to the east.

Existing Designations and Zoning

Within the Vaughan Official Plan, the Subject Property is designated “Employment Areas” on Schedule 1 (Urban Structure) and “Employment Commercial Mixed-Use” on Schedule 13 (Land Use). It is also subject to Site-Specific Plan 13 which permits major retail uses in addition to office uses and certain other employment and commercial uses contemplated for intensification corridors and arterial streets. However, these planning permissions were established at the time the Highway 27 frontage was developed with retail uses and they do not reflect the historical employment zoning of the Subject Property. Moreover, as a remnant vacant parcel, the Subject Property itself is not located along an intensification corridor or arterial street.

The Subject Property is currently zoned EM3 Retail Warehouse Employment Area pursuant to Zoning By-law 1-88, as amended, and is subject to Exception No. 9(1356). As such, the proposed employment uses (warehouse and manufacturing), together with accessory offices, are expressly permitted under the relevant zoning, as confirmed in the attached letter from the City’s Zoning Plans Examiner dated December 12, 2022.

Although new Zoning By-law 001-2021, as amended, also applies to the Subject Property and zones it GC General Commercial (subject to Exception No. 14.988), the City has confirmed that the use permissions and development standards from By-law 1-88 have been transitioned pursuant to section 1.6.2.6 of By-law 001-2021, as amended.

The Proposed Development and Variances

As noted above, Mircom proposes to construct its new headquarters on the Subject Property in the form of a two-storey architecturally-designed mixed-use building encompassing industrial and office uses. The site plan, roof plan and perspective views submitted with this application

depict the proposed building massing and volume, with a two-storey glass portion along the primary street frontage and a slightly taller clad industrial component to the rear (which occupies the majority of the building footprint).

The building height is proposed to be just under 15 metres for the industrial space (14.8 metres to the top of the parapet) to accommodate racking with the necessary ceiling clearance. The office and laboratory space is proposed to be 12 metres in height where it joins the industrial space, stepping down to just under 10 metres at the front of the building. On the assumption that the 15 metre building height permission under Zoning By-law 1-88 continues to apply (pursuant to transition section 1.6.2.6 in By-law 001-2021), it is our understanding that no height variance is required. However, out of an abundance of caution, we have flagged a potential height variance in this application since By-law 001-2021 would otherwise limit the height to 11 metres.

Otherwise, we believe the proposed building complies with the applicable built-form standards, including all setbacks, lot area, lot frontage, lot coverage, gross floor area and landscaping requirements.

However, this application is necessary because the proposed development does not comply with the applicable parking standards in either By-law 1-88 or 001-2021, both of which impose a rate of 3.9 spaces per 100 m² for the Subject Property. This parking rate was clearly established when the retail permissions were granted for the area. While it may be an appropriate rate for a large retail centre, it is excessive for the proposed industrial/office facility, which is evident based on the parking rates applicable to other industrial, manufacturing, warehouse and office uses in By-laws 1-88 and 001-2021.

Instead of the 469 parking spaces that are required under the existing zoning, Mircom is proposing to provide 127 surface parking spaces (including the required accessible parking spaces) on all sides of the building. Based on the anticipated number of employees and visitors to the new facility, Mircom believes that 127 parking spaces will be more than sufficient to meet its daily parking demand. In order to provide additional spaces beyond 127, Mircom would have to obtain permission from Hydro One to allow parking within the hydro corridor (which is not likely to be granted) or build an underground parking garage at a very high cost. While an underground garage is technically feasible, Mircom does not believe the expense is justified given the anticipated lack of demand for additional parking.

Planning Rationale

The requested minor variance(s) satisfy the four tests set out in Section 45(1) of the *Planning Act* as they are minor, desirable for the appropriate development of the land, and maintain the general intent and purpose of the Official Plan and the Zoning By-laws.

Although the Official Plan designates the Subject Property as “Employment Commercial Mixed-Use”, this designation does not actually make sense for the Subject Property given that it is not

located on an intensification corridor or arterial road. Rather, it is located in the interior of a long-established Employment Area, adjacent to several other similar and compatible industrial/office uses, and separated from the RioCan Centre by an extensive hydro corridor.

The general intent and purpose of the Official Plan is to accommodate employment growth in appropriate locations and to encourage good employment opportunities. Retaining the Mircom headquarters and its associated jobs and economic benefits in Vaughan should be a high priority for the City and the Subject Property provides an excellent opportunity to do just that. Moreover, as it relates to building height and parking standards, the general intent and purpose is to ensure compatibility with surrounding lands and a sufficient amount of on-site parking to enable the site to function properly for its intended purposes. Given Mircom's 30+ year presence in Vaughan, it has a long track record of understanding its operational and parking needs.

Similarly, the general intent and purpose of the Zoning By-laws is to ensure new development fits comfortably with the buildings and uses on surrounding lands. Although the site-specific zoning was previously amended to allow for additional retail opportunities on the Subject Property, those uses have not come to fruition and in any event are ill-suited for this interior location at the end of Milani Boulevard. There are already similar employment uses existing on the surrounding lots, and the proposed Mircom facility will fit perfectly within this context. The building height is appropriate for this location and the proposed 127 surface parking spaces are sufficient to accommodate the proposed uses. The zoning rate of 3.9 spaces per 100 m² may be appropriate for retail uses but it is unwarranted and inappropriate for industrial and office uses.

Furthermore, it is noted that the Committee of Adjustment recently approved a similar minor variance for 240 Milani Boulevard, the adjacent property to the south, whereby 86 surface parking spaces were permitted instead of the required 196 spaces (File No. A146/22). This facility was not required to construct an underground garage to accommodate extra parking, as the proposed surface parking spaces were found to be sufficient. The same logic is sought to be applied to the proposed new Mircom headquarters, which has been designed with a sufficient on-site parking and loading configuration to ensure it functions properly.

In terms of impact, the reduced parking rate will not create any negative externalities on the adjacent properties or public streets. The number of proposed parking spaces will be more than sufficient to accommodate the employees and visitors to the Mircom facility. If a variance is required for building height, this will similarly not cause any negative impacts as the taller portion of the building is located to the rear, and as an industrial building it will not result in any overlook or unacceptable shadowing on adjacent properties.

Finally, the proposed development is extremely desirable as it will enable Mircom to maintain its headquarters and successful business operations within the City of Vaughan so that it can continue to employ local residents and assist in advancing the City's economic development objectives and business profile for decades to come.

Summary and Enclosures

It is our respectful submission that the request minor variances satisfy the statutory tests and that the proposed development is entirely appropriate for the Subject Property and the broader Employment Area. We believe that the application is worthy of staff support and the approval of the Committee of Adjustment.

As indicated above, time is of the essence for Mircom to commence the construction of its new facility. Accordingly, we would very much appreciate being placed on the earliest possible Committee of Adjustment hearing agenda.

We trust you will find this application satisfactory. Enclosed with this cover letter are the required online submission form, charts, declarations and plans. Please do not hesitate to contact us if you have any questions or any additional information is required.

Yours truly,

Goodmans LLP



Ian Andres
INA/rr

cc: Mark Falbo and Yalda Riahi, Mircom