

ITEM: 6.22	COMMITTEE OF ADJUSTMENT REPORT SUMMARY MINOR VARIANCE APPLICATION FILE NUMBER A318/22 2901 Rutherford Road, Concord
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**THIS REPORT CONTAINS COMMENTS FROM THE FOLLOWING
DEPARTMENTS & AGENCIES:**

*Please see **Schedule B** of this report for a copy of Development Planning and Agency correspondence.

Additional comments from departments and agencies may be received after the publication of the Staff Report. These comments will be processed as an addendum and posted on the City's Website.

DEPARTMENTS	Circulated	Comments Received	Conditions	Nature of Comments
Committee of Adjustment	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Building Standards (Zoning Review)	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Building Inspection (Septic)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	No Comments Received to Date
Development Planning	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Application Under Review
Development Engineering	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	Recommend Approval w/Conditions
Parks, Forestry and Horticulture Operations	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
By-law & Compliance, Licensing & Permits	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	No Comments Received to Date
Development Finance	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Real Estate	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Fire Department	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	No Comments Received to Date
AGENCIES	Circulated	Comments Received	Conditions	Nature of Comments
TRCA	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	Recommend Approval w/Conditions
Ministry of Transportation (MTO)	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	No Comments or Concerns
Region of York	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	No Comments Received to Date
Alectra	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	General Comments
Bell Canada	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	No Comments Received to Date
YRDSB	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
YCDSB	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
CN Rail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
CP Rail	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
TransCanada Pipeline	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	No Comments Received to Date
Metrolinx	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Propane Operator	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	

PUBLIC & APPLICANT CORRESPONDENCE

*Please see **Schedule C** of this report for a copy of the public & applicant correspondence listed below.

The deadline to submit public comments is **noon on the last business day prior to the scheduled hearing date.**

Comments and written public submissions received after the publication of this Staff Report will be processed as an addendum and posted on the City's Website.

All personal information collected because of this public meeting (including both written and oral submissions) is collected under the authority of the Municipal Act, the Municipal Freedom of Information and Protection of Privacy Act (MFIPPA), the Planning Act and all other relevant legislation, and will be used to assist in deciding on this matter. All personal information (as defined by MFIPPA), including (but not limited to) names, addresses, opinions and comments collected will become property of the City of Vaughan, will be made available for public disclosure (including being posted on the internet) and will be used to assist the Committee of Adjustment and staff to process this application.

Correspondence Type	Name	Address	Date Received (mm/dd/yyyy)	Summary
None				

PREVIOUS COA DECISIONS ON THE SUBJECT LAND

*Please see **Schedule D** for a copy of the Decisions listed below

File Number	Date of Decision MM/DD/YYYY	Decision Outcome

PREVIOUS COA DECISIONS ON THE SUBJECT LAND

*Please see **Schedule D** for a copy of the Decisions listed below

None		
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ADJOURNMENT HISTORY

* Previous hearing dates where this application was adjourned by the Committee and public notice issued.

None	
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**COMMITTEE OF ADJUSTMENT REPORT
MINOR VARIANCE APPLICATION
A318/22
2901 Rutherford Road, Concord**

ITEM NUMBER: 6.22	CITY WARD #: 4
APPLICANT:	Rutherford Land Development Corporation
AGENT:	Nicole Sgrignuoli
PROPERTY:	2901 Rutherford Rd Vaughan ON L4K 5P1
ZONING DESIGNATION:	See Below
VAUGHAN OFFICIAL PLAN (2010) DESIGNATION:	Vaughan Official Plan 2010 ('VOP 2010'): "High-Rise Mixed-Use" by Volume 2, Section 11.7 Vaughan Mills Centre Secondary Plan and is further identified as a "Primary Centre".
RELATED DEVELOPMENT APPLICATIONS:	DA.19.010
PURPOSE OF APPLICATION:	Relief from the Zoning By-law is being requested to permit a proposed development consisting of (1) 30-storey and two (2) 26-storey residential apartment buildings with a total of 815 apartment units, and commercial/retail uses on the ground floor and to facilitate site plan application DA.19.010.

The following variances have been requested from the City's Zoning By-law:

The subject lands are zoned RM2(H) Multiple Unit Residential Zone ON HOLD under Zoning By-law 001-2021, as amended.

#	Zoning By-law 01-2021	Variance requested
	N/A	

The subject lands are zoned RA3(H) Residential Apartment Dwelling ON HOLD and subject to the provisions of Exception 9(1488) under Zoning By-law 1-88, as amended.

	Zoning By-law 1-88	Variance requested
1	Non-residential uses shall have a maximum gross floor area of 1,800m ² restricted to the ground floor. Exc. 9(1488)	To permit a maximum gross floor area of 452.4 m ² for non-residential uses located on the second level.
2	A minimum setback from the streetline to the first two-storeys of any building above finished grade shall be 3.0 metres. Exc. 9(1488)	To permit a minimum setback from the streetline to the first two-storeys of any building above finished grade of 2.9 metres.
3	To permit a minimum front yard of 1.8 metres above the first two-storeys. Schedule A	A minimum front yard of 7.5 metres is required above the first two-storeys.
4	To permit a minimum westerly exterior side yard of 1.4 metres and a minimum easterly exterior side yard of 1.7 metres. Schedule A	A minimum exterior side yard of 7.5 metres is required above the first two-storeys.
5	A minimum of 965 parking spaces are required. Exc. 9(1488)	To permit a minimum of 907 parking spaces on site.
6	A strip of land not less than 1.5 meters in width shall be provided along a lot line which abuts a street line. Exc. 9(1488)	To permit a strip of land not less than 0.0 metres in width be provided along a lot line which abuts a street line.
7	A maximum driveway width of 7.5 metres is permitted. S.3.8 g)	To permit a driveway and parking garage entrance width of 6.0 metres.

HEARING INFORMATION

DATE OF MEETING: Thursday, March 2, 2023

TIME: 6:00 p.m.

MEETING LOCATION: Vaughan City Hall, Council Chamber, 2141 Major Mackenzie Drive, Vaughan

LIVE STREAM LINK: Vaughan.ca/LiveCouncil

PUBLIC PARTICIPATION

If you would like to speak to the Committee of Adjustment at the meeting, either remotely or in person, please complete the [Request to Speak Form](#) and submit to cofa@vaughan.ca

If you would like to submit written comments, please quote file number above and submit by mail or email to:

Email: cofa@vaughan.ca

Mail: City of Vaughan, Office of the City Clerk, Committee of Adjustment, 2141 Major Mackenzie Drive, Vaughan, ON, L6A 1T1

THE DEADLINE TO REGISTER TO SPEAK OR SUBMIT WRITTEN COMMENTS ON THE ABOVE NOTED FILE(S) IS NOON ON THE LAST BUSINESS DAY BEFORE THE MEETING.

INTRODUCTION

Staff and Agencies act as advisory bodies to the Committee of Adjustment. The comments contained in this report are presented as recommendations to the Committee.

Section 45(1) of the Planning Act sets the criteria for authorizing minor variances to the City of Vaughan's Zoning By-law. Accordingly, review of the application may consider the following:

That the general intent and purpose of the by-law will be maintained.

That the general intent and purpose of the official plan will be maintained.

That the requested variance(s) is/are acceptable for the appropriate development of the subject lands.

That the requested variance(s) is/are minor in nature.

Public written and oral submissions relating to this application are taken into consideration by the Committee of Adjustment as part of its deliberations and final decision on this matter.

COMMITTEE OF ADJUSTMENT COMMENTS

Date Public Notice Mailed:	February 17, 2022	
Date Applicant Confirmed Posting of Sign:	February 16, 2022	
Applicant Justification for Variances: <small>*As provided by Applicant in Application Form</small>	Due to the constraints of the site Jane Street to the west, Caldari to the East and Abeja Street to the south and the design of the buildings the underground parking garage was unable to accommodate the require number of parking spaces and setbacks. Additional commercial space was added to the ground floor to create and active streetscape and provide a variety of uses within the development.	
Adjournment Requests (from staff): <small>*Adjournment requests provided to applicant prior to issuance of public notice</small>	None	
Was a Zoning Review Waiver (ZRW) Form submitted by Applicant: <small>*ZRW Form may be used by applicant in instances where a revised submission is made, and zoning staff do not have an opportunity to review and confirm variances prior to the issuance of public notice. *A revised submission may be required to address staff / agency comments received as part of the application review process. *Where a zoning review has not been completed on a revised submission, an opportunity is provided to the applicant to adjourn the proposal prior to the issuance of public notice.</small>	No	
Adjournment Fees: <small>In accordance with Procedural By-law 069-2019, an Adjournment Fee is applicable to reschedule an application after the issuance of public notice where a request for adjournment has been provided to the applicant prior to the issuance of public notice. An Adjournment Fee can only be waived in instances where adjournment of an application is requested by the Committee or staff after the issuance of public notice.</small>		
Committee of Adjustment Comments:	None	
Committee of Adjustment Recommended Conditions of Approval:	None	

BUILDING STANDARDS (ZONING) COMMENTS

**See Schedule B for Building Standards (Zoning) Comments

Building Standards Recommended Conditions of Approval:	None
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DEVELOPMENT PLANNING COMMENTS

**See Schedule B for Development Planning Comments.

Development Planning Recommended Conditions of Approval:	TBD
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DEVELOPMENT ENGINEERING COMMENTS

[Link to Grading Permit](#) [Link to Pool Permit](#) [Link to Curb Curt Permit](#) [Link Culvert Installation](#)

Development Engineering Department has reviewed Minor Variance application A318/22 and has no concerns with the proposed parking (907 spaces), as the application meets the parking requirements in By-law 001-2021 and can be supported by the walking distance to the Vaughan Mills terminal. Applicant should rearrange the proposed parking breakdown to ensure inclusion of all required visitor spaces: 0.2 spaces per unit visitor rate, and 2 spaces/100 m² retail GFA visitor rate.

The Development Engineering Department does not object to the Minor Variance application A318/22 subject to the following condition(s).

Development Engineering Recommended Conditions of Approval:	The Owner/Applicant shall obtain approval for the related Site Development Application (DA.19.010) from the Development Engineering (DE) Department.
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PARKS, FORESTRY & HORTICULTURE (PFH) COMMENTS

Forestry is working with development planning on this application. Forestry has no comment for COA purposes.

PFH Recommended Conditions of Approval:	None
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DEVELOPMENT FINANCE COMMENTS

That the payment of the City Development Charge is payable to the City of Vaughan prior to issuance of a building permit in accordance with the Development Charges Act and City-wide Development Charge By-law in effect at time of payment.

That the payment of Region of York Development Charge is payable to the City of Vaughan prior to issuance of a building permit in accordance with the Development Charges Act and Regional Development Charges By-laws in effect at time of payment.

That the payment of Education Development Charge is payable to the City of Vaughan prior to issuance of a building permit in accordance with the Education Act and York Region District School Board and York Catholic District School Board Development Charges By-laws in effect at time of payment.

That the payment of applicable Area Specific Development Charges are payable to the City of Vaughan prior to issuance of a building permit in accordance with the Development Charges Act and Area Specific Development Charge By-laws in effect at time of payment.

Development Finance Recommended Conditions of Approval:	None
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BY-LAW AND COMPLIANCE, LICENSING AND PERMIT SERVICES COMMENTS

No objections/comments

BCLPS Recommended Conditions of Approval:	None
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BUILDING INSPECTION (SEPTIC) COMMENTS

No Comments Received to Date

Building Inspection Recommended Conditions of Approval:	None
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FIRE DEPARTMENT COMMENTS

No Comments Received to Date

Building Inspection Recommended Conditions of Approval:

None

SCHEDULES TO STAFF REPORT

*See Schedule for list of correspondence

Schedule A	Drawings & Plans Submitted with the Application
Schedule B	Staff & Agency Comments
Schedule C (if required)	Correspondence (Received from Public & Applicant)
Schedule D (if required)	Previous COA Decisions on the Subject Land

Should the Committee find it appropriate to approve this application in accordance with request and the sketch submitted with the application, as required by Ontario Regulation 200/96, the following conditions have been recommended:

SUMMARY OF RECOMMENDED CONDITIONS OF APPROVAL

All conditions of approval, unless otherwise stated, are considered to be incorporated into the approval “if required”. If a condition is no longer required after an approval is final and binding, the condition may be waived by the respective department or agency requesting conditional approval. A condition cannot be waived without written consent from the respective department or agency.

#	DEPARTMENT / AGENCY	CONDITION(S) DESCRIPTION
1	Development Engineering Rex.bondad@vaughan.ca	The Owner/Applicant shall obtain approval for the related Site Development Application (DA.19.010) from the Development Engineering (DE) Department.
2	TRCA Kristen.Regier@trca.ca	That the applicant provides the required fee amount of \$1250.00 payable to the Toronto and Region Conservation Authority.
3	Development Planning roberto.simbana@vaughan.ca	TBD

IMPORTANT INFORMATION – PLEASE READ

CONDITIONS: It is the responsibility of the owner/applicant and/or authorized agent to obtain and provide a clearance letter from respective department and/or agency (**see condition chart above for contact**). This letter must be provided to the Secretary-Treasurer to be finalized. All conditions must be cleared prior to the issuance of a Building Permit.

APPROVALS: Making any changes to your proposal after a decision has been made may impact the validity of the Committee’s decision.

An approval obtained from the Committee of Adjustment, where applicable, is tied to the building envelope shown on the plans and drawings submitted with the application and subject to the variance approval.

A building envelope is defined by the setbacks of the buildings and/or structures shown on the plans and drawings submitted with the application, as required by Ontario Regulation 200/96. Future development outside of an approved building envelope, where a minor variance was obtained, must comply with the provisions of the City’s Zoning By-law.

Elevation drawings are provided to reflect the style of roof (i.e. flat, mansard, gable etc.) to which a building height variance has been applied. Where a height variance is approved, building height is applied to the style of roof (as defined in the City’s Zoning By-law) shown on the elevation plans submitted with the application.

Architectural design features that are not regulated by the City’s Zoning By-law are not to be considered part of an approval unless specified in the Committee’s decision.

DEVELOPMENT CHARGES: That the payment of the Regional Development Charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the Regional Development Charges By-law in effect at the time of payment.

That the payment of the City Development Charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the City’s Development Charges By-law in effect at the time of payment.

That the payment of the Education Development Charge if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the Boards of Education By-laws in effect at the time of payment

IMPORTANT INFORMATION – PLEASE READ

That the payment of Special Area Development charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and The City's Development Charge By-law in effect at the time of Building permit issuance to the satisfaction of the Reserves/Capital Department.

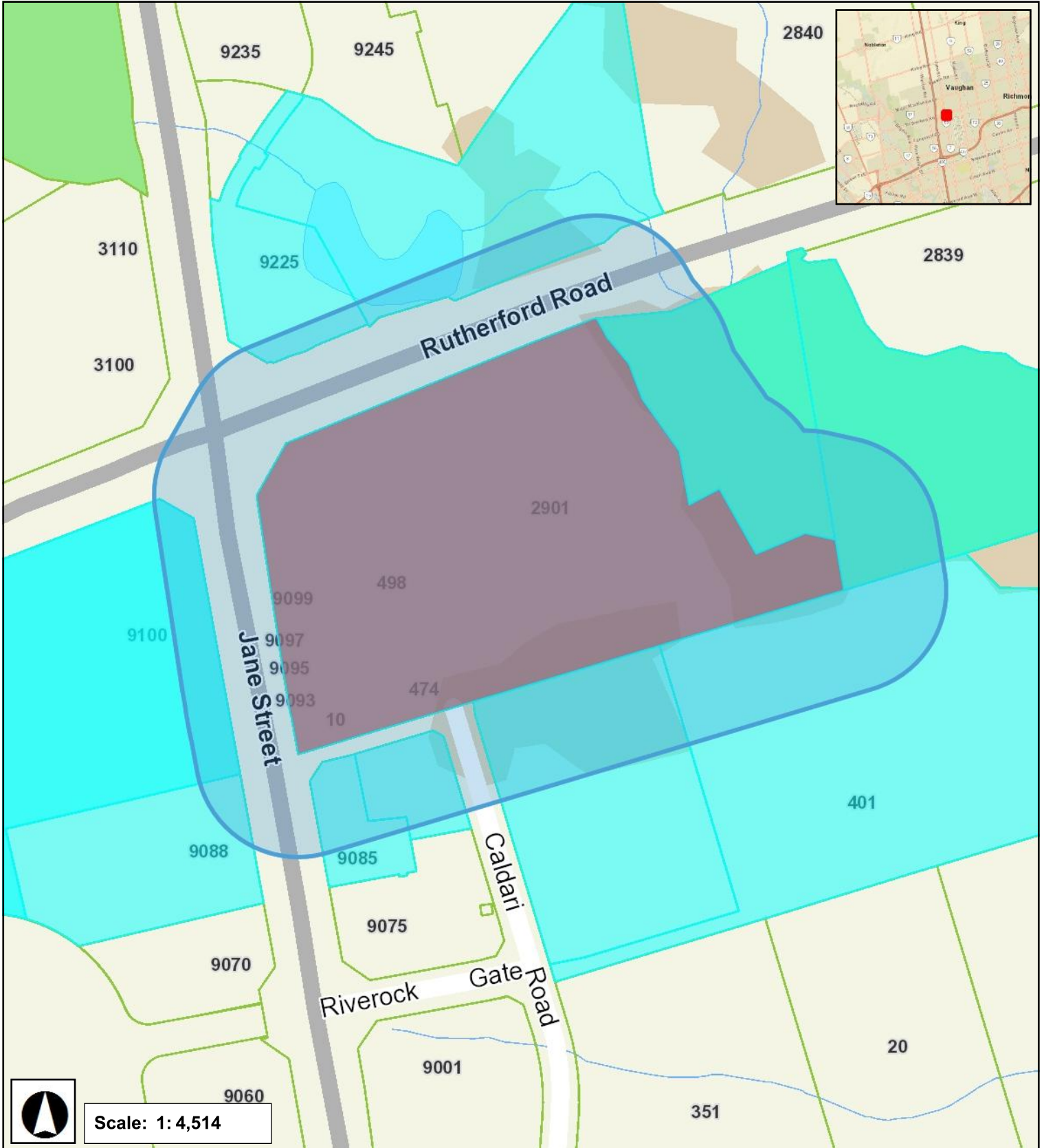
NOTICE OF DECISION: If you wish to be notified of the decision in respect to this application or a related Ontario Land Tribunal (OLT) hearing you must complete a Request for Decision form and submit to the Secretary Treasurer (ask staff for details). In the absence of a written request to be notified of the Committee's decision you will **not** receive notice.

SCHEDULE A: DRAWINGS & PLANS



LOCATION MAP A318/22

2901 RUTHERFORD ROAD, VAUGHAN



Scale: 1:4,514

AREA STATS

	Floor	GBA/Typ. Floor (sm) ⁴	No. Typ. Floors	GBA Gross Building Area (no exclusions)		GFA ¹ Exclusions (sm)	Vaughan By-law 1-88		Floor Area by Use ²						Saleable Area ⁶				Notes					
				sm	sf		GFA ¹		Residential		Retail		Com. Space		studio	1 B 1 B+D	2B 2B+D	3B		Total Suites	Res. Saleable	Com. Saleable	Indoor Amenity sm	Outdoor Amenity
							sm	sf	sm	sf	sm	sf	sm	sf										
Tower A3	Level 30	748.9	1	748.9			748.9		748.9					0	2	1	4	7						
Tower A3	Level 5-29	748.9	25	18722.5			18722.5		18722.5					24	150	99	1	274	16347.5					
Tower A1	Level 7-27	749.5	21	15739.5			15739.5		15739.5					0	105	84	21	210	13479.9					
Tower A2	Level 26	748.9	1	748.9			748.9		748.9					0	2	1	4	7	649.7					
Tower A2	Level 8-25	748.9	18	13480.2			13480.2		13480.2					18	108	72	0	198	11757.6					
Tower A2	Level 7	748.9	1	748.9			748.9		748.9					0	6	3	1	10	653.2					
Podium	Level 6	1078.9	1	1078.9			1078.9		1078.9					0	11	4	0	15	921.0					
	Level 5	1819.9	1	1819.9			1819.9		1819.9					0	10	5	0	15	1559.0			741.0	1048.4	
	Level 4	4038.1	1	4038.1			4038.1		4038.1					1	13	24	6	44	3078.9					
	Level 3	4101.7	1	4101.7			4101.7		4101.7					1	13	24	6	44	3081.0			165.0		
	Level 2	2855.2	1	2855.2			2855.2		2402.8					0	0	0	0	0	468.8	452.4				
	Ground	5745.0	1	5745.0		2415.8	3329.2		2315.2		969.4				0	5	2	0	7	349.9	44.6			
	P1	7868.1	1	7868.1		3815.5	4052.6																	
	P2-P4	7891.5	3	23674.4		12486.3	11188.1																	
TOTALS				69827.7			67411.9		65945.5										52,346.5	497.0	906.0	1,048	0	TOTALS
			30	100,621.3			67,411.9		65,945.5		969.4		497.0	44	425	319	43	831						
														5.3%	51.1%	38.4%	5.2%	100%						% of Suite Type
Floor Area by Use Breakdown	Above Grade	Floor Area (Retail) ³		969.4 sm		Floor Area (Com. Space) ³		497.0 sm		Floor Area (Residential) ⁵		65945.5 sm						63.0		Average Suite Size (sm)				
																		678		Average Suite Size (sf)				
	Below Grade	Floor Area Parking		15240.7 sm																				
Area Totals & FSI	Site Area= 7,940	Gross Floor Area ¹ Total		82,652.6 sm		Floor Space Index (FSI)		10.4																

#1

452.4

Notes From Vaughan By-law 1-88:

¹ FLOOR AREA, GROSS (G.F.A.) - Means the aggregate of the floor areas of all storeys of a building, measured to the exterior of the outside walls, but not including the areas of any cellar, or car parking area above or below grade within the building or within a separate structure.

² FLOOR AREA - Means the total area devoted to a particular use within a building, measured to the exterior limits of that use.

³ Max Permitted Non-Residential Gross floor Area is 1,800 m2.

⁴ Maximum Floor Plate of Residential Apartment Tower Above Podium shall not exceed 750 m2.

⁵ Max Permitted Residential Gross Floor Area is 66,000 m2.

⁶ Saleable areas are not updated

The statistics below are based on Vaughan By-law 1-88, as amended by the site specific By-law- 2018 (OMB)

VEHICULAR PARKING ¹				Required		Provided	
Residential - Occupant							
1 BD/ Bach. (0.85/unit)	0.85	x	469	399			
2 BD (0.95/unit)	0.95	x	319	304			
3 BD+ (1.15/unit)	1.15	x	43	50			
Residential - Occupant Total				753	753		
Visitor (0.2/ unit)	0.20	x	831	167			
Retail (3/100sm)	3.0	x	9.69	30			
Com. Space (3/100sm)	3.0	x	4.97	15			
Total Retail/Community Space				45			
Total commercial (Retail+Com. Space)/visitor required ²				167	155		
TOTAL				920	908		
Barrier Free Parking Spaces Included in TOTAL above (10 spaces Type A and 10 spaces Type B)				20	20		

BICYCLE PARKING ³				Required		Provided	
Residential							
Long Term (0.5/unit)	0.5	x	831	416	434		
Short Term (0.2/unit)	0.2	x	831	167	180		
Retail - Long Term (0.1/100sm)	0.1	x	9.69	1	1		
Retail - Short Term (0.15/100sm)	0.15	x	9.69	2	2		
Community Space* (0.1/100sm)	0.1	x	4.97	1	1		
Community Space* (0.15/100sm)	0.15	x	4.97	1	1		
TOTAL				588	619		

LOADING				Required		Provided	
Residential							
831 Dwelling Units				1	1		
Retail	969 sm			1	1		
TOTAL				1	1		

Loading Space: As per bylaw 1-88 (3.9), Industrial/Commercial Floor Area From 501-2500 m2 one loading space (9x3.5x4.2) m loading space

VEHICULAR PARKING LOCATION					AMENITY SPACE ⁴ / POPs				NOTES	
Parking Space Location					Residential Amenity Areas					Vaughan By-law 1-88 FLOOR AREA, GROSS (G.F.A.) - Means the aggregate of the floor areas of all storeys of a building, measured to the exterior of the outside walls, but not including the areas 8 of any cellar, or car parking area above or below grade within the building or within a separate structure. Parking Spaces: min 900 spaces subject to the following 1. Multi-Unit Residential Parking Standard: 0.85/unit (1B), 0.95/unit (2B), 1.15/unit (3B), 0.2/unit (Visitor). Retail/ Community Space Uses Parking Spaces: Total GFA of all buildings less than or equal to 5,000 m ² eating establishment no greater than 20% of GFA= min 3/100 m ² GFA Barrier Free Parking- 201-1000 spaces = 2spaces plus 2% of total number of parking spaces. of which, 50% shall be Type A and 50% shall be Type B. 2. Parking spaces for residential visitors, retail and community space uses may be shared and do not need to be individually designated. 3. Bicycle Parking : As per IBI Group report, bicycle parking: short-term bicycle parking spaces are required for 0.2 spaces per unit or 6 spaces. Long-term bicycle parking spaces are required 0.5 spaces per unit for buildings with more than 10 units. 4. As per By-law 1-88, amenity is defined as means space outside a dwelling unit within or outside the building designed for the passive enjoyment or active recreational needs of the residents. 2.5m2 per unit 5. As per By-law 1-88, a minimum of ten percent (10%) of the area of every lot on which a building or structure is erected shall be used for no other purpose than landscaping. Landscaping or landscaped area means an area of land comprising trees, shrubs, flowers, grass or other horticultural elements. Landscaping may include paths, patios, walkways, decorative stonework or other architectural elements designed to enhance the visual amenity of a property but does not include open storage display areas, parking or loading areas, or areas covered by driveways
Level	RESIDENTIAL		NON-RESIDENTIAL		TOTAL					
	Occupant	Visitor	Retail	Com. Space						
Ground		11	30	15	56					
P1	104	99			203					
P2	214				214					
P3	214				214					
P4	221				221					
TOTAL	753	110	30	15	908					

LANDSCAPE AREA ⁵		Required	Provided
Property Area:	7840 sm		
10% of the area:		784 sm	800 sm

#5

DRAWING LIST

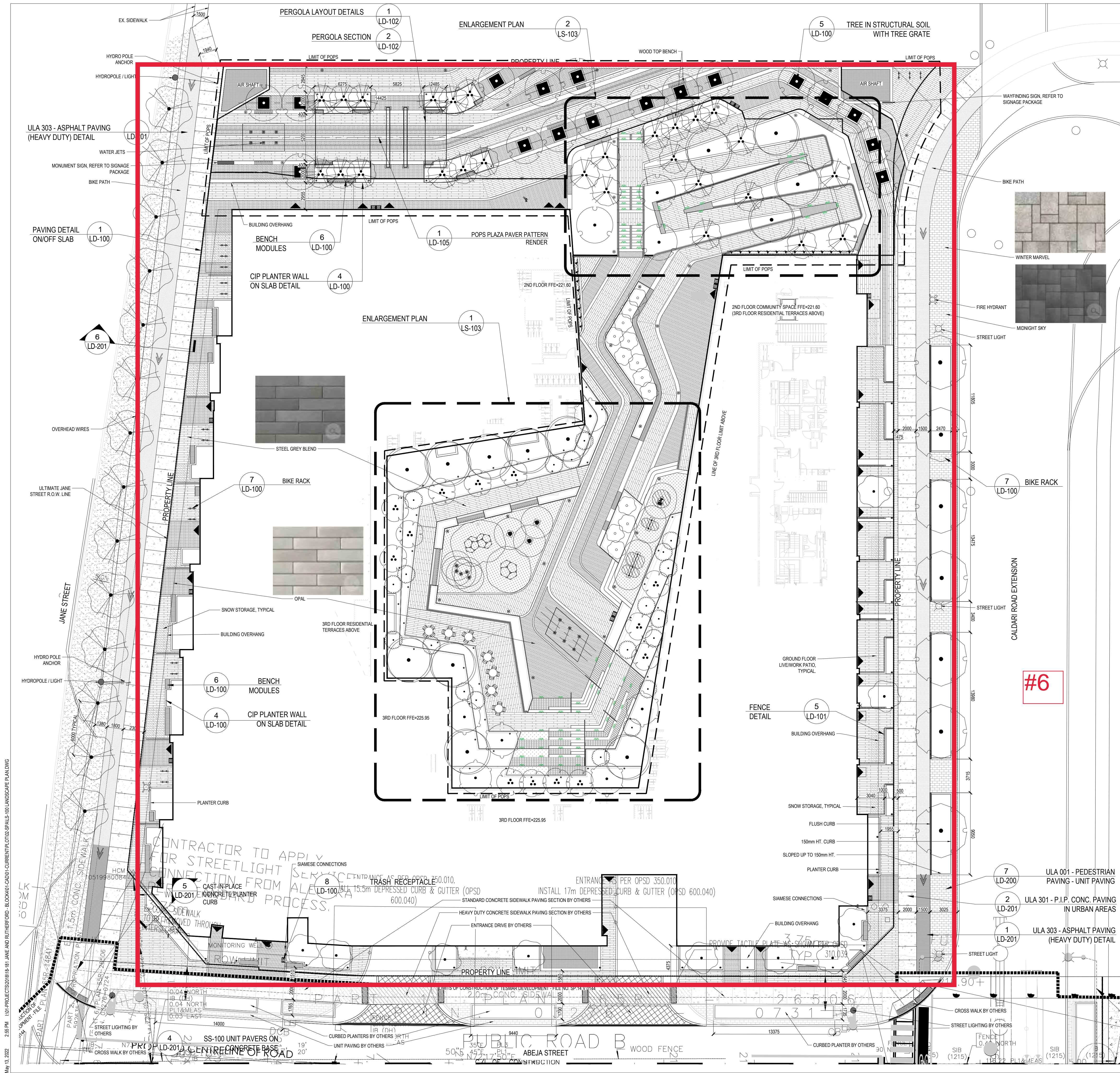
Sheet Number	Sheet Title
LS-100	LANDSCAPE PLAN
LS-101	PLANTING PLAN
LS-102	ENLARGEMENT PLAN
LS-103	LIGHTING PLAN
E-100	PHOTOMETRICS PLAN
LS-200	OVERALL AMENITY LEVEL PLAN
LS-201	ENLARGED WEST AMENITY PLAN
LS-202	ENLARGED SOUTH AMENITY PLAN
LP-201	ENLARGED WEST PLANTING PLAN
LP-202	ENLARGED SOUTH PLANTING PLAN
LD-100	LANDSCAPE DETAILS
LD-101	LANDSCAPE DETAILS
LD-102	LANDSCAPE DETAILS
LD-103	LANDSCAPE DETAILS
LD-104	LANDSCAPE DETAILS
LD-200	CITY OF VAUGHAN STANDARD DETAILS
LD-201	CITY OF VAUGHAN AND YORK REGION STANDARD DETAILS

LEGEND

	PEDESTRIAN CONCRETE PAVING
	HEAVY DUTY CONCRETE PAVING
	UNILOCK PROMENADE PLANK PAVER SIZE: 400 X 100 X 100mm COLOUR: STEEL GREY BLEND PATTERN: STACK BOND 1/3 OFFSET
	UNILOCK PROMENADE PLANK PAVER SIZE: 600 X 200 X 100mm COLOUR: OPAL PATTERN: STACK BOND 1/3 OFFSET
	UNILOCK UMBRIANO PAVER SIZE: 200 X 200 X 70mm COLOUR: WINTER MARVEL & MIDNIGHT SKY PATTERN: RUNNING BOND
	COLOURED ASPHALT BIKE PATH - STREET BOND 150B, REGULAR COLOURED ASPHALT
	COLOURED ASPHALT BIKE PATH - STREET BOND 150B, GREEN COLOURED ASPHALT
	TPV PLAY SURFACE
	HERB GARDEN
	COLUMN LIGHT - 4800mm HT.
	COLUMN LIGHT - 3600mm HT.
	BOLLARD LIGHT - 890mm HT.
	BOLLARD LIGHT - 500mm HT.
	RECESSED LIGHT
	1.2m HT. METAL PICKET FENCE
	1.2m HT. BLACK VINYL CHAINLINK FENCE
	GLASS HAND RAIL
	TREE GRATE
	BIKE RACK
	BIKE REPAIR STATION
	TACTILE INDICATOR STRIP
	RELATIVE GRADE ELEVATION
	TRASH & RECYCLE RECEPTACLE
	FLUSH CURB
	SOIL TRENCH BELOW
	SNOW STORAGE AREAS

NOTE: ANY ENHANCED STREETScape WITHIN THE PUBLIC RIGHTS OF WAY IS SUBJECT TO MAINTENANCE AGREEMENT WITH THE CITY.

1:200



no.	revision	date	by
08	REISSUED FOR SPA	2022-05-13	TM
08	REISSUED FOR SPA	2022-03-21	TM
07	REISSUED TO YORK REGION FOR SPA	2021-09-28	TM
06	REISSUED FOR SPA	2021-03-08	TM
05	ISSUED FOR SPA	2020-05-25	AF
04	ISSUED FOR SPA	2020-02-12	AF
03	ISSUED FOR PROGRESS SET	2019-12-12	AF
02	ISSUED FOR SPA	2019-09-13	AF
01	ISSUED FOR SPA	2019-01-10	AF

CONTRACTOR MUST VERIFY ALL DIMENSIONS ON THE JOB AND REPORT ANY DISCREPANCY TO THE LANDSCAPE ARCHITECT BEFORE PROCEEDING WITH THE WORK.
ALL DRAWINGS AND SPECIFICATIONS ARE INSTRUMENTS OF SERVICE AND THE PROPERTY OF THE LANDSCAPE ARCHITECT WHICH MUST BE RETURNED AT THE COMPLETION OF THE WORK.
ALL DRAWINGS TO BE USED FOR CONSTRUCTION ONLY WHEN SIGNED BY THE LANDSCAPE ARCHITECT.

signed _____ date _____

STUDIO tla
20 Champlain Blvd. Suite 102 - Toronto ON - M3H 2Z1 info@studiotla.ca www.studiotla.ca

CORTEL GROUP

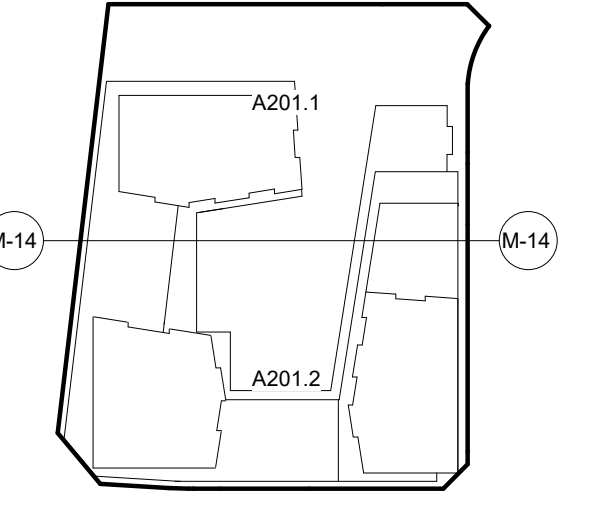
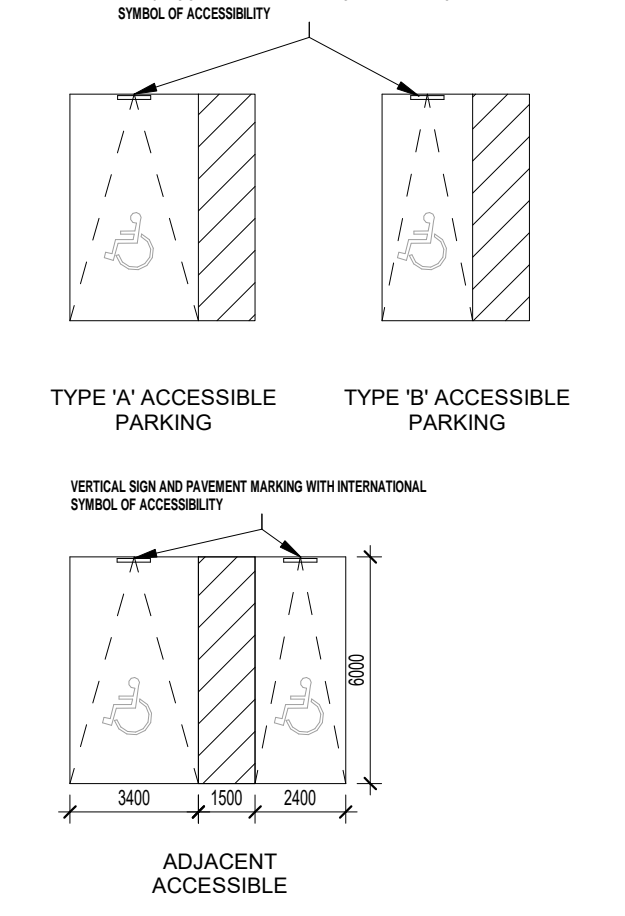
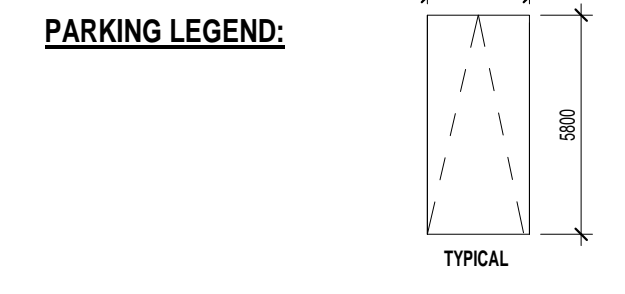
CLIENT
JANE & RUTHERFORD - BLOCK 4
VAUGHAN, ON
PROJECT REGION FILE NO.: SP.19.V.0047
LANDSCAPE PLAN

DRAWING
DRAWN: AF
CHECKED: PMJC
SCALE: 1:200
DATE: 2022-05-13
PROJECT NO.: 18-181

LS-100
SHEET NO.

May 13, 2022 2:55 PM I:\P-PROJECTS\2018\18-181 JANE AND RUTHERFORD - BLOCK 4\01-CAD\01-CURRENT\01-DWG-SPALS-100 LANDSCAPE PLAN.DWG

- PARKING NOTES:**
1. MINIMUM VAUGHAN PARKING SPACE SIZES (UNLESS OTHERWISE NOTED) AS PER BY-LAW 18
 2. 2700mm WIDE X 5800mm LONG (NO SIDES OBSTRUCTED)
 3. 3000mm WIDE X 6000mm LONG (TYPICAL ACCESSIBLE PARKING SPACE)
 4. 3200mm WIDE X 6000mm LONG (TYPICAL ACCESSIBLE SPACE ADJACENT TO ANOTHER)
 5. MAINTAIN MINIMUM DRIVE ASLE WIDTH OF 6000mm FOR PARKING SPACES WITH 90-DEGREE ANGLE
 6. MAINTAIN MINIMUM DRIVE ASLE WIDTH OF 3000mm FOR PARKING SPACES WITH 45-DEGREE ANGLE
 7. MAINTAIN MINIMUM DRIVE ASLE WIDTH OF 6000mm FOR PARKING SPACES WITH 45-DEGREE ANGLE
 8. MAINTAIN MINIMUM HEADROOM CLEARANCE OF 2100mm THROUGHOUT



- CEILING ASSEMBLIES LEGEND:**
- PINNED VINYL FACED INSULATION (CL 10)
 - PINNED INSULATION + DROPPED ALUMINUM PANEL (SCFFIT (SP))
 - DROPPED ALUMINUM PANEL SCFFIT (SP)

Received
January 11, 2023

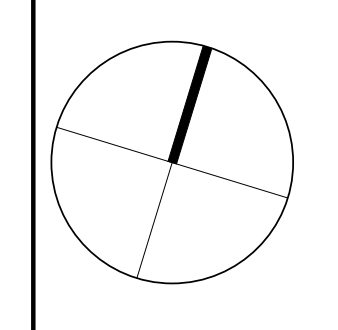
A318/22

REVISION RECORD

Date	No.	Description
2022-10-11	20	Issued for Addendum 34
2022-09-20	19	Issued for Addendum 32
2022-07-21	18	Issued for Addendum 27
2022-06-30	17	Issued for Addendum 24
2022-04-25	16	Issued for Addendum 20
2022-04-12	15	Issued for Addendum 19
2022-01-28	14	Issued for Addendum 13
2022-01-18	13	Issued for Addendum 12
2021-08-31	12	Issued for Addendum 007
2021-03-11	11	Issued for Addendum 004
2021-01-29	10	Issued for Addendum 003

ISSUE RECORD

Date	Description
2022-10-14	Issued for Construction
2022-07-15	Issued for Building Permit Responses
2022-04-18	Issued for SPA Resubmission-Rev 06
2021-07-07	Re-issue for Tender
2021-04-06	Issued for Building Permit Resubmission
2021-03-09	Issued for SPA Resubmission-Rev 05
2020-12-15	Issued for SPA Resubmission-Rev 04
2020-09-22	Issued for Tender
2020-08-24	Issued for CNMAGNA
2020-08-24	Issued For



BDP. Quadrangle

Quadrangle Architects Limited
8601 King Street West, Suite 701
416-598-5240 www.bdpquadrangle.com

Jane and Rutherford- Block 4
Vaughan, ON

for
Rutherford Land Development
Corporation

17086 1:150 AA KCR
PROJECT SCALE DRAWN REVIEWED

Ground Floor Plan

A201

Note: This drawing is the property of the Architect and may not be reproduced or used without the expressed consent of the Architect. The Contractor is responsible for checking and verifying all levels and dimensions and shall report all discrepancies to the Architect and other consultants prior to commencing work.

C:\Users\A4451\Documents\CAL_SITE_17086_PARKING-SITE_Jane&Rutherford_Blk_4_2022_A4613.rvt

1 GROUND FLOOR PLAN
A201 SCALE: 1:150

SCHEDULE B: STAFF & AGENCY COMMENTS				
DEPT/AGENCY	Circulated	Comments Received	Conditions	Nature of Comments
TRCA	X	X	X	Recommend Approval w/Conditions
Ministry of Transportation (MTO)	X	X		No Comments or Concerns
Region of York	X			No Comments Received to Date
Alectra *Schedule B	X	X		General Comments
Bell Canada *Schedule B	X			No Comments Received to Date
YRDSB *Schedule B				
YCDSB *Schedule B				
CN Rail *Schedule B				
CP Rail *Schedule B				
TransCanada Pipeline *Schedule B	X			No Comments Received to Date
Metrolinx *Schedule B				
Propane Operator *Schedule B				
Development Planning	X			Application under review
Building Standards (Zoning)	X	X		General Comments

Date: January 23rd 2023

Attention: **Christine Vigneault**

RE: Request for Comments

File No.: **A318-22**

Related Files:

Applicant Rutherford Land Development Corp

Location 2901 Rutherford Road



Discover the possibilities

COMMENTS:

- We have reviewed the proposed Variance Application and have no comments or objections to its approval.
- We have reviewed the proposed Variance Application and have no objections to its approval, subject to the following comments (attached below).
- We have reviewed the proposed Variance Application and have the following concerns (attached below).

Alectra Utilities (formerly PowerStream) has received and reviewed the proposed Variance Application. This review, however, does not imply any approval of the project or plan.

All proposed billboards, signs, and other structures associated with the project or plan must maintain minimum clearances to the existing overhead or underground electrical distribution system as specified by the applicable standards, codes and acts referenced.

In the event that construction commences, and the clearance between any component of the work/structure and the adjacent existing overhead and underground electrical distribution system violates the Occupational Health and Safety Act, the customer will be responsible for 100% of the costs associated with Alectra making the work area safe. All construction work will be required to stop until the safe limits of approach can be established.

In the event construction is completed, and the clearance between the constructed structure and the adjacent existing overhead and underground electrical distribution system violates the any of applicable standards, acts or codes referenced, the customer will be responsible for 100% of Alectra's cost for any relocation work.

References:

- Ontario Electrical Safety Code, latest edition (Clearance of Conductors from Buildings)
- Ontario Health and Safety Act, latest edition (Construction Protection)
- Ontario Building Code, latest edition (Clearance to Buildings)
- PowerStream (Construction Standard 03-1, 03-4), attached
- Canadian Standards Association, latest edition (Basic Clearances)

If more information is required, please contact either of the following:

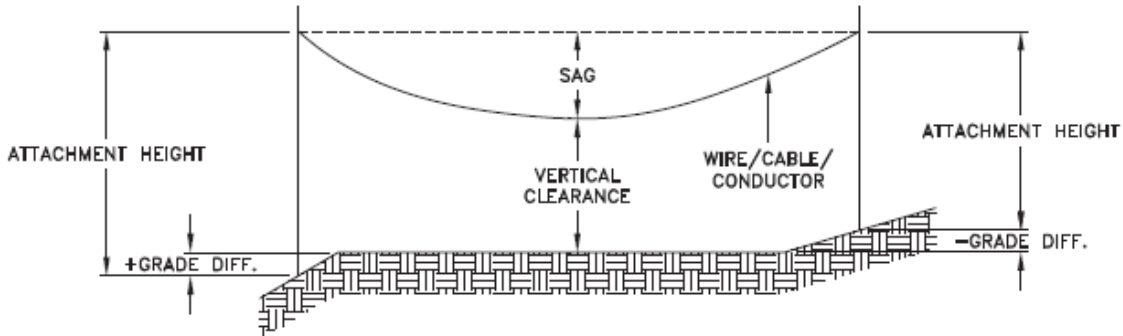
Mr. Stephen Cranley, C.E.T
Supervisor, Distribution Design, ICI & Layouts (North)
Phone: 1-877-963-6900 ext. 31297

E-mail: stephen.cranley@alectrautilities.com

Mitchell Penner
Supervisor, Distribution Design-Subdivisions
Phone: 416-302-6215

Email: Mitchell.Penner@alectrautilities.com

LOCATION OF WIRES, CABLES OR CONDUCTORS	SYSTEM VOLTAGE			
	SPAN GUYS AND COMMUNICATIONS WIRES	UP TO 600V AND NEUTRAL	4.16/2.4kV TO 27.6/16kV (SEE NOTE 1)	44kV
MINIMUM VERTICAL CLEARANCES (SEE NOTE 2)				
OVER OR ALONGSIDE ROADS, DRIVEWAYS OR LANDS ACCESSIBLE TO <u>VEHICLES</u>	442cm	442cm	480cm	520cm
OVER GROUND ACCESSIBLE TO <u>PEDESTRIANS AND BICYCLES ONLY</u>	250cm	310cm	340cm	370cm
ABOVE TOP OF RAIL AT <u>RAILWAY CROSSINGS</u>	730cm	730cm	760cm	810cm



MINIMUM ATTACHMENT HEIGHT = MAXIMUM SAG
 + MINIMUM VERTICAL CLEARANCE (FROM ABOVE TABLE)
 ± GRADE DIFFERENCE
 + 0.3m (VEHICLE OR RAILWAY LOCATION)
 + SNOW DEPTH (PEDESTRIAN LOCATION, SEE NOTE 3)

NOTES:

1. THE MULTIGROUNDED SYSTEM NEUTRAL HAS THE SAME CLEARANCE AS THE 600V SYSTEM.
2. THE VERTICAL CLEARANCES IN THE ABOVE TABLE ARE UNDER MAXIMUM SAG CONDITIONS.
3. REFER TO CSA STANDARD C22.3 No.1, ANNEX D FOR LOCAL SNOW DEPTH VALUES.
4. ALL CLEARANCES ARE IN ACCORDANCE TO CSA STANDARD C22.3.

CONVERSION TABLE

METRIC	IMPERIAL (APPROX)
810cm	27'-0"
760cm	25'-4"
730cm	24'-4"
520cm	17'-4"
480cm	16'-0"
442cm	15'-5"
370cm	12'-4"
340cm	11'-4"
310cm	10'-4"
250cm	8'-4"

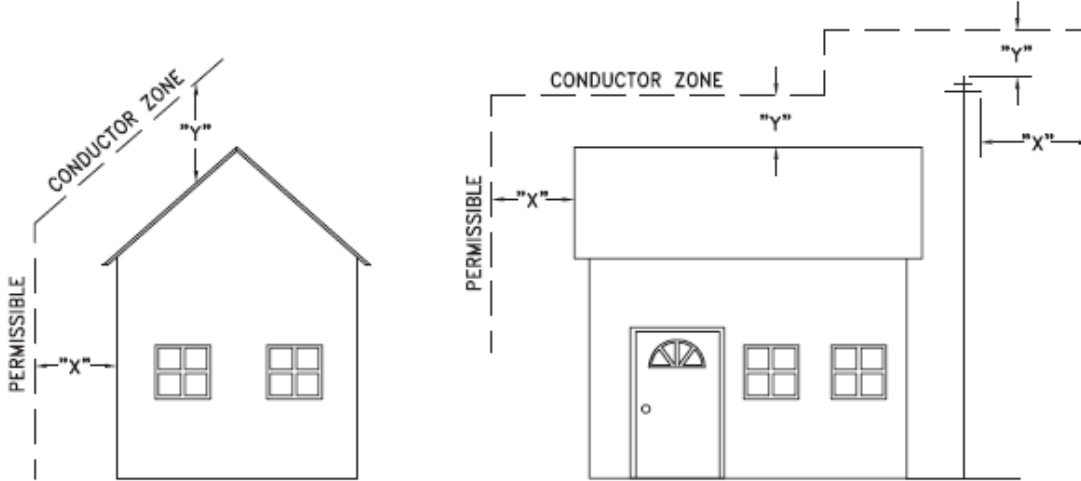
REFERENCES

SAGS AND TENSIONS | SECTION 02

MINIMUM VERTICAL CLEARANCES OF WIRES, CABLES AND CONDUCTORS ABOVE GROUND OR RAILS

ORIGINAL ISSUE DATE: 2010-DEC-24 REVISION NO: R1 REVISION DATE: 2012-JAN-09

Certificate of Approval	
This construction Standard meets the safety requirements of Section 4 of Regulation 22/04	
Joe Crozier, P.Eng.	2012-JAN-09
Name	Date
P.Eng. Approval By:	Joe Crozier



VOLTAGE	MINIMUM HORIZONTAL CLEARANCE UNDER MAXIMUM SWING CONDITIONS DIMENSION "X" (SEE NOTES 1, 3 & 4)	MINIMUM VERTICAL CLEARANCE UNDER MAXIMUM DESIGN SAG CONDITIONS DIMENSION "Y" (SEE NOTES 1, 2, 4 & 5)
0-600V AND NEUTRAL	100cm	250cm
4.16/2.4 TO 44kV	300cm	480cm

NOTES

1. UNDER NO CIRCUMSTANCES SHALL A CONDUCTOR BE PERMITTED TO PENETRATE THE ENVELOPE SHOWN BY THE DOTTED LINE.
2. THE VERTICAL CLEARANCES ARE UNDER CONDITIONS OF MAXIMUM DESIGN SAG.
3. THE HORIZONTAL CLEARANCES ARE UNDER CONDITIONS OF MAXIMUM SWING. WHERE THE CONDUCTOR SWING IS NOT KNOWN A HORIZONTAL CLEARANCE OF 480CM SHALL BE USED.
4. BUILDINGS THAT EXCEED 3 STOREYS OR 15M IN HEIGHT, THE MINIMUM HORIZONTAL CLEARANCE OF THE SECONDARY CONDUCTORS SHOULD BE INCREASED TO 300cm WHERE IT IS NECESSARY TO ALLOW FOR THE RAISING OF LADDERS BY LOCAL FIRE DEPARTMENTS.
5. IN SITUATIONS SUCH AS MULTI-LEVEL GARAGES, WHERE ROOFS ARE NORMALLY USED BY PERSONS AND VEHICLES, THE VERTICAL CLEARANCES OF POWERSTREAM STANDARD 03-1 SHALL APPLY.
6. DISTRIBUTION LINES CONSTRUCTED NEAR BUILDINGS SHALL BE BUILT TO AVOID OVERHANG WHEREVER POSSIBLE. WHERE LINES MUST BE CONSTRUCTED OVER OR ADJACENT TO BUILDINGS THE APPLICABLE HORIZONTAL AND VERTICAL CLEARANCES SHALL BE AT CONDITIONS OF MAXIMUM CONDUCTOR SWING AND MAXIMUM SAG. THE ABOVE CLEARANCES ARE DESIGNED TO PREVENT PERSONS ON OR IN BUILDINGS AS WELL AS EXTERNAL MACHINERY USED IN CONJUNCTION WITH A BUILDING TO COME IN CONTACT WITH CONDUCTORS. EFFORTS SHOULD BE MADE TO INCREASE THESE CLEARANCES WHERE POSSIBLE.
7. ALL CLEARANCES ARE IN ACCORDANCE TO CSA C22.3 NO.1-06 (TABLE-9).

METRIC	IMPERIAL (APPROX)
480cm	16'-0"
300cm	10'-0"
250cm	8'-4"
100cm	3'-4"

MINIMUM VERTICAL & HORIZONTAL CLEARANCES OF CONDUCTORS FROM BUILDINGS OR OTHER PERMANENT STRUCTURES (CONDUCTORS NOT ATTACHED TO BUILDINGS)

Certificate of Approval
This construction Standard meets the safety requirements of Section 4 of Regulation 22/04
Debbie Dadwani, P.Eng. 2010-MAY-05
Name Date
P. Eng. Approval By: *D. Dadwani*

ORIGINAL ISSUE DATE: 2010-MAY-05 REVISION NO: REVISION DATE:
PS:\System Planning and Standards\Standard Design\PowerStream Standards\PowerStream Standards working files\Section 3-1-02\WG 03-4 R0 May 5, 2010.dwg, 5/5/2010 10:22:02 AM, Adobe PDF

To: Committee of Adjustment

From: Pia Basilone, Building Standards Department

Date: February 8, 2023

Applicant: Rutherford Land Development Corporation

Location: CONC 4 Part of Lot 15
PLAN 65R26506 Part 1 municipally known as 2901 Rutherford Road

File No.(s): A318/22

Zoning Classification:

The subject lands are zoned RM2(H) Multiple Unit Residential Zone ON HOLD under Zoning By-law 001-2021, as amended.

#	Zoning By-law 001-2021	Variance requested
1	N/A	

The subject lands are zoned RA3(H) Residential Apartment Dwelling ON HOLD and subject to the provisions of Exception 9(1488) under Zoning By-law 1-88, as amended.

Zoning By-law 1-88		Variance requested
1	Non-residential uses shall have a maximum gross floor area of 1,800m ² restricted to the ground floor. Exc. 9(1488)	To permit a maximum gross floor area of 452.4m ² for non-residential uses located on the second level.
2	A minimum setback from the streetline to the first two-storeys of any building above finished grade shall be 3.0 metres. Exc. 9(1488)	To permit a minimum setback from the streetline to the first two-storeys of any building above finished grade of 2.9 metres.
3	To permit a minimum front yard of 1.8 metres above the first two-storeys. Schedule A	A minimum front yard of 7.5 metres is required above the first two-storeys.
4	To permit a minimum westerly exterior side yard of 1.4 metres and a minimum easterly exterior side yard of 1.7 metres. Schedule A	A minimum exterior side yard of 7.5 metres is required above the first two-storeys.
5	A minimum of 965 parking spaces are required. Exc. 9(1488)	To permit a minimum of 907 parking spaces on site.
6	A strip of land not less than 1.5 metres in width shall be provided along a lot line which abuts a street line. Exc. 9(1488)	To permit a strip of land not less than 0.0 metres in width be provided along a lot line which abuts a street line.
7	A maximum driveway width of 7.5 metres is permitted. S.3.8 g)	To permit a driveway and parking garage entrance width of 6.0 metres.

Staff Comments:

Stop Work Order(s) and Order(s) to Comply:

Order No. 22-110116, Stop Work Order, Issue Date: Apr 05, 2022
Order No. 22-106007, Order to Comply, Issue Date: Feb 28, 2022

Building Permit(s) Issued:

Building Permit No. 19-000824 for Temporary Sales Office - Issue Date: Dec 13, 2019
Building Permit No. 18-000768 for Single Detached Dwelling - Residential Demolition, Issue Date: Apr 09, 2018
Building Permit No. 20-109661 for Retaining Wall - Issue Date: Oct 08, 2021
Building Permit No. 21-108061 for Condo. Apartment Building - (Not Yet Issued)
Building Permit No. 21-108061 for Condo. Apartment Building - (Not Yet Issued)

Other Comments:

General Comments	
1	This application relates to DA.19.010.

Conditions of Approval:

None



2141 Major Mackenzie Drive
Vaughan, Ontario
Canada L6A 1T1
(905) 832-2281

* Comments are based on the review of documentation supplied with this application.

From: [Blaney, Cameron \(MTO\)](#)
To: [Christine Vigneault](#)
Cc: [Mulrenin, Colin \(MTO\)](#)
Subject: [External] RE: A318/22 (2901 Rutherford Road) - REQUEST FOR COMMENTS, CITY OF VAUGHAN
Date: Wednesday, January 18, 2023 9:13:25 AM

Thanks Christine,

2901 Rutherford Road is outside of MTO permit control area and does not require MTO permits or review.

Cameron Blaney | Corridor Management Planner | Simcoe & York

Highway Corridor Management Section – Central Operations

159 Sir William Hearst Avenue | Ministry of Transportation

7th Floor, Building D | Downsview, Ontario | M3M 0B7

416-358-7871

Cameron.Blaney@ontario.ca

February 6, 2023

X-Ref: 60805.02, 58559, 39666

SENT BY E-MAIL: Christine.Vigneault@vaughan.ca

Christine Vigneault
Secretary Treasurer
Committee of Adjustment
City of Vaughan
2141 Major Mackenzie Drive
Vaughan, Ontario L6A 1T1

Dear Christine:

**Re: Minor Variance Application A318/22
Part of Lot 15, Concession 4
2901 Rutherford Road
City of Vaughan, Region of York
Owner: Rutherford Land Development Corp**

This letter acknowledges receipt of the above-noted application circulated by the City of Vaughan. The materials were received by Toronto and Region Conservation Authority (TRCA) on January 17, 2023. TRCA staff have reviewed the application and offer the following comments for the consideration of the Committee of Adjustment.

Purpose of the Application

It is our understanding that the purpose of the above noted application is to request the following variance under By-Law 1-88:

By-Law 1-88:

- To permit a maximum gross floor area of 505.8 sq. m. for non-residential uses located on the second level, whereas non-residential uses shall have a maximum gross floor area of 1, 800 sq. m. restricted to the ground floor.
- To permit a minimum setback from the streetline to the first two-storeys of any building above finished grade of 2.9 m., whereas a minimum setback from the streetline to the first two-storeys of any building above finished grade shall be 3.0 m.
- To permit a minimum front yard of 7.5 m above the first two storeys, whereas a minimum front yard of 1.8 m above the first two storeys is permitted.
- To permit a minimum exterior side yard of 7.5 m above the first two storeys, whereas a minimum westerly exterior side yard of 1.4 metres and a minimum easterly exterior side yard of 1.7 metres is permitted.
- To permit a minimum of 907 parking spaces on site, whereas a minimum of 965 parking spaces are required.

- To permit a strip of land not less than 0.0 m. in width be provided along a lot line which abuts a street line, whereas a strip of land not less than 1.5 m. in width shall be provided along a lot line which abuts a street line.
- To permit a driveway and parking garage entrance width of 6.0 m., whereas a maximum width driveway width of 7.5 m. is permitted

The noted variances are being requested to facilitate the construction of three mixed use buildings; one 30 storey, and two 26 storey residential apartment buildings with retail at grade.

Ontario Regulation 166/06

The subject lands are partially located within TRCA's Regulated Area due to a valley corridor associated with a tributary of the Humber River that is located to the east of the site. In accordance with Ontario Regulation 166/06 (Regulation of Development, Interference with Wetlands and Alterations to Shorelines and Watercourses), development, interference or alteration may be permitted in the Regulated Area where it can be demonstrated to TRCA's satisfaction that the control of flooding, erosion, dynamic beaches, pollution, or the conservation of land will not be affected.

The works subject to this application are located within Block 4 on the western side of the site. This portion of the property is not located within TRCA's Regulated Area.

Application-Specific Comments

TRCA staff have been involved in the review of *Planning Act* applications for the subject site including Site Development Application DA.19.010 and Draft Plan of Subdivision 19T-18V001. TRCA provided comments to the City of Vaughan on May 26, 2022, confirming staff had no objection to the approval of the Draft Plan of Subdivision, subject to conditions outlined in the letter.

Based on a review of the materials circulated with the current minor variance application, TRCA is satisfied that the proposed variances do not impact the natural hazards associated with the valley corridor or established buffers. As such, TRCA staff has no concern with the proposed variances.

Fees

By copy of this letter, the applicant is advised that the TRCA has implemented a fee schedule for our planning application review services. This application is subject to a \$1250.00 (Minor Variance – Industrial/Commercial/Subdivision/Institutional - Minor) review fee. The applicant is responsible for fee payment and should forward the application fee to this office as soon as possible.

Recommendations

Based on the comments noted above, TRCA provides conditional approval to the approval of Minor Variance Application A318.22 subject to the following conditions:

1. That the applicant provides the required fee amount of \$1250.00 payable to the Toronto and Region Conservation Authority.

We trust these comments are of assistance. Should you have any questions, please contact me at 437-880-2129 or at Kristen.regier@trca.ca

Sincerely,

Kristen Regier

Kristen Regier

Planner I

Development Planning and Permits

KR/sb

SCHEDULE C: PUBLIC & APPLICANT CORRESPONDENCE

None