

**ITEM #: 6.19**

**COMMITTEE OF ADJUSTMENT REPORT  
MINOR VARIANCE APPLICATION  
A292/22  
30 Malaren Road, Maple**

**COA REPORT SUMMARY**

**THIS REPORT CONTAINS COMMENTS FROM THE FOLLOWING  
DEPARTMENTS & AGENCIES:**

\*Please see **Schedule B** of this report for a copy of Staff and Agency correspondence.

*Additional comments from departments and agencies may be received after the publication of the Staff Report. These comments will be processed as an addendum and posted on the City's Website.*

<b>DEPARTMENTS</b>	<b>Circulated</b>	<b>Comments Received</b>	<b>Conditions</b>	<b>Nature of Comments</b>
Committee of Adjustment	X	X		General Comments
Building Standards -Zoning Review *Schedule B	X	X		General Comments
Building Inspection (Septic)	X			No Comments Recieved to Date
Development Planning *Schedule B	X			Application Under Review
Development Engineering	X	X	X	Recommend Approval w/Conditions
Parks, Forestry and Horticulture Operations	X	X		No Comments or Concerns
By-law & Compliance, Licensing & Permits	X			No Comments Recieved to Date
Development Finance	X	X		No Comments or Concerns
Real Estate				
Fire Department	X			No Comments Recieved to Date
<b>AGENCIES</b>	<b>Circulated</b>	<b>Comments Received</b>	<b>Conditions</b>	<b>Nature of Comments</b>
TRCA *Schedule B	X			No Comments Recieved to Date
Ministry of Transportation (MTO) *Schedule B	X			No Comments Recieved to Date
Region of York *Schedule B	X	X		General Comments
Alectra *Schedule B	X	X		General Comments
Bell Canada *Schedule B	X			No Comments Recieved to Date
YRDSB *Schedule B				
YCDSB *Schedule B				
CN Rail *Schedule B				
CP Rail *Schedule B				
TransCanada Pipeline *Schedule B	X			No Comments Recieved to Date
Metrolinx *Schedule B				
Propane Operator *Schedule B				

**PUBLIC & APPLICANT CORRESPONDENCE**

\*Please see **Schedule C** of this report for a copy of the public & applicant correspondence listed below.

The deadline to submit public comments is **noon on the last business day prior to the scheduled hearing date.**

Comments and written public submissions received after the publication of this Staff Report will be processed as an addendum and posted on the City's Website.

All personal information collected because of this public meeting (including both written and oral submissions) is collected under the authority of the Municipal Act, the Municipal Freedom of Information and Protection of Privacy Act (MFIPPA), the Planning Act and all other relevant legislation, and will be used to assist in deciding on this matter. All personal information (as defined by MFIPPA), including (but not limited to) names, addresses, opinions and comments collected will become property of the City of Vaughan, will be made available for public disclosure (including being posted on the internet) and will be used to assist the Committee of Adjustment and staff to process this application.

<b>Correspondence Type</b>	<b>Name</b>	<b>Address</b>	<b>Date Received (mm/dd/yyyy)</b>	<b>Summary</b>
None				

**PREVIOUS COA DECISIONS ON THE SUBJECT LAND**

\*Please see **Schedule D** for a copy of the Decisions listed below

<b>File Number</b>	<b>Date of Decision MM/DD/YYYY</b>	<b>Decision Outcome</b>
None		

**ADJOURNMENT HISTORY**

\* Previous hearing dates where this application was adjourned by the Committee and public notice issued.

None	
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**COMMITTEE OF ADJUSTMENT REPORT  
MINOR VARIANCE APPLICATION  
A292/22  
30 Malaren Road, Maple**

**FILE MANAGER: Pravina Attwala, Administrative Coordinator - Committee of Adjustment**

<b>ITEM NUMBER: 6.19</b>	<b>CITY WARD #: 1</b>
<b>APPLICANT:</b>	Cristina Leonardis
<b>AGENT:</b>	Joseph Battaglia
<b>PROPERTY:</b>	30 Malaren Rd Maple ON L6A 1E2
<b>ZONING DESIGNATION:</b>	See Below
<b>VAUGHAN OFFICIAL PLAN (2010) DESIGNATION:</b>	Vaughan Official Plan 2010 ('VOP 2010'): "Low-Rise Residential"
<b>RELATED DEVELOPMENT APPLICATIONS:</b>	None
<b>PURPOSE OF APPLICATION:</b>	Relief from the Zoning By-law is being requested to permit an addition to the existing single-family dwelling, porch and proposed staircase.

The following variances have been requested from the City's Zoning By-law:

**The subject lands are zoned R1E(EN), First Density Residential Zone and subject to the provisions of Exception 14.534 under Zoning By-law 001-2021, as amended.**

#	Zoning By-law 001-2021	Variance requested
1	The minimum front yard setback required is 11.56 metres. [Table 7-3, Note 1.]	To permit a minimum front yard setback of 8.9 metres.
2	The minimum exterior side yard setback required is 9.0 metres. [Table 7-3]	To permit a minimum exterior side yard setback of 5.08 metres.

**The subject lands are zoned R1V, Old Village Residential Zone under Zoning By-law 1-88, as amended.**

#	Zoning By-law 1-88	Variance requested
4	The minimum front yard setback required is 11.56 metres. [Schedule 'A' Note 11.]	To permit a minimum front yard setback of 8.9 metres.
5	The minimum exterior side yard setback required is 9.0 metres. [Schedule 'A']	To permit a minimum exterior side yard setback of 5.08 metres.

**HEARING INFORMATION**

**DATE OF MEETING:** Thursday, December 8, 2022  
**TIME:** 6:00 p.m.  
**MEETING LOCATION:** Vaughan City Hall, Council Chamber, 2141 Major Mackenzie Drive, Vaughan  
**LIVE STREAM LINK:** [Vaughan.ca/LiveCouncil](http://Vaughan.ca/LiveCouncil)

**PUBLIC PARTICIPATION**

If you would like to speak to the Committee of Adjustment at the meeting, either remotely or in person, please complete the [Request to Speak Form](#) and submit to [cofa@vaughan.ca](mailto:cofa@vaughan.ca)  
 If you would like to submit written comments, please quote file number above and submit by mail or email to:

**Email:** [cofa@vaughan.ca](mailto:cofa@vaughan.ca)

**Mail:** City of Vaughan, Office of the City Clerk, Committee of Adjustment, 2141 Major Mackenzie Drive, Vaughan, ON, L6A 1T1

**THE DEADLINE TO REGISTER TO SPEAK OR SUBMIT WRITTEN COMMENTS ON THE ABOVE NOTED FILE(S) IS NOON ON THE LAST BUSINESS DAY BEFORE THE MEETING.**

## INTRODUCTION

Staff and Agencies act as advisory bodies to the Committee of Adjustment. The comments contained in this report are presented as recommendations to the Committee.

Section 45(1) of the Planning Act sets the criteria for authorizing minor variances to the City of Vaughan's Zoning By-law. Accordingly, review of the application may consider the following:

- That the general intent and purpose of the by-law will be maintained.
- That the general intent and purpose of the official plan will be maintained.
- That the requested variance(s) is/are acceptable for the appropriate development of the subject lands.
- That the requested variance(s) is/are minor in nature.

Public written and oral submissions relating to this application are taken into consideration by the Committee of Adjustment as part of its deliberations and final decision on this matter.

## COMMITTEE OF ADJUSTMENT COMMENTS

<b>Date Public Notice Mailed:</b>	November 24, 2022
<b>Date Applicant Confirmed Posting of Sign:</b>	TBD
<b>Applicant Justification for Variances:</b> <small>*As provided by Applicant in Application Form</small>	The owner wants the design.
<b>Adjournment Requests (from staff):</b> <small>*Adjournment requests provided to applicant prior to issuance of public notice</small>	None
<b>Was a Zoning Review Waiver (ZRW) Form submitted by Applicant:</b>  <small>*ZRW Form may be used by applicant in instances where a revised submission is made, and zoning staff do not have an opportunity to review and confirm variances prior to the issuance of public notice.  *A revised submission may be required to address staff / agency comments received as part of the application review process.  *Where a zoning review has not been completed on a revised submission, an opportunity is provided to the applicant to adjourn the proposal prior to the issuance of public notice.</small>	No
<b>Adjournment Fees:</b> In accordance with Procedural By-law 069-2019, an Adjournment Fee is applicable to reschedule an application after the issuance of public notice where a request for adjournment has been provided to the applicant prior to the issuance of public notice.  An Adjournment Fee can only be waived in instances where adjournment of an application is requested by the Committee or staff <b>after</b> the issuance of public notice.	
<b>Committee of Adjustment Comments:</b>	None
<b>Committee of Adjustment Recommended Conditions of Approval:</b>	None

## BUILDING STANDARDS (ZONING) COMMENTS

<b>**See Schedule B for Building Standards (Zoning) Comments</b>	
<b>Building Standards Recommended Conditions of Approval:</b>	None

## DEVELOPMENT PLANNING COMMENTS

<b>**See Schedule B for Development Planning Comments.</b>	
<b>Development Planning Recommended Conditions of Approval:</b>	Application under review.

## DEVELOPMENT ENGINEERING COMMENTS

<a href="#">Link to Grading Permit</a> <a href="#">Link to Pool Permit</a> <a href="#">Link to Curb Curt Permit</a> <a href="#">Link Culvert Installation</a>
As the proposed Addition in the subject property 23.03m <sup>2</sup> , the owner/ applicant needs to obtain a lot grading permit from Development Inspection and Lot Grading division of the City's Development Engineering Department. Please note any in ground structure over 10 m <sup>2</sup> requires a grading permit. Please contact COA application engineering reviewer after receiving the grading permit to clear the condition. (Condition attached)

## DEVELOPMENT ENGINEERING COMMENTS

[Link to Grading Permit](#)  
 [Link to Pool Permit](#)  
 [Link to Curb Curt Permit](#)  
 [Link Culvert Installation](#)

The Development Engineering Department does not object to the Minor Variance application A292/22, Subject to the following Condition:

<b>Development Engineering Recommended Conditions of Approval:</b>	The Owner/applicant shall submit a revised Lot Grading and/or Servicing Plan to the Development Inspection and Lot Grading division of the City's Development Engineering Department for final lot grading and/or servicing approval prior to any work being undertaken on the property. Please visit or contact the Development Engineering Department through email at DEPermits@vaughan.ca or visit the grading permit link provided above to learn how to apply for lot grading and/or servicing approval.
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## PARKS, FORESTRY & HORTICULTURE (PFH) COMMENTS

Forestry: Forestry has no comment at this time.

<b>PFH Recommended Conditions of Approval:</b>	None
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## DEVELOPMENT FINANCE COMMENTS

No comment no concerns

<b>Development Finance Recommended Conditions of Approval:</b>	None
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## BY-LAW AND COMPLIANCE, LICENSING AND PERMIT SERVICES COMMENTS

No comments received to date.

<b>BCLPS Recommended Conditions of Approval:</b>	N/A
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## BUILDING INSPECTION (SEPTIC) COMMENTS

No comments received to date.

<b>Building Inspection Recommended Conditions of Approval:</b>	N/A
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## FIRE DEPARTMENT COMMENTS

No comments received to date.

<b>Fire Department Recommended Conditions of Approval:</b>	N/A
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## SCHEDULES TO STAFF REPORT

\*See Schedule for list of correspondence

<b>Schedule A</b>	Drawings & Plans Submitted with the Application
<b>Schedule B</b>	Staff & Agency Comments
<b>Schedule C</b> (if required)	Correspondence (Received from Public & Applicant)
<b>Schedule D</b> (if required)	Previous COA Decisions on the Subject Land

Should the Committee find it appropriate to approve this application in accordance with request and the sketch submitted with the application, as required by Ontario Regulation 200/96, the following conditions have been recommended:

## SUMMARY OF RECOMMENDED CONDITIONS OF APPROVAL

All conditions of approval, unless otherwise stated, are considered to be incorporated into the approval "if required". If a condition is no longer required after an approval is final and binding, the condition may be waived by the respective department or agency requesting conditional approval. A condition cannot be waived without written consent from the respective department or agency.

#	DEPARTMENT / AGENCY	CONDITION(S) DESCRIPTION
1	Development Planning <a href="mailto:roberto.simbana@vaughan.ca">roberto.simbana@vaughan.ca</a>	Application under review.
2	Development Engineering <a href="mailto:ian.reynolds@vaughan.ca">ian.reynolds@vaughan.ca</a>	The Owner/applicant shall submit a revised Lot Grading and/or Servicing Plan to the Development Inspection and Lot Grading division of the City's Development Engineering

## SUMMARY OF RECOMMENDED CONDITIONS OF APPROVAL

		Department for final lot grading and/or servicing approval prior to any work being undertaken on the property. Please visit or contact the Development Engineering Department through email at <a href="mailto:DEPermits@vaughan.ca">DEPermits@vaughan.ca</a> or visit the grading permit link provided above to learn how to apply for lot grading and/or servicing approval.
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## IMPORTANT INFORMATION – PLEASE READ

**CONDITIONS:** It is the responsibility of the owner/applicant and/or authorized agent to obtain and provide a clearance letter from respective department and/or agency (**see condition chart above for contact**). This letter must be provided to the Secretary-Treasurer to be finalized. All conditions must be cleared prior to the issuance of a Building Permit.

**APPROVALS:** Making any changes to your proposal after a decision has been made may impact the validity of the Committee’s decision.

An approval obtained from the Committee of Adjustment, where applicable, is tied to the building envelope shown on the plans and drawings submitted with the application and subject to the variance approval.

A building envelope is defined by the setbacks of the buildings and/or structures shown on the plans and drawings submitted with the application, as required by Ontario Regulation 200/96. Future development outside of an approved building envelope, where a minor variance was obtained, must comply with the provisions of the City’s Zoning By-law.

Elevation drawings are provided to reflect the style of roof (i.e. flat, mansard, gable etc.) to which a building height variance has been applied. Where a height variance is approved, building height is applied to the style of roof (as defined in the City’s Zoning By-law) shown on the elevation plans submitted with the application.

Architectural design features that are not regulated by the City’s Zoning By-law are not to be considered part of an approval unless specified in the Committee’s decision.

**DEVELOPMENT CHARGES:** That the payment of the Regional Development Charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the Regional Development Charges By-law in effect at the time of payment.

That the payment of the City Development Charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the City’s Development Charges By-law in effect at the time of payment.

That the payment of the Education Development Charge if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and the Boards of Education By-laws in effect at the time of payment

That the payment of Special Area Development charge, if required, is payable to the City of Vaughan before issuance of a building permit in accordance with the Development Charges Act and The City’s Development Charge By-law in effect at the time of Building permit issuance to the satisfaction of the Reserves/Capital Department.

**NOTICE OF DECISION:** If you wish to be notified of the decision in respect to this application or a related Ontario Land Tribunal (OLT) hearing you must complete a Request for Decision form and submit to the Secretary Treasurer (ask staff for details). In the absence of a written request to be notified of the Committee’s decision you will **not** receive notice.

**SCHEDULE A: DRAWINGS & PLANS**

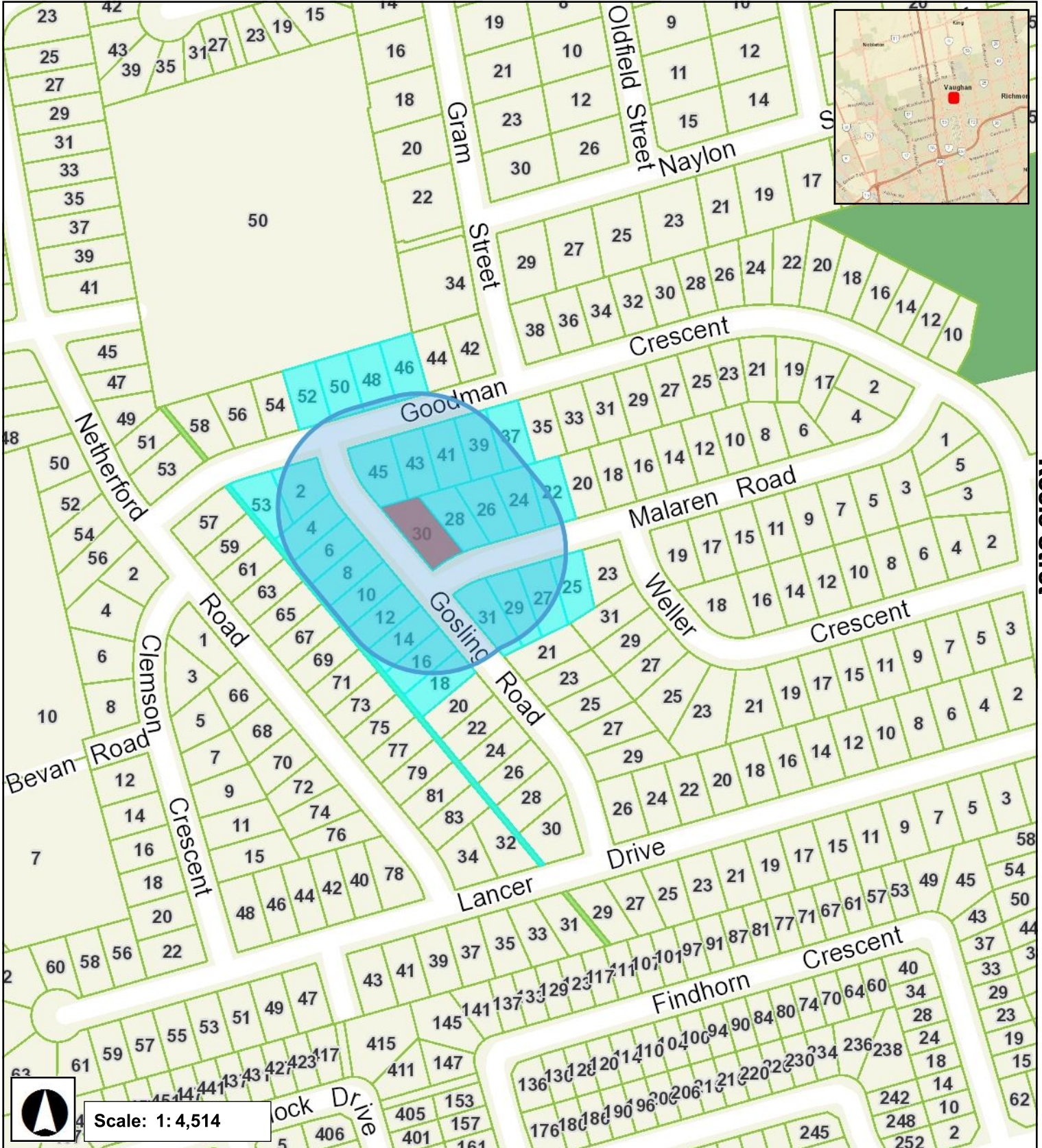




# LOCATION MAP - A292/22

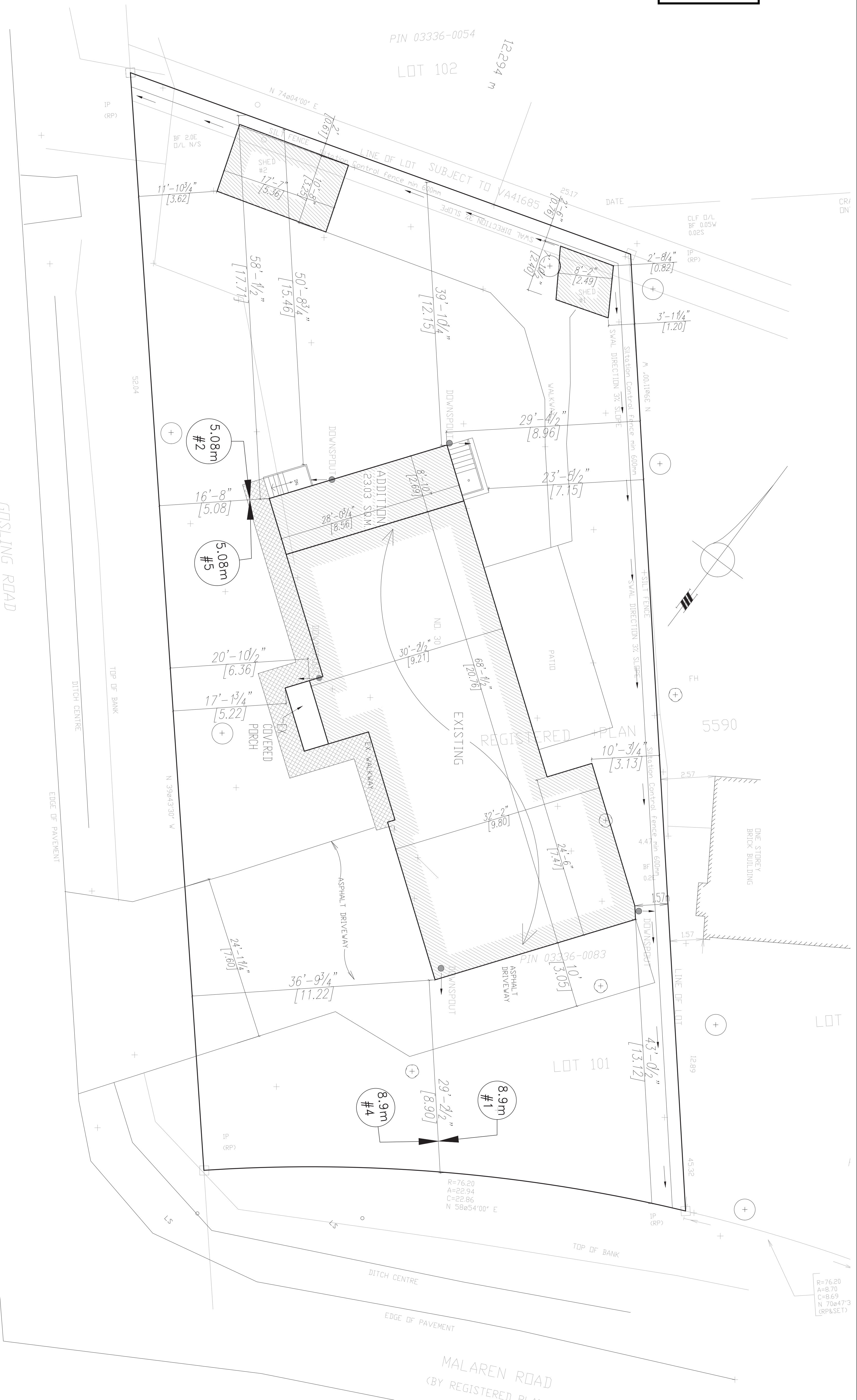
30 MALAREN ROAD, MAPLE

**Major Mackenzie Drive**

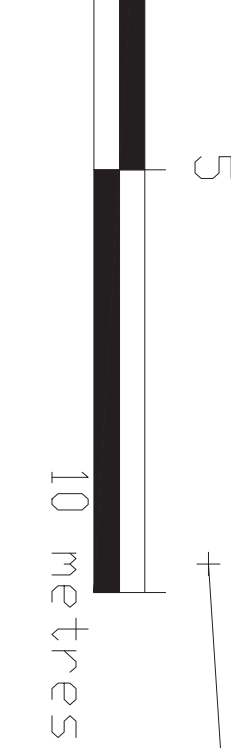




PLANS PREPARED BY APPLICANT



SCALE 1 : 200



ACCULINE INC.

© COPYRIGHT 2013

METRIC DISTANCES SHOWN ON THIS PLAN ARE IN METRES AND CAN BE CONVERTED TO FEET BY DIVIDING BY 0.3048.

ELEVATIONS SHOWN ARE GEODETIC AND ARE REFERRED TO CITY OF VAUGHAN BENCHMARK NO. 25-5, HAVING AN ELEVATION OF 229.168.

GOSLING ROAD  
PIN 03336-0142  
PAVEMENT CENTRE  
(BY REGISTERED PLAN 5590)

MALAREN ROAD  
(BY REGISTERED PLAN 5590)  
PAVEMENT CENTRE  
PIN 03336-0143

LOT 101  
REGISTERED PLAN 5590  
CITY OF VAUGHAN  
REGIONAL MUNICIPALITY OF YORK  
SCALE: 1:200  
PART 2 - SURVEY REPORT  
- DESCRIPTION  
- LOT 101, REGISTERED PLAN 5590  
- REGISTERED EASEMENTS AND/OR RIGHTS-OF-WAY  
- SUBJECT TO VA41685  
- ADDITIONAL REMARKS  
- FENCE LOCATIONS SHOWN AT TIME OF SURVEY

SITE STATISTICS

LOT AREA	1099.03 SQ.M (11829.90 SQ.FT)
EXISTING FOOTPRINT	183.40 SQ.M (1974.085 SQ.M)
EXISTING COVERED PORCH	3.56 SQ.M (38.33 SQ.M)
EXISTING SHEDS	23.80 SQ.M (256.19 SQ.M)
ADDITION FOOTPRINT	23.03 SQ.M (247.94 SQ.FT)
TOTAL FOOTPRINT	233.79 SQ.M (2516.545 SQ.FT)
COVERAGE	21.27 %
1ST FLOOR AREA WITHOUT GARAGE	133.91 SQ.M (1441.41 SQ.FT)
2ND FLOOR AREA	95.81 SQ.M (1031.29 SQ.FT)
TOTAL G.F.A	229.72 SQ.M (2472.7 SQ.FT)

NO	ISSUE/ REVISION	DATE

Drawing	SITE PLAN
Project	ADDITION TO LEONARDIS RESIDENCE 30 MALAREN ROAD VAUGHAN, ONT.

Architect  
**BATTAGLIA ARCHITECT INC.**  
1050 MCNICOLL AVE., UNIT 14  
SCARBOROUGH, ONT.  
TEL:(416) 492-7772



Date	OCT. 13, 2022
Scale	3/16"=1'-0" UNLESS NOTED
Drawn By	JOB
Project no.	1412
Drawing no.	A1

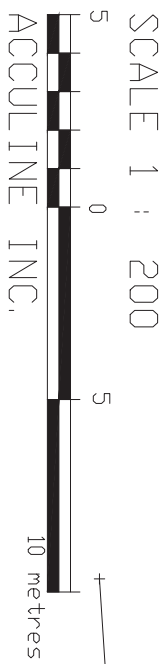


RECEIVED

By RECEIVED at 12:43 pm, Nov 10, 2022

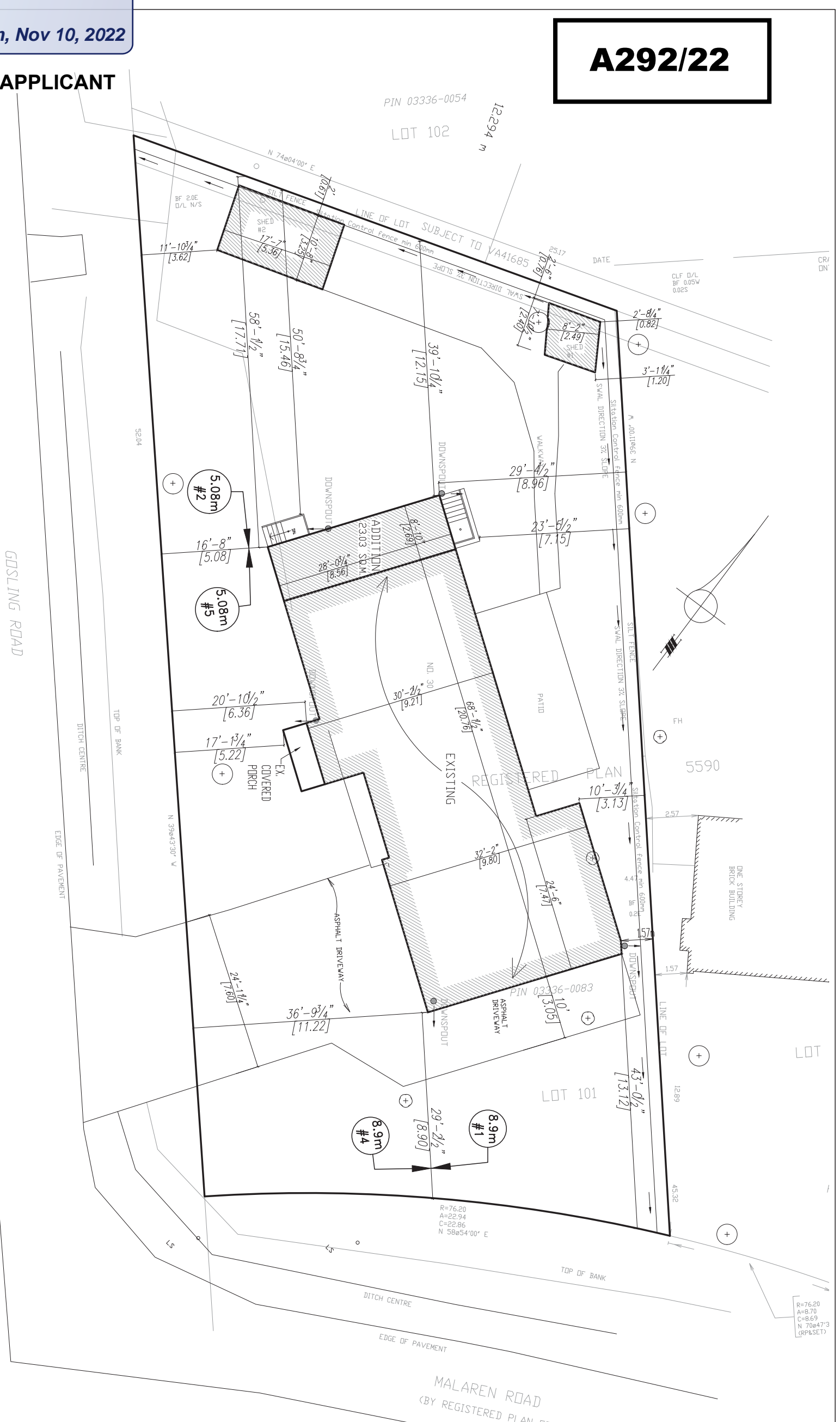
A292/22

PLANS PREPARED BY APPLICANT



ACCULINE INC.

© COPYRIGHT 2013  
 DISTANCES SHOWN ON THIS PLAN ARE IN METRES AND METRIC CAN BE CONVERTED TO FEET BY DIVIDING BY 0.3048.  
 ELEVATIONS SHOWN ARE GEODETIC AND ARE REFERRED TO CITY OF VAUGHAN BENCHMARK NO. 25-5, HAVING AN ELEVATION OF 229.168.



SITE STATISTICS

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LOT 101  
 REGISTERED PLAN 5590  
 CITY OF VAUGHAN  
 REGIONAL MUNICIPALITY OF YORK  
 SCALE: 1:200  
 PART 2 - SURVEY REPORT  
 - DESCRIPTION:  
 - LOT 101, REGISTERED PLAN 5590  
 - REGISTERED EASEMENTS AND/OR RIGHTS-OF-WAY  
 - SUBJECT TO V441685  
 - ADDITIONAL REMARKS  
 - FENCE LOCATIONS SHOWN AT TIME OF SURVEY

NO	ISSUE/ REVISION	DATE

Drawing	Project
SITE PLAN	ADDITION TO
GROUND FLOOR PLAN	LEONARDIS RESIDENCE
	30 MALAREN ROAD
	VAUGHAN, ONT.

Architect  
**BATTAGLIA ARCHITECT INC.**  
 1050 MCNICOLL AVE., UNIT 14  
 SCARBOROUGH, ONT.  
 TEL:(416) 492-7772



Date	NOV. 10, 2022
Scale	3/16"=1'-0"
UNLESS NOTED	
Drawn By	JOB
Project no.	1412
Drawing no.	A1

**SCHEDULE B: STAFF & AGENCY COMMENTS**

<b>DEPT/AGENCY</b>	<b>Circulated</b>	<b>Comments Received</b>	<b>Conditions</b>	<b>Nature of Comments</b>
TRCA *Schedule B	X			No Comments Recieved to Date
Ministry of Transportation (MTO) *Schedule B	X			No Comments Recieved to Date
Region of York *Schedule B	X	X		General Comments
Alectra *Schedule B	X	X		General Comments
Bell Canada *Schedule B	X			No Comments Recieved to Date
YRDSB *Schedule B				
YCDSB *Schedule B				
CN Rail *Schedule B				
CP Rail *Schedule B				
TransCanada Pipeline *Schedule B	X			No Comments Recieved to Date
Metrolinx *Schedule B				
Propane Operator *Schedule B				
Development Planning	X			Application under review.
Building Standards (Zoning)	X	X		General Comments

**Date:** November 9<sup>th</sup> 2022

**Attention:** **Christine Vigneault**

**RE:** Request for Comments

**File No.:** **A292-22**

**Related Files:**

**Applicant** Cristina Leonardis

**Location** 30 Malaren Road





Discover the possibilities

### COMMENTS:

- We have reviewed the proposed Variance Application and have no comments or objections to its approval.
- We have reviewed the proposed Variance Application and have no objections to its approval, subject to the following comments (attached below).
- We have reviewed the proposed Variance Application and have the following concerns (attached below).

Alectra Utilities (formerly PowerStream) has received and reviewed the proposed Variance Application. This review, however, does not imply any approval of the project or plan.

All proposed billboards, signs, and other structures associated with the project or plan must maintain minimum clearances to the existing overhead or underground electrical distribution system as specified by the applicable standards, codes and acts referenced.

In the event that construction commences, and the clearance between any component of the work/structure and the adjacent existing overhead and underground electrical distribution system violates the Occupational Health and Safety Act, the customer will be responsible for 100% of the costs associated with Alectra making the work area safe. All construction work will be required to stop until the safe limits of approach can be established.

In the event construction is completed, and the clearance between the constructed structure and the adjacent existing overhead and underground electrical distribution system violates the any of applicable standards, acts or codes referenced, the customer will be responsible for 100% of Alectra's cost for any relocation work.

### References:

- Ontario Electrical Safety Code, latest edition (Clearance of Conductors from Buildings)
- Ontario Health and Safety Act, latest edition (Construction Protection)
- Ontario Building Code, latest edition (Clearance to Buildings)
- PowerStream (Construction Standard 03-1, 03-4), attached
- Canadian Standards Association, latest edition (Basic Clearances)

If more information is required, please contact either of the following:

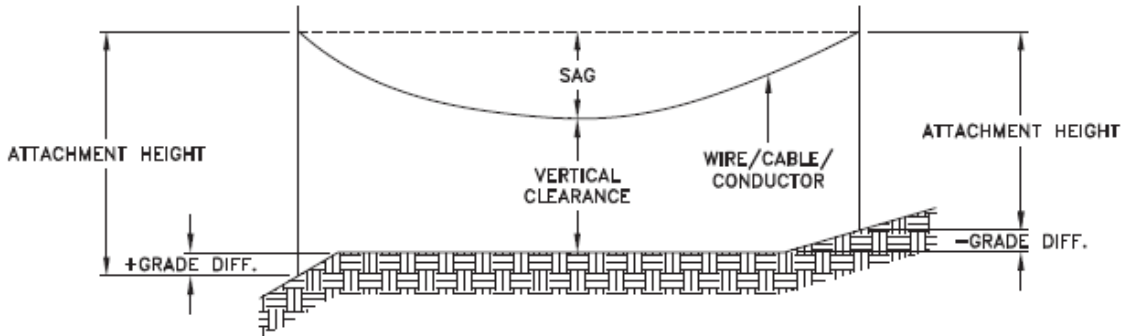
Mr. Stephen Cranley, C.E.T  
Supervisor, Distribution Design, ICI & Layouts (North)  
**Phone:** 1-877-963-6900 ext. 31297

**E-mail:** [stephen.cranley@alectrautilities.com](mailto:stephen.cranley@alectrautilities.com)

Mitchell Penner  
Supervisor, Distribution Design-Subdivisions  
**Phone:** 416-302-6215

**Email:** [Mitchell.Penner@alectrautilities.com](mailto:Mitchell.Penner@alectrautilities.com)

LOCATION OF WIRES, CABLES OR CONDUCTORS	SYSTEM VOLTAGE			
	SPAN GUYS AND COMMUNICATIONS WIRES	UP TO 600V AND NEUTRAL	4.16/2.4kV TO 27.6/16kV (SEE NOTE 1)	44kV
MINIMUM VERTICAL CLEARANCES (SEE NOTE 2)				
OVER OR ALONGSIDE ROADS, DRIVEWAYS OR LANDS ACCESSIBLE TO <u>VEHICLES</u>	442cm	442cm	480cm	520cm
OVER GROUND ACCESSIBLE TO <u>PEDESTRIANS AND BICYCLES ONLY</u>	250cm	310cm	340cm	370cm
ABOVE TOP OF RAIL AT <u>RAILWAY CROSSINGS</u>	730cm	730cm	760cm	810cm



MINIMUM ATTACHMENT HEIGHT = MAXIMUM SAG  
 + MINIMUM VERTICAL CLEARANCE (FROM ABOVE TABLE)  
 ± GRADE DIFFERENCE  
 + 0.3m (VEHICLE OR RAILWAY LOCATION)  
 + SNOW DEPTH (PEDESTRIAN LOCATION, SEE NOTE 3)

**NOTES:**

1. THE MULTIGROUNDED SYSTEM NEUTRAL HAS THE SAME CLEARANCE AS THE 600V SYSTEM.
2. THE VERTICAL CLEARANCES IN THE ABOVE TABLE ARE UNDER MAXIMUM SAG CONDITIONS.
3. REFER TO CSA STANDARD C22.3 No.1, ANNEX D FOR LOCAL SNOW DEPTH VALUES.
4. ALL CLEARANCES ARE IN ACCORDANCE TO CSA STANDARD C22.3.

**CONVERSION TABLE**

METRIC	IMPERIAL (APPROX)
810cm	27'-0"
760cm	25'-4"
730cm	24'-4"
520cm	17'-4"
480cm	16'-0"
442cm	15'-5"
370cm	12'-4"
340cm	11'-4"
310cm	10'-4"
250cm	8'-4"

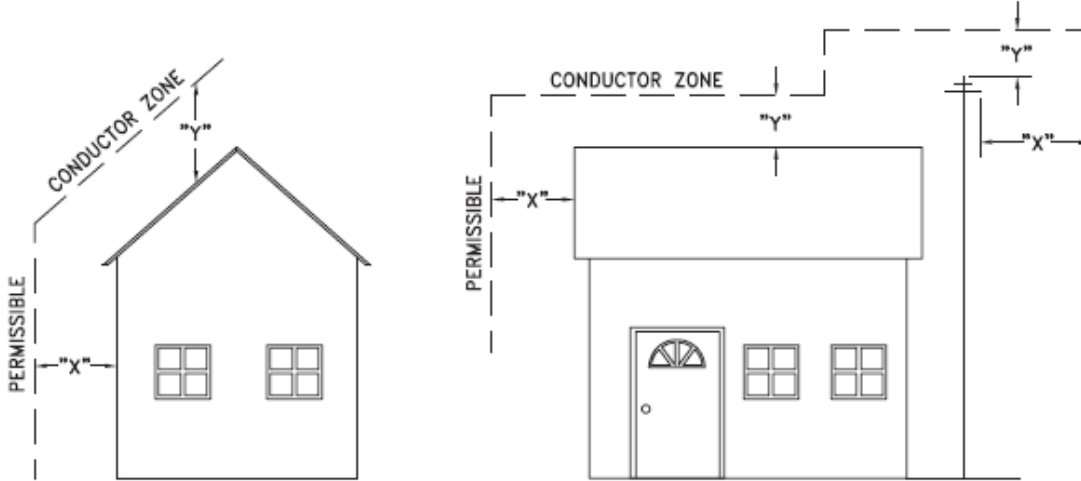
**REFERENCES**

SAGS AND TENSIONS | SECTION 02

**MINIMUM VERTICAL CLEARANCES OF WIRES, CABLES AND CONDUCTORS ABOVE GROUND OR RAILS**

ORIGINAL ISSUE DATE: 2010-DEC-24 REVISION NO: R1 REVISION DATE: 2012-JAN-09

Certificate of Approval	
This construction Standard meets the safety requirements of Section 4 of Regulation 22/04	
Joe Crozier, P.Eng.	2012-JAN-09
Name	Date
P.Eng. Approval By:	Joe Crozier



VOLTAGE	MINIMUM HORIZONTAL CLEARNACE UNDER MAXIMUM SWING CONDITIONS DIMENSION "X" (SEE NOTES 1, 3 & 4)	MINIMUM VERTICAL CLEARANCE UNDER MAXIMUM DESIGN SAG CONDITIONS DIMENSION "Y" (SEE NOTES 1, 2, 4 & 5)
0-600V AND NEUTRAL	100cm	250cm
4.16/2.4 TO 44kV	300cm	480cm

**NOTES**

1. UNDER NO CIRCUMSTANCES SHALL A CONDUCTOR BE PERMITTED TO PENETRATE THE ENVELOPE SHOWN BY THE DOTTED LINE.
2. THE VERTICAL CLEARANCES ARE UNDER CONDITIONS OF MAXIMUM DESIGN SAG.
3. THE HORIZONTAL CLEARANCES ARE UNDER CONDITIONS OF MAXIMUM SWING. WHERE THE CONDUCTOR SWING IS NOT KNOWN A HORIZONTAL CLEARANCE OF 480CM SHALL BE USED.
4. BUILDINGS THAT EXCEED 3 STOREYS OR 15M IN HEIGHT, THE MINIMUM HORIZONTAL CLEARANCE OF THE SECONDARY CONDUCTORS SHOULD BE INCREASED TO 300cm WHERE IT IS NECESSARY TO ALLOW FOR THE RAISING OF LADDERS BY LOCAL FIRE DEPARTMENTS.
5. IN SITUATIONS SUCH AS MULTI-LEVEL GARAGES, WHERE ROOFS ARE NORMALLY USED BY PERSONS AND VEHICLES, THE VERTICAL CLEARANCES OF POWERSTREAM STANDARD 03-1 SHALL APPLY.
6. DISTRIBUTION LINES CONSTRUCTED NEAR BUILDINGS SHALL BE BUILT TO AVOID OVERHANG WHEREVER POSSIBLE. WHERE LINES MUST BE CONSTRUCTED OVER OR ADJACENT TO BUILDINGS THE APPLICABLE HORIZONTAL AND VERTICAL CLEARANCES SHALL BE AT CONDITIONS OF MAXIMUM CONDUCTOR SWING AND MAXIMUM SAG. THE ABOVE CLEARANCES ARE DESIGNED TO PREVENT PERSONS ON OR IN BUILDINGS AS WELL AS EXTERNAL MACHINERY USED IN CONJUNCTION WITH A BUILDING TO COME IN CONTACT WITH CONDUCTORS. EFFORTS SHOULD BE MADE TO INCREASE THESE CLEARANCES WHERE POSSIBLE.
7. ALL CLEARANCES ARE IN ACCORDANCE TO CSA C22.3 NO.1-06 (TABLE-9).

METRIC	IMPERIAL (APPROX)
480cm	16'-0"
300cm	10'-0"
250cm	8'-4"
100cm	3'-4"

**MINIMUM VERTICAL & HORIZONTAL CLEARANCES OF CONDUCTORS FROM BUILDINGS OR OTHER PERMANENT STRUCTURES (CONDUCTORS NOT ATTACHED TO BUILDINGS)**

**Certificate of Approval**  
This construction Standard meets the safety requirements of Section 4 of Regulation 22/04  
Debbie Dadwani, P.Eng. 2010-MAY-05  
Name Date  
P. Eng. Approval By: *D. Dadwani*

ORIGINAL ISSUE DATE: 2010-MAY-05 REVISION NO: REVISION DATE:  
PS:\System Planning and Standards\Standard Design\PowerStream Standards\PowerStream Standards working files\Section 3-1-02\WG 03-4 R0 May 5, 2010.dwg, 5/5/2010 10:22:02 AM, Adobe PDF

**To:** Committee of Adjustment  
**From:** Christian Tinney, Building Standards Department  
**Date:** November 08, 2022  
**Applicant:** Cristina Leonardis  
**Location:** 30 Malaren Road  
 PLAN RP5590 Lot 101  
**File No.(s):** A292/22

**Zoning Classification:**

The subject lands are zoned R1E(EN), First Density Residential Zone and subject to the provisions of Exception 14.534 under Zoning By-law 001-2021, as amended.

#	Zoning By-law 001-2021	Variance requested
1	The minimum front yard setback required is 11.56 metres. [Table 7-3, Note 1.]	To permit a minimum front yard setback of 8.9 metres.
2	The minimum exterior side yard setback required is 9.0 metres. [Table 7-3]	To permit a minimum exterior side yard setback of 5.08 metres.

The subject lands are zoned R1V, Old Village Residential Zone under Zoning By-law 1-88, as amended.

#	Zoning By-law 1-88	Variance requested
4	The minimum front yard setback required is 11.56 metres. [Schedule 'A' Note 11.]	To permit a minimum front yard setback of 8.9 metres.
5	The minimum exterior side yard setback required is 9.0 metres. [Schedule 'A']	To permit a minimum exterior side yard setback of 5.08 metres.

**Staff Comments:**

**Stop Work Order(s) and Order(s) to Comply:**

Order No. 20-000025, Order to Comply for Construction of the building has not been carried out in accordance with the permit documents and applicable law, Issue Date: May 21, 2020

**Building Permit(s) Issued:**

Building Permit No. 17-003566 for Single Detached Dwelling - Addition, Issue Date: Jan 05, 2018  
 Building Permit No. 17-003566 for Single Detached Dwelling - Alteration, Issue Date: Sep 28, 2018  
 Building Permit No. 17-003566 for Single Detached Dwelling - Addition, Issue Date: (Not Yet Issued)

**Other Comments:**

General Comments	
1	The applicant shall be advised that additional variances may be required upon review of detailed drawing for building permit/site plan approval.

**Conditions of Approval:**

If the committee finds merit in the application, the following conditions of approval are recommended.

\* Comments are based on the review of documentation supplied with this application.



**From:** [Development Services](#)  
**To:** [Pravina Attwala](#)  
**Cc:** [Committee of Adjustment](#)  
**Subject:** [External] RE: A292/22 (30 MALAREN ROAD) - REQUEST FOR COMMENTS  
**Date:** Friday, November 11, 2022 9:50:11 AM  
**Attachments:** [image002.png](#)  
[image004.png](#)

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Hi Pravina,

The Regional Municipality of York has completed its review of the above minor variance and has no comment.

Many thanks,

**Christine Meehan, B.U.R.PL.** | Associate Planner, Programs and Process Improvement, Planning and Economic Development, Corporate Services

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The Regional Municipality of York | 17250 Yonge Street | Newmarket, ON L3Y 6Z1  
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**SCHEDULE C: PUBLIC & APPLICANT CORRESPONDENCE**

None